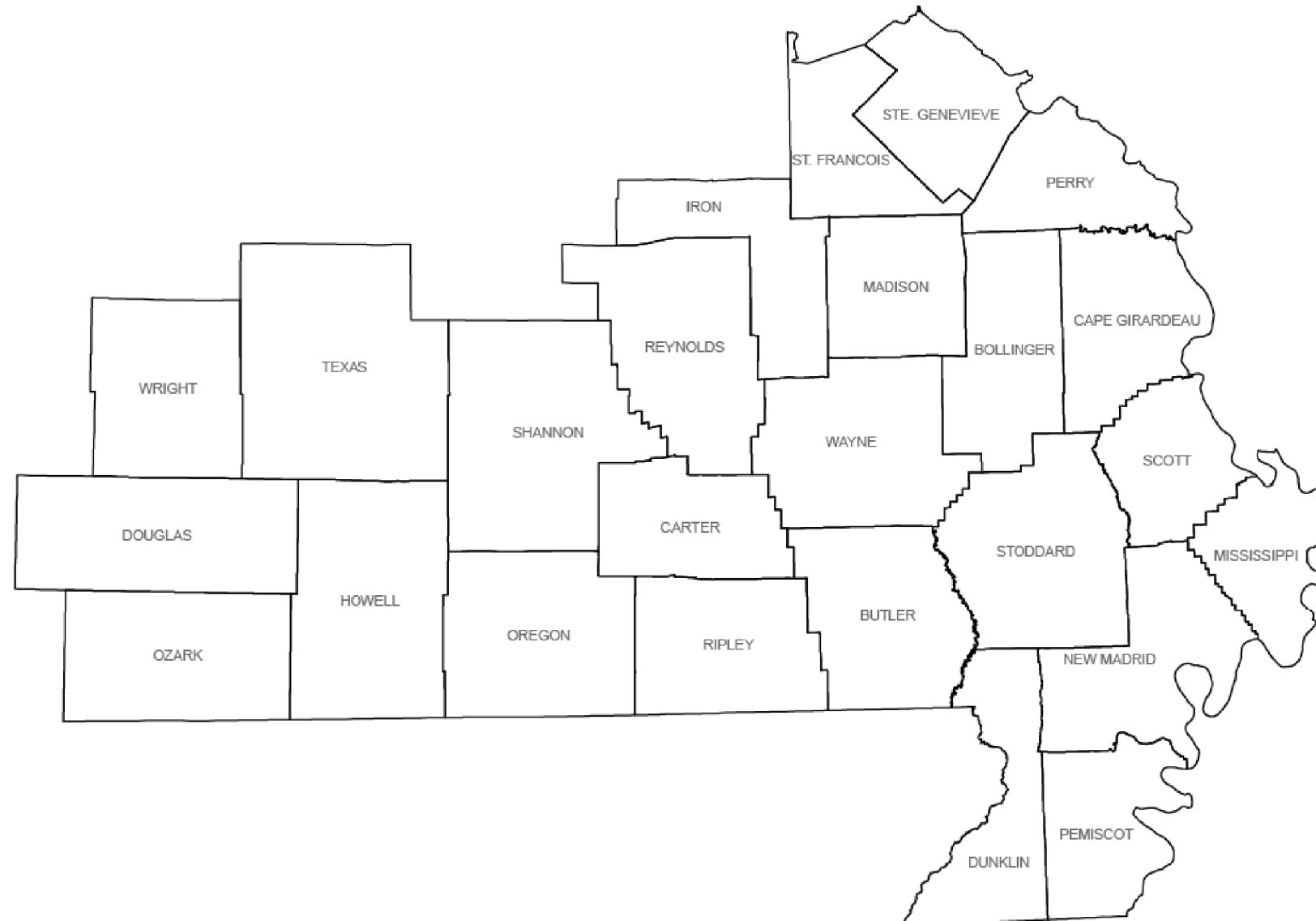


MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
PLANS FOR PROPOSED
STATE HIGHWAY
VARIOUS COUNTIES
SOUTHEAST DISTRICT

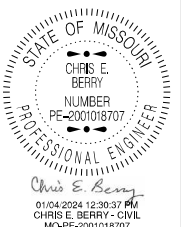


ADA TRANSITION PLAN IMPROVEMENTS JOB ORDER CONTRACTING

THE EXISTENCE AND APPROXIMATE LOCATION OF UTILITY FACILITIES KNOWN TO EXIST, AS SHOWN ON THE PLANS, ARE BASED ON THE BEST INFORMATION AVAILABLE TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE. RELIANCE UPON THIS INFORMATION IS DONE AT THE RISK AND PERIL OF THE USER, AND THE COMMISSION SHALL NOT BE LIABLE FOR ANY DAMAGES THAT MAY ARISE FROM ANY ERROR IN THE INFORMATION. IT IS, THEREFORE, THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE, LOCATION AND STATUS OF ANY FACILITY. SUCH VERIFICATION INCLUDES DIRECT CONTACT WITH THE LISTED UTILITIES.

INDEX OF SHEETS

DESCRIPTION	SHEET NUMBER
TITLE SHEET -----	1
TYPICAL SECTION (TS) (2 SHEETS)---	2
TRAFFIC CONTROL SHEETS (TC)-----	3-6



DATE PREPARED	
1/4/2024	
ROUTE	STATE
VAR	MO
DISTRICT	SHEET NO.
SE	1
COUNTY	
VARIOUS	
JOB NO.	
J9S3833	
CONTRACT ID.	

PROJECT NO.

[illegible]

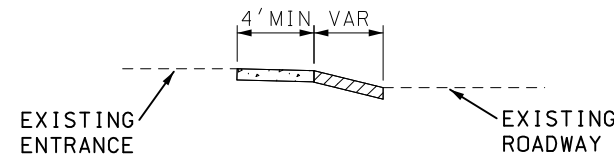
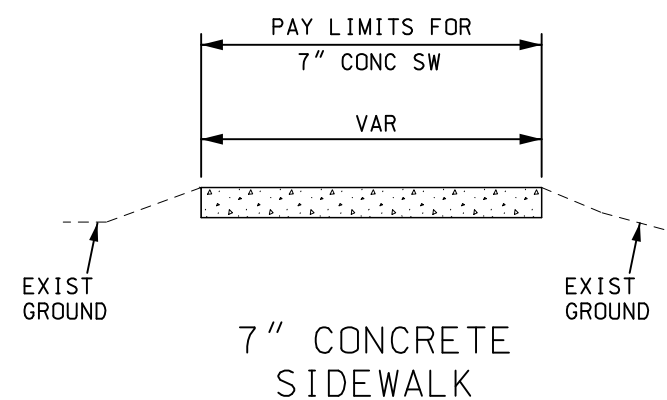
MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-4636)

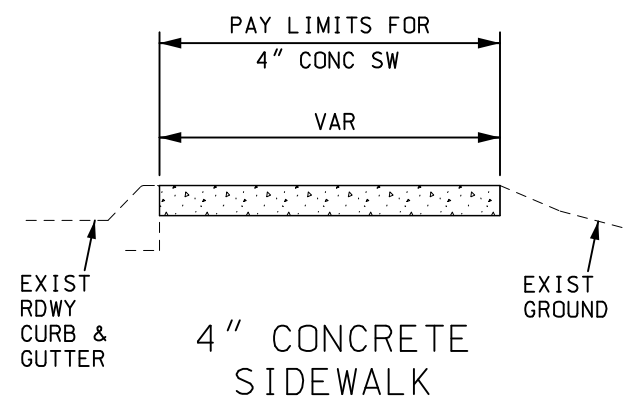
CONVENTIONAL SYMBOLS (USED IN PLANS)

	EXISTING	NEW
BUILDINGS AND STRUCTURES		
GUARD RAIL		
GUARD CABLE		
CONCRETE RIGHT-OF-WAY MARKER		
STEEL RIGHT-OF-WAY MARKER		
LOCATION SURVEY MARKER		
UTILITIES		
FIBER OPTICS	— FO —	— FO —
OVERHEAD CABLE TV	— OTV —	— OTV —
UNDERGROUND CABLE TV	— UTV —	— UTV —
OVERHEAD TELEPHONE	— OT —	— OT —
UNDERGROUND TELEPHONE	— UT —	— UT —
OVERHEAD POWER	— OE —	— OE —
UNDERGROUND POWER	— UE —	— UE —
SANITARY SEWER	— S —	— S —
STORM SEWER	— SS —	— SS —
GAS	— G —	— G —
WATER	— W —	— W —
MANHOLE		
FIRE HYDRANT		
WATER VALVE		
WATER METER		
DROP INLET		
DITCH BLOCK		
GROUND MOUNTED SIGN		
LIGHT POLE		
H-FRAME POWER POLE		
TELEPHONE PEDESTAL		
FENCE		
CHAIN LINK	— V —	— V —
WOVEN WIRE	— X —	— X —
GATE POST		
BENCHMARK		

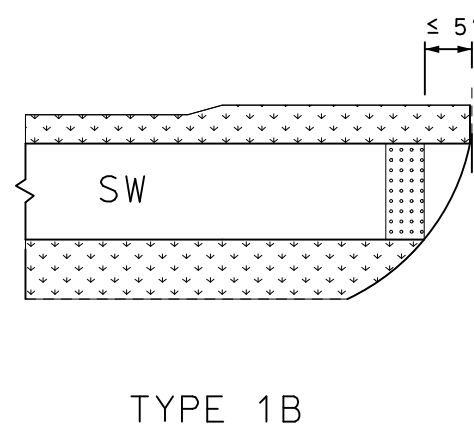
NOTE: DASHED OR OPEN SYMBOLS INDICATE
EXISTING FEATURES



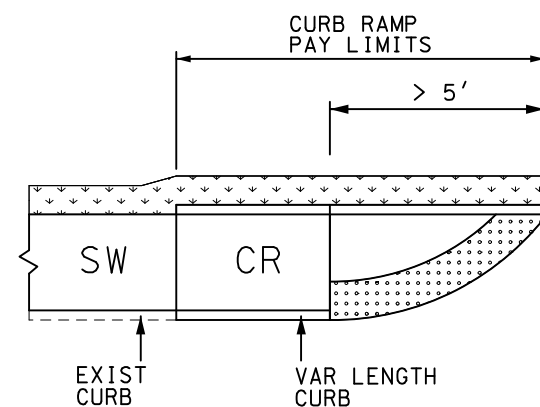
CR - CURB RAMP

 - PAVED APPROACH

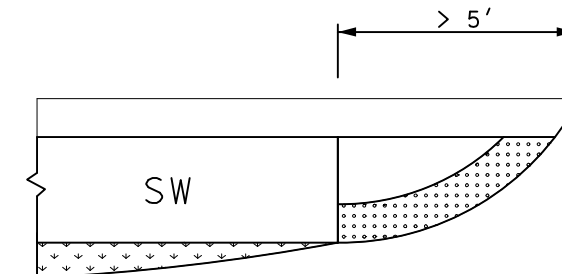
The diagram illustrates the construction and dimensions of a 4 inch concrete sidewalk. It shows a cross-section with a 2% maximum cross slope. The width is defined by 'PAY LIMITS FOR 4" CONC SW' and 'MIN 5' WIDTH'. The diagram also indicates 'VAR WIDTH' and 'VAR SLOPE' on the left side, and 'EXIST WALL, CURB, OR STEP' on the right side. The sidewalk is shown with a dashed line for the 'EXIST RDWY CURB & GUTTER'.



TYPE 1B



TYPE 2A



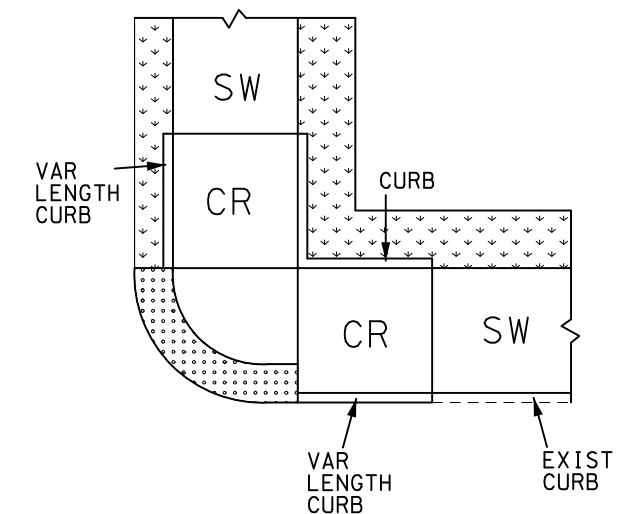
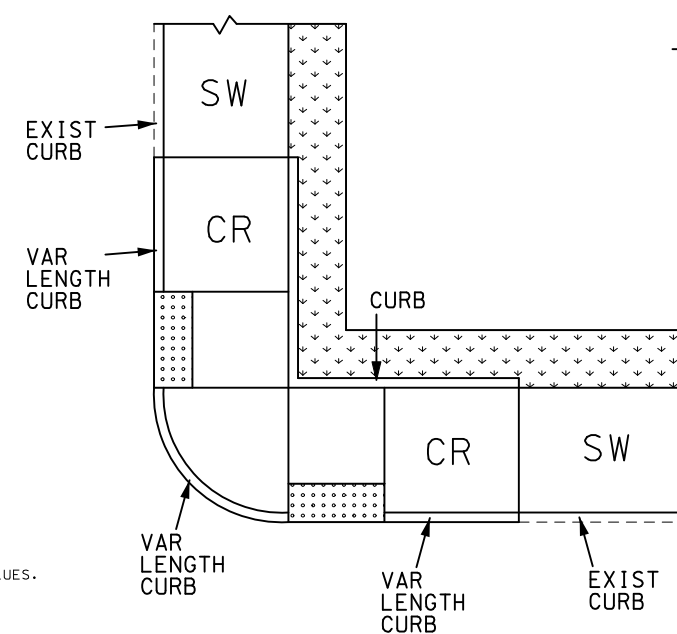
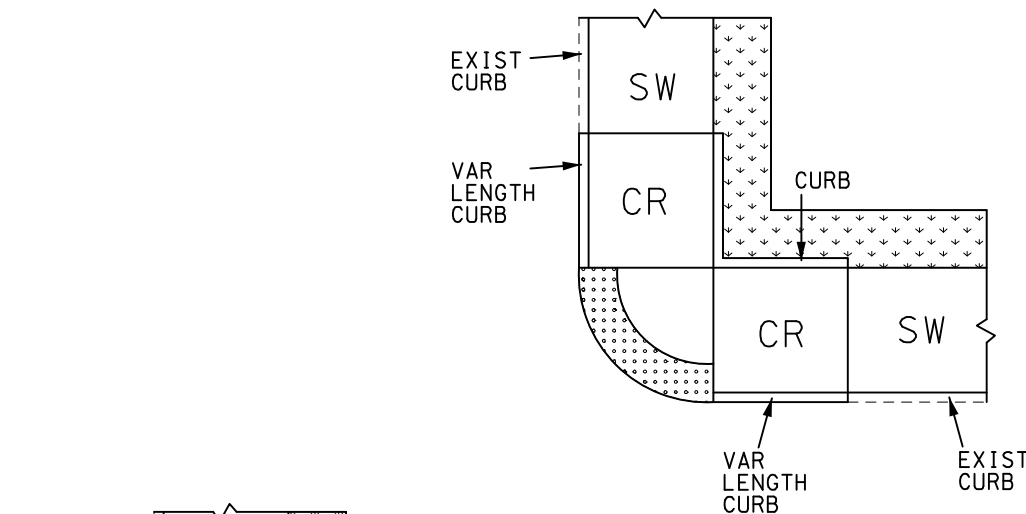
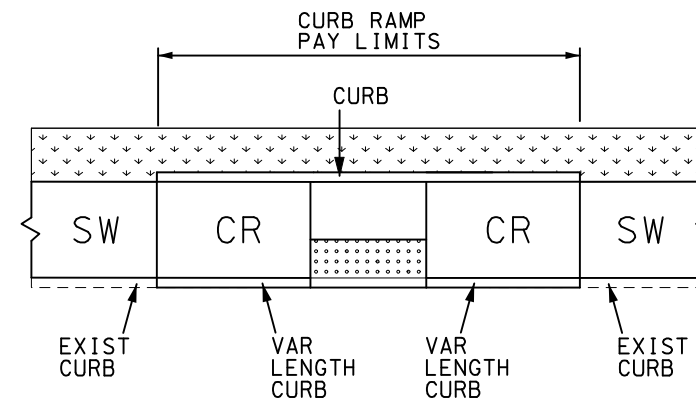
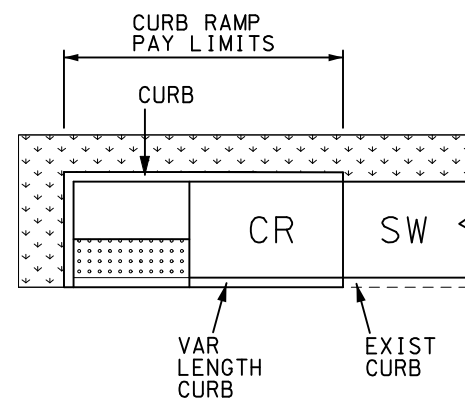
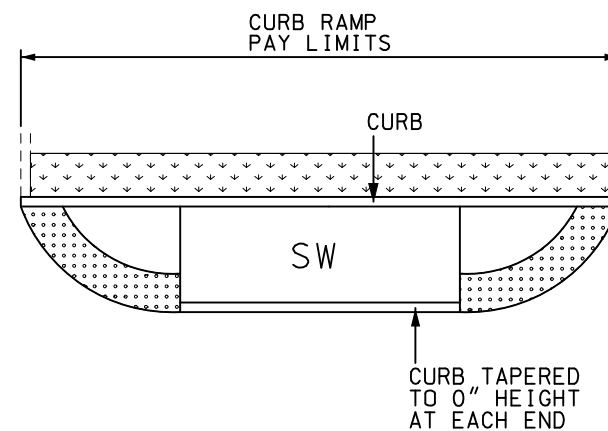
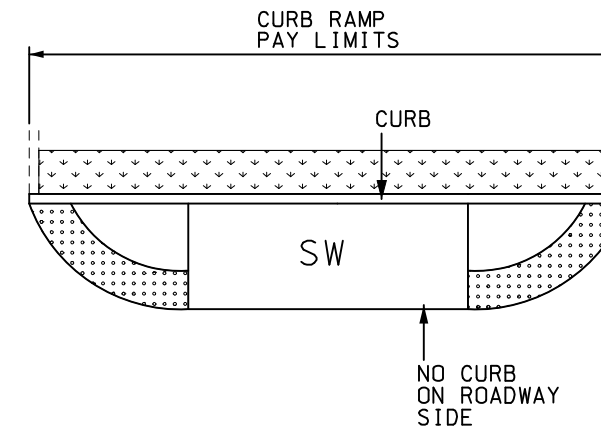
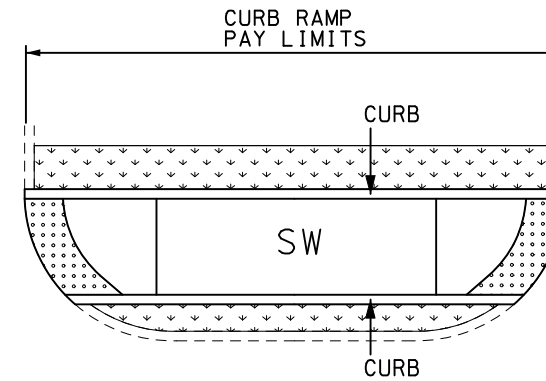
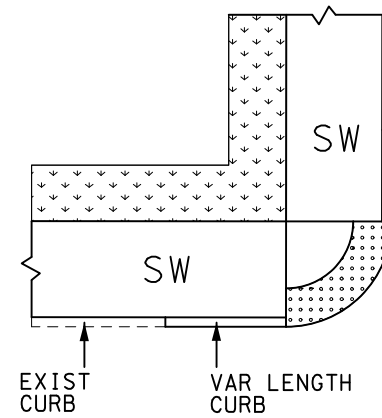
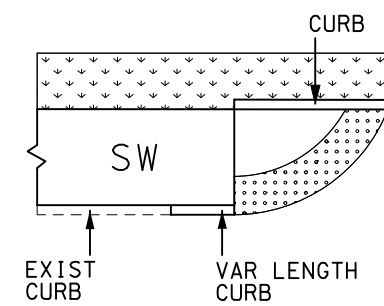
TYPE 2B

UTILITIES, SIGNS, FIXED OBJECTS, OR CROSS SLOPE TRANSITIONS MAY NOT BE PLACED ON A CURB RAMP, OR IN A MANNER THAT INTERFERES WITH THE USE OF THE CURB RAMP.

[illegible]MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

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JEFFERSON CITY, MO 65102
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TYPICAL SECTION
SHEET 1 OF 2



NOTES:

TRUNCATED DOMES SHALL WRAP THE
RADIUS ON CURB RAMPS IN WHICH THE FULL
WIDTH LOCATION ON THE SIDEWALK IS GREATER
THAN 5' FROM THE INTERSECTION WITH THE CROSSING
(DEMONSTRATED IN TYPE 2)

IF THE FULL WIDTH LOCATION IS LESS THAN
5' THE TRUNCATED DOMES SHALL BE PLACED
PERPENDICULAR TO THE SIDEWALK. (DEMONSTRATED
IN TYPE 1)

TRUNCATED DOMES ARE NOT PLACED AT PRIVATE ENTRANCES
SEE PLAN SHEETS FOR LOCATIONS.

NOTES:

MAXIMUM ALLOWABLE SLOPES - NO SLOPES ON SIDEWALKS OR CURB RAMPS SHALL EXCEED THESE VALUES.
CURB RAMP RUNNING GRADE 8.33%
CROSS-SLOPE 2.00%

TYPICAL SECTIONS TO BE USED IN CONJUNCTION WITH THE STANDARD PLANS.

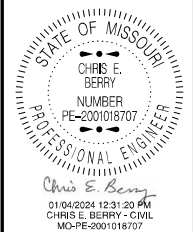
ANY PAVED APPROACHES THAT THE ENGINEER FINDS TO NOT BE TECHNICALLY FEASIBLE MAY BE EXCLUDED AND NOTED AS AN ADA EXCEPTION. A TRANSITION IN CROSS SLOPE SHALL BE MADE FROM THE END OF THE CURB RAMP TO THE EXISTING CROSS SLOPE OF THE ENTRANCE.

UTILITIES, SIGNS, FIXED OBJECTS, OR CROSS SLOPE TRANSITIONS MAY NOT BE PLACED ON A CURB RAMP, OR IN A MANNER THAT INTERFERES WITH THE USE OF THE CURB RAMP.

SW - CONCRETE SIDEWALK

CR - CURB RAMP

⌵ ⌵ - VARIABLE EXISTING MATERIAL

 - TRUNCATED DOME

DATE PREPARED
1/4/2024

ROUTE	STATE
VAR	MO

DISTRICT	SHEET NO
SE	2

COUNTY
VARIOUS

JOB NO.
J9S3833

7999899
CONTRACT ID.

PROJECT NO.

BRIDGE NO.

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COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

TYPICAL SECTION
SHEET 2 OF 2

DRAWING NOT TO SCALE. FOLLOW DIMENSIONS.

TRUNCATED DOME LOCATIONS VARY ON TYPE 5D - SEE PLAN SHEET



W020-1
(2)



W020-5
(5)

CENTER LANE
OR
LEFT LANE



W020-6a
(6)

CENTER
OR
LEFT



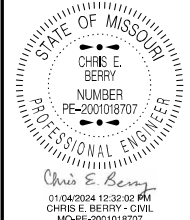
W021-5b
(21)



W05-5
(34)

TRAFFIC CONTROL LEGEND

- NON-PORTABLE SIGN
- E MOVABLE BARRICADE
- CHANNELIZER
- ... FLASHING ARROW PANEL



DATE PREPARED
1/4/2024

ROUTE
VAR

DISTRICT
SE

STATE
MO

SHEET NO.
4

COUNTY
VARIOUS

JOB NO.
J9S3833

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

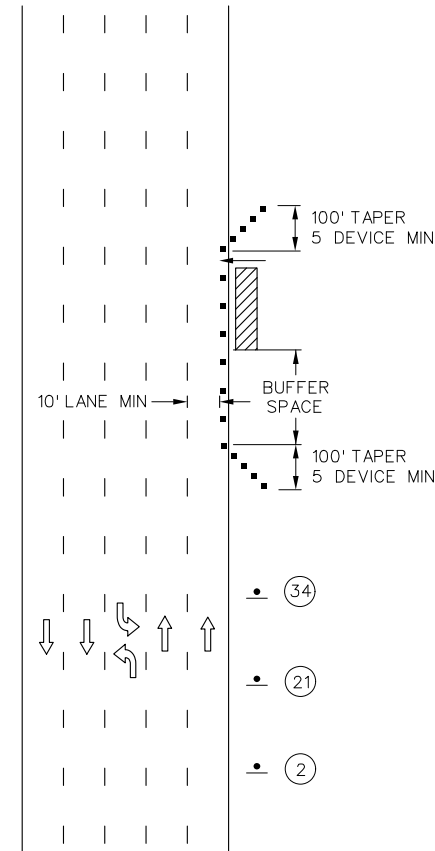
DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

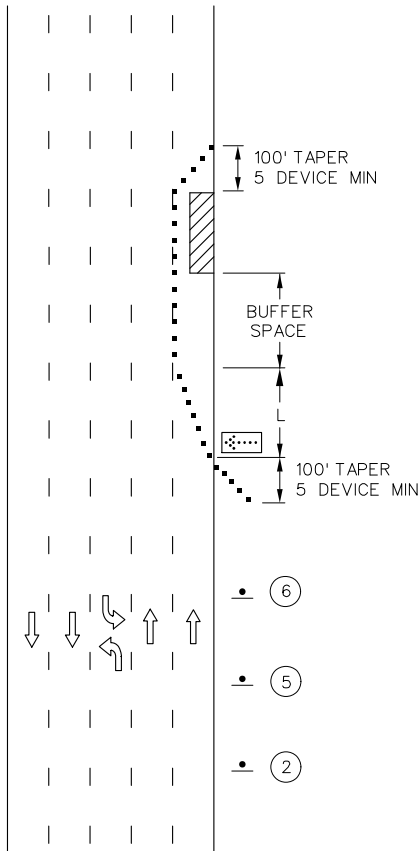
105 WEST CAPITOL
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1-888-ASK-MODOT (1-888-275-6636)

MODOT

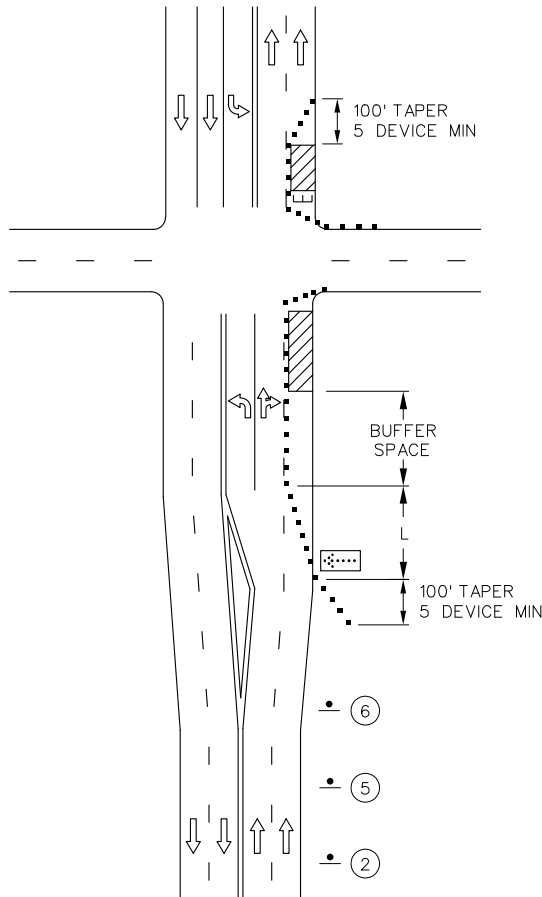
TRAFFIC CONTROL
SHEET 2 OF 4



MINOR
ENCROACHMENT

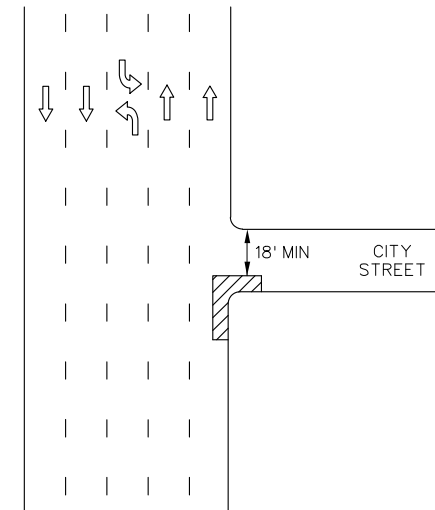


LANE
CLOSURE



RIGHT LANE CLOSURE
AT INTERSECTION

WORK ON NEAR OR FAR SIDE



SIDE STREET
ENCROACHMENT

SIGN SPACING FOR ADVANCE SIGN SERIES (2) (3)		
SPEED (1) (MPH)	NON-DIVIDED HIGHWAYS	DIVIDED HIGHWAYS
0 - 35	200 FT	200 FT
40 - 45	350 FT	500 FT
50 - 55	500 FT	1000 FT
60 - 70	SA - 1000 FT, SB - 1500 FT AND SC - 2640 FT	

- (1) POSTED SPEED LIMIT PRIOR TO ROAD WORK.
- (2) SPACING BETWEEN SIGNS, BETWEEN LAST SIGN AND FLAGGER, BEGINNING OF TAPER OR SIGNED CONDITION.
- (3) SPACINGS MAY BE ADJUSTED AS NECESSARY TO MEET FIELD CONDITIONS AND VISIBILITY.

TAPER LENGTH AND SPACING						
SPEED (1) (MPH)	TAPER LENGTH L FOR LATERAL SHIFTS (2) (4)			CHANNELIZER SPACING (3)		
	10 FT	11 FT	12 FT	TAPERS	BUFFER SPACE WORK AREA	
0 - 35	205 FT	225 FT	245 FT	35 FT (4)	50 FT (4)	
40 - 45	450 FT	495 FT	540 FT	40 FT (4)	100 FT (4)	
50 - 55	550 FT	605 FT	660 FT	50 FT (5)	100 FT (5)	
60 - 70	700 FT	770 FT	840 FT	60 FT (5)	100 FT (5)	

- (1) POSTED SPEED LIMIT PRIOR TO ROAD WORK.
- (2) TAPER LENGTHS MAY BE ADJUSTED TO ACCOMMODATE CROSSROADS, CURVES, INTERSECTIONS, RAMPs, OR OTHER GEOMETRIC FEATURES.
- (3) SPACING MAY BE REDUCED TO DISCOURAGE TRAFFIC ENCROACHMENT.
- (4) SPACING MAY BE REDUCED TO 1/2 AT INTERSECTIONS.

BUFFER SPACE LENGTHS	
POSTED SPEED PRIOR TO CONSTRUCTION (MPH)	LENGTH
0 - 35	250 FT
40 - 45	360 FT
50 - 55	495 FT
60 - 70	730 FT

NOTES:

ALL SIGNS SHALL BE POST MOUNTED AND IN ACCORDANCE WITH STANDARD PLAN 616.10 AND 903.03.

DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS.

ANY EXISTING SIGNING THAT CONFLICTS WITH THE TRAFFIC CONTROL SIGNING SHALL BE COMPLETELY COVERED OR REMOVED.

NO PAYMENT WILL BE MADE FOR RELOCATION OF CONSTRUCTION SIGNS.

