# MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION PLANS FOR PROPOSED

STATE HIGHWAY
VARIOUS COUNTIES

SOUTHEAST DISTRICT

## STE. GENEVIEVE T. FRANCOI PERRY IRON MADISON CAPE GIRARDEAU REYNOLDS BOLLINGER TEXAS WRIGHT SHANNON WAYNE SCOTT CARTER DOUGLAS STODDARD MISSISSIPP HOWELL BUTLER OREGON RIPLEY OZARK NEW MADRID PEMISCOT DUNKLIN

CONVENTIONAL SYMBOLS

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-UTV-- OT-

- UT -- OE -- UE -- S -- SS - ••••

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<del>-OTV-</del>

-<del>UTV</del>--<del>OT</del>-

UT OE

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(USED IN PLANS)

BUILDINGS AND STRUCTURES

LOCATION SURVEY MARKER

OVERHEAD CABLE TV

SANITARY SEWER STORM SEWER GAS WATER MANHOLE

FIRE HYDRANT

WATER VALVE
WATER METER

DROP INLET

DITCH BLOCK
GROUND MOUNTED SIGN

H-FRAME POWER POLE
TELEPHONE PEDESTAL

FENCE CHAIN LINK WOVEN WIRE GATE POST

BENCHMARK

NOTE: DASHED OR OPEN SYMBOLS INDICATE EXISTING FEATURES

UNDERGROUND CABLE TV OVERHEAD TELEPHONE

UNDERGROUND TELEPHONE OVERHEAD POWER UNDERGROUND POWER

CONCRETE RIGHT-OF-WAY MARKER STEEL RIGHT-OF-WAY MARKER

GUARD RAIL

GUARD CARLE

UTILITIES FIBER OPTICS

## ADA TRANSITION PLAN IMPROVEMENTS JOB ORDER CONTRACTING

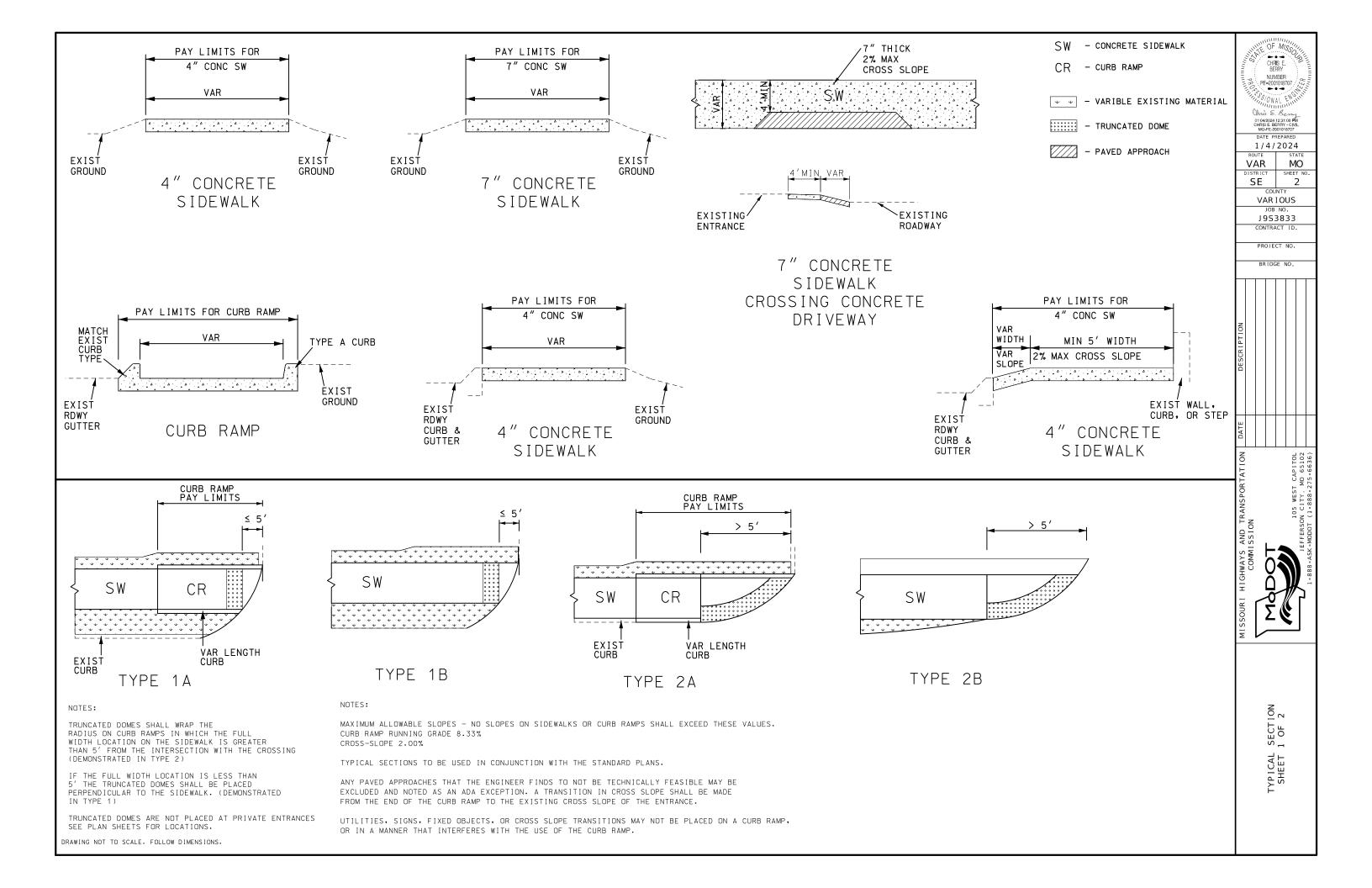
THE EXISTENCE AND APPROXIMATE LOCATION OF UTILITY FACILITIES KNOWN TO EXIST, AS SHOWN ON THE PLANS, ARE BASED ON THE BEST INFORMATION AVAILABLE TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE. RELIANCE UPON THIS INFORMATION IS DONE AT THE RISK AND PERIL OF THE USER, AND THE COMMISSION SHALL NOT BE LIABLE FOR ANY DAMAGES THAT MAY ARISE FROM ANY ERROR IN THE INFORMATION. IT IS, THEREFORE, THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE, LOCATION AND STATUS OF ANY FACILITY. SUCH VERIFICATION INCLUDES DIRECT CONTACT WITH THE LISTED UTILITIES.

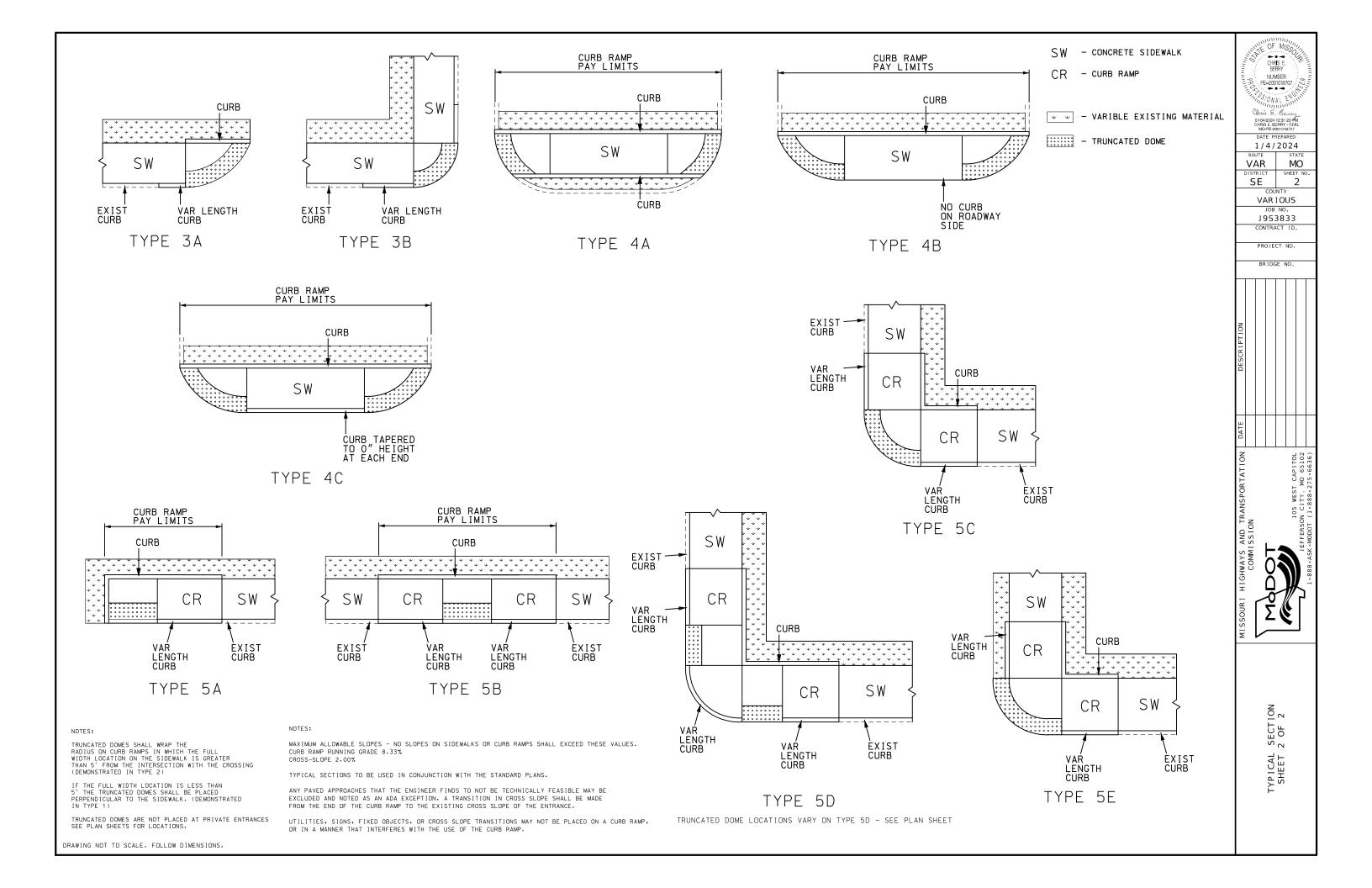
#### INDEX OF SHEETS

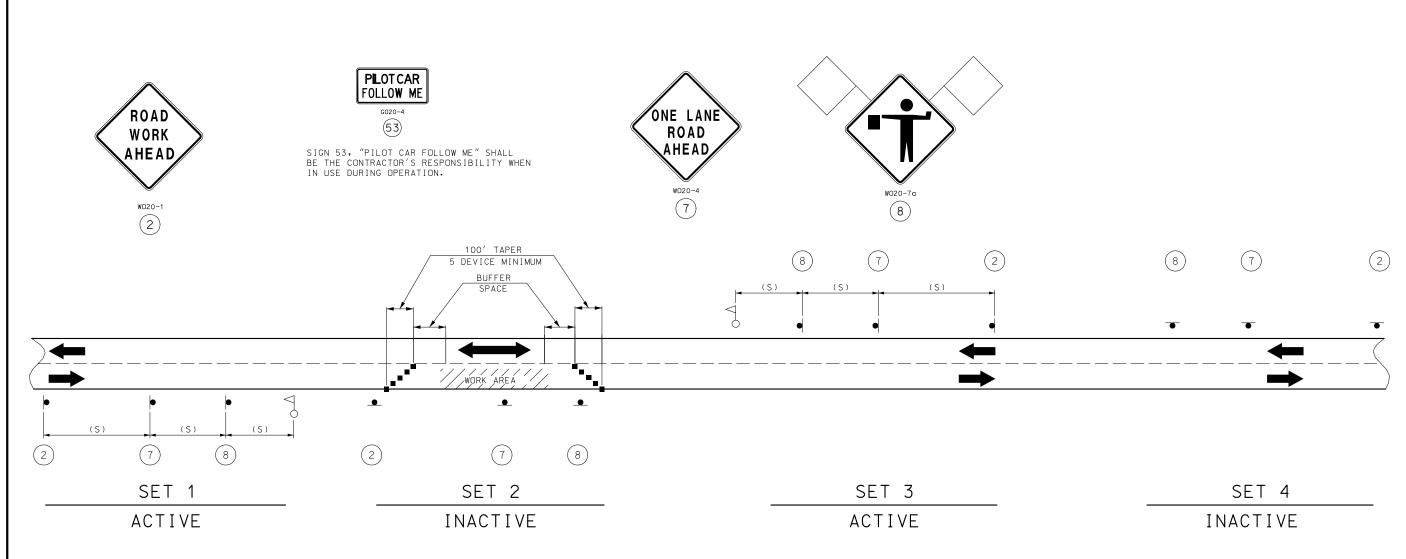
DESCRIPTION	SHEET NUMBER
TITLE SHEET	1
TYPICAL SECTION (TS) (2 SHEETS)	2
TRAFFIC CONTROL SHEETS (TC)	3-6

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NOTES:

DAYLIGHT FLAGGING OPERATIONS ONLY.

CHANNELIZING DEVICES LOCATED DOWNSTREAM OF THE ONE-LANE, TWO-WAY TAPER ARE OPTIONAL. THESE DEVICES SHOULD BE ELIMINATED WHEN THEIR USE WILL REDUCE SHENUSEBEGILAND WIBRE, ACRICHEDAND ANYEACCERCHBLEAGHOONDEMSNOTO LESS THAN TBAFDRCWISLGNIGNISUZAMINDY 4AKRECTNAHETINESUURFACINGIONERBOUNDT FACE EITHER DIRECTION OF TRAFFIC) WHEN THE RESURFACING OPERATION IS LOCATED BETWEEN SIGN SETS 1 AND 3.

WHEN SIGN SETS 2 AND 4 ARE ACTIVE, SIGN SETS 1 AND 3 BECOME INACTIVE AND ARE ADVANCED TO BECOME SETS 2 AND 4 WITH SIGN LEGENDS TURNED AWAY FROM BOTH DIRECTIONS OF TRAFFIC. WHEN THE RESURFACING OPERATION ADVANCES TO BETWEEN SIGN SETS 2 AND 4, SIGN SETS 2 AND 4, SIGN SETS 2 AND 4 BECOME ACTIVE (I.E., NEW SIGN SETS 1 AND 3) AND SIGN SETS 1 AND 3 ADVANCED IN THE DIRECTION OF THE OPERATION (I.E., NEW SIGN SETS 2 AND 4).

WHEN TEMPORARY RUMBLE STRIPS ARE USED, REFER TO EPG FIGURE 616.6.87.1 FOR DISTANCES AND SPACING.

- CHANNELIZERS

- FLAGGER

SPEED	SIGN SPACING (FT.) (1)
PERMANENT POSTED (MPH)	NON-DIVIDED HIGHWAYS (S)
0-35	200
40-45	350
50-55	500
60-70	1000

#### NOTES:

(1) SPACING BETWEEN SIGNS, BETWEEN LAST SIGN AND FLAGGER, BEGINNING OF TAPER OR SIGNED CONDITION.

SPACINGS MAY BE ADJUSTED AS NECESSARY TO MEET FIELD CONDITIONS.

PERMANENT POSTED	BUFFER SPACE			
SPEED LIMIT	LENGTH			
(MPH)	(FT)			
0 - 35	280			
40 - 45	400			
50 - 55	560			
60 - 70	840			

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Chris E. Benz 01/04/2024 12:31:43 PM CHRIS E. BERRY - CIVIL MO-PE-2001018707

1/4/2024

VAR MO
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JOB NO.

J9S3833

CONTRACT ID.

BRIDGE NO.

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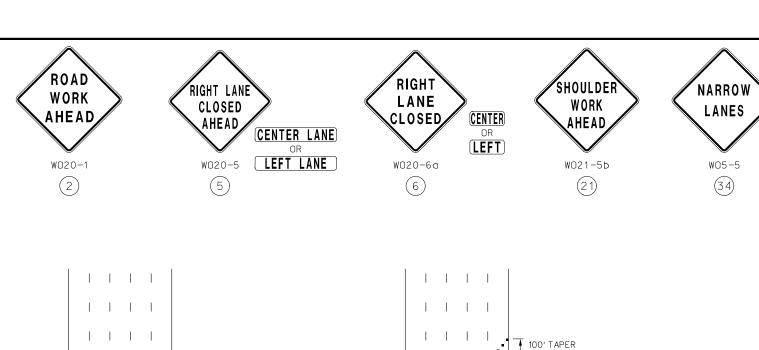
105 WEST CAPITOL

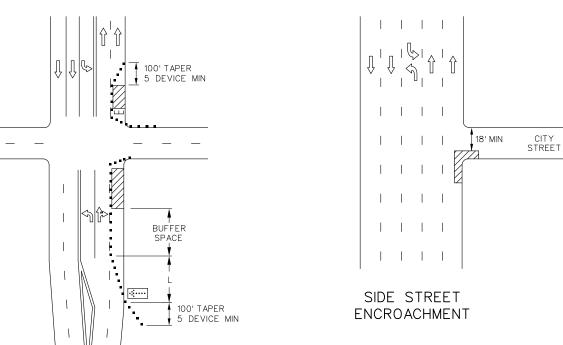
16FERSON CITY, MO 65102

TRAFFIC CONTROL SHEET 1 OF 4

- FLAGGER

DRAWING NOT TO SCALE. FOLLOW DIMENSIONS.





MINOR **ENCROACHMENT** 

SIGN SPACING FOR ADVANCE SIGN SERIES (2) (3)

NON-DIVIDED HIGHWAYS

200 FT

350 FT

500 FT

(2) SPACING BETWEEN SIGNS, BETWEEN LAST SIGN AND FLAGGER, BEGINNING OF TAPER OR SIGNED CONDITION.

SA - 1000 FT, SB - 1500 FT AND SC - 2640 FT

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10' LANE MIN → I

▲ 100' TAPER

100' TAPER

5 DEVICE MIN

• (34)

• (21)

<u>•</u> (2)

DIVIDED

200 FT

500 FT

1000 FT

5 DEVICE MIN

TAPER LENGTH AND SPACING 60 - 70 700 FT 770 FT 840 FT 60 FT (5) 100

LANE

**CLOSURE** 

(1)	POSTED	SPEED	LIMIT	PRIOR	TO	ROAD	WORK

- (2) TAPER LENGTHS MAY BE ADJUSTED TO ACCOMMODATE CROSSROADS, CURVES, INTERSECTIONS, RAMPS, OR OTHER GEOMETRIC FEATURES.
- (3) SPACING MAY BE REDUCED TO DISCOURAGE TRAFFIC ENCROACHMENT.
- (4) SPACING MAY BE REDUCED TO 1/2 AT INTERSECTIONS.

SPEED (1)	TAPER	LENGTH L	FOR	CHANNELIZE	ER SPACING (3)				
(MPH)	LATERAL SHIFTS (2) (4)			TAPERS	BUFFER SPACE				
	10 FT	11 FT	12 FT		WORK AREA				
0 - 35	205 FT	225 FT	245 FT	35 FT (4)	50 FT (4)				
40 - 45	450 FT	495 FT	540 FT	40 FT (4)	100 FT (4)				
50 - 55	550 FT	605 FT	660 FT	50 FT (5)	100 FT (5)				

	BUFFER SPACE	LENGTHS
ACING (3)	POSTED SPEED PRIOR	
FER SPACE	TO CONSTRUCTION	LENGTH
ORK AREA	(MPH)	
O FT (4)	0 - 35	250 FT
0 FT (4)	40 - 45	360 FT
0 FT (5)	50 - 55	495 FT
0 FT (5)	60 - 70	730 FT

5 DEVICE MIN

5 DEVICE MIN

BUFFER SPACE

**•** (5)

<u>•</u> 2

NOTES:

**•** (5)

RIGHT LANE CLOSURE

AT INTERSECTION WORK ON NEAR OR FAR SIDE

> ALL SIGNS SHALL BE POST MOUNTED AND IN ACCORDANCE WITH STANDARD PLAN 616.10 AND 903.03. DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS.

> ANY EXISTING SIGNING THAT CONFLICTS WITH THE TRAFFIC CONTROL SIGNING SHALL BE COMPLETELY COVERED OR REMOVED.

NO PAYMENT WILL BE MADE FOR RELOCATION OF CONSTRUCTION SIGNS.

CHRIS E. BERRY NUMBER PE-2001018707 SONAL 01/04/2024 12:32:02 PM CHRIS E. BERRY - CIVIL MO-PE-2001018707 1/4/2024 VAR MO SE 4

TRAFFIC CONTROL LEGEND

NON-PORTABLE SIGN

MOVABLE BARRICADE

FLASHING ARROW PANEL

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TRAFFIC SHEET 2

(3)	SPACIN	IGS MAY	BE A	DJUS	TED	AS	NECESSARY	TO	MEET	
	FIELD	CONDITI	ONS	AND	VISI	BIL	ITY.			

(1) POSTED SPEED LIMIT PRIOR TO ROAD WORK.

SPEED (1)

0 - 35

40 - 45

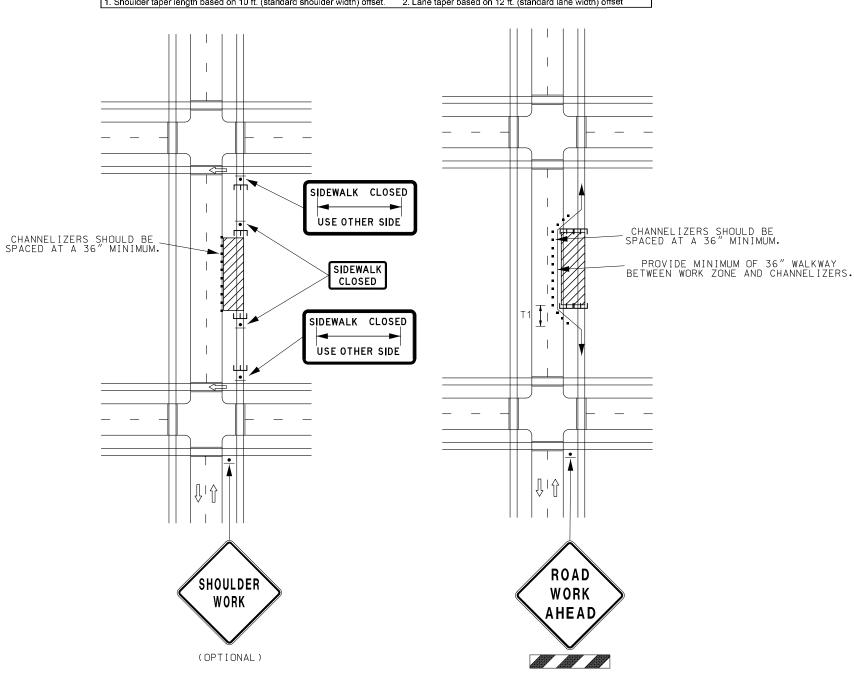
50 - 55

60 - 70

DRAWING NOT TO SCALE. FOLLOW DIMENSIONS.

## SIDEWALK DETOUR OR DIVERSION

SPEED	D SIGN SPACING (ft.)		SPEED   SIGN SPACING (ft.)   TAPER LENGTH (ft.)   (		OPTIONAL	CHANNELIZEF	R SPACING (ft.)
Normal Posted (mph)	Undivided (S)	Divided (S)	Shoulder <sup>1</sup> (T1)	Lane <sup>2</sup> (T2)	BUFFER LENGTH (ft.) (B)	Tapers	Buffer/ Work Areas
0-35	200	200	70	-	250	15	25
40-45	350	500	150	-	360	20	50
50-55	500	1000	185	-	495	50	100
60-70	SA - 1000, and SC		235	-	730	60	100
1. Shoulder t	aper length base	d on 10 ft. (stan	dard shoulder wic	lth) offset. 2.	Lane taper based on	12 ft. (standard lane	width) offset



### TRAFFIC CONTROL LEGEND

NON-PORTABLE SIGN

MOVABLE BARRICADE

CHANNEL I ZER

FLASHING ARROW PANEL



ADVANCED WARNING RAIL SYSTEM (FOR LONG TERM OPERATIONS)

CHRIS E. BERRY NUMBER PE-2001018707 SSONAL

Chus E. Benz 01/04/2024 12:32:24 PM CHRIS E. BERRY - CIVIL MO-PE-2001018707 DATE PREPARED

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J9S3833 CONTRACT ID.

PROJECT NO. BRIDGE NO.

NOTES:

WHEN CROSSWALKS OR OTHER PEDESTRIAN FACILITIES ARE CLOSED OR RELOCATED, TEMPORARY FACILITIES SHALL BE DETECTABLE AND SHALL INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH FEATURES PRESENT IN THE EXISTING PEDESTRIAN FACILITY.

WHERE SIDEWALKS EXIST, PROVISIONS SHOULD BE MADE FOR DISABLED PERSONS.

ONLY THE TEMPORARY TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL TRAFFIC.

SIGNS MAY BE MOUNTED ON PORTABLE MOUNTS AT 1 FT. PROVIDED THEY DO NOT INTERFERE WITH PEDESTRIAN MOVEMENT OR BE OBSTRUCTED BY PARKING. OTHERWISE, SIGNS SHALL BE MOUNTED AT 7 FT.

FOR HIGH SPEED FACILITIES, CHANNELIZER SPACING MAY BE REDUCED TO 1/2 SPACING NOTED IN TABLE.

OTHER APPROPRIATE SIGNS MAY BE USED IN LIEU OF THE "SHOULDER WORK AHEAD" OR "ROAD WORK AHEAD" SIGNS.

AUDIBLE INFORMATION DEVICES SHOULD BE CONSIDERED WHERE MIDBLOCK CLOSINGS AND CHANGED CROSSWALK AREAS CAUSE INADEQUATE COMMUNICATION TO BE PROVIDED TO PEDESTRIANS WHO HAVE VISUAL DISABILITIES.

FOR LONG-TERM OPERATIONS, REFER TO EPG 616.6.2.2 FLAGS AND ADVANCE WARNING RAIL SYSTEM.

DRAWING NOT TO SCALE. FOLLOW DIMENSIONS.

## CROSSWALK CLOSURES AND PEDESTRIAN DETOURS

### TRAFFIC CONTROL LEGEND

NON-PORTABLE SIGN

MOVABLE BARRICADE

CHANNEL I ZER

FLASHING ARROW PANEL

SPEED	SIGN SPA	CING (ft.)	TAPER LENGTH (ft.)		OPTIONAL	CHANNELIZER SPACING	
Normal	Undivided	Divided	Shoulder <sup>1</sup>	Lane <sup>2</sup>	BUFFER	Tapers	Buffer/
Posted	(S)	(S)	(T1)	(T2)	LENGTH (ft.)		Work Areas
(mph)	(0)	(0)	()	(/	(B)		
0-35	200	200	-	-	250	15	25
40-45	350	500	-	-	360	20	50
50-55	500	1000	-	-	495	50	100
60-70	SA - 1000, SB - 1500 and SC-2640		-	-	730	60	100
1. Shoulder ta	aper length base	d on 10 ft. (stan	dard shoulder wid	th) offset. 2.	Lane taper based on	12 ft. (standard lane	width) offset

WHERE SIDEWALKS EXIST, PROVISIONS SHOULD BE MADE FOR DISABLED PERSONS.

WHEN CROSSWALKS OR OTHER PEDESTRIAN FACILITIES ARE CLOSED OR RELOCATED, TEMPORARY FACILITIES SHALL BE DETECTABLE AND SHALL INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH FEATURES PRESENT IN THE EXISTING PEDESTRIAN FACILITY

CURB PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FT. IN ADVANCE OF THE MID-BLOCK CROSSWALK.

PEDESTRIAN TRAFFIC SIGNAL DISPLAYS CONTROLLING CLOSED CROSSWALKS SHOULD BE COVERED OR DEACTIVATED.

ONLY THE TEMPORARY TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN OTHER DEVICES MAY BE NECESSARY TO CONTROL TRAFFIC.

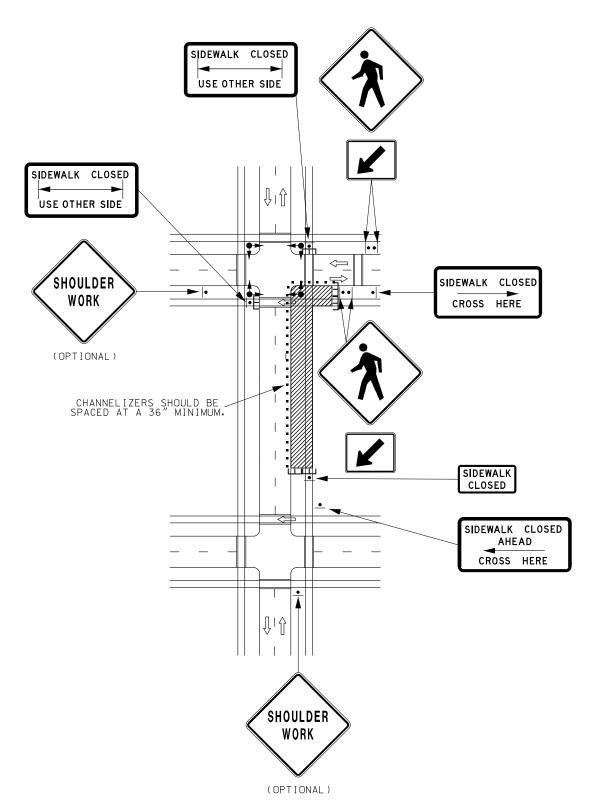
SIGNS MAY BE MOUNTED ON PORTABLE MOUNTS AT 1FT. PROVIDED THEY DO NOT INTERFERE WITH PEDESTRIAN MOVEMENT OR BE OBSTRUCTED BY PARKING. OTHERWISE, SIGNS SHALL BE MOUNTED AT 7 FT.

FOR HIGH SPEED FACILITIES, CHANNELIZER SPACING MAY BE REDUCED TO 1/2 SPACING NOTED IN TABLE.

OTHER APPROPRIATE SIGNS MAY BE USED IN LIEU OF THE "SHOULDER WORK AHEAD" SIGN.

AUDIBLE INFORMATION DEVICES SHOULD BE CONSIDERED WHERE MIDBLOCK CLOSINGS AND CHANGED CROSSWALK AREAS CAUSE INADEQUATE COMMUNICATION TO BE PROVIDED TO PEDESTRIANS WHO HAVE VISUAL

DRAWING NOT TO SCALE. FOLLOW DIMENSIONS.



CHRIS E. BERRY NUMBER PE-200101870 

Chris E. Berry 01/04/2024 12:32:45 PM CHRIS E. BERRY - CIVIL MO-PE-2001018707

1/4/2024

VAR MO SHEET NO 6 SE **VARIOUS** LOB NO J9S3833

CONTRACT ID. PROJECT NO.

BRIDGE NO.

TRAFFIC SHEET