

DESIGN DESIGNATION

A.A.D.T. - 2024 = 4526
A.A.D.T. - 2034 = 4758
T = 4%
V = 55 M.P.H.

FUNCTIONAL CLASSIFICATION- MAJOR COLLECTOR

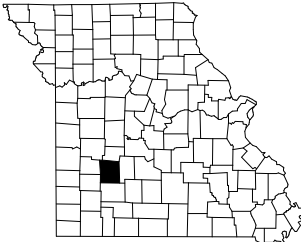
NO NEW R/W REQUIRED

CONVENTIONAL SYMBOLS
(USED IN PLANS)

	EXISTING	NEW
BUILDINGS AND STRUCTURES		
GUARD RAIL		
GUARD CABLE		
CONCRETE RIGHT-OF-WAY MARKER		
STEEL RIGHT-OF-WAY MARKER		
LOCATION SURVEY MARKER		
UTILITIES		
FIBER OPTICS	-FO-	-FO-
OVERHEAD CABLE TV	-OTV-	-OTV-
UNDERGROUND CABLE TV	-UTV-	-UTV-
OVERHEAD TELEPHONE	-OT-	-OT-
UNDERGROUND TELEPHONE	-UT-	-UT-
OVERHEAD POWER	-OE-	-OE-
UNDERGROUND POWER	-UE-	-UE-
SANITARY SEWER	-S-	-S-
STORM SEWER	-SS-	-SS-
GAS	-G-	-G-
WATER	-W-	-W-
MANHOLE		
FIRE HYDRANT		
WATER VALVE		
WATER METER		
DROP INLET		
DITCH BLOCK		
GROUND MOUNTED SIGN		
LIGHT POLE		
H-FRAME POWER POLE		
TELEPHONE PEDESTAL		
FENCE		
CHAIN LINK		
WOVEN WIRE		
GATE POST		
BENCHMARK		

NOTE: DASHED OR OPEN SYMBOLS INDICATE EXISTING FEATURES

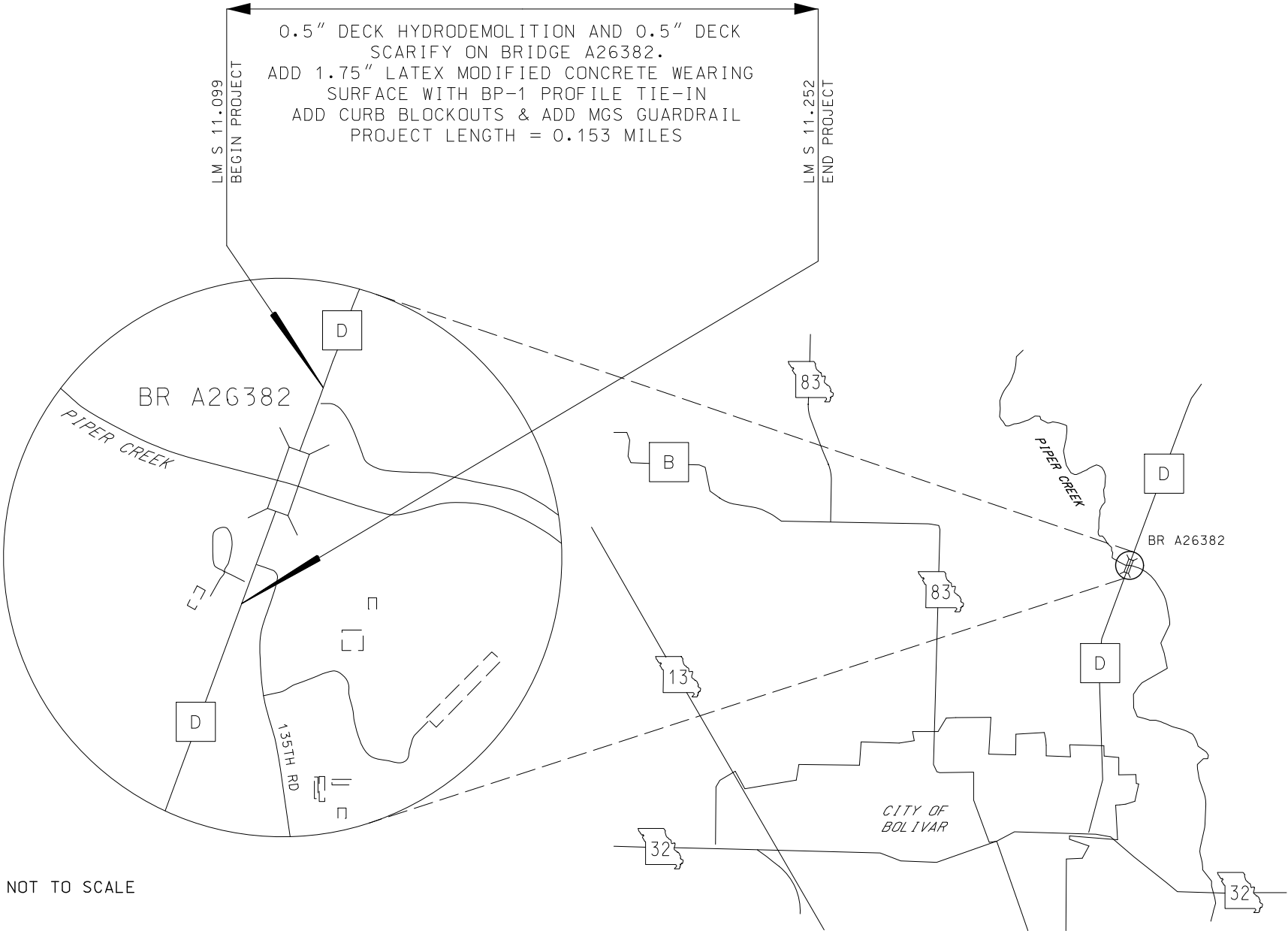
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
PLANS FOR PROPOSED
STATE HIGHWAY
POLK COUNTY



KEY MAP
LOCATION OF POLK COUNTY

PROJECT LIMITS - J7S3509

0.5" DECK HYDRODEMOLITION AND 0.5" DECK
SCARIFY ON BRIDGE A26382.
ADD 1.75" LATEX MODIFIED CONCRETE WEARING
SURFACE WITH BP-1 PROFILE TIE-IN
ADD CURB BLOCKOUTS & ADD MGS GUARDRAIL
PROJECT LENGTH = 0.153 MILES



NOT TO SCALE

THE EXISTENCE AND APPROXIMATE LOCATION OF UTILITY FACILITIES KNOWN TO EXIST, AS SHOWN ON THE PLANS, ARE BASED ON THE BEST INFORMATION AVAILABLE TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE. RELIANCE UPON THIS INFORMATION IS DONE AT THE RISK AND PERIL OF THE USER, AND THE COMMISSION SHALL NOT BE LIABLE FOR ANY DAMAGES THAT MAY ARISE FROM ANY ERROR IN THE INFORMATION. IT IS, THEREFORE, THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE, LOCATION AND STATUS OF ANY FACILITY. SUCH VERIFICATION INCLUDES DIRECT CONTACT WITH THE LISTED UTILITIES.

INDEX OF SHEETS

DESCRIPTION	SHEET NUMBER
TITLE SHEET	1
TYPICAL SECTIONS (TS) (3 SHEETS)	2
QUANTITIES (QU) (4 SHEETS)	3
SPECIAL SHEETS (SS)	4
TRAFFIC CONTROL SHEETS (TC)	5-22
EROSION CONTROL SHEETS (EC)	23
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BRIDGE DRAWINGS (B)	
A26382	1-7



DATE PREPARED
2/21/2024

ROUTE D STATE MO
DISTRICT SW SHEET NO. 1

COUNTY
POLK

JOB NO.
J7S3509

CONTRACT ID.

PROJECT NO.

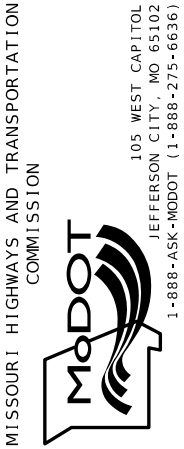
BRIDGE NO.

DESCRIPTION	DATE

LENGTH OF PROJECT


LOG MILES FROM 2021 ARAN	
BEGINNING OF PROJECT	LM S 11.099
END OF PROJECT	LM S 11.252
APPARENT LENGTH	803 FEET
EQUATIONS AND EXCEPTIONS:	

TOTAL CORRECTIONS	0.000 FEET
NET LENGTH OF PROJECT	803 FEET
STATE LENGTH	0.153 MILES
FOR INFORMATION ONLY ESTIMATED DISTURBED ACRES	0.092 ACRES



NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLD MILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

NO S.E. CORRECTION



DATE PREPARED
2/21/2024

ROUTE D	STATE MO
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DISTRICT	SHEET NO.
SW	2

COUNTY
POLK

JOB NO.
J7S3509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

Plan view of proposed roadwork on Route D, showing lane widths, shoulder widths, and material specifications.

Overall width: VAR

Section widths (from left to right):

- VAR (1) SHLDR
- 12' THRU LANE
- 12' THRU LANE
- VAR (1) SHLDR

Match lines: MATCH EXIST SLOPE

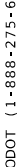
Materials and thicknesses:

- 2" BP-1 (PG64-22)
- 8" PMBB (PG64-22)
- 4" TYPE 5 AGGR. BASE
- 2" BP-1 (PG64-22)
- 2" COLDMILL
- EXIST 1 3/4" BP-1 (PG 64-22) (2014)
- 2" BP-1 (PG64-22)
- 8" PMBB (PG64-22)
- 4" TYPE 5 AGGR. BASE

EDGE LINE RUMBLE STRIP (2)

ROUTE D

SB LM 11.099 TO SB LM 11.142



MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO. 65102
1-888-ASK-MODOT (1-888-275-6636)

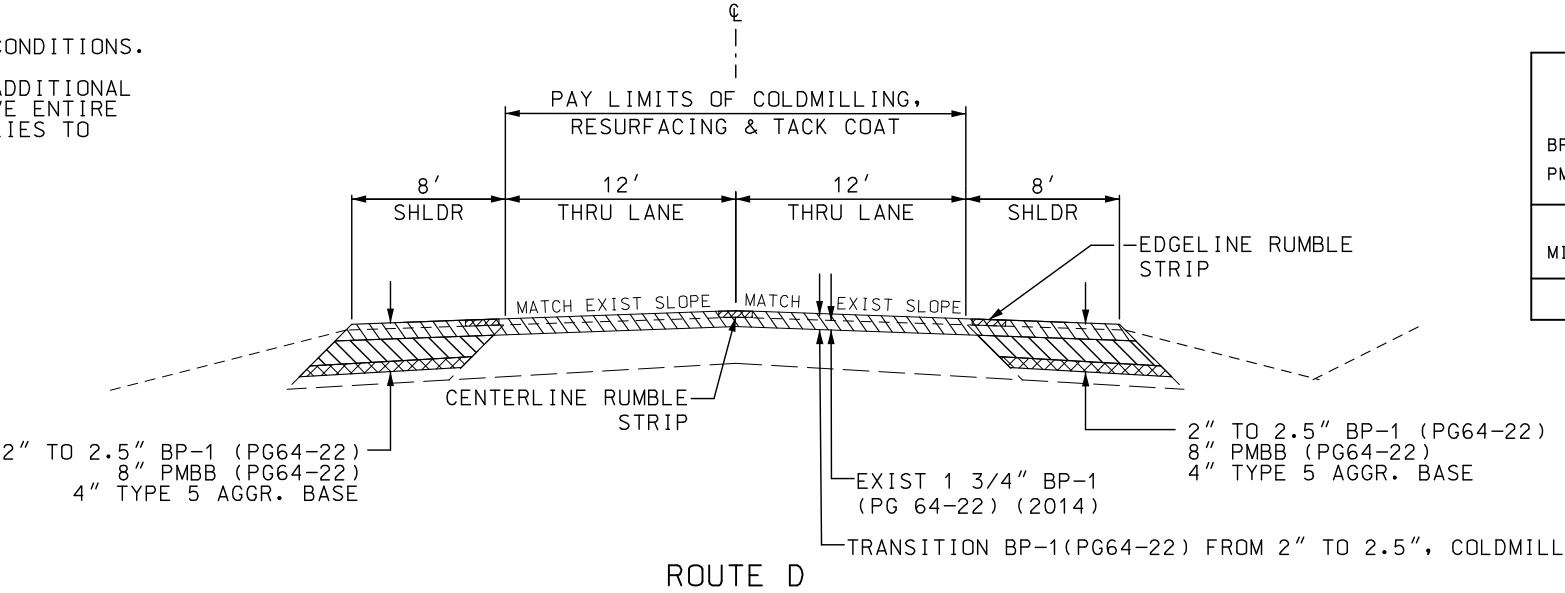
TYPICAL SECTIONS
SHEET 1 OF 3

NOTES: LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION



ASPHALT FACTORS	
COMBINED FACTOR	
BP-1 (PG64-22)	1.990 TONS/CY
PMBB (PG64-22)	1.995 TONS/CY
TACK COAT	
MILLING	0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI	



Warner D. Sherman II
02/21/2024 9:31:01 PM
WARNER D. SHERMAN II - CIVIL
MO-PE-26645

DATE PREPARED
2/21/2024

ROUTE D STATE MO

DISTRICT SW SHEET NO. 2

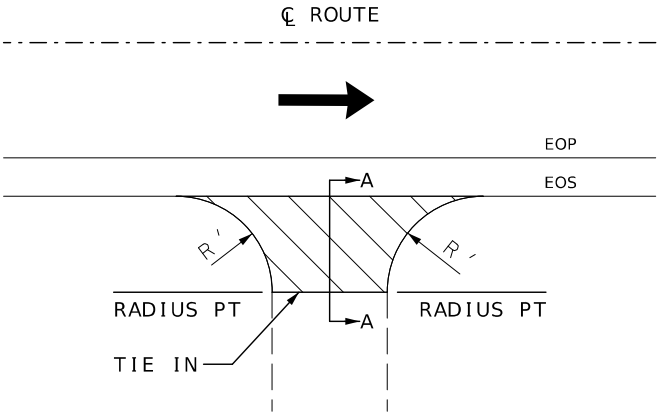
COUNTY POLK

JOB NO. J7S3509

CONTRACT ID.

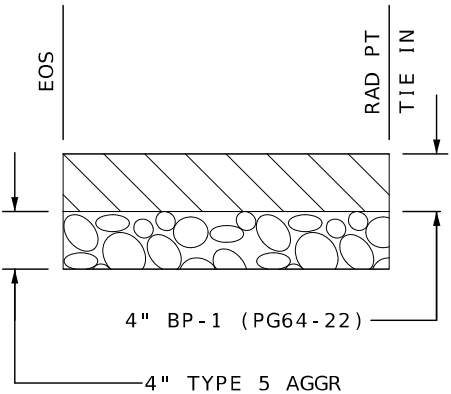
PROJECT NO.

BRIDGE NO.

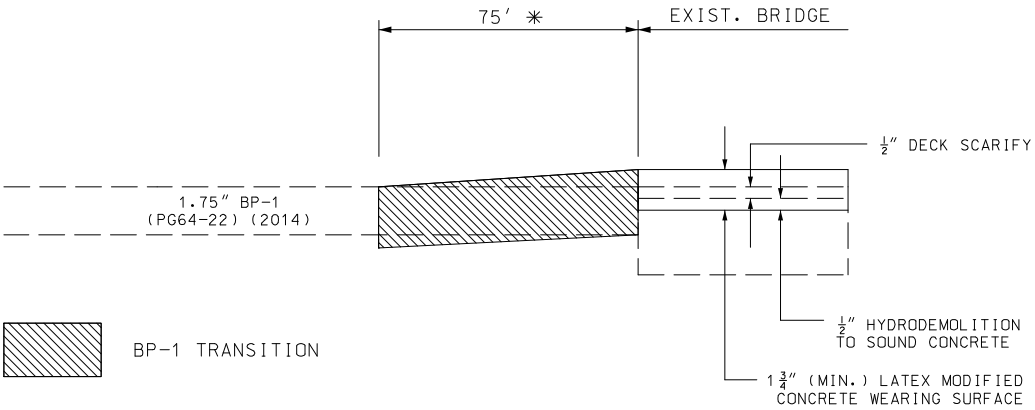


TYPE I ENTRANCE

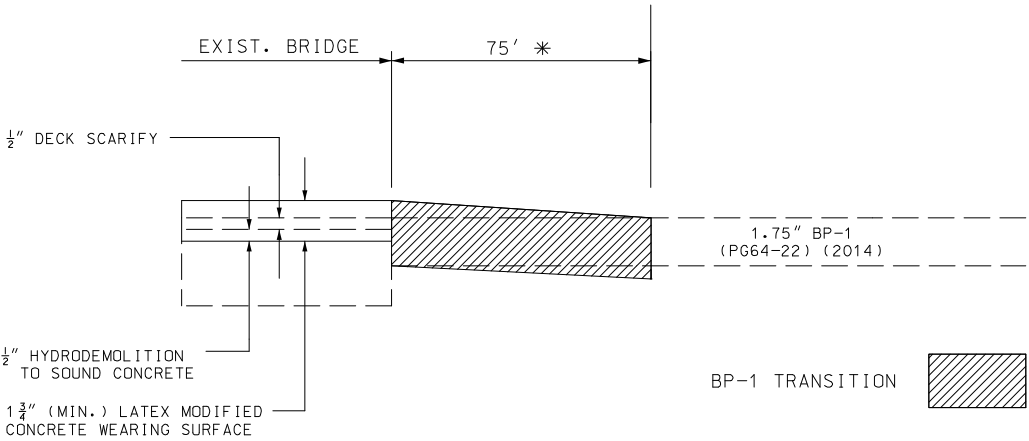
LM 11.128
LM 11.237
LM 11.250



SECTION A-A



BP-1 TRANSITION



BP-1 TRANSITION

* COLDMILL ENTIRE PREVIOUS TOP LIFT OF BP-1 (1.75")

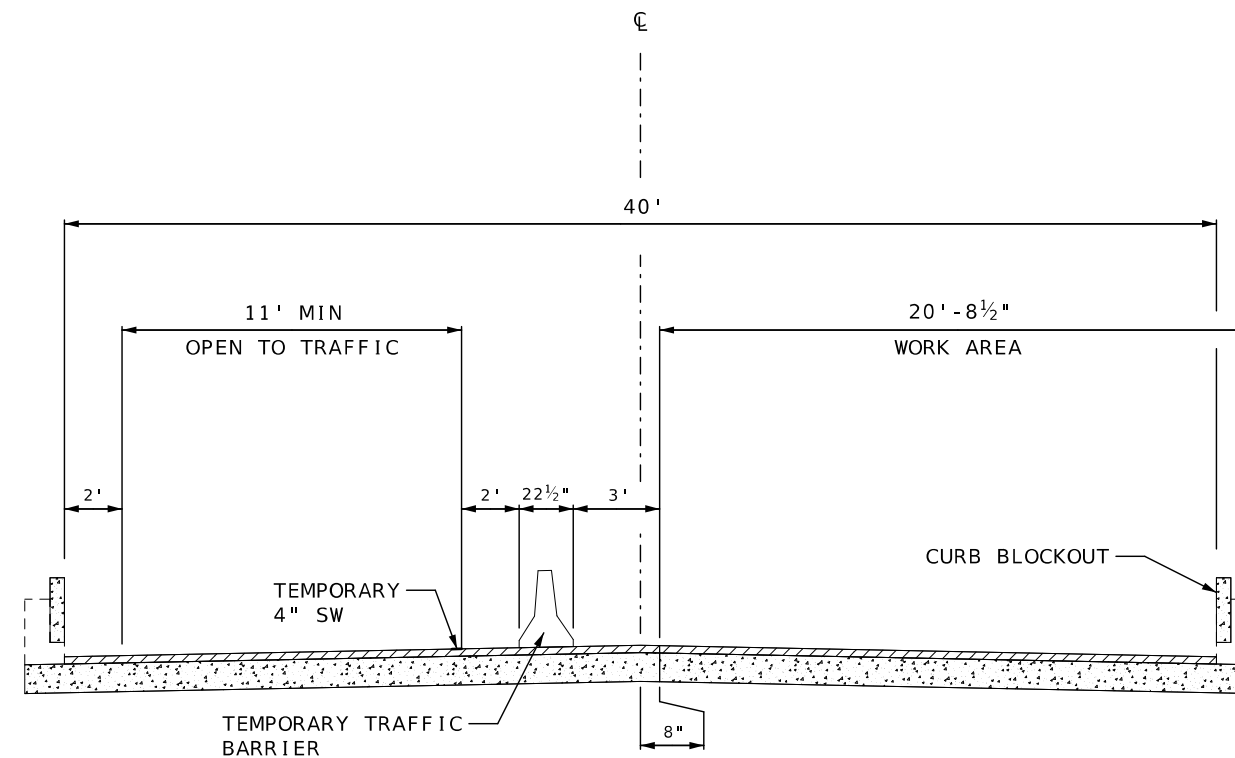
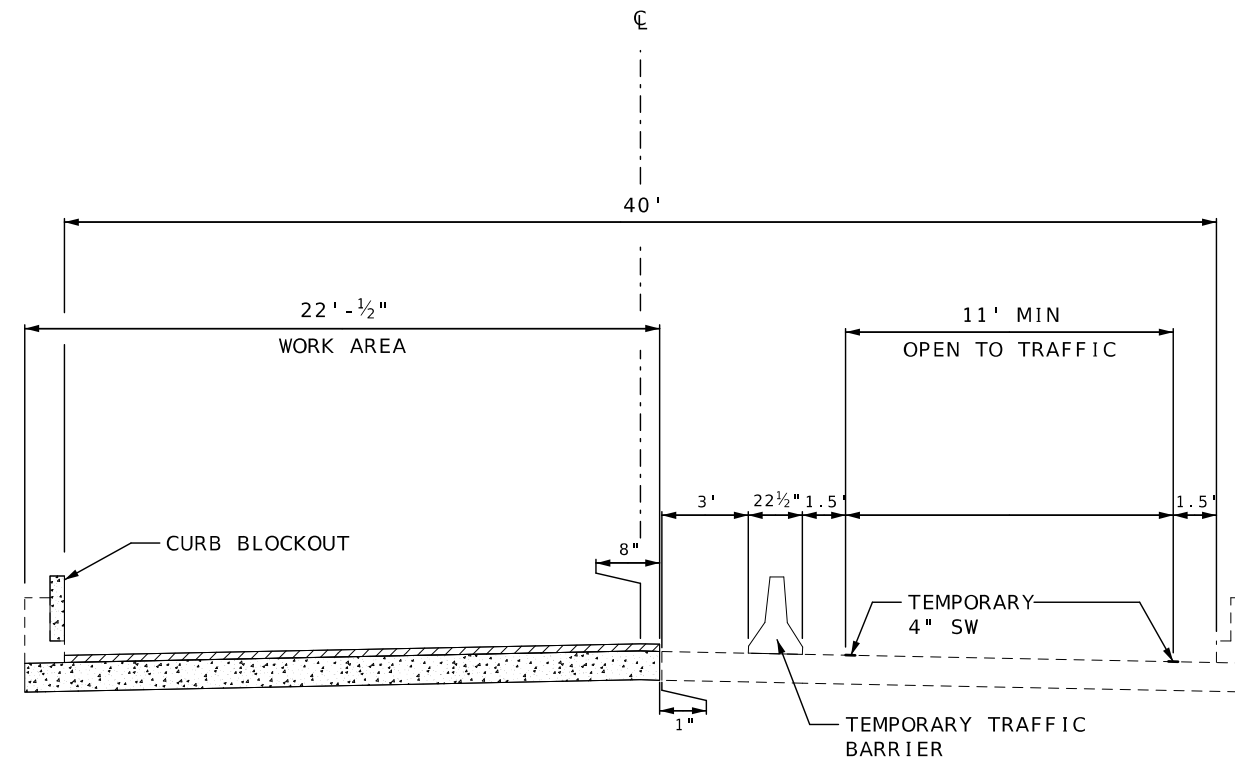
* TRANSITION BP-1 FILL FROM 2" TO 2.5" (AT BRIDGE APPR SLAB)

TYPICAL SECTIONS
SHEET 2 OF 3

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)





TYPICAL SECTIONS
SHEET 3 OF 3

PAVEMENT MARKING							
ROUTE	LOG MILE	TO	LOG MILE	LOCATION	WATERBORNE PAINT, TYPE P BEADS		REMARKS
					4" SOLID WHITE	4" INT YELLOW	
					LF	LF	
D	11.099		11.252	RT/LT	1616		
D	10.977		11.361	CL		507	
TOTALS					1616	507	

TEMPORARY PAVEMENT MARKING PAINT							
LOG MILE	LOG MILE	LOC	4" WHITE	4" YELLOW	24" WHITE	REMOVAL	REMARKS
			LF	LF	LF	LF	
STAGE 2							
10.977	11.072	CL		1000		125	NO PASSING ZONE
11.072		RT			12		STOPBAR
11.072	11.134	CL				82	REMOVE EXISTING CENTERLINE MARKING
11.099	11.252	LT				803	REMOVE EXISTING SW OUTER EDGE LINE
11.099	11.252	LT	803			450	SW OUTER EDGE LINE
11.118	11.233	CL	606			130	SW EDGE LINE
11.216	11.266	CL				66	REMOVE EXISTING CENTERLINE MARKING
11.266		LT			12		STOPBAR
11.266	11.361	CL		1000		125	NO PASSING ZONE
STAGE 3							
11.142	11.209	CL	645				SW EDGE LINE
STAGE 4							
10.977	11.072	CL				500	REMOVE NO PASSING MARKING
11.072		RT				12	REMOVE STOPBAR
11.266		LT				12	REMOVE STOPBAR
11.266	11.361	CL				500	REMOVE NO PASSING MARKING
TOTALS			2054	2000	24	2805	

COLDMILLING BITUMINOUS PAVEMENT FOR REMOVAL OF SURFACE (3 IN. THICK OR LESS)							
ROUTE	LOG MILE	TO	LOG MILE	NET LENGTH	AVERAGE WIDTH	AREA	REMARKS
				LF	LF	SY	
D	11.099	-	11.142	225.0	24.0	600.0	2" MILL/FILL
D	11.209	-	11.252	225.0	24.0	600.0	2" MILL/FILL
TOTAL						1200.0	
USE						1200	

MODIFIED COLDMILLING (DEPTH TRANSITIONS)							
ROUTE	LOG MILE	TO	LOG MILE	NET LENGTH	AVERAGE WIDTH	AREA	REMARKS
				LF	LF	SY	
D	11.142	-	11.156	75.0	12.7	105.8	STAGE 2 - 2" to 2.5" MILL/FILL
D	11.195	-	11.209	75.0	12.7	105.8	STAGE 2 - 2" to 2.5" MILL/FILL
D	11.142	-	11.156	75.0	19.3	160.8	STAGE 3 - 2" to 2.5" MILL/FILL
D	11.195	-	11.209	75.0	19.3	160.8	STAGE 3 - 2" to 2.5" MILL/FILL
TOTAL						533.2	
USE						534	

RUMBLE STRIPS							
ROUTE	LOG MILE	TO	LOG MILE	NET LENGTH	SHOULDER	CENTERLINE	REMARKS
				LF	STA	STA	
D	11.099	-	11.126	142.6	1.4		LT
D	11.139	-	11.156	89.8	0.9		LT
D	11.195	-	11.227	169.0	1.7		LT
D	11.241	-	11.252	58.1	0.6		LT
D	11.099	-	11.152	279.8	2.8		RT
D	11.195	-	11.242	248.2	2.5		RT
D	11.099	-	11.156	300.0		3.0	CL
D	11.195	-	11.252	300.0		3.0	CL
TOTAL					9.9	6.0	

CONTRACTOR FURNISHED SURVEYING & STAKING
1 LUMP SUM

MULCHING
0.4 AC

SEEDING - COOL SEASON GRASSES
0.3 AC

TEMPORARY SEEDING
0.1 AC

TEMPORARY EROSION CONTROL									
SHEET	ROUTE	LOG MILE	LOG MILE	LOCATION	NO. OF CHECKS	SILT FENCE	ROCK DITCH CHECK	SEDIMENT REMOVAL	REMARKS
					EA	LF	LF	CY	
11	D	11.099	11.124	LT		129		1.3	
11	D	11.099	11.156	RT		302		3.1	
11	D	11.131	11.156	LT		128		1.3	
11	D	11.169		LT	1		20	1.0	RDC
11	D	11.169		RT	1		20	1.0	RDC
11	D	11.190		RT	1		20	1.0	RDC
11	D	11.190		LT	1		20	1.0	RDC
11	D	11.195	11.234	LT		200		2.0	
11	D	11.195	11.242	RT		272		2.8	
TOTALS						1031	80	14.5	
USE						1031	80	15	

TEMPORARY PIPE				
ROUTE	LOG MILE	TEMPORARY PIPE	PIPE COLLAR TYPE A	REMARKS
		LF	EACH	
D	11.250	20	1	
D	11.250	48		
TOTALS		68	1	

SUMMARY OF QUANTITIES
SHEET 2 OF 4

STATE OF MISSOURI

WARNER D. SHERMAN II

NUMBER PE-26645

PROFESSIONAL ENGINEER

Warner D. Sherman

02/21/2024 9:32:29 PM

WARNER D. SHERMAN II - CIVIL

MO-PE-26645

DATE PREPARED

2/21/2024

ROUTE

D

STATE

MO

DISTRICT

SW

SHEET NO.

3

COUNTY

POLK

JOB NO.

J7S3509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

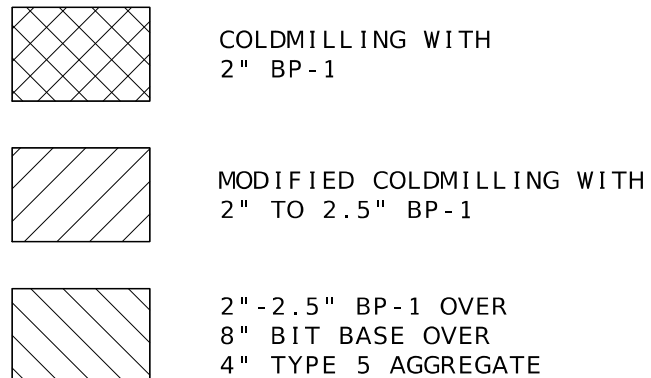
1-888-ASK-MODOT (1-888-273-6636)

MoDOT

EFFECTIVE: 04-01-2024									
SIGN	SIZE IN.	AREA SQ.FT.	QTY EACH	TOTAL AREA SQ.FT.	QTY RELOC EACH	TOTAL RELOC SQ.FT.	SIGN NUM.		
WARNING SIGNS								DESCRIPTION	
WO1-1L	48X48	16.00						TURN (SYMBOL LEFT)	
WO1-1R	48X48	16.00						TURN (SYMBOL RIGHT)	
WO1-2L	48X48	16.00						CURVE (SYMBOL LEFT)	
WO1-2R	48X48	16.00						CURVE (SYMBOL RIGHT)	
WO1-3L	48X48	16.00						REVERSE TURN (SYMBOL LEFT)	
WO1-3R	48X48	16.00						REVERSE TURN (SYMBOL RIGHT)	
WO1-4L	48X48	16.00						REVERSE CURVE (SYMBOL LEFT)	
WO1-4R	48X48	16.00	1	16				REVERSE CURVE (SYMBOL RIGHT ARROW)	
WO1-4bL	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL LEFT)	
WO1-4bR	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL RIGHT)	
WO1-4cL	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL LEFT)	
WO1-4cR	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL RIGHT)	
WO1-6	60X30	12.50						HORIZONTAL ARROW (SYMBOL)	
WO1-6a	72X36	18.00						HORIZ. ARROW (SYMBOL ON PERMANENT BARRICADE)	
WO1-7	60X30	12.50						DOUBLE HEAD HORIZONTAL ARROW (SYMBOL)	
WO1-7a	72X36	18.00						DOUBLE HEAD HORIZ. ARROW (SYMBOL ON PERM. BARR.)	
WO1-8	18X24	3.00						CHEVRON (SYMBOL)	
WO1-8a	30X36	7.50						CHEVRON (SYMBOL FOR DIVIDED HIGHWAYS)	
WO3-1	48X48	16.00						STOP AHEAD (SYMBOL)	
WO3-2	48X48	16.00						YIELD AHEAD (SYMBOL)	
WO3-3	48X48	16.00	2	32				SIGNAL AHEAD (SYMBOL)	
WO3-4	48X48	16.00						BE PREPARED TO STOP	
WO3-5	48X48	16.00						SPEED LIMIT AHEAD	
WO4-1L	48X48	16.00						MERGE (SYMBOL FROM LEFT)	
WO4-1R	48X48	16.00						MERGE (SYMBOL FROM RIGHT)	
WO4-1aL	48X48	16.00						MERGE (LEFT)	
WO4-1aR	48X48	16.00						MERGE (RIGHT)	
WO5-1	48X48	16.00	2	32				ROAD/BRIDGE/RAMP NARROWS	
WO5-3	48X48	16.00						ONE LANE BRIDGE	
WO5-5	48X48	16.00						NARROW LANES	
WO6-1	48X48	16.00						DIVIDED HIGHWAY (SYMBOL)	
WO6-2	48X48	16.00						DIVIDED HIGHWAY END (SYMBOL)	
WO6-3	48X48	16.00						TWO WAY TRAFFIC (SYMBOL)	
WO7-3a	30X24	5.00						NEXT XX MILES (PLAQUE)	
WO8-1	48X48	16.00						BUMP	
WO8-2	48X48	16.00						DIP	
WO8-3	48X48	16.00						PAVEMENT ENDS	
WO8-4	48X48	16.00						SOFT SHOULDER	
WO8-5	48X48	16.00						SLIPPERY WHEN WET (SYMBOL)	
WO8-6	48X48	16.00						TRUCK CROSSING	
WO8-6c	48X48	16.00						TRUCK ENTRANCE	
WO8-7	36X36	9.00						LOOSE GRAVEL	
WO8-7a	36X36	9.00						FRESH OIL / LOOSE GRAVEL	
WO8-9	48X48	16.00						LOW SHOULDER	
WO8-11	48X48	16.00	2	32				UNEVEN LANES	
WO8-12	48X48	16.00	2	32				NO CENTER LINE	
WO8-15	48X48	16.00						GROOVED PAVEMENT	
WO8-15P	30X24	5.00						MOTORCYCLE (PLAQUE)	
WO8-17L	48X48	16.00	1	16				SHOULDER DROP-OFF (SYMBOL LEFT)	
WO8-17R	48X48	16.00	1	16				SHOULDER DROP-OFF (SYMBOL RIGHT)	
WO8-17P	30X24	5.00	2	10				SHOULDER DROP-OFF (PLAQUE)	
WO10-1	42RND.	9.62						RAILROAD CROSSING	
WO12-1	24X24	4.00						DOUBLE DOWN ARROW (SYMBOL)	
WO12-2	48X48	16.00						LOW CLEARANCE (SYMBOL)	
WO12-2x	24X18	3.00						LOW CLEARANCE (PLAQUE)	
WO12-2a	84X24	14.00						OVERHEAD LOW CLEARANCE (FEET AND INCHES)	
WO12-4	120X60	50.00						LOW CLEARANCE XX FT XX IN XX MILES AHEAD	
WO12-5	120X60	50.00						WIDTH RESTRICTION XX FT XX IN XX MILES AHEAD	
WO13-1	30X30	6.25						ADVISORY SPEED (PLAQUE)	
WO16-2	30X24	5.00						XXX FEET (PLAQUE)	
WO16-3	30X24	5.00						X MILE (PLAQUE)	
WO20-1	48X48	16.00	6	96				ROAD/BRIDGE/RAMP WORK AHEAD	
WO20-2	48X48	16.00						DETOUR AHEAD	
WO20-3	48X48	16.00						ROAD CLOSED AHEAD	
WO20-4	48X48	16.00	2	32				ONE LANE ROAD AHEAD	
WO20-5	48X48	16.00						RIGHT/CENTER/LEFT LANE CLOSED AHEAD	
WO20-5a	48X48	16.00						2 RIGHT/CENTER/LEFT LANES CLOSED AHEAD	
WO20-6a	48X48	16.00						RIGHT/CENTER/LEFT LANE CLOSED	
WO20-7a	48X48	16.00	2	32				FLAGGER (SYMBOL)	
WO21-2	36X36	9.00						FRESH OIL	
WO21-5	48X48	16.00	2	32				SHOULDER WORK / SHOULDER WORK AHEAD	
WO22-1	48X48	16.00						BLASTING ZONE AHEAD	
WO22-2	42X36	10.50						TURN OFF 2-WAY RADIO AND PHONE	
WO22-3	42X36	10.50						END BLASTING ZONE	
GO22-1	21X15	2.19						WET PAINT (ARROW PIVOTS)	


SIGN	SIZE IN.	AREA SQ.FT.	QTY EACH	TOTAL SQ.FT.	QTY RELOC EACH	TOTAL RELOC SQ.FT.	SIGN NUM.	DESCRIPTION	ITEM NUMBER	TOTAL QTY	DESCRIPTION
GUIDE SIGNS											
E05-1	36X48	12.00						GORE EXIT	6122008		IMPACT ATTENUATOR 40 MPH (SAND BARRELS)
E05-2	48X36	12.00						EXIT OPEN	6122009		IMPACT ATTENUATOR 45 MPH (SAND BARRELS)
E05-2a	48X36	12.00						EXIT CLOSED	6122010		IMPACT ATTENUATOR 50 MPH (SAND BARRELS)
GO20-1	60X24	10.00						ROAD WORK NEXT XX MILES	6122012	2	IMPACT ATTENUATOR 55 MPH (SAND BARRELS)
GO20-2	48X24	8.00	2	16				END ROAD WORK	6122014		IMPACT ATTENUATOR 60 MPH (SAND BARRELS)
GO20-4	36X18	4.50						PILOT CAR FOLLOW ME	6122017		IMPACT ATTENUATOR 65 MPH (SAND BARRELS)
GO20-4a	42X30	8.75						PILOT CAR IN USE WAIT & FOLLOW	6122019		IMPACT ATTENUATOR 70 MPH (SAND BARRELS)
GO20-4a	18X12	1.50						PILOT CAR IN USE WAIT & FOLLOW	6122020	2	REPLACEMENT SAND BARREL
GO20-5aP	36X24	6.00	2	12				WORK ZONE (PLAQUE)	6122030	2	IMPACT ATTENUATOR (RELOCATION)
MO4-8a	24X18	3.00						END DETOUR	6123001		TRUCK MOUNTED ATTENUATOR (TMA)
MO4-9L	48X36	12.00						DETOUR (LEFT)	6161008	2	ADVANCED WARNING RAIL SYSTEM
MO4-9R	48X36	12.00						DETOUR (RIGHT)	6161012		BUOYS (BOATS KEEP OUT)
MO4-9P	48X12	4.00						STREET NAME (PLAQUE)	6161013		BUOYS (NO WAKE)
MO4-10L	48X18	6.00						DETOUR ARROW (LEFT)	6161014		SPECIAL SIGN ASSEMBLY (BOATS KEEP OUT)
MO4-10R	48X18	6.00						DETOUR ARROW (RIGHT)	6161025	50	CHANNELIZER (TRIM LINE)
REGULATORY SIGNS											
R1-1	48X48	13.25						STOP	6161030	9	TYPE III MOVEABLE BARRICADE
R1-2	48TRI.	6.93						YIELD	6161033		DIRECTION INDICATOR BARRICADE
R1-2a	36X36	9.00						TO ONCOMING TRAFFIC (PLAQUE)	6161040		FLASHING ARROW PANEL
R1-3P	30X12	2.50						ALL WAY (PLAQUE)	6161047		TYPE III OBJECT MARKER
R2-1	36X48	12.00	4	48				SPEED LIMIT XX	6161055		SEQUENTIAL FLASHING WARNING LIGHT
R3-1	48X48	16.00						NO RIGHT TURN (SYMBOL)	6161070		TUBULAR MARKER
R3-2	48X48	16.00						NO LEFT TURN (SYMBOL)	6161095		RADAR SPEED ADVISORY SYSTEM
R3-3	36X36	9.00						NO TURNS	6161096		CHANGEABLE MESSAGE SIGN, COMMISSION FURNISHED/RETAINED
R3-4	48X48	16.00						NO U-TURN (SYMBOL)	6161098A		CHANGEABLE MESSAGE SIGN W/O COMM. INTERFACE - CONTRACTOR FURNISHED/RETAINED
R3-7L	30X30	6.25						LEFT LANE MUST TURN LEFT	6161099	2	CHANGEABLE MESSAGE SIGN WITH COMM. INTERFACE - CONTRACTOR FURNISHED/RETAINED
R3-7R	30X30	6.25						RIGHT LANE MUST TURN RIGHT	6162000A	1	WORK ZONE TRAFFIC SIGNAL SYSTEM
R4-1	36X48	12.00						DO NOT PASS	6162002		TEMPORARY LONG-TERM RUMBLE STRIPS
R4-2	36X48	12.00						PASS WITH CARE	6173600D	513	TEMPORARY TRAFFIC BARRIER
R4-7a	36X48	12.00						KEEP RIGHT (HORIZONTAL ARROW)	6173602B		CONTRACTOR FURNISHED/RETAINED
R4-8a	36X48	12.00						KEEP LEFT (HORIZONTAL ARROW)	6174000A		TEMP. TRAFFIC BARRIER HEIGHT TRANSITION
R5-1	30X30	6.25						DO NOT ENTER	6175010A	513	RELOCATING TEMPORARY TRAFFIC BARRIER
R5-1a	36X24	6.00						WRONG WAY	6176000B		TEMPORARY TRAFFIC BARRIER
R6-1L	54X18	6.75						ONE WAY ARROW (LEFT)	6176000B		COMMISSION FURNISHED/RETAINED
R6-1R	54X18	6.75						ONE WAY ARROW (RIGHT)	6177000B		TEMP. TRAFFIC BARRIER HEIGHT TRANSITION
R6-2L	24X30	5.00						ONE WAY (LEFT)	6208064A		COMMISSION FURNISHED/RETAINED
R6-2R	24X30	5.00						ONE WAY (RIGHT)	9029400		TEMPORARY RAISED PAVEMENT MARKER
R9-9	24X12	2.00						SIDEWALK CLOSED	9029401		TEMPORARY TRAFFIC SIGNALS
R9-11L	24X18	3.00						SIDEWALK CLOSED AHEAD, (ARROW LEFT) CROSS HERE			TEMPORARY TRAFFIC SIGNALS AND LIGHTING
R9-11R	24X18	3.00						SIDEWALK CLOSED AHEAD, (ARROW RIGHT) CROSS HERE			
R10-6	24X36	6.00	2	12				STOP HERE ON RED (45^ ARROW)			
R11-2	48X30	10.00	1	10				ROAD CLOSED			
R11-3a	60X30	12.50						ROAD CLOSED XX MILES AHEAD			
R11-4	60X30	12.50						LOCAL TRAFFIC ONLY			
CONST-3A	60X48	20.00						ROAD CLOSED TO THRU TRAFFIC			
CONST-3X	56X12	4.67						FINE SIGN			
								SPEEDING/PASSING (PLATE)			
MISCELLANEOUS SIGNS											
CONST-5	48X36	12.00						POINT OF PRESENCE			
CONST-5	96X48	32.00	2	64				POINT OF PRESENCE			
CONST-7	48X24	8.00						RATE OUR WORK ZONE			
CONST-7	72X36	18.00	2	36				RATE OUR WORK ZONE			
CONST-8	48X36	12.00	2	24				WORK ZONE NO PHONE ZONE			

ALL STATIONS AND/OR LOCATIONS SHOWN ON PLANS ARE APPROXIMATE AND CAN VARY AS APPROVED BY ENGINEER



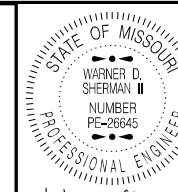
*COLDMILL ENTIRE PREVIOUS LIFT OF BP-1 (1.75")
*TRANSITION BP-1 FILL FROM 2" TO 2.5" (AT BRIDGE APPROACH SLAB)

SCALE



0 30 60 90

SPECIAL SHEETS
SHEET 1 OF 1



Warner Sherman
02/21/2024 3:34:21 PM
WARNER D. SHERMAN II - CIV

MO-PE-26645
DATE PREPARED
2/21/2024

ROUTE	STATE
D	MO

DISTRICT	SHEET NO.
SW	4

COUNTY	POLK
--------	------

JOB NO.
1753509

CONTRACT ID.

PROJECT NO.

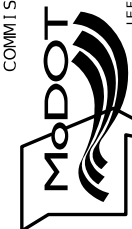
BRIDGE NO.

[illegible]

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

MoDOT

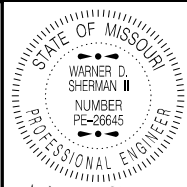
105 WEST CAPITOL
JEFFERSON CITY, MO 65102



GENERAL NOTES:

1. ANY EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL SIGNING SHALL BE COMPLETELY COVERED OR REMOVED.
2. TEMPORARY SIGNING SHOWN IS FOR WORK ON ONE SIDE OF THE ROAD. FOR WORK ON THE OTHER SIDE, REVERSE ORDER OF THE SIGNS AND CHANNELIZERS.
3. SIGNING SHOWN SHALL REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE.
4. ALTERNATE TRAFFIC CONTROL MAY BE USED AS NEEDED AT THE APPROVAL OF THE ENGINEER.
5. REFER TO STANDARD DRAWING 616.10, 619.10, AND 620.10 FOR ADDITIONAL DETAILS AND 903.03 FOR SIGN AND SIGN MOUNTING REQUIREMENTS.
6. SPEED LIMIT SIGNS INDICATING THE NORMAL SPEED LIMIT SHALL BE INSTALLED AT THE END OF THE WORK ZONE, PROVIDED NO FURTHER WORK ZONES WILL BE ENCOUNTERED WITHIN THE NEXT 1/2 MILE.
7. TEMPORARY SPEED LIMIT SIGNS SHALL BE COVERED OR REMOVED WHEN THE CONDITIONS REQUIRING REDUCED SPEEDS DO NOT EXIST.
8. NO DIRECT PAYMENT WILL BE MADE FOR THE RELOCATION OF CHANNELIZERS, CONSTRUCTION SIGNS, OR FLASHING ARROW PANEL.
9. ALL SIGNS, EXCEPT "RATE OUR WORK ZONES" SHALL BE PORTABLE MOUNT AND ARE TO BE MOVED AS WORK PROGRESSES, UNLESS OTHERWISE NOTED. ALL TRAFFIC CONTROL ITEMS SHALL BE REMOVED FROM THE ROADWAY DURING NON-WORKING HOURS.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING PROPER TRAFFIC CONTROL SETUPS THROUGHOUT CONSTRUCTION AS DESCRIBED IN THESE PLANS OR AS APPROVED BY THE ENGINEER.
11. WHERE MINIMUM LANE WIDTHS CANNOT BE ACHIEVED ON THE SIDE STREETS, THE CONTRACTOR SHALL CLOSE HALF OF THE APPROACH AND POSITION A FLAGGER AT EACH END OF THE WORK AREA. LOCATION TO BE DETERMINED BY THE ENGINEER. ADVANCE FLAGGER SIGNING SHALL BE INSTALLED AS OUTLINED IN THE MUTCD.
12. THE CONTRACTOR SHALL MAINTAIN PUBLIC ACCESS TO ALL BUSINESSES AND STREETS. NO STREET OR ENTRANCE SHALL BE COMPLETELY BLOCKED.
13. SPACING & DISTANCES OF TRAFFIC CONTROL DEVICES ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD & APPROVED BY THE ENGINEER.
14. TRAFFIC CONTROL SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
15. ADJUST ADVANCE WARNING SIGN SPACING TO AVOID SIGN PLACEMENT WITHIN THE LIMITS OF THE RAILROAD RIGHT OF WAY.

REVISED: 4/18/2023



Warner D. Sherman II
02/21/2024 9:34:50 PM
WARNER D. SHERMAN II - CIVIL
MO-PE-26645

DATE PREPARED
2/21/2024

ROUTE
D MO

DISTRICT
SW SHEET NO.
5

COUNTY
POLK

JOB NO.
J7S3509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

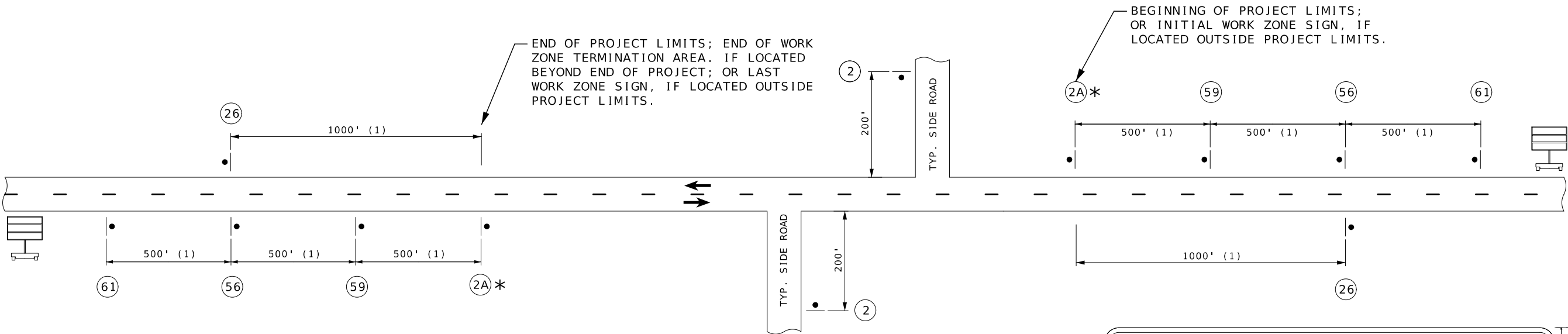
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

MODOT

COMMISSION

NOT TO SCALE

TRAFFIC CONTROL SHEET 1 OF 18



(1) DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS. WHERE TRAFFIC BACKUPS ARE EXPECTED BEYOND THE ADVANCE WARNING AREA, ADDITIONAL SIGNING MAY BE NEEDED.

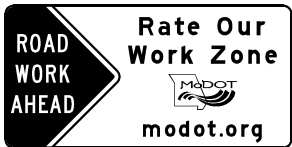
(2) SIGN CONST-7-72 IS PLACED 500 FEET BEFORE THE BEGINNING OF PROJECT LIMITS OR THE ROAD WORK AHEAD SIGN OR ROAD WORK NEXT XX MILES SIGN. IF USED, WHEN THESE SIGNS ARE LOCATED OUTSIDE THE PROJECT LIMITS.

(3) CONST-5-96 SIGN IS PLACED IN A VISIBLE AREA WITHIN THE PROJECT LIMITS PROVIDED ITS PLACEMENT DOES NOT DISRUPT A SEQUENCE OF SIGNS. IF A VISIBLE LOCATION WITHIN THE PROJECT IS NOT AVAILABLE, THE SIGN MAY BE PLACED 500 FEET BEFORE SIGN CONST-7-72.

(4) THE "WORK ZONE NO PHONE ZONE" SIGN IS PLACED A MINIMUM OF 500 FEET BEFORE THE ROAD WORK AHEAD SIGN.

(5) CMS PLACEMENT SHALL BE APPROVED BY THE ENGINEER.

TYPICAL BEGINNING AND END OF PROJECT (UNDIVIDED HIGHWAY)



CONST - 7 - 72

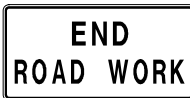
(56) (2)



WO20 - 1



(2A) *



GO20 - 2

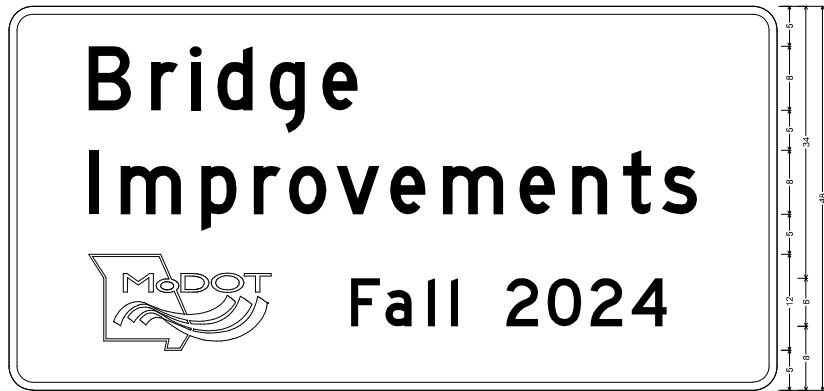
(26)



WO20 - 1

(2)

* SIGN 2A WITH AWRS MUST BE GROUND OR SKID MOUNTED.



CONST-5-96 SHF-FLAT SHEET:

3,000' Radius, 1,000' Border, White on Blue:
"Bridge", D; "Improvements", D; "Fall 2024", D;

Table of letter and object letter											
B	C	D	E	F	G	H	I	J	K	L	M
9.875	17.750	22.750	26.000	32.625	39.125	47.625	54.250	63.750	70.375	76.750	81.625
N	O	P	Q	R	S	T	U	V	W	X	Y
9.875	14.000	24.000	30.625	35.000	41.000	47.625	54.250	63.750	70.375	76.750	81.625
Z	1	2	3	4	5	6	7	8	9	0	+
9.875	42.250	47.000	52.375	55.250	58.125	62.125	67.375	72.625	77.875	83.125	88.375

Completed as Promised

CONST-5P-60 SHF-FLAT SHEET FLUORESCENT:

1,500' Radius, No border, Yellow:

"Completed as Promised" Black, D 65% spacing:

Table of letter and object letter											
C	D	E	F	G	H	I	J	K	L	M	N
1.500	4.875	7.875	12.375	15.375	16.625	19.375	21.500	24.250	26.000	28.000	31.875
O	P	Q	R	S	T	U	V	W	X	Y	Z
36.625	39.875	41.875	44.875	49.500	50.750	53.500	56.250	61.000	63.000	66.000	70.000

CONST - 5 - 96

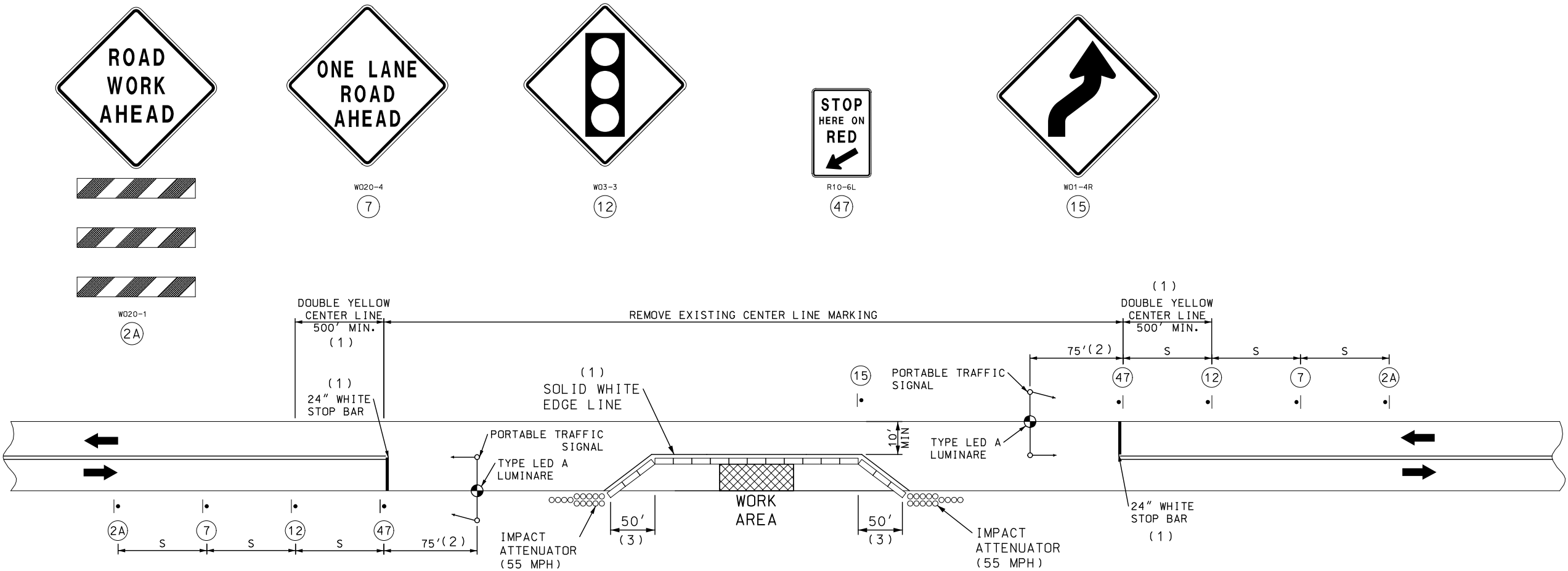
(61) (3)

NOT TO SCALE

TRAFFIC CONTROL
SHEET 1 OF 18

LANE CLOSURE ON A TWO-LANE ROAD USING TEMPORARY TRAFFIC CONTROL SIGNALS
PORTABLE TRAFFIC SIGNAL AND CONCRETE BARRIER DETAILS

NOT TO SCALE



NOTES:

DIMENSIONS IN FEET UNLESS OTHERWISE NOTED.

(1) SIGNING AND PAVEMENT MARKING IDENTICAL ON BOTH APPROACHES.

(2) 75-FOOT RECOMMENDED SPACING. SPACING MAY BE BETWEEN 40-FT AND 180-FOET.

(3) FLARE BARRIER TO EXTEND BEYOND CLEAR ZONE OR FLARE BARRIER TO EDGE LINE AND USE APPROVED END TREATMENT.

TEMPORARY TRAFFIC CONTROL SIGNALS SHALL MEET THE PHYSICAL DISPLAY AND OPERATIONAL REQUIREMENTS OF CONVENTIONAL TRAFFIC CONTROL SIGNALS.

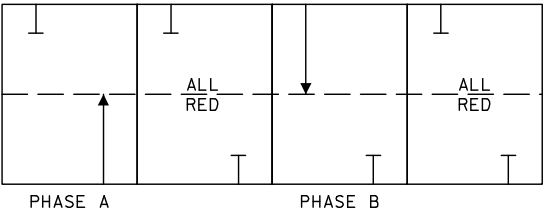
TEMPORARY TRAFFIC CONTROL SIGNAL TIMING SHALL BE ESTABLISHED BY AUTHORIZED OFFICIALS. DURATION OF RED CLEARANCE INTERVALS SHALL BE ADEQUATE TO CLEAR THE ONE-LANE SECTION OF CONFLICTING VEHICLES.

WHEN THE TEMPORARY TRAFFIC CONTROL SIGNAL IS CHANGED TO THE FLASHING MODE, EITHER MANUALLY OR AUTOMATICALLY, RED SIGNAL INDICATIONS SHALL BE FLASHED TO BOTH APPROACHES.

STOP LINES SHALL BE INSTALLED WITH TEMPORARY TRAFFIC CONTROL SIGNALS FOR INTERMEDIATE AND LONG-TERM CLOSURES. EXISTING CONFLICTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKER REFLECTORS BETWEEN THE ACTIVITY AREA AND THE STOP LINE SHALL BE REMOVED, THE STOP LINES AND OTHER TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED AND THE PERMANENT PAVEMENT MARKINGS RESTORED.

SPEED	SIGN SPACING (FT.)		TAPER LENGTH (FT.)		OPTIONAL	CHANNELIZER SPACING (FT.)	
PERM. POST (MPH)	UNDIVIDED (S)	DIVIDED (S)	SHOULDER (1) (T1)	LANE (2) (T2)	BUFFER LENGTH (FT.) (B)	TAPERS	BUFFER/ WORK AREAS
50-55	500	-	-	-	560	-	80

1. SHOULDER TAPER LENGTH BASED ON 10 FT. (STANDARD SHOULDER WIDTH) OFFSET.
2. LANE TAPER LENGTH BASED ON 12 FT. (STANDARD LANE WIDTH) OFFSET.

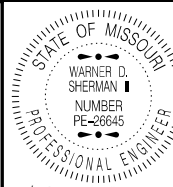


PHASE A AND PHASE B SHALL ALWAYS BE FOLLOWED BY AN ALL RED PHASE OR INTERVAL.

SIGNALS SHALL REST IN ALL RED WHEN THERE ARE NO VEHICLE CALL.

NOT TO SCALE

TRAFFIC CONTROL
SHEET 3 OF 18



Warner D. Sherman
02/21/2024 3:36:36 PM
WARNER D. SHERMAN II - CIVIL
MO-PE-26645

DATE PREPARED
2/21/2024

ROUTE D STATE MO
DISTRICT SW SHEET NO. 7

COUNTY POLK
JOB NO. J7S3509
CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

MoDOT



WO20-1
(2)

BRIDGE
OR
RAMP



WO20-4
(7)



WO20-7a
(8)

GO20-5aP
WORK
ZONE

SPEED
LIMIT
45

R2-1

(54)

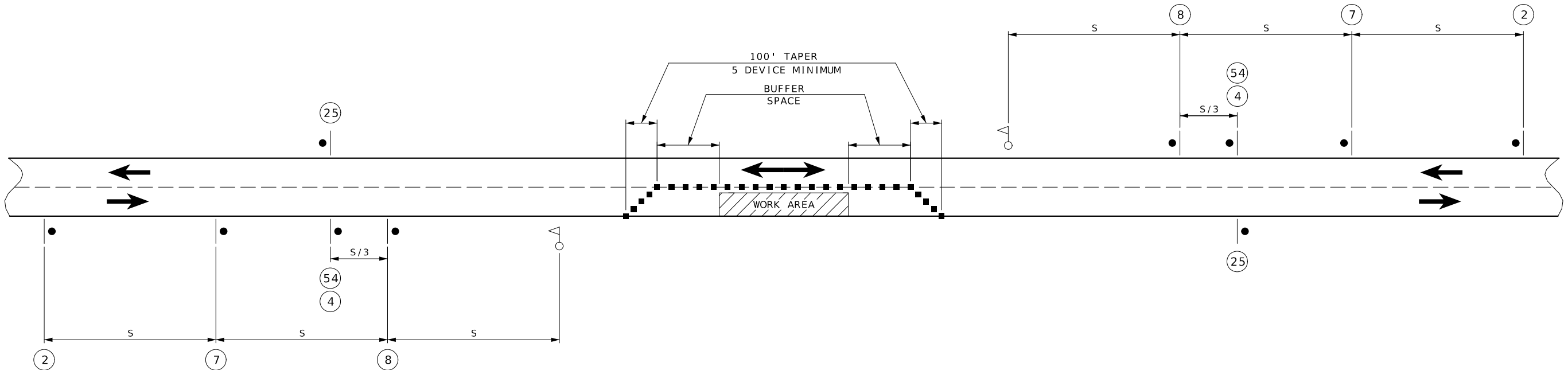
(4)

SPEED
LIMIT
55

R2-1

(25)

(NORMAL SPEED)



NOTES:

CHANNELIZING DEVICES LOCATED DOWNSTREAM OF THE ONE-LANE, TWO-WAY TAPER ARE OPTIONAL. THESE DEVICES SHOULD BE ELIMINATED WHEN THEIR USE WILL REDUCE THE USABLE LANE WIDTH, INCLUDING ANY ACCEPTABLE SHOULDERS, TO LESS THAN 10' OR WILL SIGNIFICANTLY AFFECT THE RESURFACING OPERATION.

FLAG ASSEMBLIES FOR SIGN W020-7a ARE SUBSIDIARY TO THE SIGN.

■ - CHANNELIZERS

○ - FLAGGER

IF USED AT NIGHT, THE FLAGGER STATIONS SHALL BE ILLUMINATED WITH AN AVERAGE MAINTAINED INTENSITY OF 0.6 FOOTCANDLES (6.5 LUX).

LANE CLOSURE USING FLAGGERS
(UNDIVIDED HIGHWAY)

SPEED	SIGN SPACING (FT) (1)	CHANNELIZER SPACING (FT)	OPTIONAL
PERMANENT POSTED (MPH)	UNDIVIDED HIGHWAYS (S)	BUFFER/ WORK AREAS	BUFFER LENGTH (FT)
0-35	200	40	280
40-45	350	80	400
50-55	500	80	560
60-70	1000	120	840

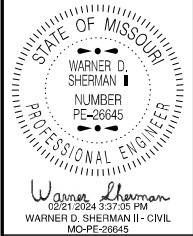
SPACING NOTES:

(1) SPACING BETWEEN SIGNS, BETWEEN LAST SIGN AND FLAGGER, BEGINNING OF TAPER OR SIGNED CONDITION.

SPACINGS MAY BE ADJUSTED AS NECESSARY TO MEET FIELD CONDITIONS, AS APPROVED BY ENGINEER.

NOT TO SCALE

TRAFFIC CONTROL
SHEET 4 OF 18



DATE PREPARED
2/21/2024

ROUTE D STATE MO

DISTRICT SW SHEET NO. 8

COUNTY POLK

JOB NO. J7S3509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

MODOT





Warner D. Sherman
02/21/2024 9:37:58 PM
WARNER D. SHERMAN II - CIVIL
MO-PE-26645

DATE PREPARED
2/21/2024

ROUTE D	STATE MO
------------	-------------

DISTRICT SW	SHEET NO. 10
----------------	-----------------

COUNTY
POLK

JOB NO.
J7S3509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

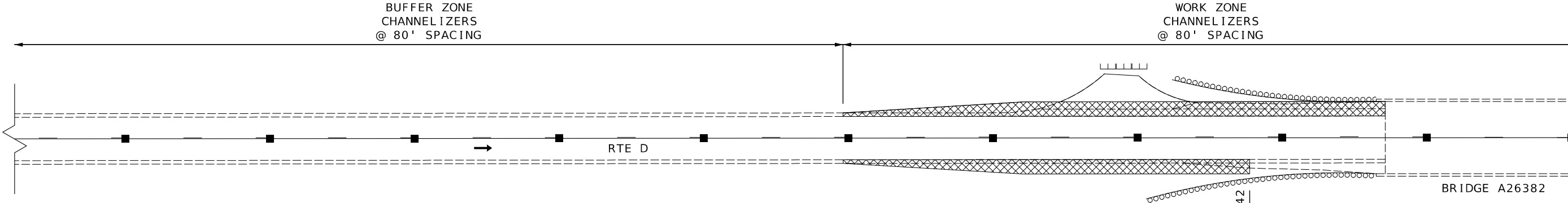
DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION










105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

TRAFFIC CONTROL STAGE 1
SHOULDER WORK WITH LANE CLOSURE USING FLAGGERS
CONSTRUCT FULL DEPTH SHOULDERS
REVERSE TAPER AND LANE CLOSURE FOR SB SHOULDER WORK AREA



TRAFFIC CONTROL LEGEND

-  SIGN (SINGLE SIDED)
-  FLAGGER
-  CHANNELIZER
-  BARRICADE
-  CHANGEABLE MESSAGE BOARD
-  PROTECTIVE VEHICLE (OPTIONAL)
-  WORK AREA

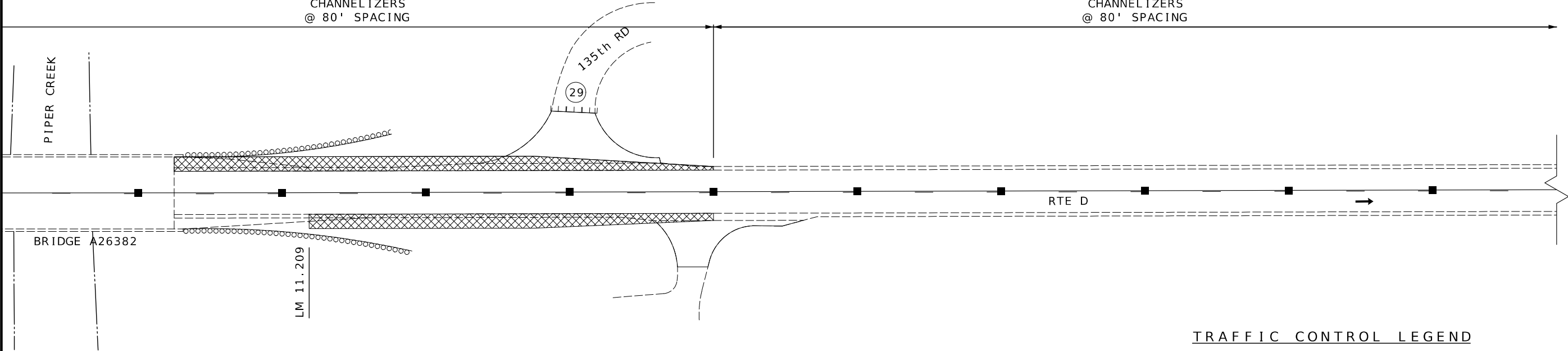


R11-2
29

TRAFFIC CONTROL STAGE 1
SHOULDER WORK WITH LANE CLOSURE USING FLAGGERS
CONSTRUCT FULL DEPTH SHOULDERS
REVERSE TAPER AND LANE CLOSURE FOR SB SHOULDER WORK AREA

WORK ZONE
CHANNELIZERS
@ 80' SPACING

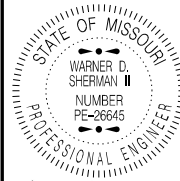
BUFFER ZONE
CHANNELIZERS
@ 80' SPACING



TRAFFIC CONTROL LEGEND

- SIGN (SINGLE SIDED)
- ◁ FLAGGER
- CHANNELIZER
- E BARRICADE
- CHANGEABLE MESSAGE BOARD
- PROTECTIVE VEHICLE (OPTIONAL)
- WORK AREA

NOT TO SCALE
TRAFFIC CONTROL
SHEET 7 OF 18



Warner D. Sherman
02/21/2024 5:38:26 PM
WARNER D. SHERMAN II - CIVIL
MO-PE-26645

DATE PREPARED
2/21/2024

ROUTE	STATE
D	MO
DISTRICT	SHEET NO.
SW	11

COUNTY
POLK

JOB NO.
J7S3509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



BRIDGE
OR
RAMP



BRIDGE
OR
RAMP



WO20-1

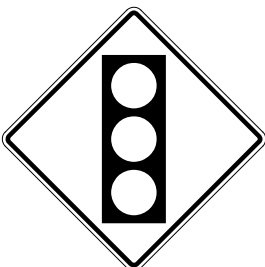
(2A) *

*SIGN 2A WITH AWRS MUST
BE GROUND OR SKID MOUNTED.



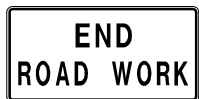
WO20-4

(7)



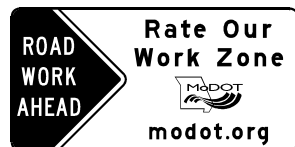
WO3-3

(12)



GO20-2

(26)



CONST-7-72

(56) (2)



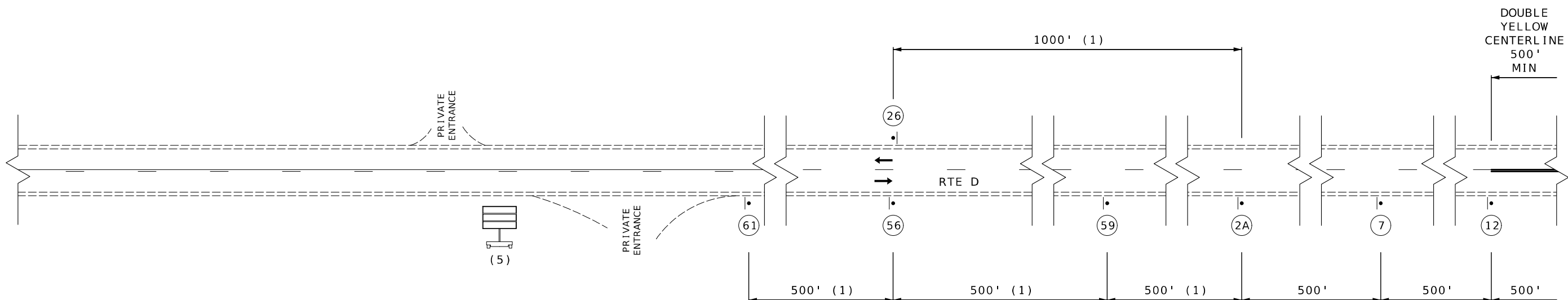
CONST-8

(59) (4)



TRAFFIC CONTROL STAGE 2

CLOSE SB LANE AT BRIDGE
UTILIZE TEMPORARY SIGNAL AND SHIFT TRAFFIC ONTO NB LANE
CONSTRUCT NEW FULL DEPTH SB SHOULDER
MODIFIED MILL/FILL SB LANE TO NEW BRIDGE ROADWAY HEIGHT



FOR SIGN 61 DETAILS, SEE TRAFFIC CONTROL SHEET 1 OF 18.

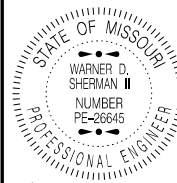
TRAFFIC CONTROL LEGEND

- (1) DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS. WHERE TRAFFIC BACKUPS ARE EXPECTED BEYOND THE ADVANCE WARNING AREA, ADDITIONAL SIGNING MAY BE NEEDED.
- (2) SIGN CONST-7-72 IS PLACED 500 FEET BEFORE THE BEGINNING OF PROJECT LIMITS OR THE ROAD WORK AHEAD SIGN OR ROAD WORK NEXT XX MILES SIGN. IF USED, WHEN THESE SIGNS ARE LOCATED OUTSIDE THE PROJECT LIMITS.
- (3) CONST-5-96 SIGN IS PLACED IN A VISIBLE AREA WITHIN THE PROJECT LIMITS PROVIDED ITS PLACEMENT DOES NOT DISRUPT A SEQUENCE OF SIGNS. IF A VISIBLE LOCATION WITHIN THE PROJECT IS NOT AVAILABLE, THE SIGN MAY BE PLACED 500 FEET BEFORE SIGN CONST-7-72.
- (4) THE "WORK ZONE NO PHONE ZONE" SIGN IS PLACED A MINIMUM OF 500 FEET BEFORE THE ROAD WORK AHEAD SIGN.
- (5) CMS PLACEMENT SHALL BE APPROVED BY THE ENGINEER.

- SIGN (SINGLE SIDED)
- ◁ FLAGGER
- CHANNELIZER
- E BARRICADE
- CHANGEABLE MESSAGE BOARD
- PROTECTIVE VEHICLE (OPTIONAL)
- WORK AREA

NOT TO SCALE

TRAFFIC CONTROL
SHEET 9 OF 18



Warner D. Sherman II
02/21/2024 3:39:16 PM
WARNER D. SHERMAN II - CIVIL
MO-PE-26645

DATE PREPARED

2/21/2024

ROUTE D STATE MO

DISTRICT SW SHEET NO. 13

COUNTY POLK

JOB NO. J7S3509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

DATE

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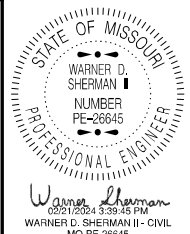
DATE

DATE

DATE

DATE

DATE



DATE PREPARED	
2/21/2024	
ROUTE	STATE
D	MO
DISTRICT	SHEET NO.
SW	14
COUNTY	
POLK	
JOB NO.	
J7S3509	
CONTRACT ID.	

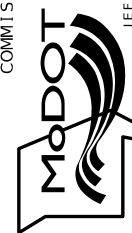
PROJECT NO.
BRIDGE NO.

[illegible]

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

MoDOT








105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

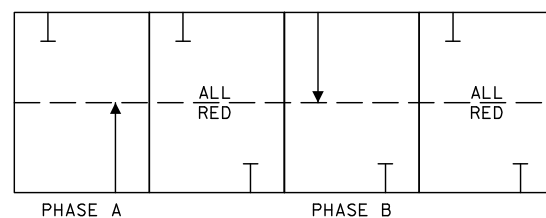


47

CLOSE SB LANE AT BRIDGE
UTILIZE TEMPORARY SIGNAL AND SHIFT TRAFFIC ONTO NB LANE
CONSTRUCT NEW FULL DEPTH SB SHOULDER
MODIFIED MILL/FILL SB LANE TO NEW BRIDGE ROADWAY HEIGHT



- | | |
|---|-------------------------------|
|  | SIGN (SINGLE SIDED) |
|  | FLAGGER |
|  | CHANNELIZER |
|  | BARRICADE |
|  | CHANGEABLE MESSAGE BOARD |
|  | PROTECTIVE VEHICLE (OPTIONAL) |
|  | WORK AREA |



PHASE A AND PHASE B SHALL ALWAYS BE FOLLOWED BY
AN ALL RED PHASE OR INTERVAL.

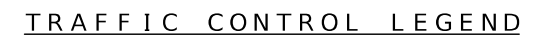
SIGNALS SHALL REST IN ALL RED WHEN THERE ARE NO
VEHICLE CALL.








NOT TO SCALE

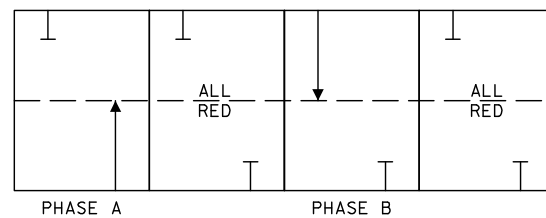
TRAFFIC CONTROL
SHEET 10 OF 18



CLOSE SB LANE AT BRIDGE
UTILIZE TEMPORARY SIGNAL AND SHIFT TRAFFIC ONTO NB LANE
CONSTRUCT NEW FULL DEPTH SB SHOULDER
MODIFIED MILL/FILL SB LANE TO NEW BRIDGE ROADWAY HEIGHT

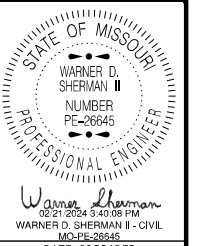


- | | |
|---|-------------------------------|
|  | SIGN (SINGLE SIDED) |
|  | FLAGGER |
|  | CHANNELIZER |
|  | BARRICADE |
|  | CHANGEABLE MESSAGE BOARD |
|  | PROTECTIVE VEHICLE (OPTIONAL) |
|  | WORK AREA |



SIGNALS SHALL REST IN ALL RED WHEN THERE ARE NO VEHICLE CALL.

TRAFFIC CONTROL
SHEET 11 OF 18



DATE PREPARED

2 / 21 / 2024

ROUTE	STATE
D	MO

DISTRICT	SHEET NO.
SW	15

COUNTY

POLK

JOB NO.
J7S3509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

[illegible]

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION



JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



BRIDGE
OR
RAMP



BRIDGE
OR
RAMP



WO20-1

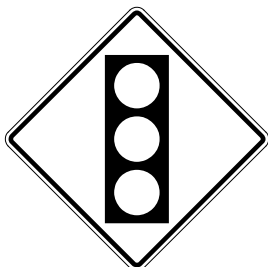
(2A) *

*SIGN 2A WITH AWRS MUST
BE GROUND OR SKID MOUNTED.



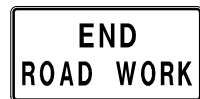
WO20-4

(7)



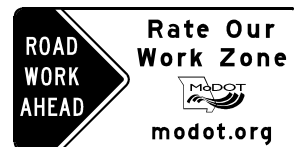
WO3-3

(12)



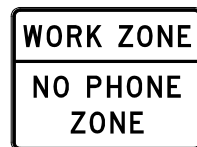
GO20-2

(26)



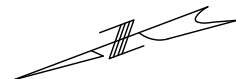
CONST-7-72

(56) (2)



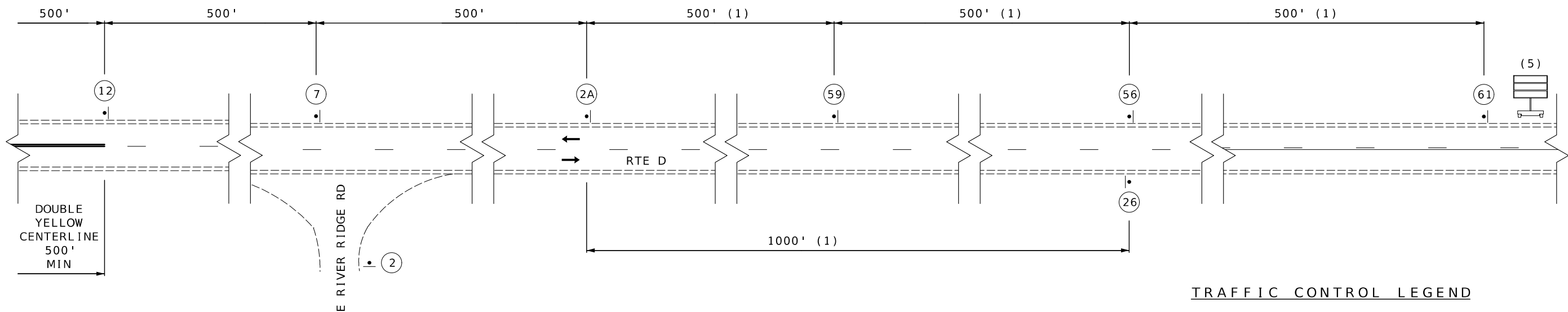
CONST-8

(59) (4)



TRAFFIC CONTROL STAGE 2

CLOSE SB LANE AT BRIDGE
UTILIZE TEMPORARY SIGNAL AND SHIFT TRAFFIC ONTO NB LANE
CONSTRUCT NEW FULL DEPTH SB SHOULDER
MODIFIED MILL/FILL SB LANE TO NEW BRIDGE ROADWAY HEIGHT



FOR SIGN 61 DETAILS, SEE TRAFFIC CONTROL SHEET 1 OF 18.

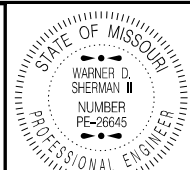
- (1) DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS. WHERE TRAFFIC BACKUPS ARE EXPECTED BEYOND THE ADVANCE WARNING AREA, ADDITIONAL SIGNING MAY BE NEEDED.
- (2) SIGN CONST-7-72 IS PLACED 500 FEET BEFORE THE BEGINNING OF PROJECT LIMITS OR THE ROAD WORK AHEAD SIGN OR ROAD WORK NEXT XX MILES SIGN. IF USED, WHEN THESE SIGNS ARE LOCATED OUTSIDE THE PROJECT LIMITS.
- (3) CONST-5-96 SIGN IS PLACED IN A VISIBLE AREA WITHIN THE PROJECT LIMITS PROVIDED ITS PLACEMENT DOES NOT DISRUPT A SEQUENCE OF SIGNS. IF A VISIBLE LOCATION WITHIN THE PROJECT IS NOT AVAILABLE, THE SIGN MAY BE PLACED 500 FEET BEFORE SIGN CONST-7-72.
- (4) THE "WORK ZONE NO PHONE ZONE" SIGN IS PLACED A MINIMUM OF 500 FEET BEFORE THE ROAD WORK AHEAD SIGN.
- (5) CMS PLACEMENT SHALL BE APPROVED BY THE ENGINEER.

TRAFFIC CONTROL LEGEND

- SIGN (SINGLE SIDED)
- ◁ FLAGGER
- CHANNELIZER
- E BARRICADE
- ☐ CHANGEABLE MESSAGE BOARD
- ☐ PROTECTIVE VEHICLE (OPTIONAL)
- ☐ WORK AREA

NOT TO SCALE

TRAFFIC CONTROL
SHEET 12 OF 18



Warner D. Sherman
02/21/2024 9:40:31 PM
WARNER D. SHERMAN II - CIVIL
MO-PE-26645

DATE PREPARED

2/21/2024

ROUTE STATE

D MO

DISTRICT SHEET NO.

SW 16

COUNTY

POLK

JOB NO.

J7S3509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION



105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



WO20 - 1

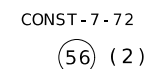
(2A) *

*SIGN 2A WITH AWRS MUST
BE GROUND OR SKID MOUNTED.



GO20-2

(26)

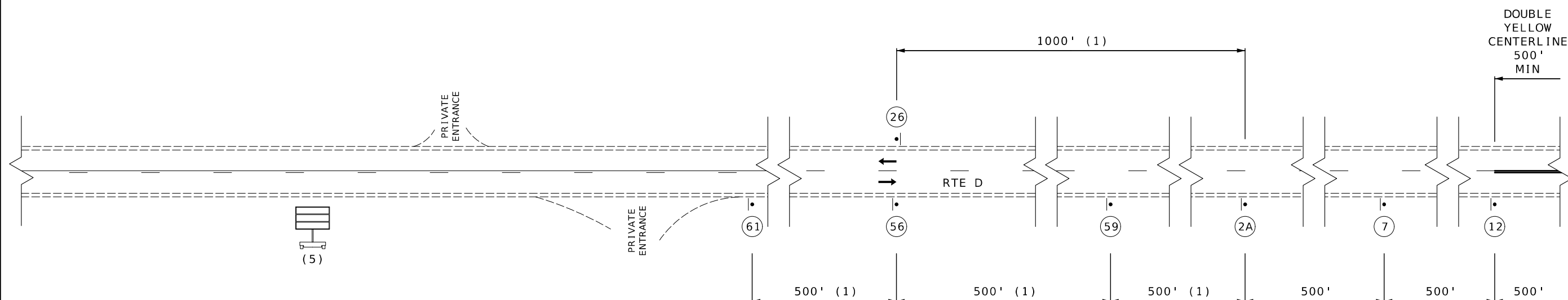


CONST-8
(59) (4)



TRAFFIC CONTROL STAGE 3








CLOSE NB LANE AT BRIDGE
UTILIZE TEMPORARY SIGNAL AND SHIFT TRAFFIC ONTO SB LANE
MODIFIED MILL/FILL NB LANE AND SHOULDER
TO NEW BRIDGE ROADWAY HEIGHT



FOR SIGN 61 DETAILS, SEE TRAFFIC CONTROL SHEET 1 OF 18.

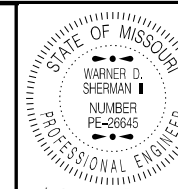
- (1) DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS. WHERE TRAFFIC BACKUPS ARE EXPECTED BEYOND THE ADVANCE WARNING AREA, ADDITIONAL SIGNING MAY BE NEEDED.
- (2) SIGN CONST-7-72 IS PLACED 500 FEET BEFORE THE BEGINNING OF PROJECT LIMITS OR THE ROAD WORK AHEAD SIGN OR ROAD WORK NEXT XX MILES SIGN. IF USED, WHEN THESE SIGNS ARE LOCATED OUTSIDE THE PROJECT LIMITS.
- (3) CONST-5-96 SIGN IS PLACED IN A VISIBLE AREA WITHIN THE PROJECT LIMITS PROVIDED ITS PLACEMENT DOES NOT DISRUPT A SEQUENCE OF SIGNS. IF A VISIBLE LOCATION WITHIN THE PROJECT IS NOT AVAILABLE, THE SIGN MAY BE PLACED 500 FEET BEFORE SIGN CONST-7-72.
- (4) THE "WORK ZONE NO PHONE ZONE" SIGN IS PLACED A MINIMUM OF 500 FEET BEFORE THE ROAD WORK AHEAD SIGN.
- (5) CMS PLACEMENT SHALL BE APPROVED BY THE ENGINEER.

TRAFFIC CONTROL LEGEND

- | | |
|---|-------------------------------|
|  | SIGN (SINGLE SIDED) |
|  | FLAGGER |
|  | CHANNELIZER |
|  | BARRICADE |
|  | CHANGEABLE MESSAGE BOARD |
|  | PROTECTIVE VEHICLE (OPTIONAL) |
|  | WORK AREA |

NOT TO SCALE

TRAFFIC CONTROL
SHEET 13 OF 18



Warner Sherman
02/21/2024 3:41:01 PM
WARNER D. SHERMAN II - CIV

MO-PE-26645
DATE PREPARED
2/21/2024

ROUTE D	STATE MO
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DISTRICT	SHEET NO.
SW	17

COUNTY
POLK

JOB NO.
1753509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.








[illegible]MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

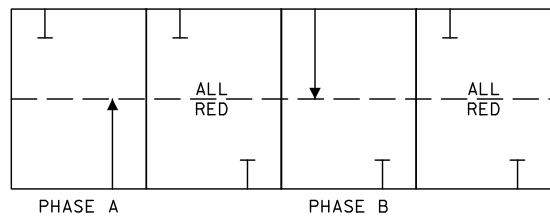
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



CLOSE NB LANE AT BRIDGE
UTILIZE TEMPORARY SIGNAL AND SHIFT TRAFFIC ONTO SB LANE
MODIFIED MILL/FILL NB LANE AND SHOULDER
TO NEW BRIDGE ROADWAY HEIGHT



- | | |
|---|-------------------------------|
|  | SIGN (SINGLE SIDED) |
|  | FLAGGER |
|  | CHANNELIZER |
|  | BARRICADE |
|  | CHANGEABLE MESSAGE BOARD |
|  | PROTECTIVE VEHICLE (OPTIONAL) |
|  | WORK AREA |

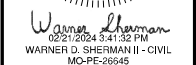


PHASE A AND PHASE B SHALL ALWAYS BE FOLLOWED BY
AN ALL RED PHASE OR INTERVAL.

SIGNALS SHALL REST IN ALL RED WHEN THERE ARE NO
VEHICLE CALL.

NOT TO SCALE

TRAFFIC CONTROL
SHEET 14 OF 18



DATE PREPARED
2/21/2024

ROUTE D	STATE MO
------------	-------------

DISTRICT	SHEET NO.
SW	18


COUNTY
POLK

JOB NO.
J7S3509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

[illegible]

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)





WO20-1

2

BRIDGE
OR
RAMP



WO20-1

2A *

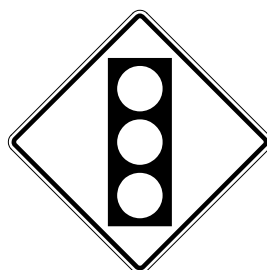
*SIGN 2A WITH AWRS MUST
BE GROUND OR SKID MOUNTED.

BRIDGE
OR
RAMP



WO20-4

7



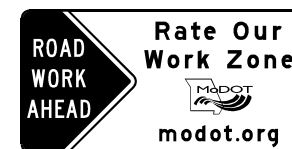
WO3-3

12

**END
ROAD WORK**

GO20-2

26



CONST-7-72

(56) (2)



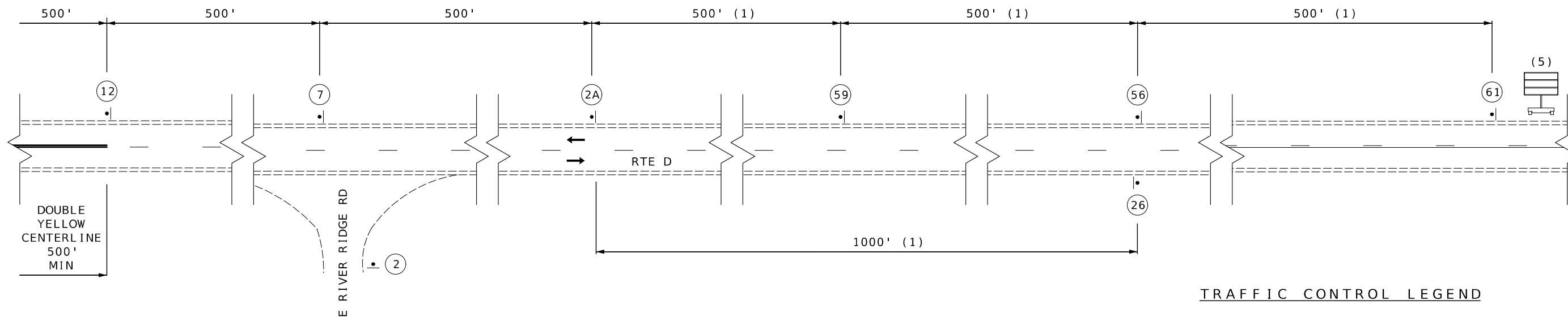
CONST-8

(59) (4)










TRAFFIC CONTROL STAGE 3

CLOSE NB LANE AT BRIDGE
UTILIZE TEMPORARY SIGNAL AND SHIFT TRAFFIC ONTO SB LANE
MODIFIED MILL/FILL NB LANE AND SHOULDER
TO NEW BRIDGE ROADWAY HEIGHT



TRAFFIC CONTROL LEGEND

- | | |
|---|-------------------------------|
|  | SIGN (SINGLE SIDED) |
|  | FLAGGER |
|  | CHANNELIZER |
|  | BARRICADE |
|  | CHANGEABLE MESSAGE BOARD |
|  | PROTECTIVE VEHICLE (OPTIONAL) |
|  | WORK AREA |

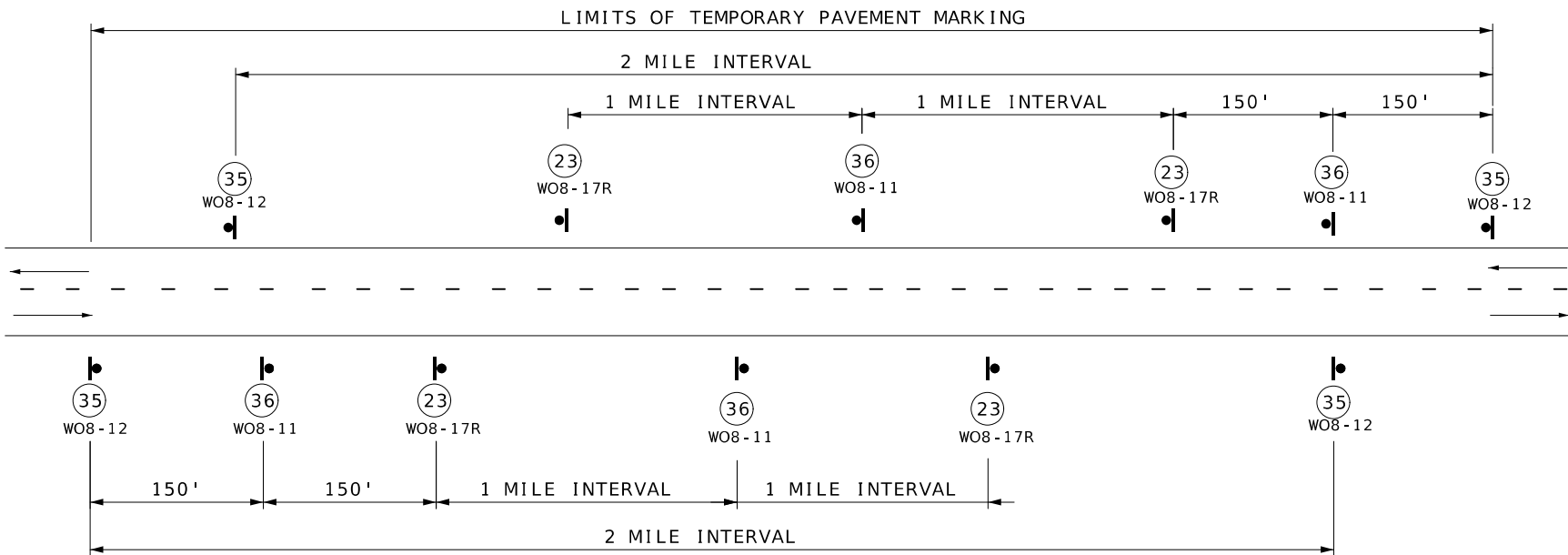
FOR SIGN 61 DETAILS, SEE TRAFFIC CONTROL SHEET 1 OF 18.

- (1) DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS. WHERE TRAFFIC BACKUPS ARE EXPECTED BEYOND THE ADVANCE WARNING AREA, ADDITIONAL SIGNING MAY BE NEEDED.
- (2) SIGN CONST-7-72 IS PLACED 500 FEET BEFORE THE BEGINNING OF PROJECT LIMITS OR THE ROAD WORK AHEAD SIGN OR ROAD WORK NEXT XX MILES SIGN. IF USED, WHEN THESE SIGNS ARE LOCATED OUTSIDE THE PROJECT LIMITS.
- (3) CONST-5-96 SIGN IS PLACED IN A VISIBLE AREA WITHIN THE PROJECT LIMITS PROVIDED ITS PLACEMENT DOES NOT DISRUPT A SEQUENCE OF SIGNS. IF A VISIBLE LOCATION WITHIN THE PROJECT IS NOT AVAILABLE, THE SIGN MAY BE PLACED 500 FEET BEFORE SIGN CONST-7-72.
- (4) THE "WORK ZONE NO PHONE ZONE" SIGN IS PLACED A MINIMUM OF 500 FEET BEFORE THE ROAD WORK AHEAD SIGN.
- (5) CMS PLACEMENT SHALL BE APPROVED BY THE ENGINEER.

NOT TO SCALE

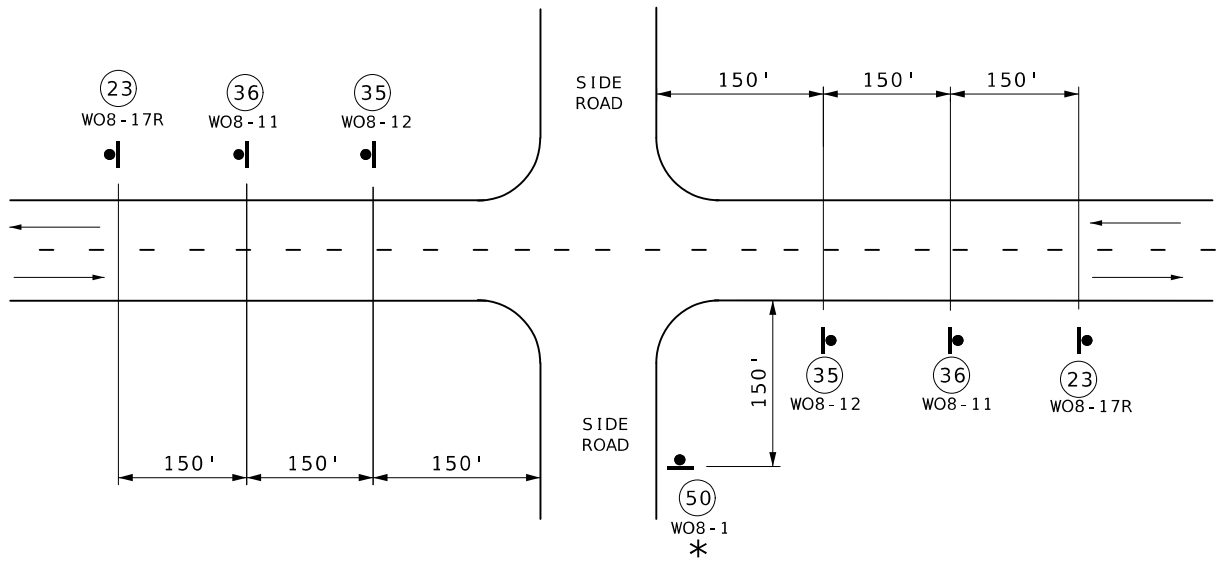
TRAFFIC CONTROL
SHEET 16 OF 18

[illegible]



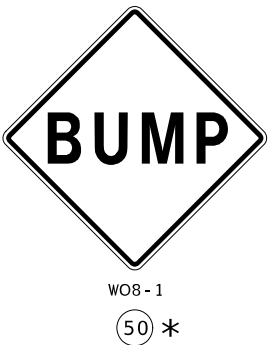
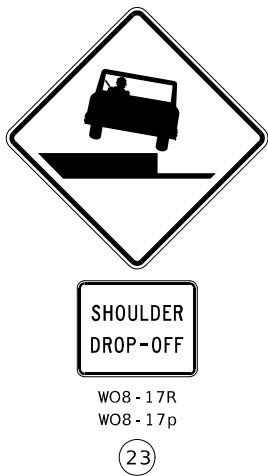
SIGN SPACING FOR MAINLINE

(DETAIL SHOWN IS BASED ON A PROJECT MEETING ALL CONDITIONS: NO CENTER STRIPE, UNEVEN LANES, SHOULDER DROP-OFF AND BUMP.)
WHEN BOTH UNEVEN LANES AND SHOULDER DROP-OFF SIGNS ARE USED, BOTH SIGHS SHALL STAY IN PLACE UNTIL BOTH CONDITIONS NO LONGER EXIST.
IF ONLY ONE CONDITION EXISTS (UNEVEN LANES OR SHOULDER DROP-OFF). THE SIGN SPACING SHALL BE AT 1 MILE INTERVALS.



SIGN SPACING AT SIDE ROAD INTERSECTIONS

* BUMP SIGN SHOULD BE IN ACCORDANCE WITH STANDARD PLAN 619.10.
UNLESS REQUIRED BY STANDARD PLAN 619.10, BUMP SIGN AT CONTRACTOR'S
EXPENSE, NO DIRECT PAY. SEE STANDARD SPECIFICATION 622.2.4.



GENERAL NOTES:

FOR DETAILS OF TEMPORARY PAVEMENT MARKING, SEE
STD. 620.10, SHEET 1 OF 5.

SIGN (35) AND TEMPORARY PAVEMENT MARKING INSTALLED
WHERE CENTERLINE STRIPING HAS BEEN COVERED OR
REMOVED. SIGNS ARE TO REMAIN IN PLACE UNTIL THE
PERMANENT CENTERLINE PAVEMENT MARKINGS ARE IN PLACE.
SIGNS SHALL BE COVERED OR REMOVED WHEN PAVEMENT
CENTERLINE MARKING HAS BEEN INSTALLED.

SIGN (35) IS PLACED AT APPROXIMATELY TWO-MILE INTERVALS
AND AT STATE ROUTE JUNCTIONS. WHEN THE INSTALLATION
AT A JUNCTION IS WITHIN ONE-EIGHTH MILE OF THE NORMAL
MAINLINE SIGN (35), THE LATTER MAY BE ELIMINATED.

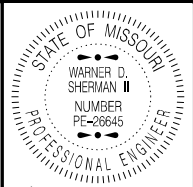
ALL SIGNS SHALL BE POST MOUNTED AND IN ACCORDANCE
WITH STANDARD PLAN 616.10 AND 903.03.

WHEN SHOULDER DROP-OFF SIGNS ARE IN PLACE FOR GREATER
THAN THREE DAYS, THE SHOULDER DROP-OFF PLAQUE SHOULD
BE USED IN ADDITION WITH THE SHOULDER DROP-OFF SIGN.

FOR BRIDGE EXCEPTIONS AND COLDMILLED AREAS, LOCATION
OF SIGN (50) TO BE APPROVED BY THE ENGINEER. UNLESS
REQUIRED BY STANDARD PLAN 619.10, SIGN (50) AT
CONTRACTOR'S EXPENSE, NO DIRECT PAY. SEE STANDARD
SPECIFICATION 622.2.4.

NOT TO SCALE

TRAFFIC CONTROL
SHEET 17 OF 18



Warner D. Sherman II
02/21/2024 3:42:48 PM
WARNER D. SHERMAN II - CIVIL
MO-PE-26645

DATE PREPARED
2/21/2024

ROUTE D STATE MO

DISTRICT SW SHEET NO. 21

COUNTY POLK

JOB NO. J7S3509

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

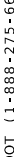
DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

MODOT

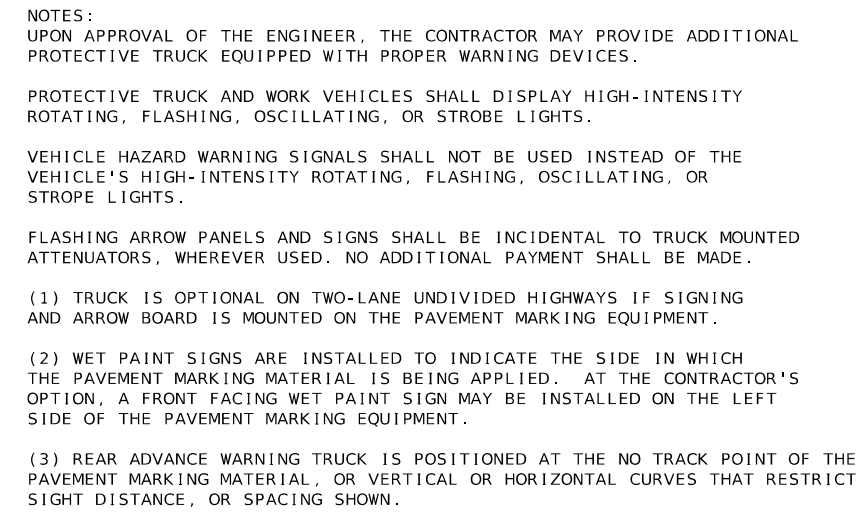
BRIDGE NO.

[illegible]

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

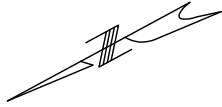
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

27



TRAFFIC CONTROL
SHEET 18 OF 18

NOTE: ALL DEVICES WILL BE INSTALLED AS NECESSARY
BASED ON THE DISCRETION OF PROJECT PERSONNEL



Warner D. Sherman
02/21/2024 9:43:43 PM
WARNER D. SHERMAN II - CIVIL
MO-PE-26645

DATE PREPARED
2/21/2024

ROUTE	STATE
D	MO

DISTRICT	SHEET NO.
SW	23

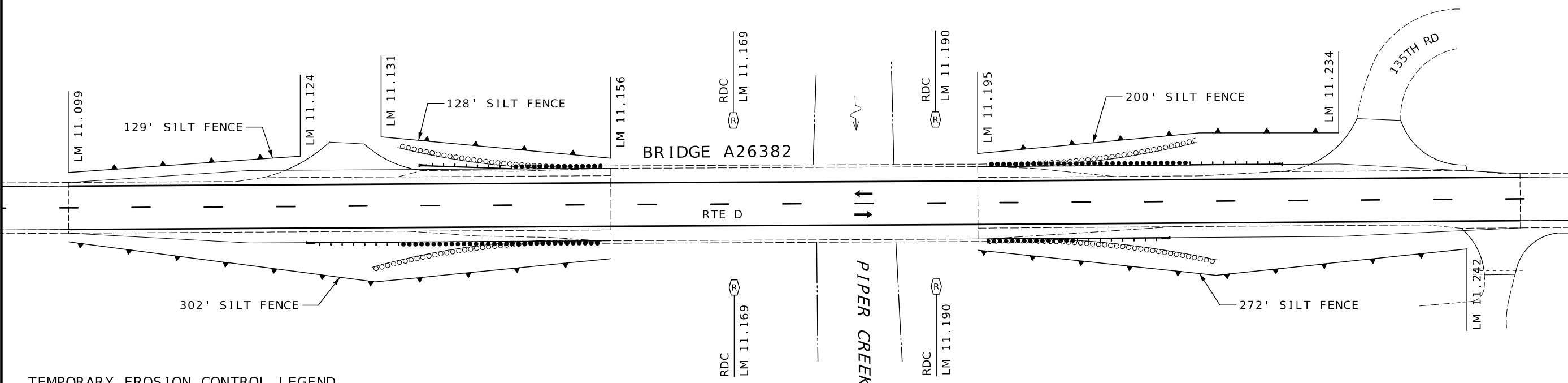
COUNTY
POLK

JOB NO.
J7S3509

CONTRACT ID.

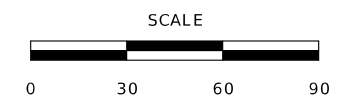
PROJECT NO.

BRIDGE NO.



TEMPORARY EROSION CONTROL LEGEND

- ALTERNATE DITCH CHECK
- CURB INLET CHECK
- ROCK DITCH CHECK
- SEDIMENT TRAP
- ENERGY DISSIPATOR
- TEMPORARY BERM TYPE B
- TEMPORARY BERM TYPE C
- SILT FENCE
- TEMPORARY SEDIMENT BASIN
- PERMANENT SEDIMENT BASIN

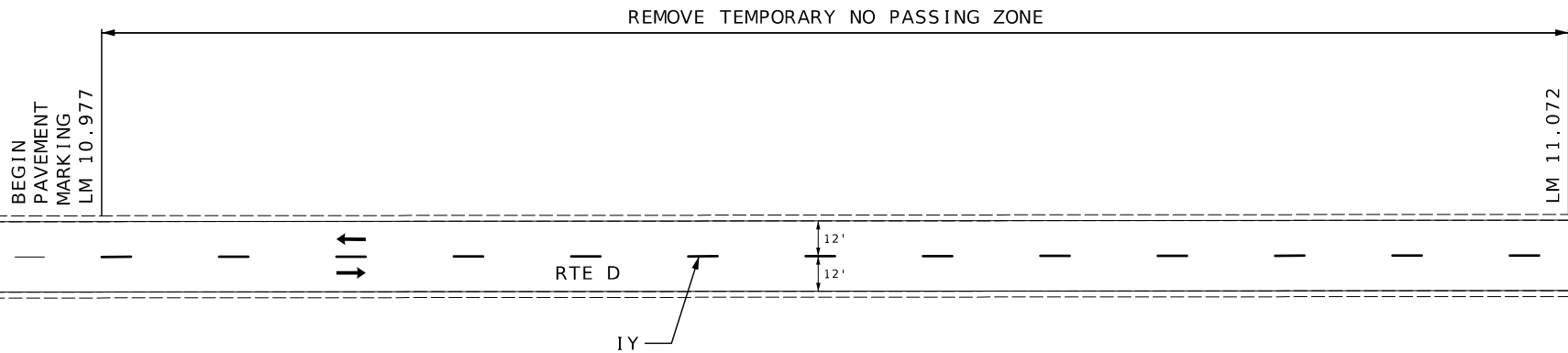


EROSION CONTROL
SHEET 1 OF 1

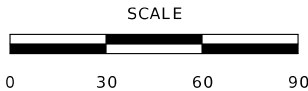
DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

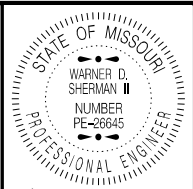
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



IY - 4" INTERMITTENT YELLOW



PAVEMENT MARKING
SHEET 1 OF 3



Warner D. Sherman
02/21/2024 3:44:07 PM
WARNER D. SHERMAN II - CIVIL
MO-PE-26645

DATE PREPARED
2/21/2024

ROUTE D STATE MO

DISTRICT SW SHEET NO. 24

COUNTY POLK

JOB NO. J7S3509

CONTRACT ID.

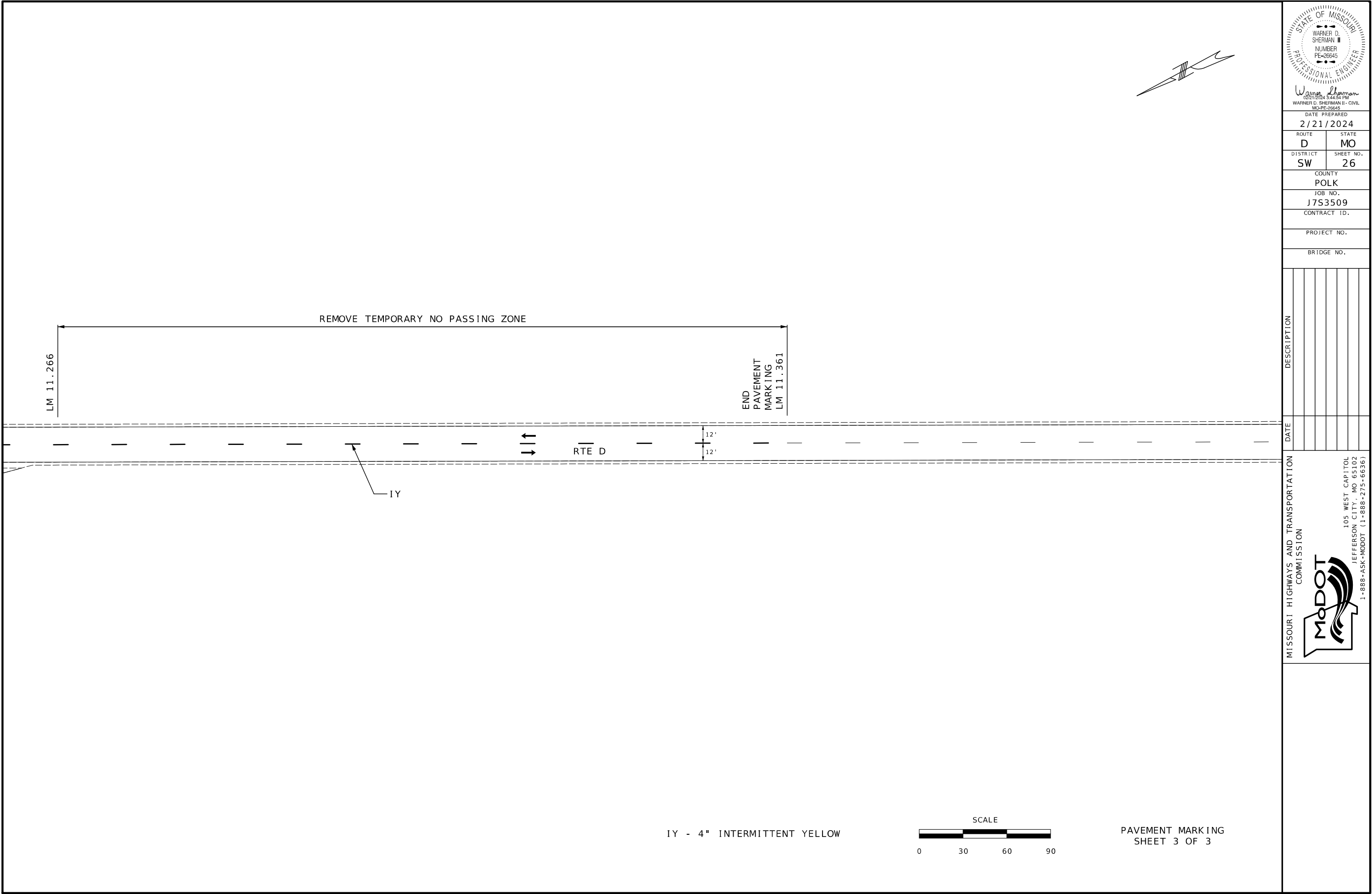
PROJECT NO.

BRIDGE NO.

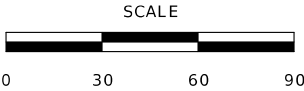
DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



IY - 4" INTERMITTENT YELLOW



PAVEMENT MARKING
SHEET 3 OF 3

STATE OF MISSOURI

WARNER D. SHERMAN II

NUMBER PE-26645

PROFESSIONAL ENGINEER

Warner D. Sherman

02/21/2024 3:44:54 PM

WARNER D. SHERMAN II - CIVIL

MO-PE-26645

DATE PREPARED
2/21/2024

ROUTE
D

DISTRICT
SW

STATE
MO

SHEET NO.
26

COUNTY
POLK

JOB NO.
J7S3509

CONTRACT ID.

PROJECT NO.

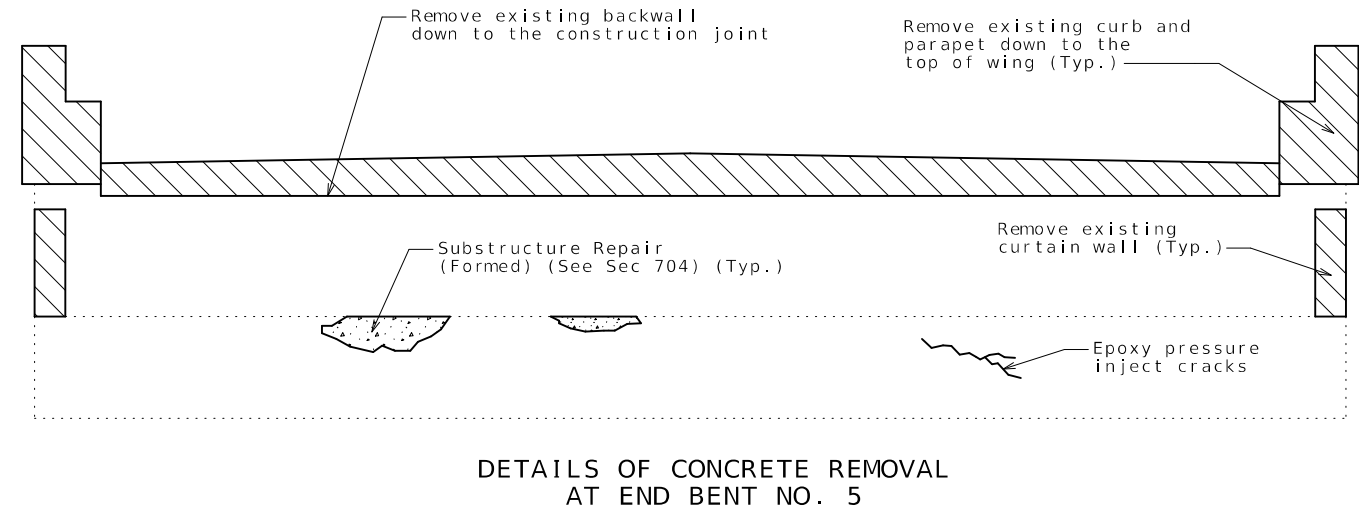
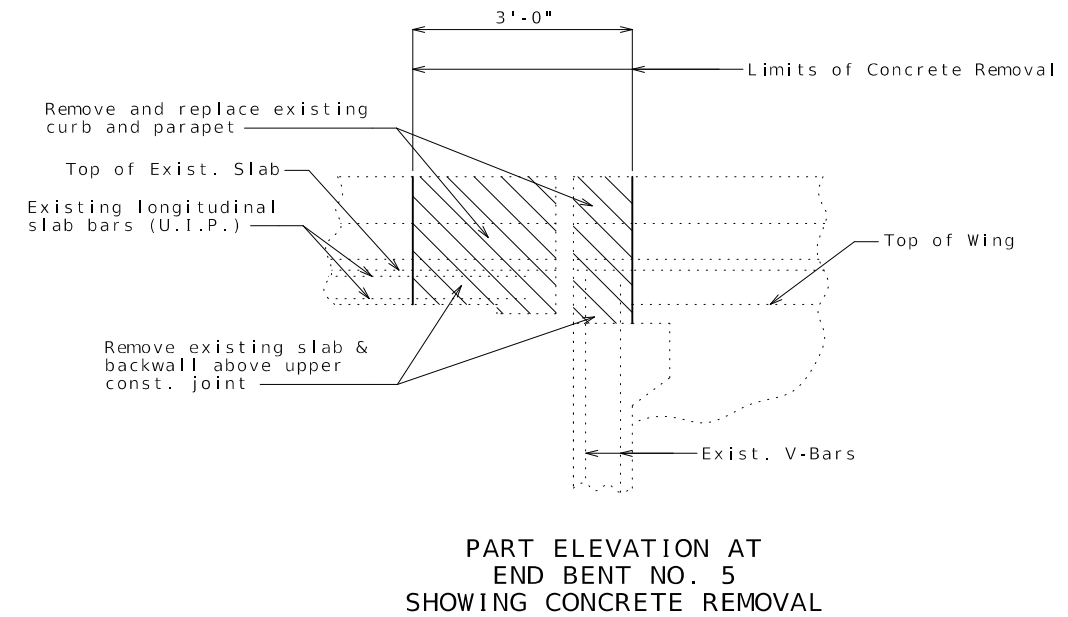
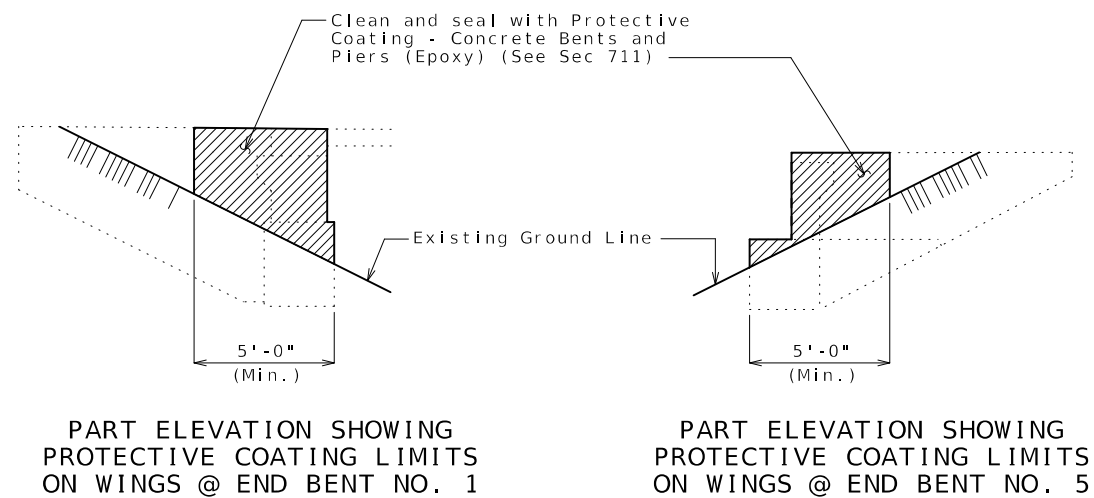
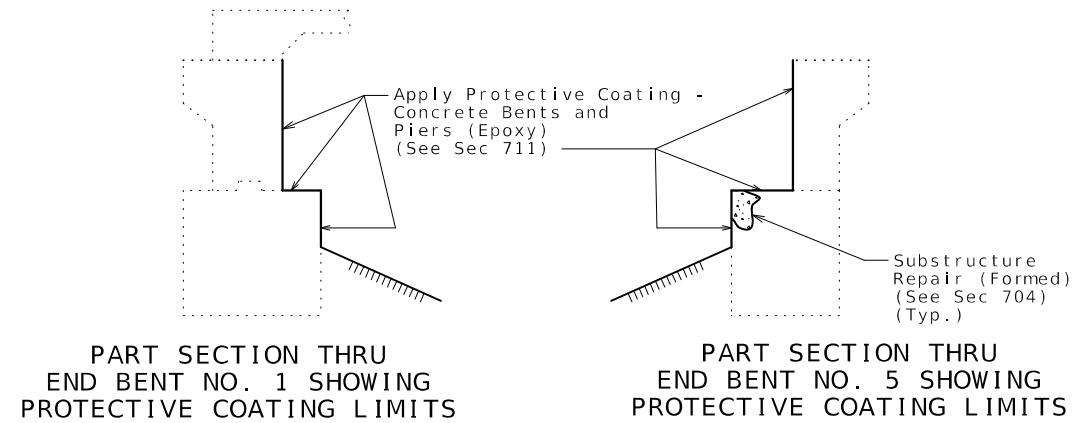
BRIDGE NO.

DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

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105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

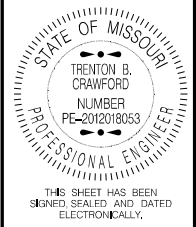


Notes:

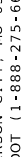
The cost of concrete removal, top of backwall and curtain walls, as shown will be considered completely covered by the contract unit price for Removal of Existing Expansion Joint and Adjacent Concrete. Vertical backwall and curtain wall reinforcement shall be cut off one inch below concrete removal surface and the resulting holes shall be filled with a qualified special mortar.

The cost of removing and replacing the curb and parapet will be considered completely covered by the contract unit price for Remove and Replace Curb and Parapet.

A smooth, level surface shall be provided at removal lines.

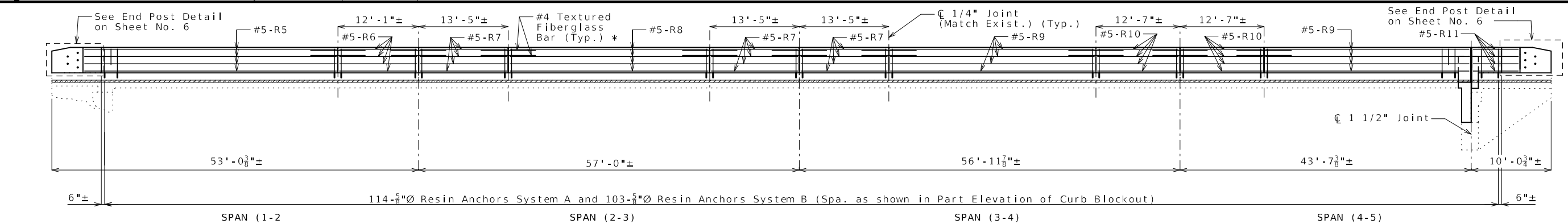


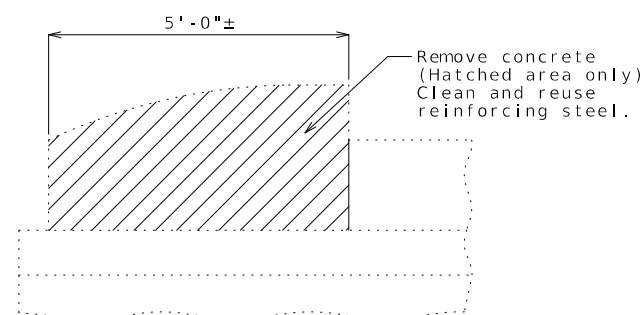
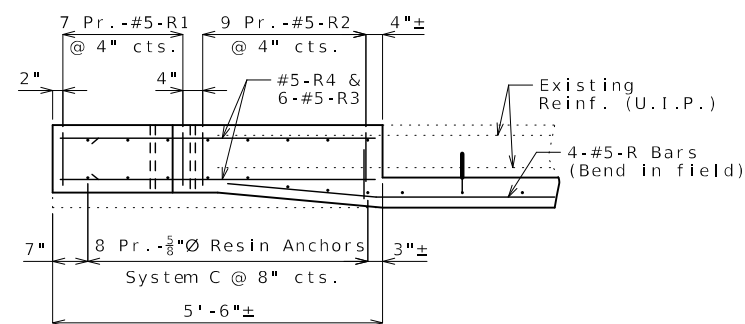
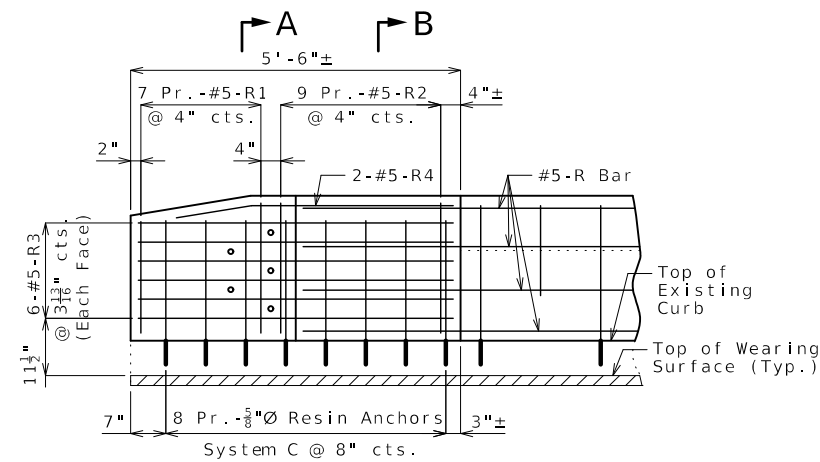
DATE PREPARED	
2/13/2024	
ROUTE	STATE
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J7S3509	
CONTRACT ID.	
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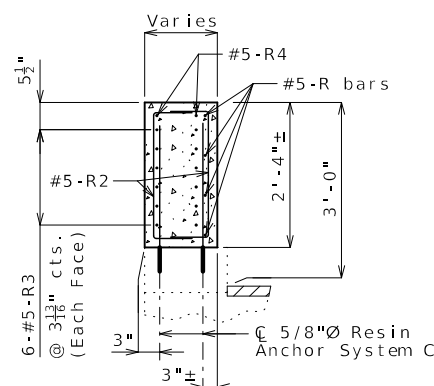
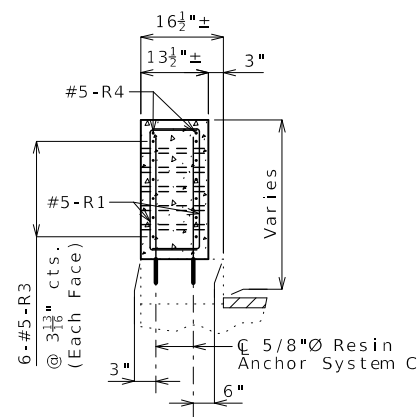
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COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)





Cost of removing existing end posts will be considered completely covered by the contract unit price for Curb Blockout.

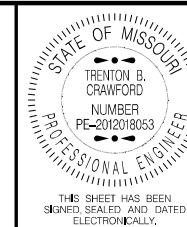
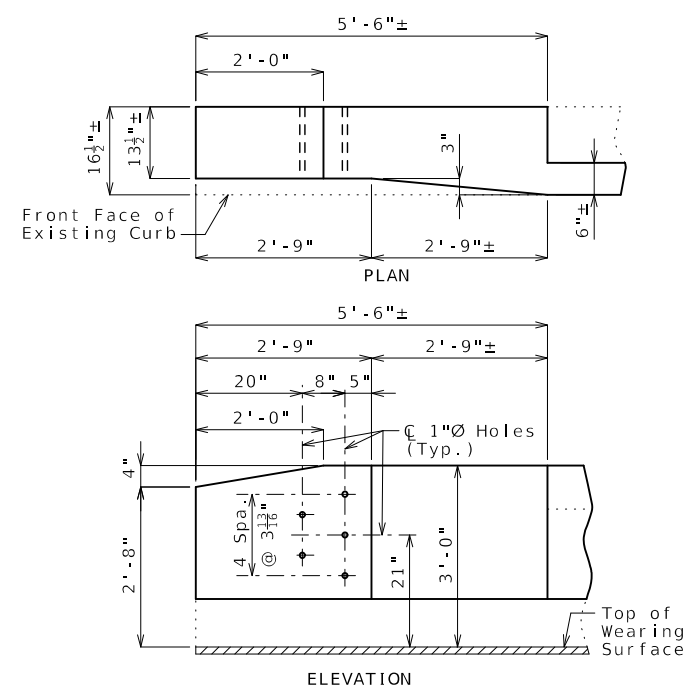
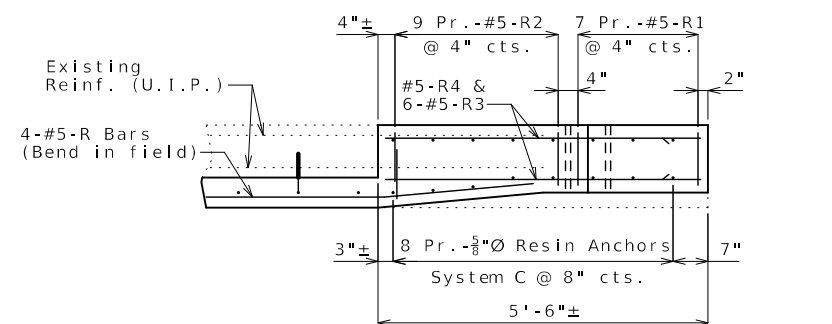
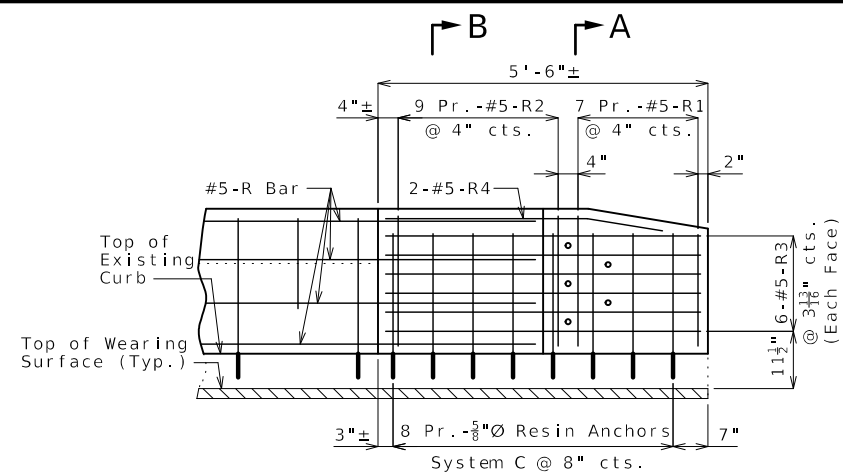


Notes:

Work this sheet with Sheet No. 5.

For details of resin anchors, see Sheet No. 5.

Resin anchors shall be shifted or bent in field to clear one-inch diameter holes by at least 1/2 inch.



DATE PREPARED

2/13/2024

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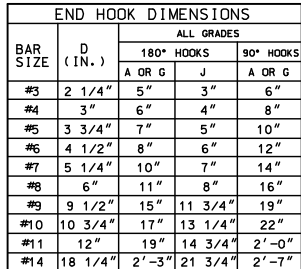
COUNTY

POLK

JOB NO.

[illegible]MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102

[illegible]

REINFORCING STEEL (GRADE 60) $F_Y = 60,000$ PSI.

[illegible]