

KEY MAP

LOCATION OF ST CHARLES COUNTY
ST LOUIS COUNTY
JEFFERSON COUNTY

CONVENTIONAL SYMBOLS (USED IN PLANS)

NOTE: DASHED OR OPEN SYMBOLS INDICATE
EXISTING FEATURES

STEVEN T. STIRNEMANN
NUMBER
PE-2003001103

TE PREPARED

DISTRICT	SHEET NO.
STL	1

JOB NO.
16P3510B

CONTRACT ID.	
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PROJECT NO.	
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BRIDGE NO.

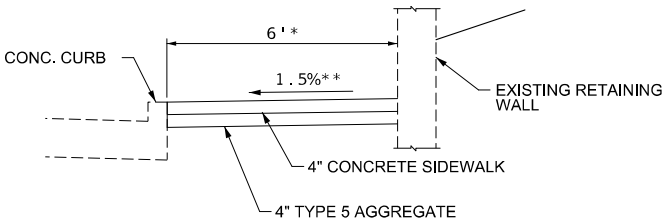


105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

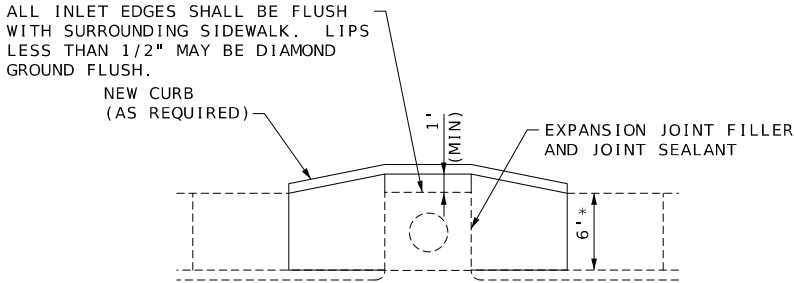
REV

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

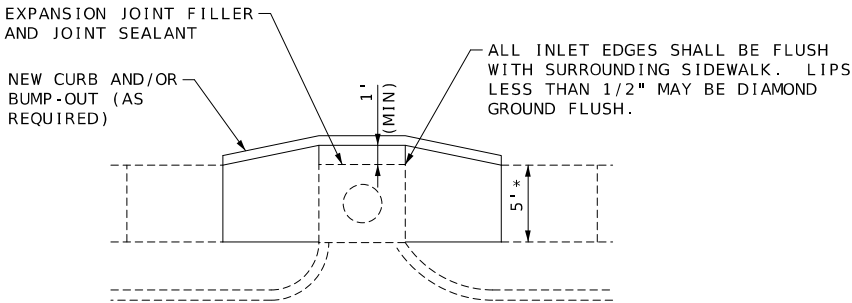
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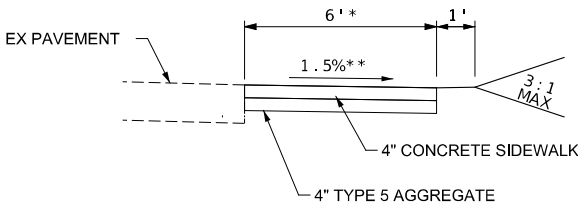
S13 CURB-SIDEWALK-RETAINING WALL



S14 SIDEWALK AT BACK OF CURB AND AT INLET



S15 SIDEWALK WITH TREELAWN AND AT INLET



S16 PAVEMENT-SIDEWALK

DISCLAIMER
THE PROFESSIONAL WHOSE SIGNATURE AND PERSONAL SEAL APPEAR HEREON ASSUMES RESPONSIBILITY ONLY FOR WHAT APPEARS ON THIS PAGE, AND DISCLAIMS (PURSUANT TO SECTION 327.411 RSMO) SPECIFICATION, ESTIMATES, REPORTS, OR OTHER DOCUMENTS OR INSTRUMENTS NOT SEALED BY THE UNDERSIGNED PROFESSIONAL RELATING TO OR INTENDED TO BE USED FOR ANY PART OR PARTS OF THE PROJECT TO WHICH THIS PAGE REFERS.



THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY

DATE PREPARED

3/28/2024

ROUTE STATE

VARIES MO

DISTRICT SHEET NO.

STL 2

COUNTY

VARIES

JOB NO.

J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

MoDOT

EDSI

ENGINEERING DESIGN SOURCE, INC.

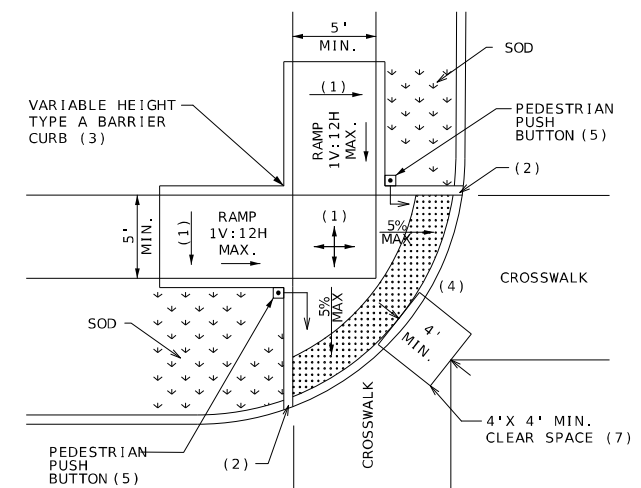
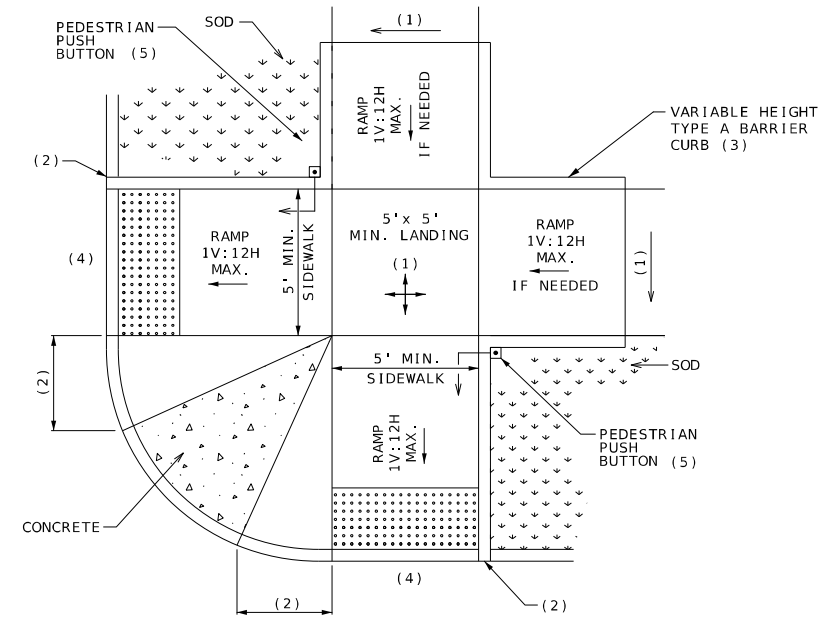
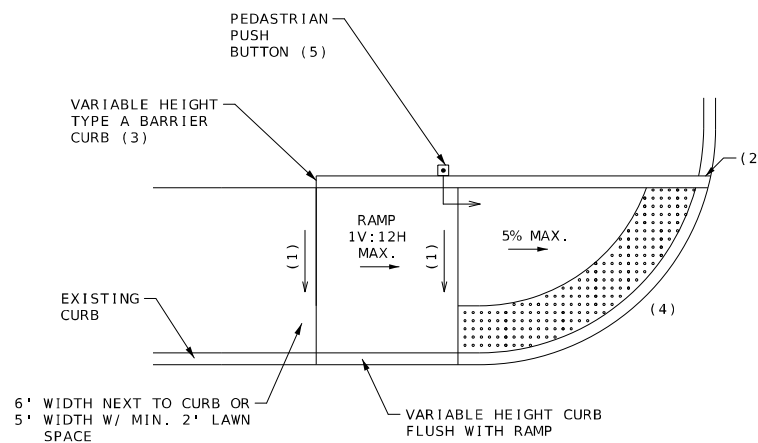
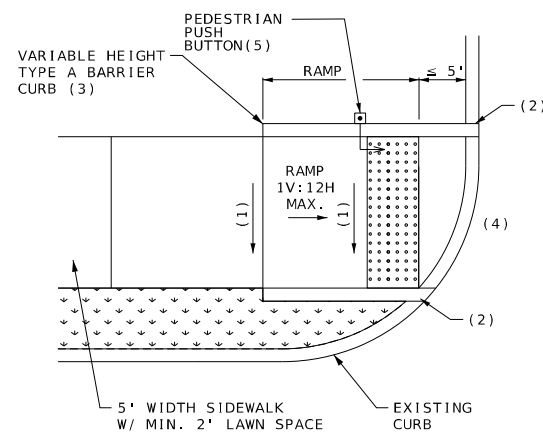
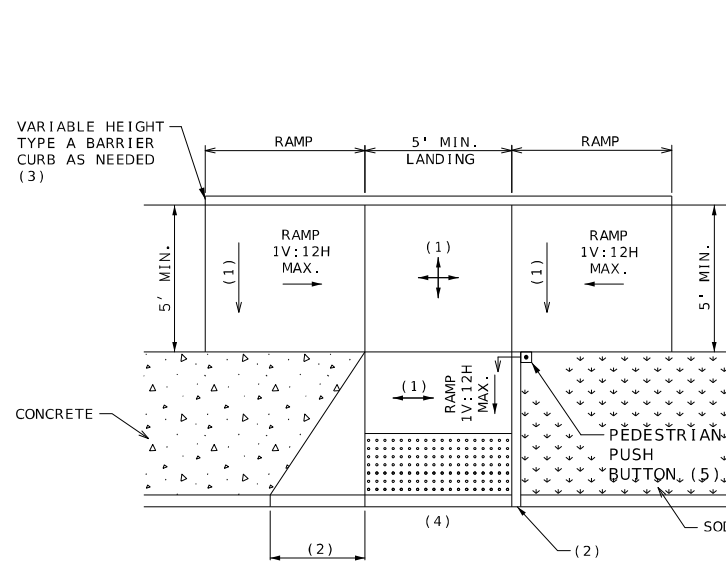
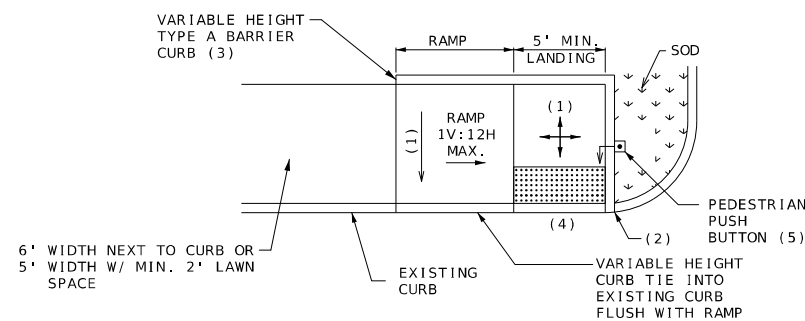
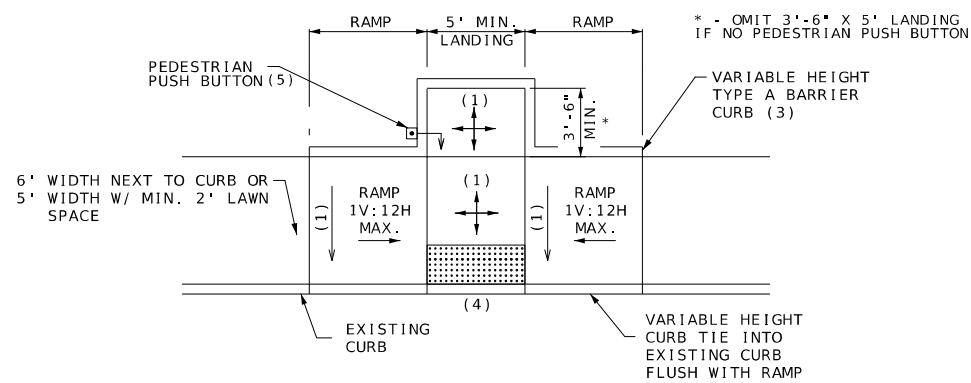
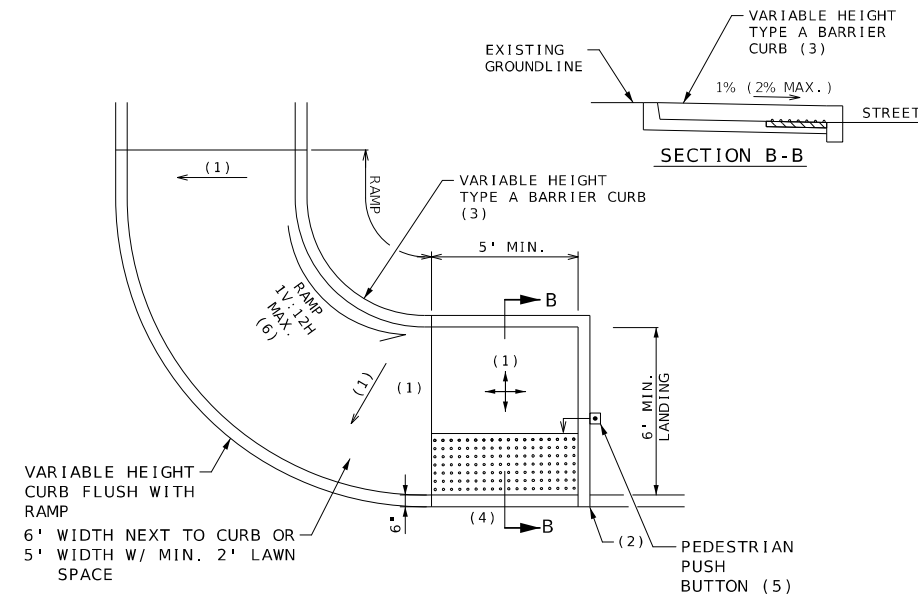
16141 Swingley Ridge Rd., Suite 300
Chesterfield, Missouri 63017
Missouri State Engineering Corporation #001523

T. 636 537 5585
F. 636 537 0215

NOTE:
* UNLESS SHOWN OTHERWISE ON PLAN SHEETS
** THE NEW SIDEWALK SLOPE DIRECTION SHOULD MATCH SLOPE DIRECTION OF EXISTING SIDEWALK.

TYPICAL APPLICATIONS

SHEET 2 OF 5



GENERAL NOTES:

- (1) 1.0% (2.0% MAX.) CROSS SLOPE OR ROAD GRADE EXCEPTION.
- (2) VARIABLE HEIGHT VERTICAL CURB. IF TRAVERSABLE USE A MAX. 1V:10H FLARE MEASURED PARALLEL TO THE CURB LINE.
- (3) HEIGHT VARIES TO MEET EXISTING GROUND.
- (4) THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUNS, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 5% MAXIMUM.
- (5) THE FACE OF PEDESTRIAN PUSH BUTTONS SHALL BE OFFSET 0" FOR FRONT APPROACH AND 10" MAX. FOR SIDE APPROACH TO THE CURB FACE.
- (6) ENSURE THAT THE INSIDE EDGE OF CURVED RAMPS MAINTAIN AN 8.3% (1V:12H) MAXIMUM SLOPE.
- (7) BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE 4'X 4' MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.
- (8) TRUNCATED DOMES INSTALLED ONLY IF SHOWN ON PLAN VIEW.

DISCLAIMER

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THIS SHEET HAS BEEN
DESIGNED, SEALED, AND DATED
ELECTRONICALLY

DATE PREPARED

3/28/2024

ROUTE	STATE
AD 156	MO

VARIES	MO
DISTRICT	SHEET NO.

STI 2

COUNTY

VARIES

JOB NO.
1000000000

J6P3510B

CONTRACT ID.

PROJECT NO. _____

BRIDGE NO.

[illegible]

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STEVEN T. STIRNEMANN
NUMBER
PE-2003001103
PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN

DATE PREPARED
3/28/2024

DISTRICT	SHEET NO.
STL	2

JOB NO.
J6P3510B

PROJECT NO. _____

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JEFFREY A. MASK - M

Diagram illustrating a high-pressure cell setup. A sample is placed between two anvils, with a pressure-transmitting medium (D) surrounding it. A pressure gauge is connected to the system.



SOURCE:
T. 636 5
F. 636 5

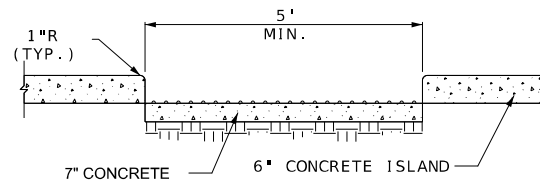
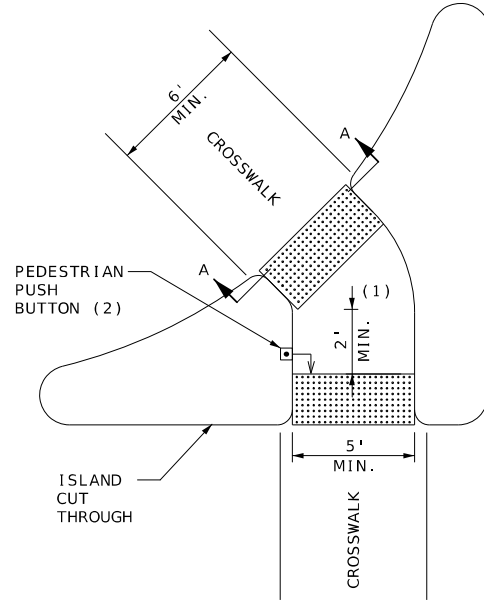
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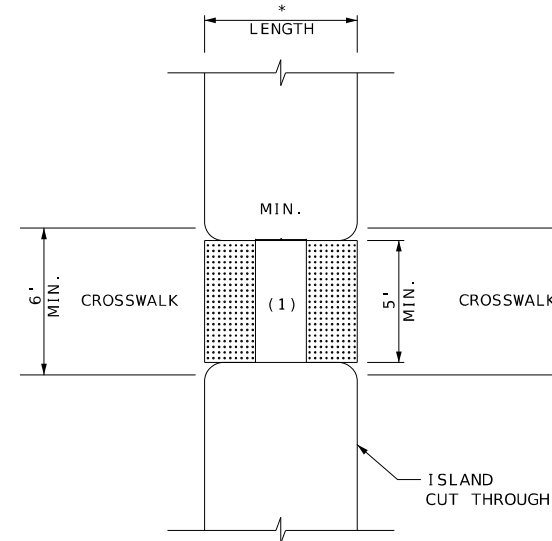
MISSOURI STATE ENGINEER

40 PM 3/28/2024



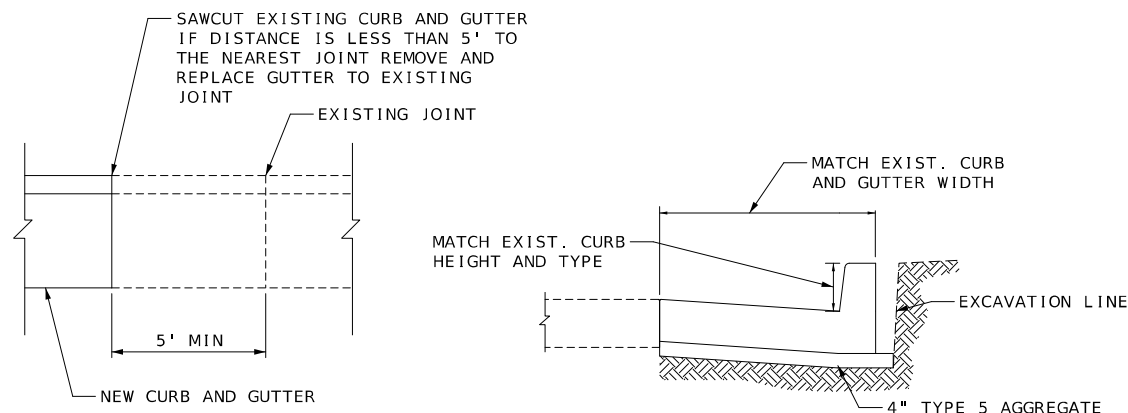
RAMP OR CUT-THROUGH DEPENDING
ON ISLAND WIDTH. IF RAMPED,
PROVIDE 4' MINIMUM LANDING AND
SLOPE RAMPS AT 1V:12H MAX.

C10 MEDIAN CUT THROUGHS

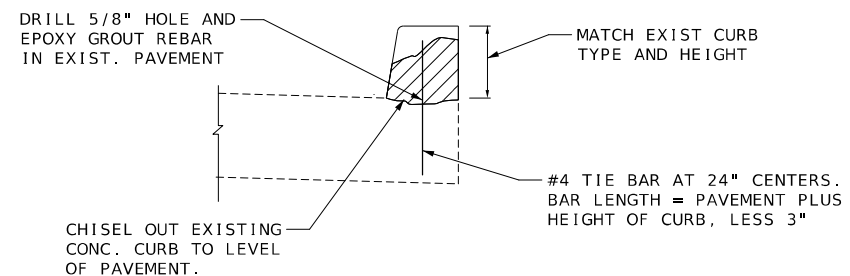


- * DETECTABLE WARNING SURFACES SHALL BE OMITTED IF LENGTH IS < 6', BECAUSE REFUGE SPACE IS DEEMED TOO SMALL.

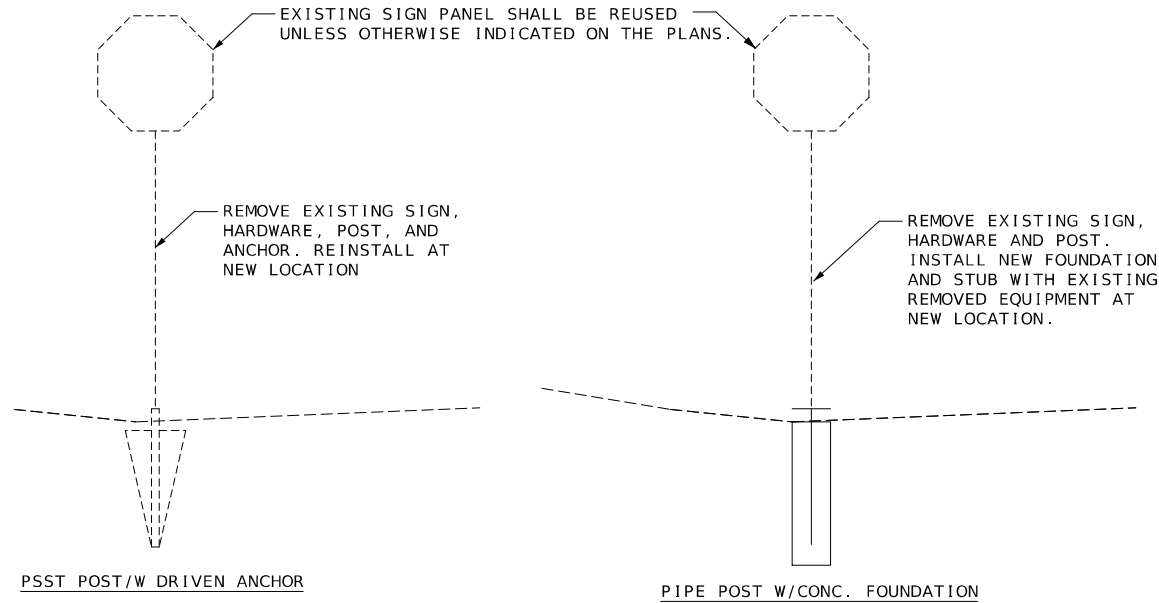
SHEET 4 OF 5



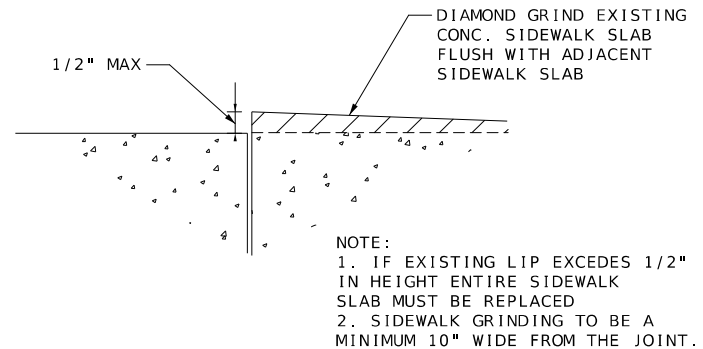
R1 CURB AND GUTTER REPAIR



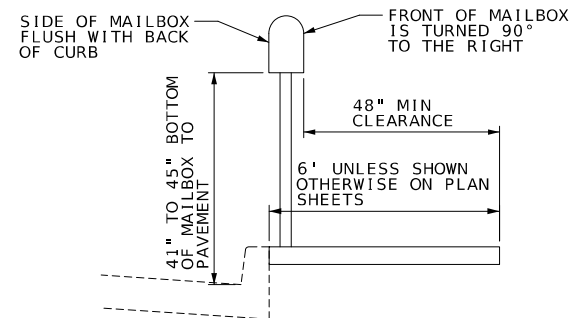
R2 INTEGRAL CURB REPAIR



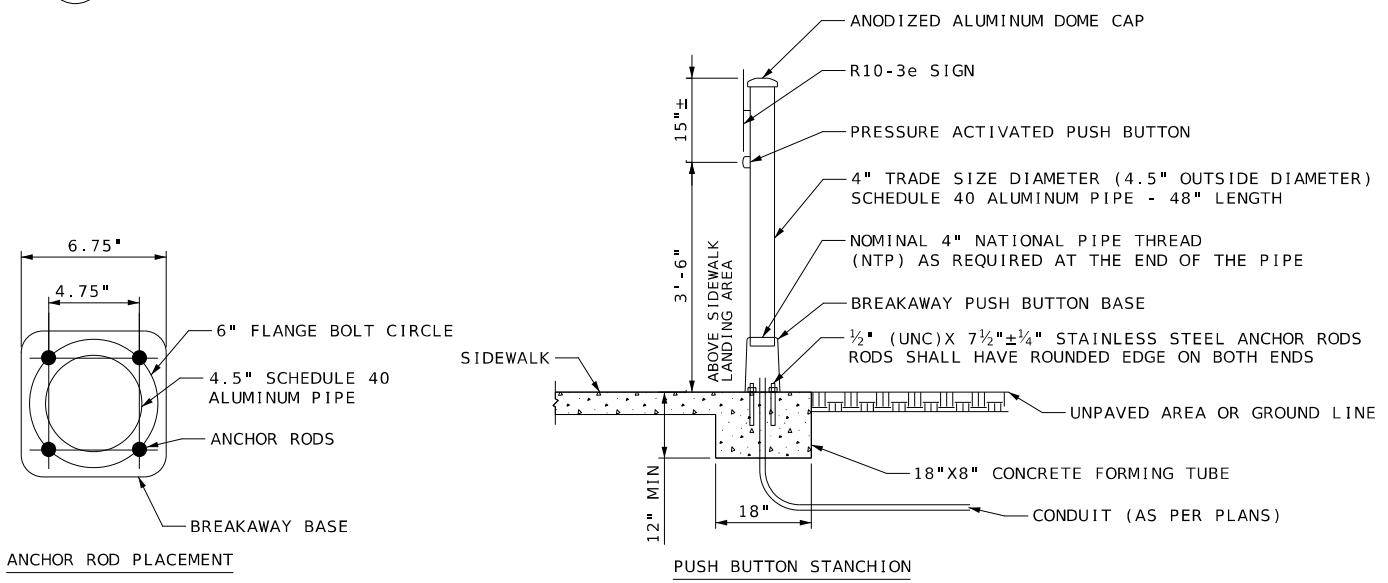
R3 EXISTING SIGN RELOCATION



R4 DIAMOND GRINDING SIDEWALK EDGE



R5 MAILBOX RELOCATION



- NOTES:
- THE PUSH BUTTON STANCHION FOUNDATION IS CONSTRUCTED AS PART OF THE SIDEWALK. INCREASE THE SIDEWALK THICKNESS TO 12" (MIN.) TO PROVIDE FOR THE PUSH BUTTON STANCHION FOUNDATION.
 - INSTALLATION OF THE PUSH BUTTON STANCHION SHALL BE PER MANUFACTURER'S REQUIREMENTS.
 - CABLES SHALL HAVE A SLIT-FIT CONNECTION IN THE PEDESTAL THAT WILL DETACH IN THE EVENT THE POLE IS STRUCK.

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DATE PREPARED 3/28/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 2
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

EDSI
ENGINEERING DESIGN SOURCE, INC.
16141 Swingley Ridge Rd., Suite 300
Chesterfield, Missouri 63047
T. 636 537 5585
F. 636 537 0215
Missouri State Engineering Corporation #001523

SIDEWALK														
PLAN SHEET NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	SIDEWALK LENGTH	SIDEWALK WIDTH	SUGGESTED ADA RAMP TYPE	TRUNCATED DOMES	SIDEWALK EDGE GRINDING	CONCRETE CURB RAMP	CONCRETE SIDEWALK, 4 IN.	CONCRETE SIDEWALK, 7 IN.	TYPE 3 AGGREGATE FOR BASE (4 IN. THICK)	REMARKS
					(FT)	(FT)		(SF)	(LF)	(EA)	(SQYD)	(SQYD)	(SQYD)	
6	13+05	13+11	IMPERIAL MAIN	RT			C2			1				I-55 NB OFF RAMP SWC; LANDING ONLY
7	10+01	10+06	CHURCH RD	LT	5	5					3		3	OZARK NWC
7	16+01	15+12	CHURCH RD	RT			C4	10		1				MICHIGAN AVE SEC
							SUBTOTALS	10	0	2	3	0	3	
9	12+29	12+34	IMPERIAL MAIN	RT			C2			1				W OUTER RD SWC; LANDING ONLY
9	12+30	12+35	IMPERIAL MAIN	LT			C2			1				W OUTER RD NWC; LANDING ONLY
9	12+37	12+44	IMPERIAL MAIN	LT			C2			1				W OUTER RD NWC; LANDING ONLY
9	12+51	12+58	IMPERIAL MAIN	RT			C2			1				W OUTER RD SWC; LANDING ONLY
9	16+94	17+04	IMPERIAL MAIN	LT			C1	10		1				I-55 SB OFF RAMP NEC
9	17+05	17+62	IMPERIAL MAIN	RT			C6	14		1				I-55 SB ON RAMP SEC
10	231+47	231+64	W OUTER RD	RT			C9	30				20	20	ISLAND CUT THROUGH
10	231+48	231+66	W OUTER RD	LT			C9	30				13	13	ISLAND CUT THROUGH
10	232+46	232+59	W OUTER RD	LT			C9	30				13	13	ISLAND CUT THROUGH
10	232+47	232+56	W OUTER RD	RT			C9	30				11	11	ISLAND CUT THROUGH
10	232+51	232+71	W OUTER RD	LT	10	6					5		5	VOGEL RD NWC
10	232+54	232+71	W OUTER RD	RT	13	6					6		6	VOGEL RD NEC
10	232+56	232+79	W OUTER RD	LT			C2	10		1				VOGEL RD NWC
10	232+57	232+82	W OUTER RD	RT			C2	11		1				VOGEL RD NEC
10	232+78	232+84	W OUTER RD	RT	10	6					5		5	VOGEL RD NEC
10	232+77	232+86	W OUTER RD	LT	11	6					5		5	VOGEL RD NWC
							SUBTOTALS	165	0	8	21	57	78	
12	160+26	160+43	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
12	160+58	160+85	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
12	161+24	161+39	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
12	161+53	161+75	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
12	161+75	161+87	WOODSON RD	LT			C5			1				RED, STAMPED, YELLOW DOMES
12	162+46	162+58	WOODSON RD	LT			C5			1				RED, STAMPED, YELLOW DOMES
12	162+72	162+90	WOODSON RD	RT			C2			1				RED, STAMPED, YELLOW DOMES
12	163+17	163+40	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
12	163+36	163+58	WOODSON RD	RT			C6			1				RED, STAMPED, YELLOW DOMES
12	163+80	163+98	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
13	167+11	167+29	WOODSON RD	RT			C2	16		1				RED, STAMPED, YELLOW DOMES
13	172+02	172+17	WOODSON RD	RT			C6			1				RED, STAMPED, YELLOW DOMES
13	172+56	172+81	WOODSON RD	RT			C5			1				RED, STAMPED, YELLOW DOMES
13	172+80	172+91	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
13	173+10	173+21	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
13	172+81	172+96	WOODSON RD	RT			C5			1				RED, STAMPED, YELLOW DOM

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THIS SHEET HAS BEEN
SIGNED, SEALED, AND DATED
ELECTRONICALLY

DATE PREPARED

ROUTE	STATE
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DISTRICT	SHEET NO.
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DISTRICT	SHEET NO.
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COUNTY

VARIES

JOB NO

J6P3510E

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

[illegible]MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



ENGINEERING DESIGN SOURCE, INC.

16141 Swingley Ridge Rd., Suite 300
Chesterfield, Missouri 63017
Missouri State Engineering Corporation #001523
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F. 636 537 0275

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

REV.

SCHEDULE OF QUANTITIES

SHEET 2 OF 10

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THIS SHEET HAS BEEN
DESIGNED, SEALED, AND DATED
ELECTRONICALLY

DATE PREPARED
3/28/2024

ROUTE	STATE
AD 156	MO

DISTRICT	SHEET NO.
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STATE	COUNTY
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ARTICLE 3

16P3510B

CONTRACT ID:

PROJECT NO. _____

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COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
TEL: 636-339-3300 FAX: 636-339-3301
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ENGINEERING DESIGN SOURCE, INC.
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Chesterfield, Missouri 63017
T. 636 537 5585
F. 636 537 0275

[illegible]

REV.

SIDEWALK (CONTINUED)														
PLAN SHEET NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	SIDEWALK LENGTH	SIDEWALK WIDTH	SUGGESTED ADA RAMP TYPE	TRUNCATED DOMES	SIDEWALK EDGE GRINDING	CONCRETE CURB RAMP	CONCRETE SIDEWALK, 4 IN.	CONCRETE SIDEWALK, 7 IN.	TYPE 5 AGGREGATE FOR BASE (4 IN. THICK)	REMARKS
					(FT)	(FT)		(SF)	(LF)	(EA)	(SQYD)	(SQYD)	(SQYD)	
15	189+89	190+14	WOODSON RD	LT			C6	34		1				RED, STAMPED, YELLOW DOMES
15	189+95	190+06	WOODSON RD	RT			C6			1				RED, STAMPED, YELLOW DOMES
15	190+06	190+46	WOODSON RD	RT	39	6					26.3		26.30	RED, STAMPED, YELLOW DOMES
15	190+46	190+57	WOODSON RD	RT			C6			1				RED, STAMPED, YELLOW DOMES
15	190+67	190+78	WOODSON RD	RT			C6			1				RED, STAMPED, YELLOW DOMES
SUBTOTALS								292	0	45	43.0		42.97	
17	169+29	169+83	I-70	RT	94	VARIES					55.4		55.44	
17	170+03	170+19	I-70	RT	23	5					12.8		12.78	
17	170+52	170+63	I-70	RT	21	VARIES					12.6		12.56	
17	172+11	172+20	I-70	LT	9	5					4.4		4.44	
17	172+94	173+09	I-70	LT	20	5					10.8		10.78	
17	170+14	170+28	I-70	RT			C6	21		1				
17	170+52	170+65	I-70	RT			C5	10		1				
18	268+76		I-70	RT					5.0					
18	268+77	268+86	I-70	RT	6	VARIES					4.3		4.33	
19	529+46		I-70	RT					10.0					
19	529+43	529+49	I-70	RT	15	5.5					9.1		9.11	
19	529+53	529+59	I-70	RT			C6	16		1				
19	530+30	530+35	I-70	RT	5	5					2.8		2.78	
20	529+14	529+23	I-70	LT	28	5	C9	20				14.11	14.11	ISLAND CUT THROUGH
20	529+22	529+27	I-70	LT			C5	10		1				
20	529+23	529+30	I-70	LT	15	6					10.0		10	
20	530+11	530+16	I-70	LT	15	5					8.3		8.33	
21	589+67		I-70	LT					6.0					
21	590+67		I-70	RT					6.0					
21	591+95		I-70	RT					6.0					
23	1+35	1+60	I-70	RT	26	6					17.3		17	
23	1+46	1+86	I-70	LT	39	6					26.0		26	
23	1+86	1+97	I-70	LT							4.0		4	
23	1+97	2+08	I-70	LT	13	6					8.7		9	
23	2+08	2+15	I-70	LT	15	6	C3	10.2		1				
23	2+62	2+74	I-70	LT	15	5	C3	10		1				
23	2+74	3+23	I-70	LT	50	5					27.8		28	
23	1+60	1+77	I-70	RT	15	6	C3	11.5		1				
23	3+41	3+55	I-70	RT	varies	varies	C9	19				5	5	
23	0+52	0+58	I-70	LT	8	2						2	2	
23	3+23	3+47	I-70	LT	25	5					13.1		13.11	
23	3+47	3+57	I-70	LT			C3	10		1				
22	12+40	12+76	I-70	LT			C9	30				32.33	32.33	
22	12+48	12+68	I-70	RT			C9	30				15.78	15.78	
22	13+24	13+43	I-70	RT			C9	30				15.22	15.22	
							SUBTOTAL	227.7	33.0	8	227.4	84.04	311.49	
25	10+47	10+54	S WOODS MILL	LT			C3	12		1				WEST CURB RAMP
25	10+54	11+98	S WOODS MILL	LT	149	6					99.0		99	WEST SIDEWALK
25	10+54	13+58	S WOODS MILL	LT	5	6					3.0		3	WEST SIDEWALK PANEL
25	14+34	14+39	S WOODS MILL	LT	5	6					3.0		3	WEST SIDEWALK PANEL
25	15+46	15+53	S WOODS MILL	LT			C3	12		1				WEST CURB RAMP
							SUBTOTALS	24	0	2	105.0	0	105	
27	10+35	10+50	CRAIGWOLD RD	LT			C6	29		1				
27	10+50	10+69	CRAIGWOLD RD	LT	VARIES	VARIES					11.7		11.7	
27	13+81	13+96	CRAIGWOLD RD	LT	VARIES	VARIES					8.1		8.1	
27	13+96	14+01	CRAIGWOLD RD	LT			C6	10		1				
							SUBTOTALS	39	0	2.0	19.8		19.8	
29	970+04	970+11	I-44	RT	7	5					4.2		4.2	
29	970+06	970+15	I-44	RT			C4	10		1				
29	971+22	971+34	I-44	LT	13	6	C9	14				7.4	7.4	
29	971+40	971+49	I-44	LT	14	6	C9	15				7.7	7.7	
30	1051+65	1051+80	I-44	LT			C1	11		1				
30	1051+75	1051+81	I-44	LT	8	6					3.6		3.6	
30	1052+25	1052+29	I-44	LT	9	4					4.0		4.0	
30	1051+72	1051+77	I-44	RT	6	5					3.3		3.3	

SCHEDULE OF QUANTITIES

SHEET 3 OF 10

SIDEWALK (CONTINUED)														
PLAN SHEET NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	SIDEWALK LENGTH	SIDEWALK WIDTH	SUGGESTED ADA RAMP TYPE	TRUNCATED DOMES	SIDEWALK EDGE GRINDING	CONCRETE CURB RAVP	CONCRETE SIDEWALK, 4 IN.	CONCRETE SIDEWALK, 7 IN.	TYPE 5 AGGREGATE FOR BASE (4 IN. THICK)	REMARKS
					(FT)	(FT)		(SF)	(LF)	(EA)	(SQYD)	(SQYD)	(SQYD)	
30	1052+23	1052+29	I-41	RT	VARIES	VARIES					3.4		3.4	
30	1052+24		I-41	RT					5.0					
							SUBTOTAL	50.0	5.0	2.0	18.6	15.1	33.7	
32	10+62	10+77	OLD HWY 94	LT			C4	17		1				HOLLOWBROOK DR SWC
32	11+32	11+44	OLD HWY 95	LT			C4	12		1				HOLLOWBROOK DR NWC
							SUBTOTAL	29	0	2	0	0	0	
34	41+88	42+00	N OUTER RD	LT			C1	11		1				SHADY SPRINGS LN NWC
							SUBTOTAL	11	0	1	0	0	0	
							DECORATIVE SUBTOTALS	292		45	43.0			RED, STAMPED, YELLOW DOMES
							SUBTOTAL	*848				*156.2	*593.9	*SEE SUBTOTAL SCHEDULE FOR FINAL PAY ITEM TOTAL
							TOTALS		38.0	27	394.8			

EARTHWORK						
OVERALL SHEET NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	LINEAR GRADING, CLASS 2 MODIFIED, STA	REMARKS
6	13+05	13+11	I-55	RT	0.10	I-55 NB OFF RAMP SWC RAMP
7	10+01	10+06	I-55	LT	0.50	OZARK NWC SIDEWALK
7	16+01	16+12	I-55	RT	0.10	MICHIGAN AVE SEC RAMP
					SUBTOTALS	0.70
9	12+29	12+35	I-55 W. OUTER RD	RT	0.06	W OUTER RD SWC LANDING
9	12+30	12+35	I-55 W. OUTER RD	LT	0.07	W OUTER RD NWC LANDING
9	12+37	12+44	I-55 W. OUTER RD	LT	0.08	W OUTER RD NWC LANDING
9	12+51	12+58	I-55 W. OUTER RD	RT	0.07	W OUTER RD SWC LANDING
9	16+94	17+04	IMPERIAL MAIN	LT	0.06	I-55 SB OFF RAMP
9	17+05	17+62	IMPERIAL MAIN	RT	0.06	I-55 SB ON RAMP
10	232+61	232+86	I-55 VJ, OUTER RD	LT	0.37	VOGEL NWC
10	232+64	232+84	I-55 VJ, OUTER RD	RT	0.37	VOGEL NEC
					SUBTOTALS	1.40
12	160+26	160+43	WOODSON RD	LT	0.18	
12	160+68	160+85	WOODSON RD	LT	0.18	
12	161+24	161+39	WOODSON RD	LT	0.15	
12	161+63	161+87	WOODSON RD	LT	0.25	
12	162+46	162+58	WOODSON RD	LT	0.13	
12	162+72	162+90	WOODSON RD	RT	0.15	
12	163+17	163+40	WOODSON RD	LT	0.22	
12	163+56	163+58	WOODSON RD	RT	0.22	
12	163+80	163+98	WOODSON RD	LT	0.18	
13	167+11	167+29	WOODSON RD	RT	0.16	
13	172+02	173+61	WOODSON RD	RT	1.73	
13	172+80	173+21	WOODSON RD	LT	0.48	
13	173+94	174+08	WOODSON RD	RT	0.15	
13	174+54	174+68	WOODSON RD	RT	0.15	
14	175+35	175+50	WOODSON RD	RT	0.14	
14	175+93	176+09	WOODSON RD	RT	0.15	
14	176+28	176+42	WOODSON RD	RT	0.15	
14	176+62	176+76	WOODSON RD	RT	0.15	
14	177+04	177+18	WOODSON RD	RT	0.15	
14	177+31	177+46	WOODSON RD	RT	0.15	
14	177+77	177+92	WOODSON RD	RT	0.15	
14	179+23	179+49	WOODSON RD	RT	0.24	
14	179+90	180+16	WOODSON RD	RT	0.24	
14	182+44	183+37	WOODSON RD	RT	1.17	
14	182+83	183+24	WOODSON RD	LT	0.41	
14	183+52	183+63	WOODSON RD	LT	0.11	
14	183+82	183+93	WOODSON RD	LT	0.11	
15	185+46	186+43	WOODSON RD	RT	1.37	
15	187+07	187+20	WOODSON RD	LT	0.13	
15	187+69	187+82	WOODSON RD	RT	0.13	
15	187+71	187+84	WOODSON RD	LT	0.13	
15	189+20	190+14	WOODSON RD	LT	1.10	
15	189+23	190+78	WOODSON RD	RT	1.51	
					SUBTOTAL	12.02

EARTHWORK (CONTINUED)						
OVERALL SHEET NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	LINEAR GRADING, CLASS 2 MODIFIED, STA	REMARKS
17	169+29	169+83	1-70 & McKelvey	RT	0.75	
17	170+03	170+28	1-70 & McKelvey	RT	0.42	
17	170+54	170+65	1-70 & McKelvey	RT	0.27	
19	529+43	529+49	1-70 & N Hanley	RT	0.15	
19	529+53	529+59	1-70 & N Hanley	RT	0.13	
19	530+30	530+35	1-70 & N Hanley	RT	0.05	
20	529+22		1-70 & N Hanley	LT	0.10	
20	529+23		1-70 & N Hanley	LT	0.15	
20	530+16		1-70 & N Hanley	LT	0.15	
22	12+40	12+76	Jennings Sta Rd	LT	0.60	
23	3+23	3+57	Jennings Sta Rd	LT	0.33	
					SUBTOTAL	3.10
25	10+47	11+98	S WOODS MILL	LT	1.60	WEST SIDEWALK AND CURB RAMP
25	13+54	13+61	S WOODS MILL	LT	0.10	WEST SIDEWALK
25	14+34	14+39	S WOODS MILL	LT	0.10	WEST SIDEWALK
25	15+46	15+53	S WOODS MILL	LT	0.10	WEST CURB RAMP
					SUBTOTAL	1.90
27	10+35	10+69	CRAGWOLD RD	LT	0.32	
27	13+81	14+01	CRAGWOLD RD	LT	0.20	
					SUBTOTAL	0.52
29	970+08	970+15	I-44	RT	0.26	
30	1051+65	1051+75	I-44	LT	0.16	
30	1051+72		I-44	RT	0.06	
30	1052+29		I-44	RT	0.00	
30	1052+29		I-44	LT	0.09	
					SUBTOTAL	0.66
32	10+62	10+77	S OLD HWY 94	LT	0.10	HOLLOWBROOK DR SWC RAMP
32	11+05		S OLD HWY 94		0.30	
32	11+32	11+44	S OLD HWY 94	LT	0.20	HOLLOWBROOK DR NWC RAMP
					SUBTOTAL	0.60
34	41+88	42+00	SHADY SPRINGS	LT	0.10	SHADY SPRINGS NWC RAMP
					SUBTOTAL	0.10
					TOTAL	21.00

SUBTOTALS			
APPLICATION	TRUNCATED DOMES (SF)	TYPE 5 AGGREGATE FOR BASE (4 IN THICK) (SQYD)	CONCRETE SIDEWALK, 7 IN. (SQYD)
ISLAND & MEDIAN	*30.0	*29.2	*9.7
PAVEMENT		*718.1	
DRIVEWAYS		*392.6	
SIDEWALK	*848	*593.9	*156.2
TOTAL	878	1734	165.9

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THIS SHEET HAS BEEN
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ELECTRONICALLY

DATE PREPARED

3/28/2024

ROUTE

VARIES

DISTRICT

STL

STATE

MO

SHEET NO.

3

COUNTY

VARIES

JOB NO.

J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION						
DATE						

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION



EDSI
ENGINEERING DESIGN SOURCE, INC.

16141 Swingle Ridge Rd., Suite 300
Chesterfield, Missouri 63017
Missouri State Engineering Corporation #001523
T. 636 537 5585
F. 636 537 0215



SCHEDULE OF QUANTITIES

SHEET 4 OF 10

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STATE OF MISSOURI

STEVEN T. STIRNEMANN

NUMBER PE-2003001103

PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY

DATE PREPARED
3/28/2024

ROUTE
VARIES

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3

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J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

PAVEMENT MARKING QUANTITIES										
OVERALL SHEET NUMBER	BASELINE	FROM STATION	TO STATION	LOCATION	6 IN. WHITE HIGH BUILD WATERBORNE PAVEMENT MARKING PAINT, TYPE I BEADS	PREFORMED THERMOPLASTIC PAVEMENT MARKING, 24 IN. WHITE	PREFORMED THERMOPLASTIC PAVEMENT MARKING, 30 IN WHITE MIDDLE	PREFORMED THERMOPLASTIC PAVEMENT MARKING, 12 IN WHITE, YIELD LINE TRIANGLES	PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING PAINT*
					(LF)	(LF)	(EA)	(EA)	(LF)	(LS)
6	IMPERIAL MAIN	13+08.00	13+16.00	LT	39					
6	IMPERIAL MAIN	13+13.00	13+15.00	LT	33					
6	IMPERIAL MAIN	13+16.00	13+32.00	RT	18					
6	IMPERIAL MAIN	13+18.00	13+34.00	RT	18					
6	IMPERIAL MAIN	13+21.00	13+34.00	RT		15				
6	IMPERIAL MAIN	13+50.00	13+63.00	LT	22					
6	IMPERIAL MAIN	13+54.00	13+68.00	LT	22					
6	IMPERIAL MAIN	13+54.00	13+70.00	RT	21					
6	IMPERIAL MAIN	13+56.00	13+57.00	KI				9		
6	IMPERIAL MAIN	13+58.00	13+74.00	RT	21					
6	IMPERIAL MAIN	13+62.00	13+71.00	LT				9		
				SUBTOTAL	194	15	0	18	0	1
9	IMPERIAL MAIN	12+30.00	12+30.00	LT/RT	51					
9	IMPERIAL MAIN	12+36.00	12+36.00	LT/RT	57					
9	IMPERIAL MAIN	12+36.00	12+45.00	RT	10					
9	IMPERIAL MAIN	12+36.00	12+44.00	LT	10					
9	IMPERIAL MAIN	12+51.00	13+11.00	LT	64					
9	IMPERIAL MAIN	12+54.00	13+11.00	LT	61					
9	IMPERIAL MAIN	12+62.00	13+15.00	RT	50					
9	IMPERIAL MAIN	12+66.00	13+16.00	RT	47					
9	IMPERIAL MAIN	13+24.00	13+24.00	LT/RT	67					
9	IMPERIAL MAIN	13+27.00	13+46.00	RT	23					
9	IMPERIAL MAIN	13+28.00	13+42.00	LT	20					
9	IMPERIAL MAIN	13+30.00	13+30.00	LT/RT	67					
9	IMPERIAL MAIN	13+30.00	13+43.00	RT				9		
9	IMPERIAL MAIN	13+31.00	13+50.00	RT	24					
9	IMPERIAL MAIN	13+32.00	13+46.00	LT	21					
9	IMPERIAL MAIN	13+37.00	13+49.00	LT				11		
9	IMPERIAL MAIN	16+34.00	16+45.00	RT				9		
9	IMPERIAL MAIN	16+37.00	16+51.00	RT	19					
9	IMPERIAL MAIN	16+37.00	16+51.00	LT	19					
9	IMPERIAL MAIN	16+41.00	16+55.00	RT	20					
9	IMPERIAL MAIN	16+42.00	16+55.00	LT	19					
9	IMPERIAL MAIN	16+45.00	16+55.00	LT				9		
9	IMPERIAL MAIN	16+70.00	17+05.00	RT	36					
9	IMPERIAL MAIN	16+70.00	17+12.00	RT	42					
9	IMPERIAL MAIN	16+71.00	16+89.00	LT		18				
9	IMPERIAL MAIN	16+72.00	16+94.00	LT	23					
9	IMPERIAL MAIN	16+73.00	16+95.00	LT	23					
9	IMPERIAL MAIN	16+75.00	16+76.00	RT	11					
9	IMPERIAL MAIN	17+05.00	17+16.00	RT	13					
10	W OUTER RD	231+30.00	231+47.00	RT	21					
10	W OUTER RD	231+33.00	231+50.00	RT	21					
10	W OUTER RD	231+34.00	231+47.00	LT	18					
10	W OUTER RD	231+38.00	231+51.00	LT	19					
10	W OUTER RD	231+41.00	231+51.00	LT				9		
10	W OUTER RD	231+42.00	231+43.00	RT		24				

* SEE JSP NN

SCHEDULE OF QUANTITIES

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

MoDOT

105 WEST CAPITAL JEFFERSON CITY, MO 65102 1-888-ASK-MODOT (1-888-275-6636)

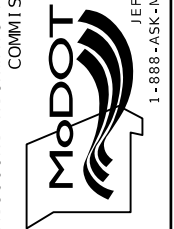
EDSI

ENGINEERING DESIGN SOURCE, INC.

16141 Swingley Ridge Rd., Suite 300 Chesterfield, Missouri 63017 Missouri State Engineering Corporation #001523

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* SEE JSP NN



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ROUTE	STATE
VARIES	MO
DISTRICT	SHEET NO.
STL	3

PROJECT NO.

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[illegible]

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Chesterfield, Missouri 63017
Missouri State Engineering Corporation #001523

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REV.

SHEET 8 OF 10

EFFECTIVE: 04-01-2023																							
SIGN	SIZE IN.	AREA SQ.FT.	QTY EACH	TOTAL AREA SQ.FT.	QTY RELOC EACH	TOTAL RELOC SQ.FT.	SIGN NUM.		SIGN	SIZE IN.	AREA SQ.FT.	QTY EACH	TOTAL SQ.FT.	QTY RELOC EACH	TOTAL RELOC SQ.FT.	SIGN NUM.							
WARNING SIGNS								DESCRIPTION								ITEM NUMBER				TOTAL QTY		DESCRIPTION	
WO1-1L	48X48	16.00						TURN (SYMBOL LEFT ARROW)	E05-1	36X48	12.00							GORE EXIT	6122008		IMPACT ATTENUATOR 40 MPH (SAND BARRELS)		
WO1-1R	48X48	16.00						TURN (SYMBOL RIGHT ARROW)	E05-2	48X36	12.00							EXIT OPEN	6122009		IMPACT ATTENUATOR 45 MPH (SAND BARRELS)		
WO1-2L	48X48	16.00						CURVE (SYMBOL LEFT ARROW)	E05-2a	48X36	12.00							EXIT CLOSED	6122010		IMPACT ATTENUATOR 50 MPH (SAND BARRELS)		
WO1-2R	48X48	16.00						CURVE (SYMBOL RIGHT ARROW)	GO20-1	60X24	10.00							ROAD WORK NEXT XX MILES	6122012		IMPACT ATTENUATOR 55 MPH (SAND BARRELS)		
WO1-3L	48X48	16.00						REVERSE TURN (SYMBOL LEFT ARROW)	GO20-2	48X24	8.00							END ROAD WORK	6122014		IMPACT ATTENUATOR 60 MPH (SAND BARRELS)		
WO1-3R	48X48	16.00						REVERSE TURN (SYMBOL RIGHT ARROW)	GO20-4	36X18	4.50							PILOT CAR FOLLOW ME	6122017		IMPACT ATTENUATOR 65 MPH (SAND BARRELS)		
WO1-4L	48X48	16.00						REVERSE CURVE (SYMBOL LEFT ARROW)	GO20-4a	42X30	8.75							PILOT CAR IN USE WAIT & FOLLOW	6122019		IMPACT ATTENUATOR 70 MPH (SAND BARRELS)		
WO1-4R	48X48	16.00						REVERSE CURVE (SYMBOL RIGHT ARROW)	GO20-4a	18X12	1.50							PILOT CAR IN USE WAIT & FOLLOW	6122020		REPLACEMENT SAND BARREL		
WO1-4bL	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL LT ARROWS)	GO20-5aP	36X24	6.00							WORK ZONE (PLAQUE)	6122030		IMPACT ATTENUATOR (RELOCATION)		
WO1-4bR	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL RT ARROWS)	MO4-8a	24X18	3.00							END DETOUR	6123000A		TRUCK OR TRAILER MOUNTED ATTENUATOR (TMA)		
WO1-4cL	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL LT ARROWS)	MO4-9L	48X36	12.00							DETOUR (LEFT ARROW)	6161008	4	ADVANCED WARNING RAIL SYSTEM		
WO1-4cR	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL RT ARROWS)	MO4-9R	48X36	12.00							DETOUR (RIGHT ARROW)	6161012		BUOYS (BOATS KEEP OUT)		
WO1-6	60X30	12.50						HORIZONTAL ARROW (SYMBOL)	MO4-9P	48X12	4.00							STREET NAME (PLAQUE)	6161013		BUOYS (NO WAKE)		
WO1-6a	72X36	18.00						HORIZ. ARROW (SYMBOL ON PERMANENT BARRICADE)	MO4-10L	48X18	6.00							DETOUR (ARROW LEFT)	6161014		SPECIAL SIGN ASSEMBLY (BOATS KEEP OUT)		
WO1-7	60X30	12.50						DOUBLE HEAD HORIZONTAL ARROW (SYMBOL)	MO4-10R	48X18	6.00							DETOUR (ARROW RIGHT)	6161025	272	CHANNELIZER (TRIM LINE)		
WO1-7a	72X36	18.00						DOUBLE HEAD HORIZ. ARROW (SYMBOL ON PERM. BARR.)	REGULATORY SIGNS										6161030	3	TYPE III MOVEABLE BARRICADE		
WO1-8	18X24	3.00						CHEVRON (SYMBOL)	R1-1	48X48	13.25							STOP	6161033		DIRECTION INDICATOR BARRICADE		
WO1-8a	30X36	7.50						CHEVRON (SYMBOL FOR DIVIDED HIGHWAYS)	R1-2	48TRI	6.93							YIELD	6161040	3	FLASHING ARROW PANEL		
WO3-1	48X48	16.00						STOP AHEAD (SYMBOL)	R1-2a	36X36	9.00							TO ONCOMING TRAFFIC (PLAQUE)	6161047		TYPE III OBJECT MARKER		
WO3-2	48X48	16.00						YIELD AHEAD (SYMBOL)	R1-3P	30X12	2.50							ALL WAY (PLAQUE)	6161055		SEQUENTIAL FLASHING WARNING LIGHT		
WO3-3	48X48	16.00						SIGNAL AHEAD (SYMBOL)	R2-1	36X48	12.00							SPEED LIMIT XX	6161070		TUBULAR MARKER		
WO3-4	48X48	16.00						BE PREPARED TO STOP	R3-1	48X48	16.00							NO RIGHT TURN (SYMBOL)	6161095		RADAR SPEED ADVISORY SYSTEM		
WO3-5	48X48	16.00						SPEED LIMIT AHEAD	R3-2	48X48	16.00							NO LEFT TURN (SYMBOL)			CHANGEABLE MESSAGE SIGN, COMMISSION FURNISHED/RETAINED		
WO4-1L	48X48	16.00						MERGE (SYMBOL FROM LEFT)	R3-3	36X36	9.00							NO TURNS	6161096		CHANGEABLE MESSAGE SIGN W/O COMM. INTERFACE - CONTRACTOR FURNISHED/RETAINED		
WO4-1R	48X48	16.00						MERGE (SYMBOL FROM RIGHT)	R3-4	48X48	16.00							NO U-TURN (SYMBOL)	6161098A		CHANGEABLE MESSAGE SIGN WITH COMM. INTERFACE - CONTRACTOR FURNISHED/RETAINED		
WO4-1aL	48X48	16.00						MERGE (ARROW SYMBOL)	R3-7L	30X30	6.25							LEFT LANE MUST TURN LEFT	6161099		WORK ZONE TRAFFIC SIGNAL SYSTEM		
WO4-1aR	48X48	16.00						MERGE (ARROW SYMBOL)	R3-7R	30X30	6.25							RIGHT LANE MUST TURN RIGHT	6162002		TEMPORARY LONG-TERM RUMBLE STRIPS		
WO5-1	48X48	16.00	2	32				ROAD/BRIDGE/RAMP NARROWS	R4-1	36X48	12.00							DO NOT PASS	6173600D		TEMPORARY TRAFFIC BARRIER CONTRACTOR FURNISHED/RETAINED		
WO5-3	48X48	16.00						ONE LANE BRIDGE	R4-2	36X48	12.00							PASS WITH CARE	6173602B		TEMPORARY TRAFFIC BARRIER CONTRACTOR FURNISHED/COMMISSION RETAINED		
WO5-5	48X48	16.00						NARROW LANES	R4-8a	36X48	12.00							KEEP LEFT (HORIZONTAL ARROW)	6174000A		TEMP. TRAFFIC BARRIER HEIGHT TRANSITION		
WO6-1	48X48	16.00						DIVIDED HIGHWAY (SYMBOL)	R4-7a	36X48	12.00							KEEP RIGHT (HORIZONTAL ARROW)	6175010A		RELOCATING TEMPORARY TRAFFIC BARRIER		
WO6-2	48X48	16.00						DIVIDED HIGHWAY END (SYMBOL)	R5-1	30X30	6.25							DO NOT ENTER	6176000B		TEMPORARY TRAFFIC BARRIER COMMISSION FURNISHED/RETAINED		
WO6-3	48X48	16.00						TWO WAY TRAFFIC (SYMBOL)	R5-1a	36X24	6.00							WRONG WAY	6177000B		TEMP. TRAFFIC BARRIER HEIGHT TRANSITION		
WO7-3a	30X24	5.00	2	10				NEXT XX MILES (PLAQUE)	R6-1L	54X18	6.75							ONE WAY ARROW (LEFT)	6208064A		COMMISSION FURNISHED/RETAINED		
WO8-1	48X48	16.00						BUMP	R6-1R	54X18	6.75							ONE WAY ARROW (RIGHT)	9029400		TEMPORARY RAISED PAVEMENT MARKER		
WO8-2	48X48	16.00						DIP	R6-2L	24X30	5.00							ONE WAY (LEFT)	9029401		TEMPORARY TRAFFIC SIGNALS		
WO8-3	48X48	16.00						PAVEMENT ENDS	R6-2R	24X30	5.00							ONE WAY (RIGHT)			TEMPORARY TRAFFIC SIGNALS AND LIGHTING		
WO8-4	48X48	16.00						SOFT SHOULDER	R9-9	24X12	2.00	9	18					SIDEWALK CLOSED					
WO8-5	48X48	16.00						SLIPPERY WHEN WET (SYMBOL)	R9-11L	24X18	3.00	2	6					SIDEWALK CLOSED AHEAD, (ARROW LEFT) CROSS HERE					
WO8-6	48X48	16.00						TRUCK CROSSING (WITH FLAGS)	R9-11R	24X18	3.00	2	6					SIDEWALK CLOSED AHEAD, (ARROW RIGHT) CROSS HERE					
WO8-6c	48X48	16.00						TRUCK ENTRANCE	R10-6	24X36	6.00							STOP HERE ON RED (45° ARROW)					
WO8-7	36X36	9.00						LOOSE GRAVEL	R11-2	48X30	10.00							ROAD CLOSED					
WO8-7a	36X36	9.00						FRESH OIL/LOOSE GRAVEL	R11-3a	60X30	12.50							ROAD CLOSED XX MILES AHEAD LOCAL TRAFFIC ONLY					
WO8-9	48X48	16.00						LOW SHOULDER	R11-4	60X30	12.50							ROAD CLOSED TO THRU TRAFFIC					
WO8-11	48X48	16.00						UNEVEN LANES	CONST-3A	60X48	20.00							FINE SIGN					
WO8-12	48X48	16.00						NO CENTER LINE	CONST-3X	56X12	4.67							SPEEDING/PASSING (PLATE)					
WO8-15	48X48	16.00						GROOVED PAVEMENT	MISCELLANEOUS SIGNS														
WO8-15P	30X24	5.00						MOTORCYCLE (PLAQUE)	CONST-5	48X36	12.00							POINT OF PRESENCE					
WO8-17	48X48	16.00						SHOULDER DROP-OFF (SYMBOL)	CONST-5	96X48	32.00							POINT OF PRESENCE					
WO8-17P	30X24	5.00						SHOULDER DROP-OFF (PLAQUE)	CONST-7	48X24	8.00							RATE OUR WORK ZONE					
W10-1	42RND.	9.62						RAILROAD CROSSING	CONST-7	72X36	18.00							RATE OUR WORK ZONE					
WO12-1	24X24	4.00						DOUBLE DOWN ARROW (SYMBOL)	CONST-8	48X36	12.00							WORK ZONE NO PHONE ZONE					
WO12-2	48X48	16.00						LOW CLEARANCE (SYMBOL)	R-10	24X18	3.00	4	12					SIDEWALK CLOSED USE OTHER SIDE					
WO12-2X	24X18	3.00						LOW CLEARANCE (PLAQUE)															
WO12-2a	84X24	14.00						OVERHEAD LOW CLEARANCE (FEET AND INCHES)															
WO12-4	120X60	50.00						LOW CLEARANCE XX FT XX IN XX MILES AHEAD															
WO12-5	120X60	50.00						WIDTH RESTRICTION XX FT XX IN XX MILES AHEAD						</									

*NOTES:

1. QUANTITIES ON THIS SHEET ARE ESTIMATED FOR THE ENTIRE PACKAGE.
2. TRAFFIC CONTROL ITEMS TO BE PAID FOR AS A LUMP SUM PAY ITEM.
3. NO DIRECT PAYMENT FOR RELOCATION OF TRAFFIC CONTROL DEVICES.

SCHEDULE OF QUANTITIES

SHEET 10 OF 10

STATE OF MISSOURI

STEVEN T. STIRNEMANN

NUMBER PE-2003001103

PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY

DATE PREPARED
3/28/2024

ROUTE
VARIES

DISTRICT
STL

STATE
MO

SHEET NO.
3

COUNTY
VARIES

JOB NO.
J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

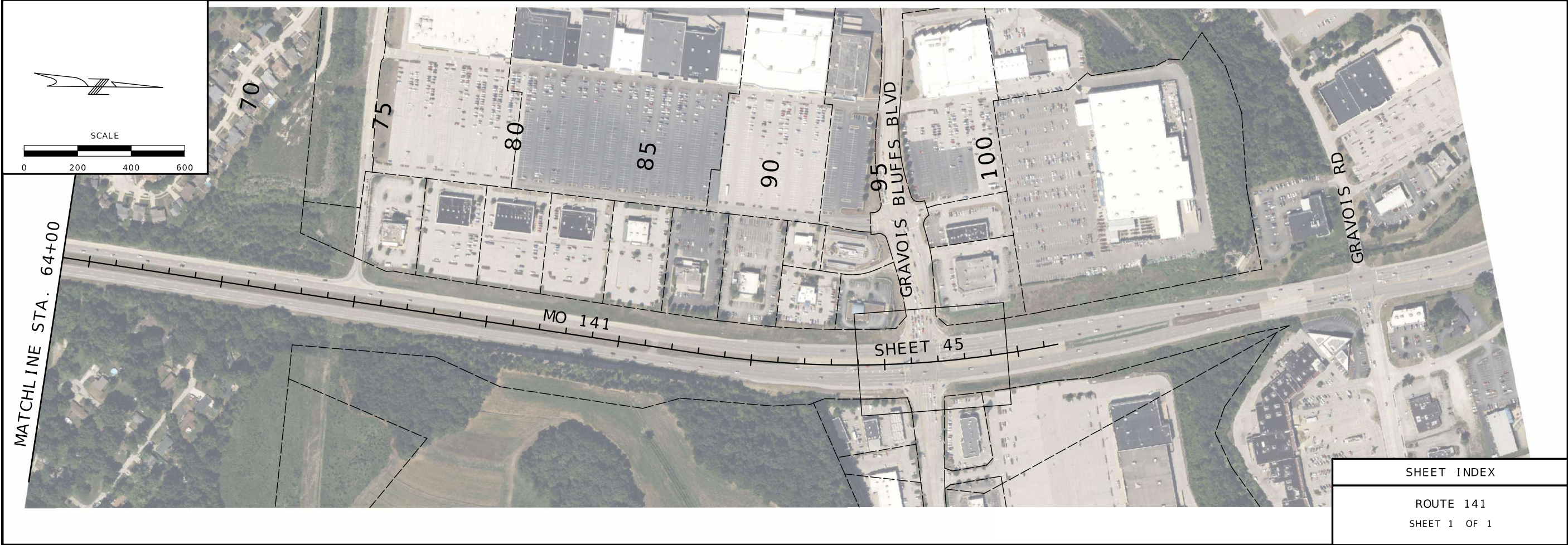


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STATE OF MISSOURI
JOHN R. KEEVEN
PE-2010000836
PROFESSIONAL ENGINEER
John Keeven
03/27/2024 1:37:13 PM
John Keeven
MO PE-2010000836

DATE PREPARED 3/27/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 4
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

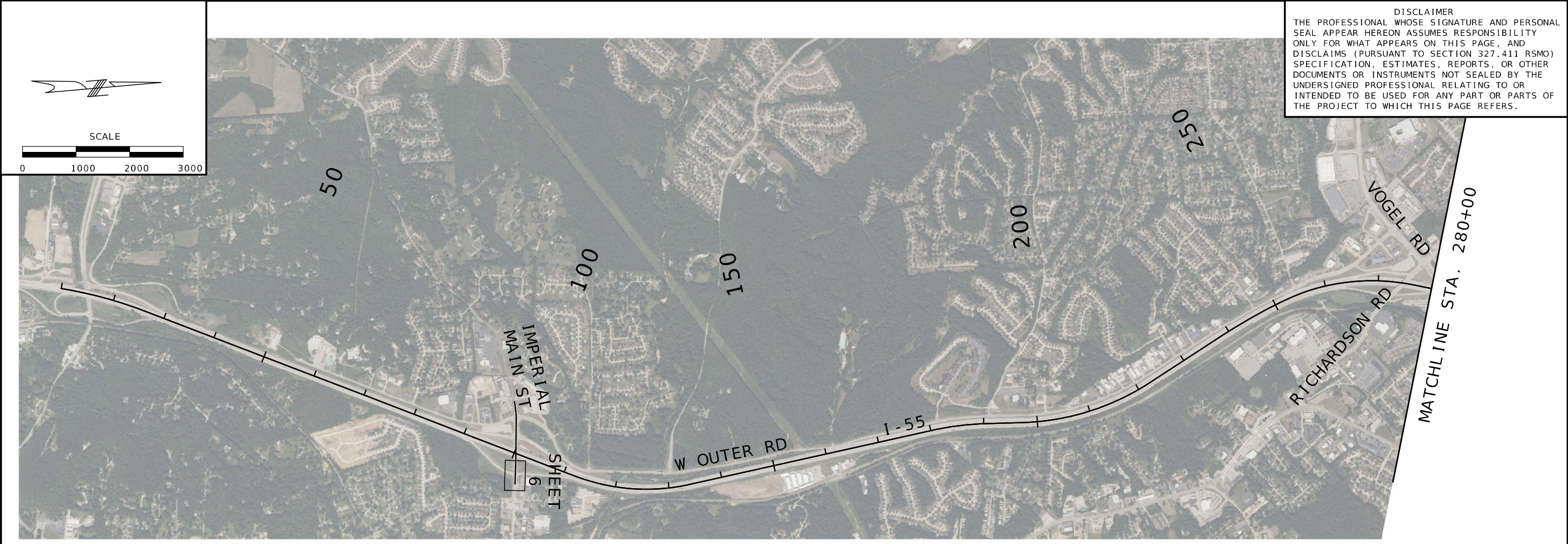
DATE	DESCRIPTION




MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
MODOT
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

CMT
CRAWFORD, MURPHY & TILLY, INC.
ONE MEMORIAL DRIVE, SUITE 500
ST. LOUIS, MO 63102 (314) 436-5500
ENGINEERING CORPORATION - 000631

SHEET INDEX
ROUTE 141 SHEET 1 OF 1



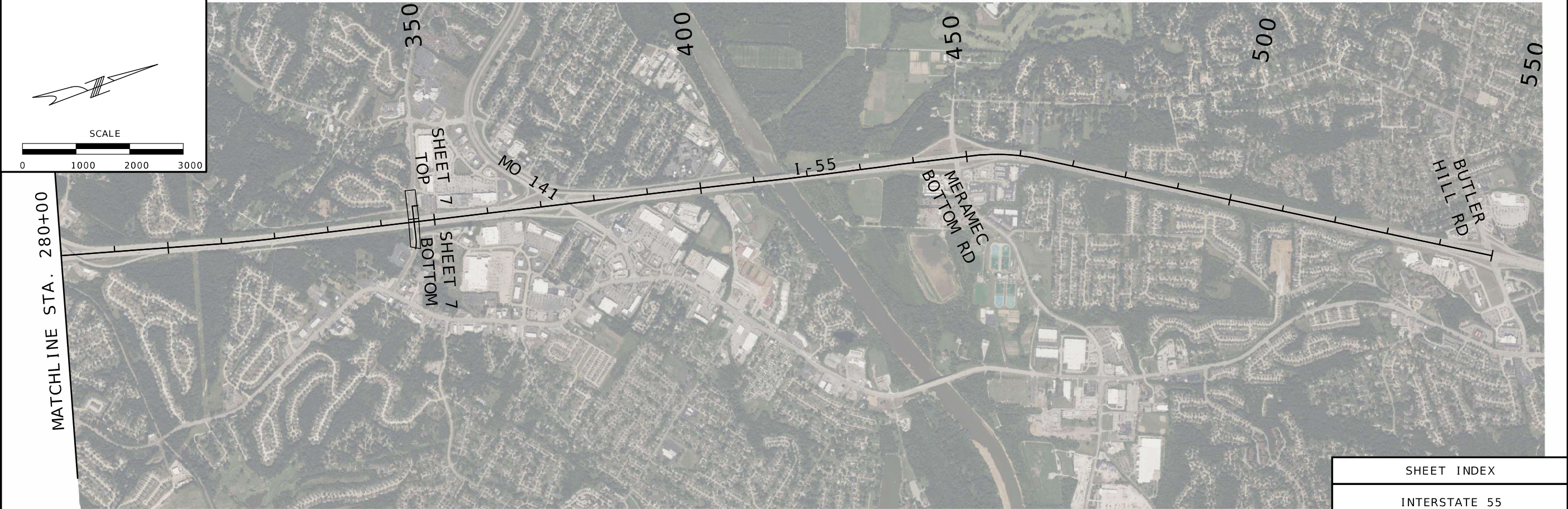
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03/27/2024 1:37:27 PM
John Keeven
MO PE-2010000836

DATE PREPARED 3/27/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 5
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DATE	DESCRIPTION



MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION



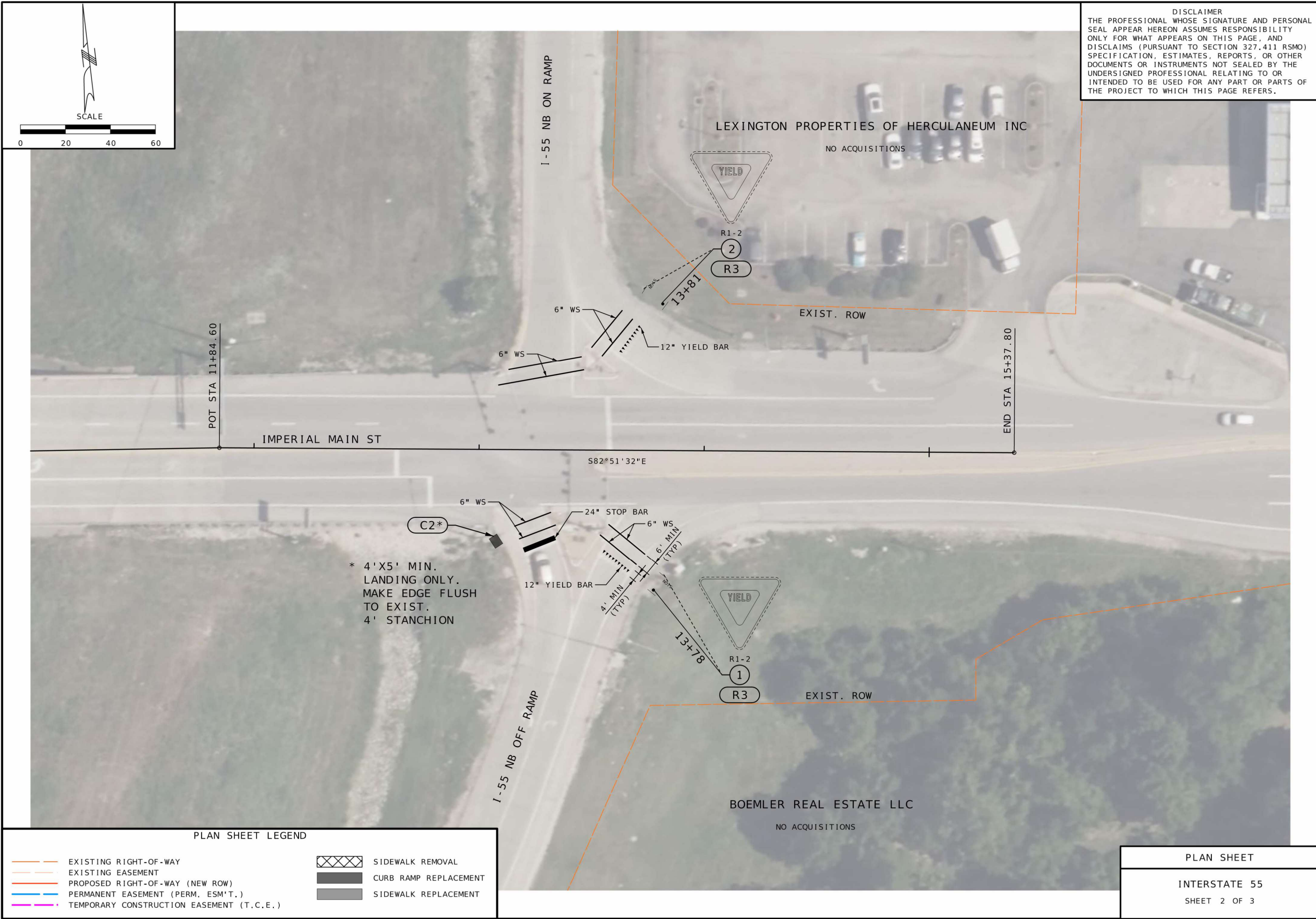
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



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CRAWFORD, MURPHY &
TILLY, INC.

ONE MEMORIAL DRIVE, SUITE 500
ST. LOUIS, MO 63102 (314) 436-5500
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SHEET INDEX
INTERSTATE 55
SHEET 1 OF 3



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STATE OF MISSOURI
JOHN R. KEEVEN
NUMBER
PE-2010000836
PROFESSIONAL ENGINEER

03/27/2024 1:37:27 PM
John Keaven
MO PE-2010000836

DATE PREPARED 3/27/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 6
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

CRAWFORD, MURPHY &
TILLY, INC.

ONE MEMORIAL DRIVE, SUITE 500
ST. LOUIS, MO 63102 (314) 436-5500
ENGINEERING CORPORATION - 000631

PLAN SHEET LEGEND

- EXISTING RIGHT-OF-WAY

EXISTING EASEMENT

PROPOSED RIGHT-OF-WAY (NEW ROW)

PERMANENT EASEMENT (PERM. ESM'T.)

TEMPORARY CONSTRUCTION EASEMENT (T.C.E.)
- SIDEWALK REMOVAL

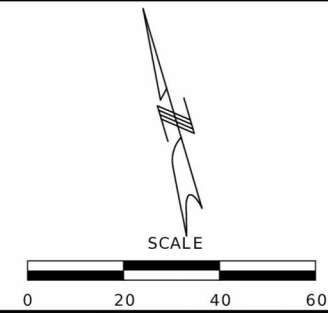
CURB RAMP REPLACEMENT

SIDEWALK REPLACEMENT

PLAN SHEET

INTERSTATE 55


SHEET 2 OF 3



03/27/2024 1:37:27 PM
John Keeven
MO PE-201000836

[illegible]

**MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION**

The logo for the Missouri Department of Transportation (MoDOT) features the word "MoDOT" in a bold, sans-serif font. The "M" and "D" are connected, and the "O" is a circle. Below the text is a stylized graphic of a road or highway with three curved lines representing lanes or a bridge structure.

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



MATCHLINE STA 12+40

CHURCH RD

1-55

MATCHLINE STA 12+40

S76°06'08"E

POT STA 345+68.94

1-55

15

PC STA 15+39.16

MICHIGAN AVE

PCC STA 16+40.72

JEFF CO CATHOLIC CHUR REAL EST






NO ACQUISITIONS

EXIST. ROW

CHURCH RD

C

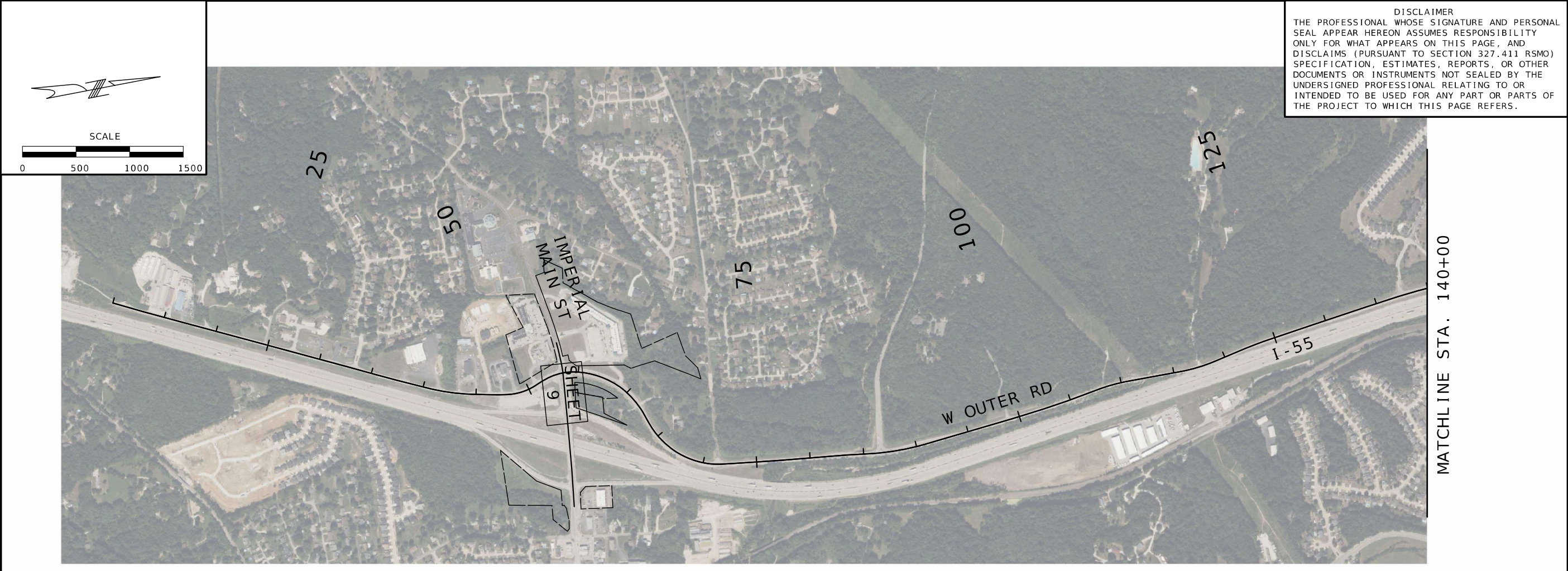
PLAN SHEET LEGEND

- | | | | |
|---|--|---|-----------------------|
| | EXISTING RIGHT-OF-WAY |  | SIDEWALK REMOVAL |
| | EXISTING EASEMENT |  | CURB RAMP REPLACEMENT |
|  | PROPOSED RIGHT-OF-WAY (NEW ROW) |  | SIDEWALK REPLACEMENT |
|  | PERMANENT EASEMENT (PERM. ESM'T.) | | |
| | TEMPORARY CONSTRUCTION EASEMENT (T.C.E.) | | |

PLAN SHEET

INTERSTATE 55

SHEET 3 OF 3

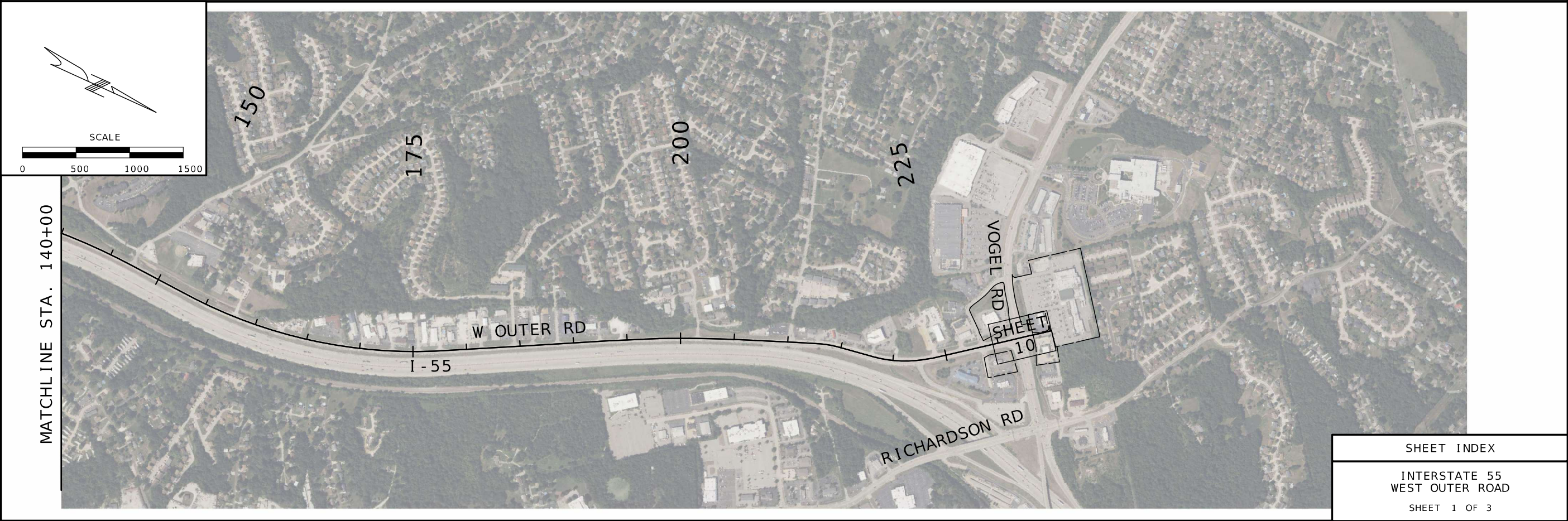


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STATE OF MISSOURI
JOHN R. KEEVEN
REGISTERED PROFESSIONAL ENGINEER
03/27/2024 1:37:27 PM
John Keeven
MO PE-2010000836

DATE PREPARED
3/27/2024
ROUTE Varies STATE MO
DISTRICT STL SHEET NO. 8
COUNTY Varies
JOB NO. J6P3510B
CONTRACT ID.
PROJECT NO.
BRIDGE NO.

DATE	DESCRIPTION
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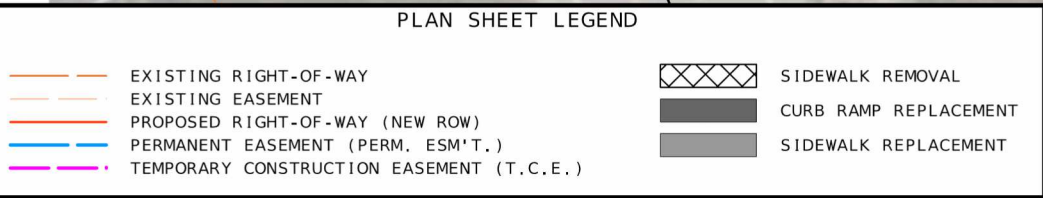
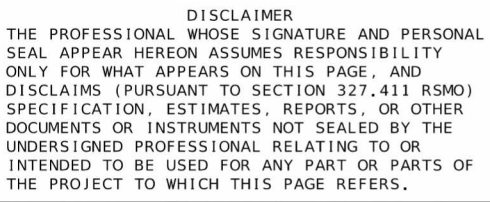
MATCHLINE STA. 140+00

MATCHLINE STA. 140+00

SHEET INDEX
INTERSTATE 55 WEST OUTER ROAD SHEET 1 OF 3

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
MODOT
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

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TILLY, INC.
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ST. LOUIS, MO 63102 (314) 436-5500
ENGINEERING CORPORATION - 000631



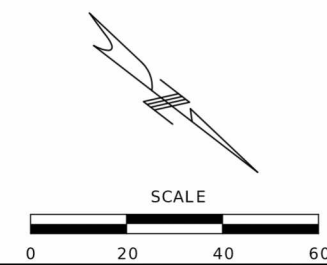
* 4'X5' MIN. LANDING ONLY.
MAKE EDGE FLUSH TO EXIST. 4' STANCHION

PLAN SHEET

INTERSTATE 55
WEST OUTER ROAD

SHEET 2 OF 3

[illegible]



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STATE OF MISSOURI
JOHN R. KEEVEN
NUMBER PE-2010000836
PROFESSIONAL ENGINEER

03/27/2024 1:37:28 PM
John Keeven
MO PE-2010000836

DATE PREPARED 3/27/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 10
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

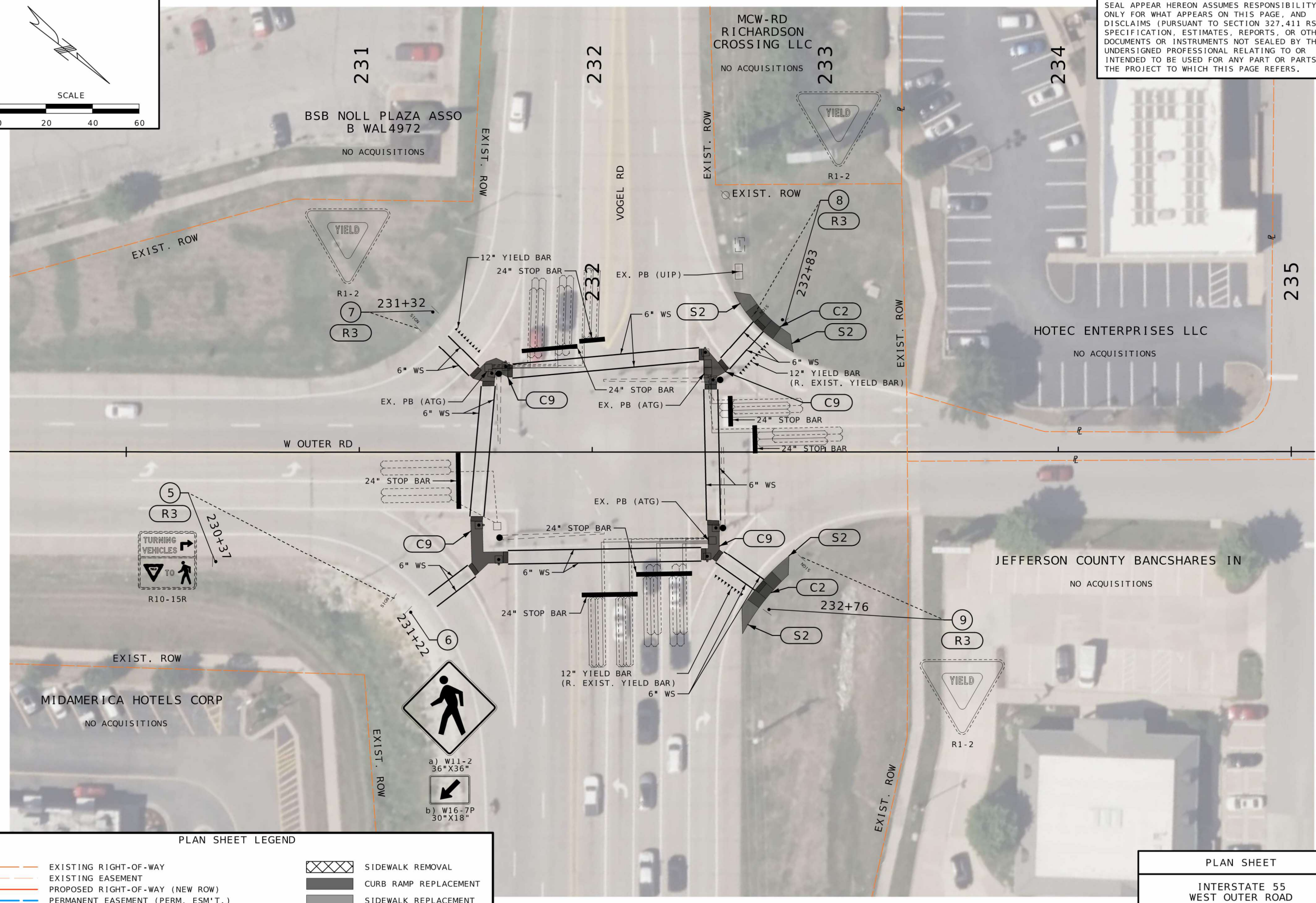
DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

CRAWFORD, MURPHY &
TILLY, INC.

ONE MEMORIAL DRIVE, SUITE 500
ST. LOUIS, MO 63102 (314) 436-5500
ENGINEERING CORPORATION - 000631



PLAN SHEET LEGEND

EXISTING RIGHT-OF-WAY

EXISTING EASEMENT

PROPOSED RIGHT-OF-WAY (NEW ROW)

PERMANENT EASEMENT (PERM. ESM'T.)

TEMPORARY CONSTRUCTION EASEMENT (T.C.E.)

SIDEWALK REMOVAL

CURB RAMP REPLACEMENT

SIDEWALK REPLACEMENT

PLAN SHEET

INTERSTATE 55
WEST OUTER ROAD

SHEET 3 OF 3



SCALE



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DATE PREPARED 3/28/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 11
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

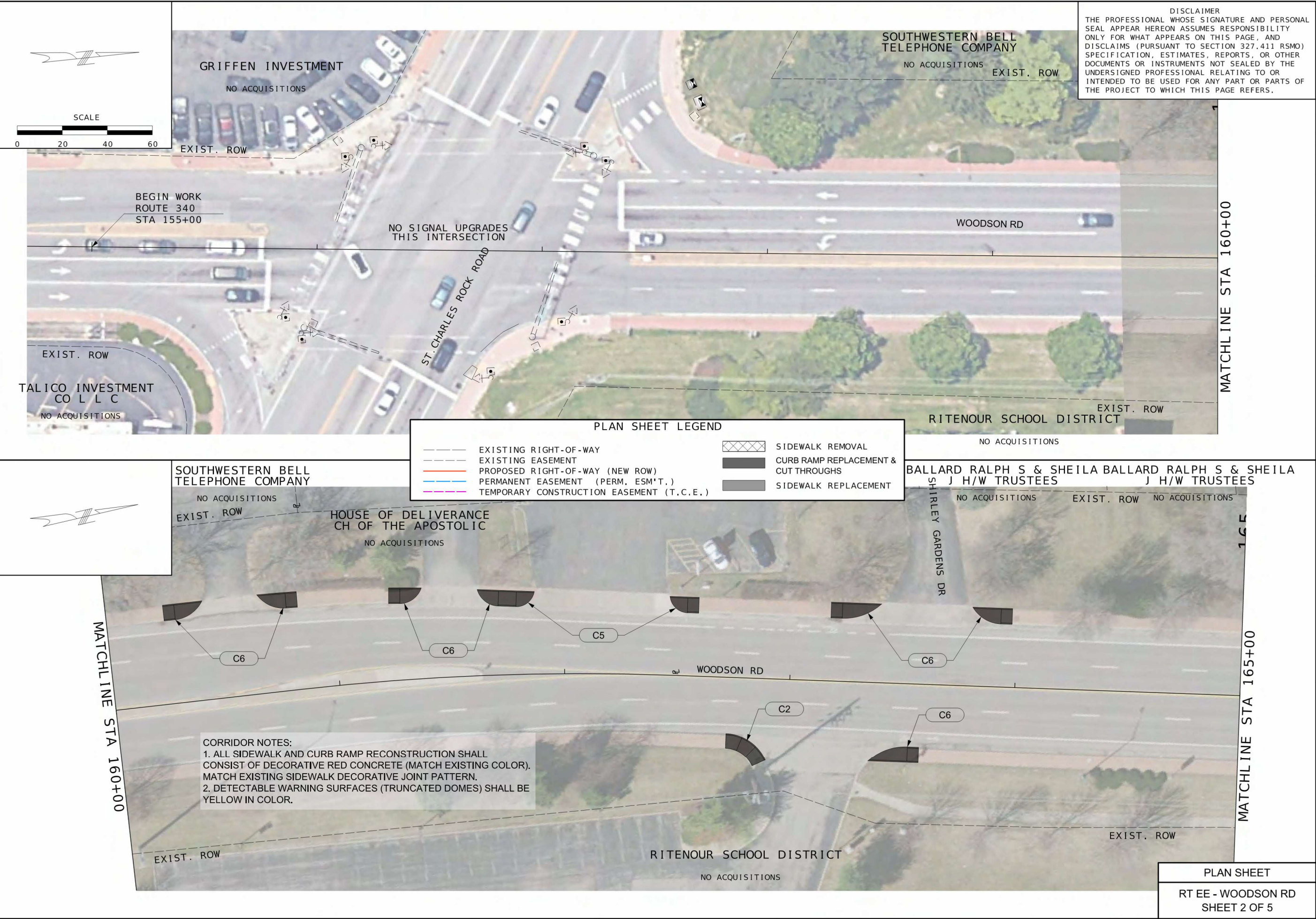
DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

EDSI
ENGINEERING DESIGN SOURCE, INC.
16141 Swingley Ridge Rd., Suite 300
Chesterfield, Missouri 63017
Missouri State Engineering Corporation #001523
T. 636 537 5585
F. 636 537 0215

SHEET INDEX
RT EE - WOODSON RD
SHEET 1 OF 5



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DATE PREPARED
3/28/2024

ROUTE
VARIES
DISTRICT
STL

STATE
MO
SHEET NO.
12

COUNTY
VARIES
JOB NO.
J6P3510B
CONTRACT ID.

PROJECT NO.

BRIDGE NO.

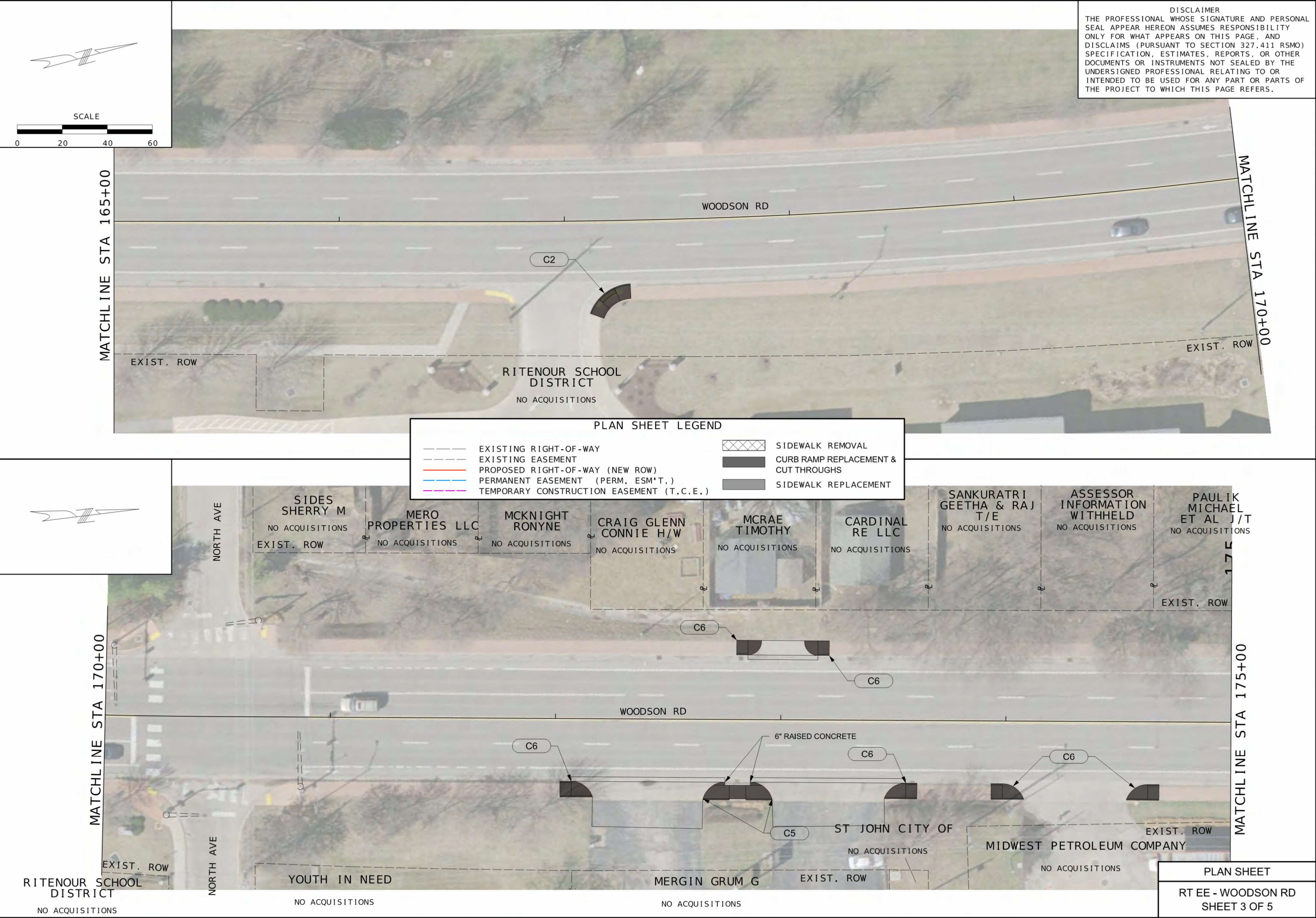
DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

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ENGINEERING DESIGN SOURCE, INC.
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Chesterfield, Missouri 63017
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Missouri State Engineering Corporation #001523

PLAN SHEET
RT EE - WOODSON RD
SHEET 2 OF 5



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DATE PREPARED		3/28/2024	
ROUTE	STATE	VARIES	MO
DISTRICT	SHEET NO.	STL	13
COUNTY			
VARIES			
JOB NO.			
J6P3510B			
CONTRACT ID.			
PROJECT NO.			
BRIDGE NO.			

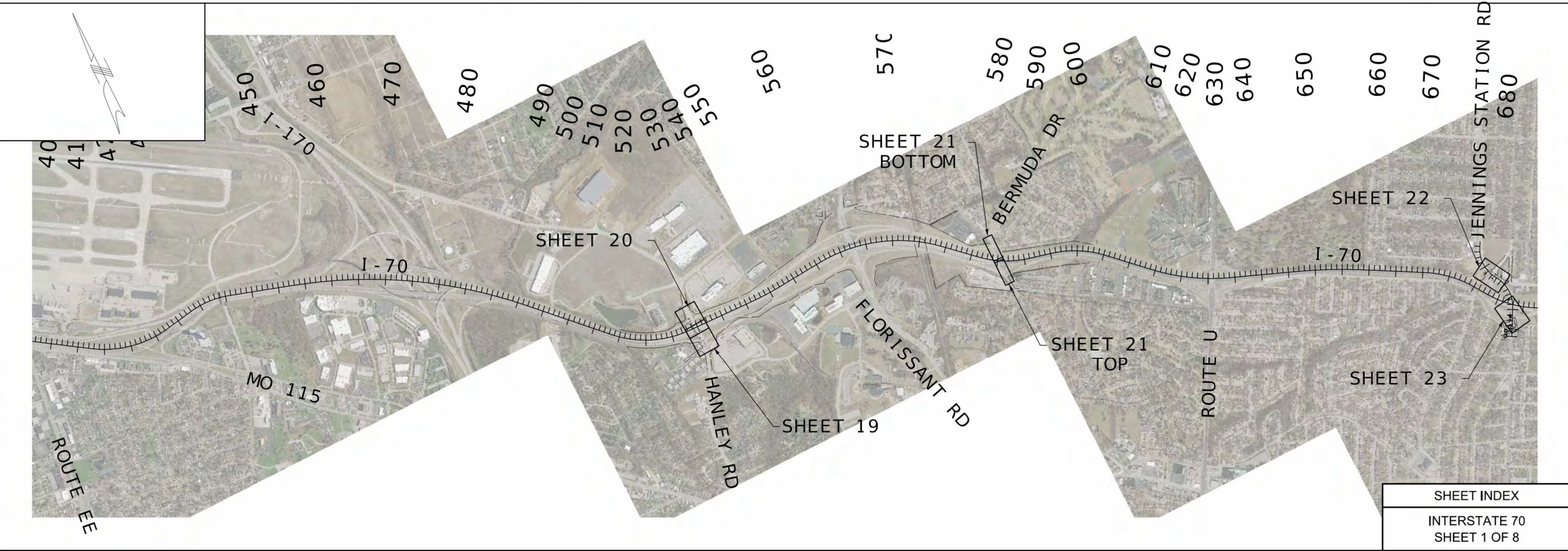
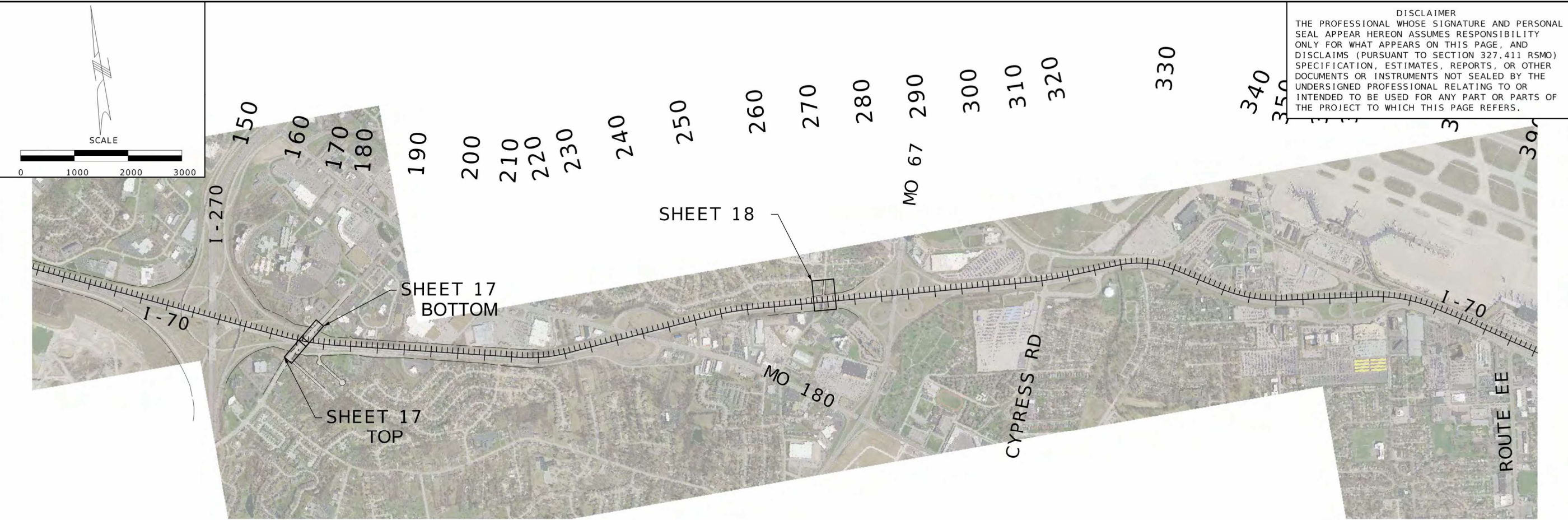
DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

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ENGINEERING DESIGN SOURCE, INC.
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Chesterfield, Missouri 63017
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F. 636 537 0215
Missouri State Engineering Corporation #001523

PLAN SHEET
RT EE - WOODSON RD
SHEET 3 OF 5



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THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY

DATE PREPARED

3/28/2024

ROUTE STATE

VARIES MO

DISTRICT SHEET NO.

STL 16

COUNTY

VARIES

JOB NO.

J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

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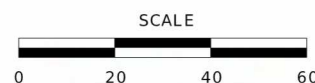
DATE

DATE

DATE

DATE

DATE



An aerial photograph of a road intersection. A label 'R4' in a rounded rectangle points to a location on the upper road. A label 'S14' in a rounded rectangle points to a location on the lower road, near a black rectangular marker.

CUT BACK
EXCESSIVE
BRUSH
ENCROACHMENT

DWIGHT D. EISENHOWER HWY (I-70)

BENEDETTA DR

12M530274
4129 FEE FEE RD
N/F
LEAVITT JEREMY N & MARIA C H/W
DB 17977, PG 2315
NO ACQUISITIONS

11M210261
4194 AVERY LN
N/F
ASSESSOR IN/FORMATION WITHHELD
DB 17412, PG 580
NO ACQUISITIONS

12M530373
4056 FEE FEE RD
N/F
BRIDGETON CITY OF
DB 8018, PG 724

12M530175
4110 FEE FEE RD
N/F
CARATACHEA OCTAVIO
DB 22252, PG 2918
NO ACQUISITIONS






11M210012
4120 FEE FEE RD
N/F
WILBUR PROPERTIES L L C
DB 23948, PG 1776
NO ACQUISITIONS

MILL EXISTING ASPHALT LIP
FLUSH WITH EXISTING SURFACE
(PAID FOR AS EDGE GRINDING)

270

FEE FEE RD

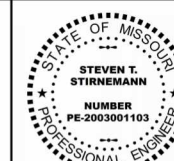
PLAN SHEET LEGEND

	EXISTING RIGHT-OF-WAY		SIDEWALK REMOVAL
	EXISTING EASEMENT		CURB RAMP REPLACEMENT
	PROPOSED RIGHT-OF-WAY (NEW ROW)		SIDEWALK REPLACEMENT
	PERMANENT EASEMENT (PERM. ESM'T.)		
	TEMPORARY CONSTRUCTION EASEMENT (T.C.E.)		

PLAN SHEET
INTERSTATE 7
SHEET 3 OF 8

DISCLAIMER

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THIS SHEET HAS BEEN
SIGNED, SEALED, AND DATED
ELECTRONICALLY

DATE PREPARED _____

ROUTE	STATE
VARIABLES	MO

DISTRICT	SHEET NO.
STL	18

COUNTY
VARIES

JOB NO.
J6P3510B

CONTRACT ID.	
--------------	--

PROJECT NO. _____

BRIDGE NO. _____

DESCRIPTION

DATE _____

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



FDSI

16141 Swingle Ridge Rd., Suite 300
Chesterfield, Missouri 63017
Missouri State Engineering Corporation #001523
T. 636 537 5585
F. 636 537 0275

[illegible]

REV.

DISCLAIMER
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DATE PREPARED
3/28/2024

ROUTE STATE
VARIES MO
DISTRICT SHEET NO.
STL 23

COUNTY
VARIES
JOB NO.
J6P3510B
CONTRACT ID.

PROJECT NO.

BRIDGE NO.

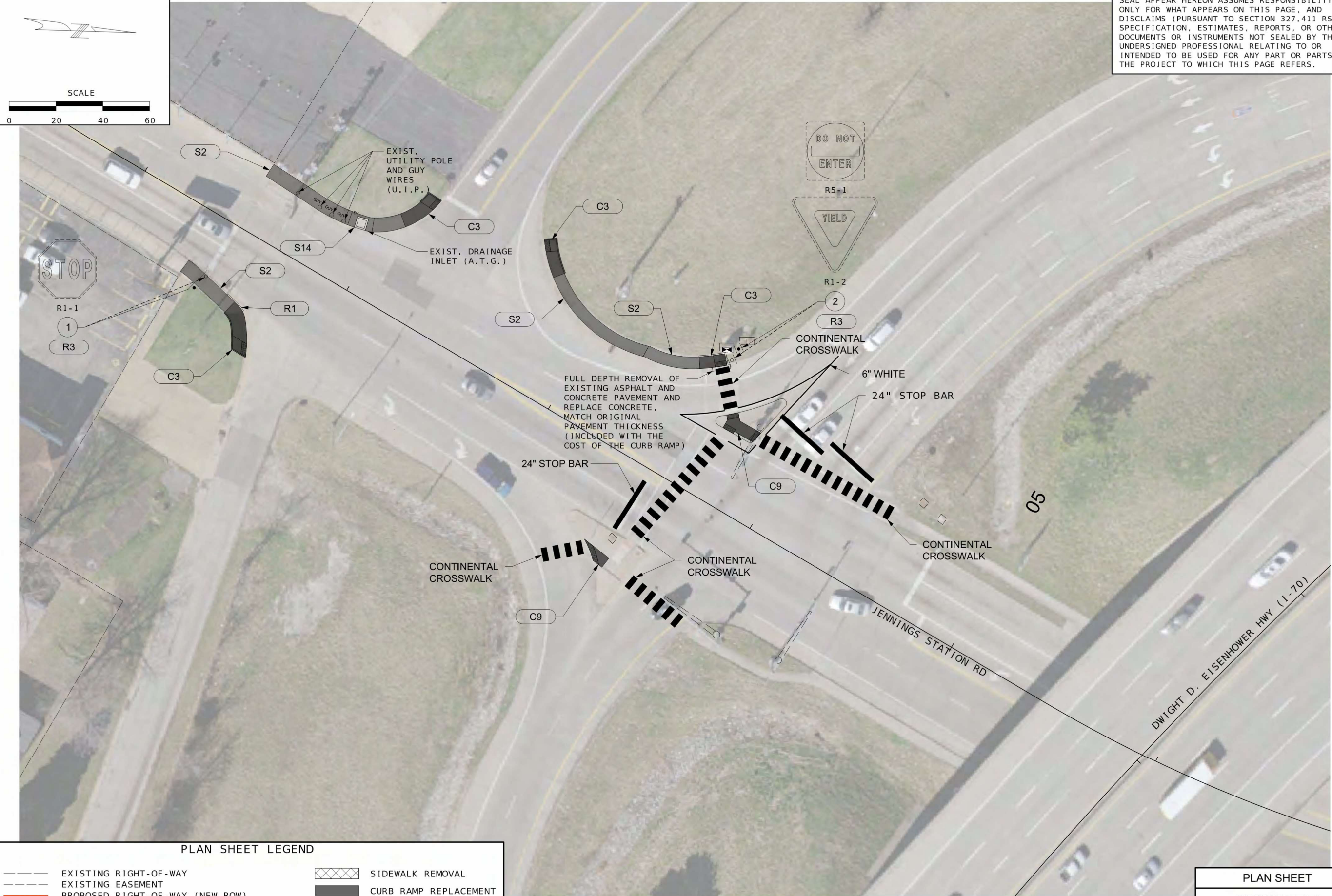
DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

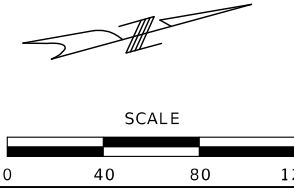
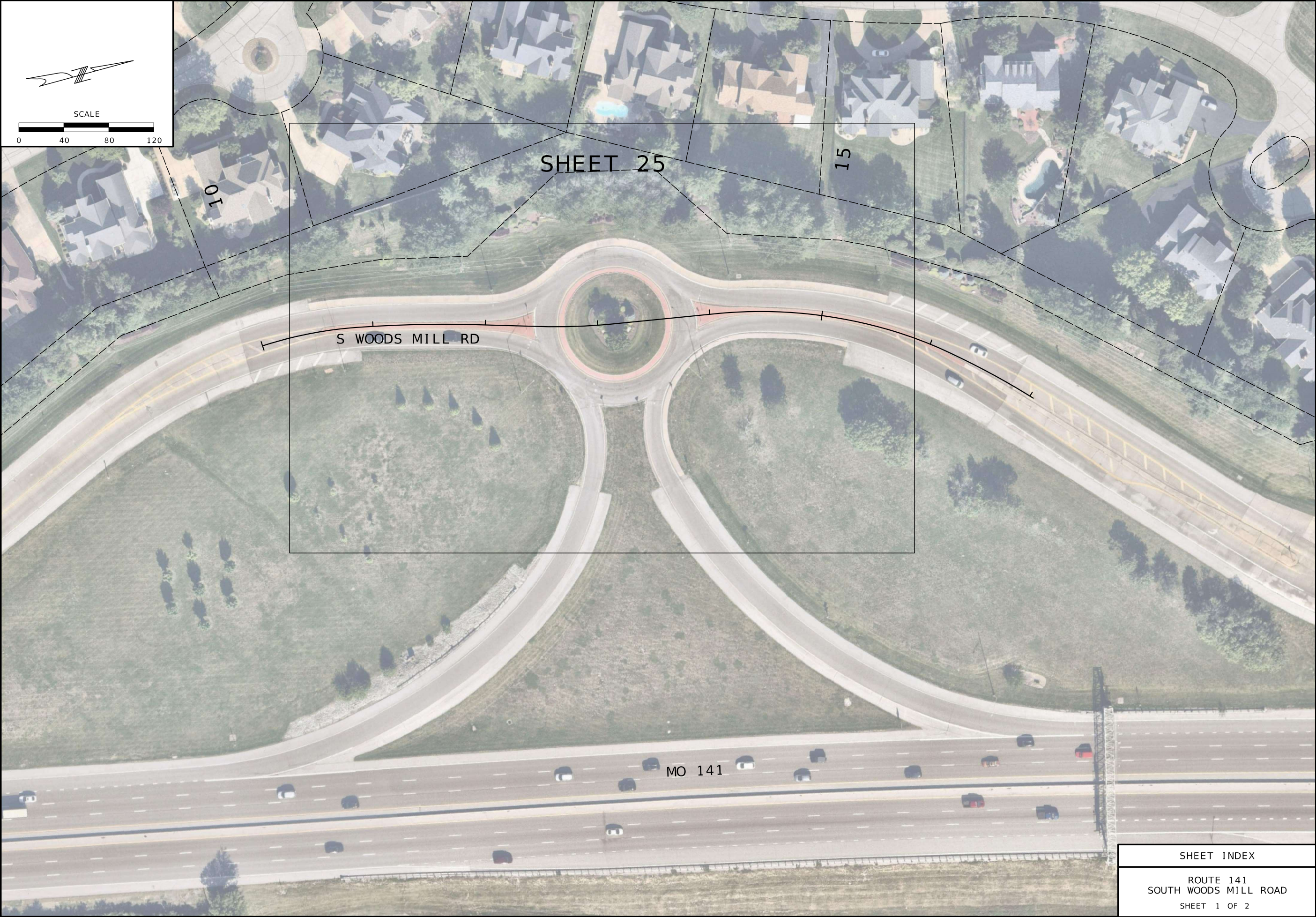
105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

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ENGINEERING DESIGN SOURCE, INC.
16141 Swingley Ridge Rd., Suite 300
Chesterfield, Missouri 63017
T. 636 537 5585
F. 636 537 0215
Missouri State Engineering Corporation #001523

PLAN SHEET
INTERSTATE 70
SHEET 8 OF 8



PLAN SHEET LEGEND			
---	EXISTING RIGHT-OF-WAY	XXXX	SIDEWALK REMOVAL
---	EXISTING EASEMENT	███	CURB RAMP REPLACEMENT
---	PROPOSED RIGHT-OF-WAY (NEW ROW)	███	SIDEWALK REPLACEMENT
---	PERMANENT EASEMENT (PERM. ESM'T.)		
---	TEMPORARY CONSTRUCTION EASEMENT (T.C.E.)		



SHEET 25

S WOODS MILL RD

MO 141



03/27/2024 1:37:28 PM
John Keeven
MO PE-2010000836

DATE PREPARED
3/27/2024

ROUTE	STATE
VARIES	MO
DISTRICT	SHEET NO.
STL	24

COUNTY
VARIES

JOB NO.
J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DATE	DESCRIPTION

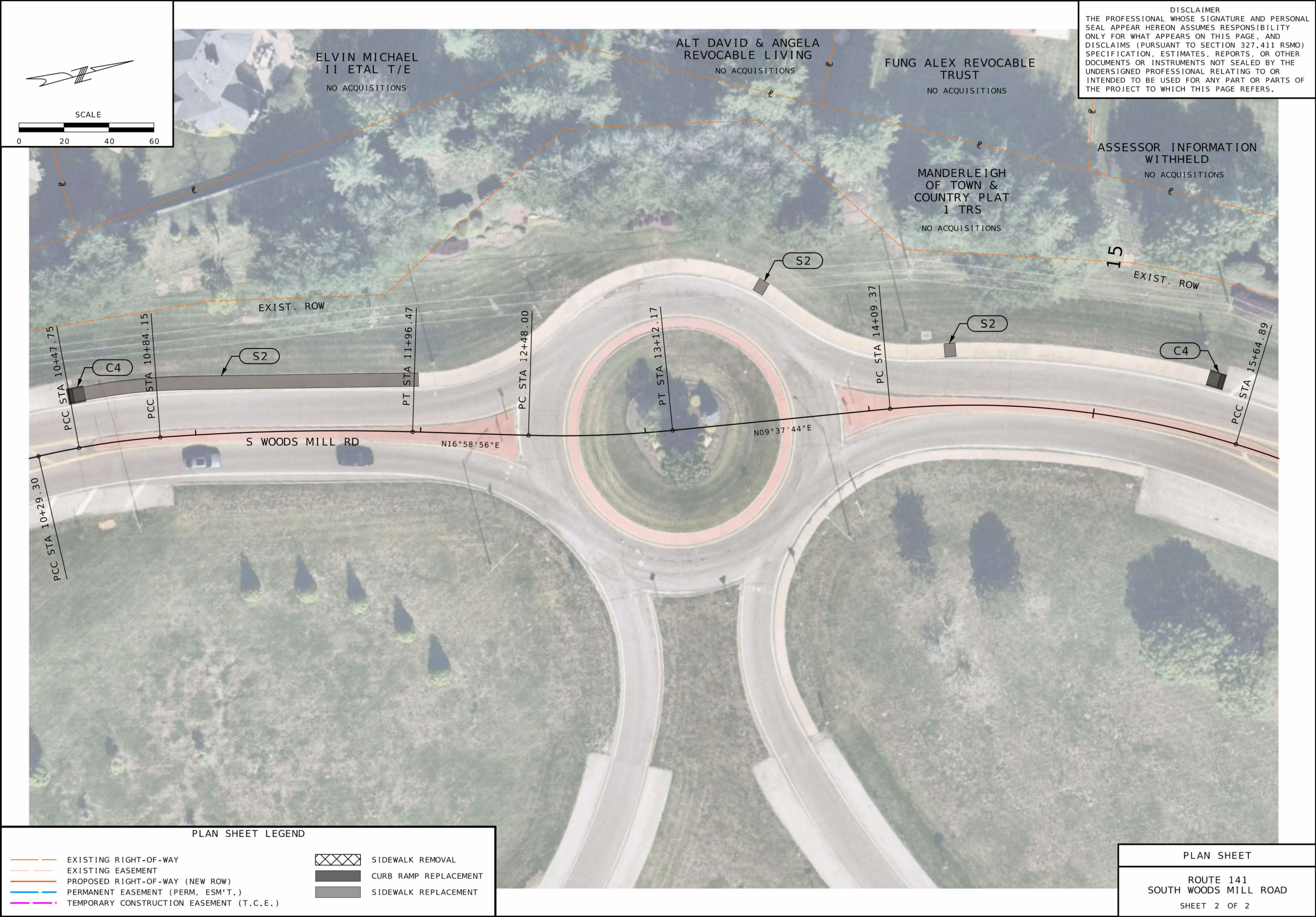
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SHEET INDEX
ROUTE 141 SOUTH WOODS MILL ROAD SHEET 1 OF 2



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STATE OF MISSOURI
JOHN R. KEEVEN
NUMBER PE-2010000836
PROFESSIONAL ENGINEER
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MO PE-2010000836

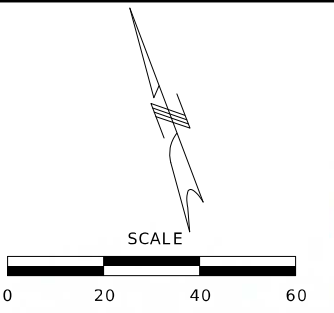
DATE PREPARED
3/27/2024
ROUTE
VARIES
STATE
MO
DISTRICT
STL
SHEET NO.
25
COUNTY
VARIES
JOB NO.
J6P3510B
CONTRACT ID.
PROJECT NO.
BRIDGE NO.

DATE	DESCRIPTION
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PLAN SHEET
ROUTE 141
SOUTH WOODS MILL ROAD
SHEET 2 OF 2



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STATE OF MISSOURI

STEVEN T. STIRNEMANN

NUMBER PE-2003001103

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VARIES	MO
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STL	26
COUNTY	
VARIES	
JOB NO.	
J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DATE	DESCRIPTION

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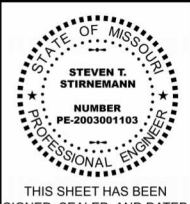
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SHEET INDEX
I-44 AT I-270
SHEET 1 OF 2



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ROUTE	STATE	DISTRICT	SHEET NO.
VARIES	MO	STL	27
COUNTY			
VARIES			
JOB NO.			
J6P3510B			
CONTRACT ID.			
PROJECT NO.			
BRIDGE NO.			

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PLAN SHEET LEGEND			
---	EXISTING RIGHT-OF-WAY		SIDEWALK REMOVAL
---	EXISTING EASEMENT		CURB RAMP REPLACEMENT
---	PROPOSED RIGHT-OF-WAY (NEW ROW)		SIDEWALK REPLACEMENT
---	PERMANENT EASEMENT (PERM. ESM*T.)		
---	TEMPORARY CONSTRUCTION EASEMENT (T.C.E.)		

PLAN SHEET
I-44 AT I-270
SHEET 2 OF 2

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STATE OF MISSOURI

STEVEN T. STIRNEMANN

NUMBER PE-2003001103

PROFESSIONAL ENGINEER

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DATE PREPARED 3/28/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 28
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

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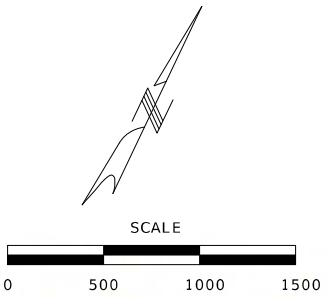
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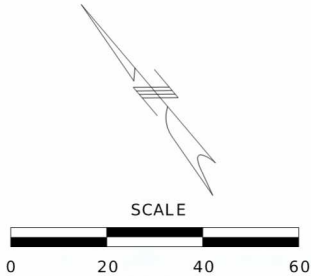
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SHEET INDEX
INTERSTATE 44 SHEET 1 OF 3





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ROUTE STATE
VARIES MO
DISTRICT SHEET NO.
STL 30

COUNTY
VARIES
JOB NO.
J6P3510B
CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DATE	DESCRIPTION

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PLAN SHEET
INTERSTATE 44
SHEET 3 OF 3



PLAN SHEET LEGEND

---	EXISTING RIGHT-OF-WAY	[Hatched Box]	SIDEWALK REMOVAL
- - -	EXISTING EASEMENT	[Solid Black Box]	CURB RAMP REPLACEMENT & CUT THROUGHS
---	PROPOSED RIGHT-OF-WAY (NEW ROW)	[Solid Grey Box]	SIDEWALK REPLACEMENT
---	PERMANENT EASEMENT (PERM. ESM'T.)		
---	TEMPORARY CONSTRUCTION EASEMENT (T.C.E.)		



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John R. Keeven

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ROUTE STATE

VARIES MO

DISTRICT SHEET NO.

STL 31

COUNTY

VARIES

JOB NO.

J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

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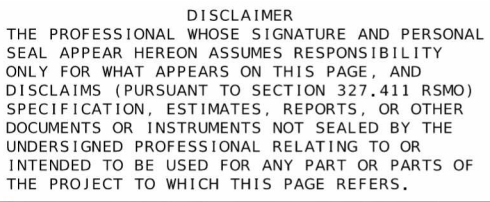
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SHEET INDEX

ROUTE 94
SOUTH OLD HIGHWAY 94

SHEET 1 OF 2

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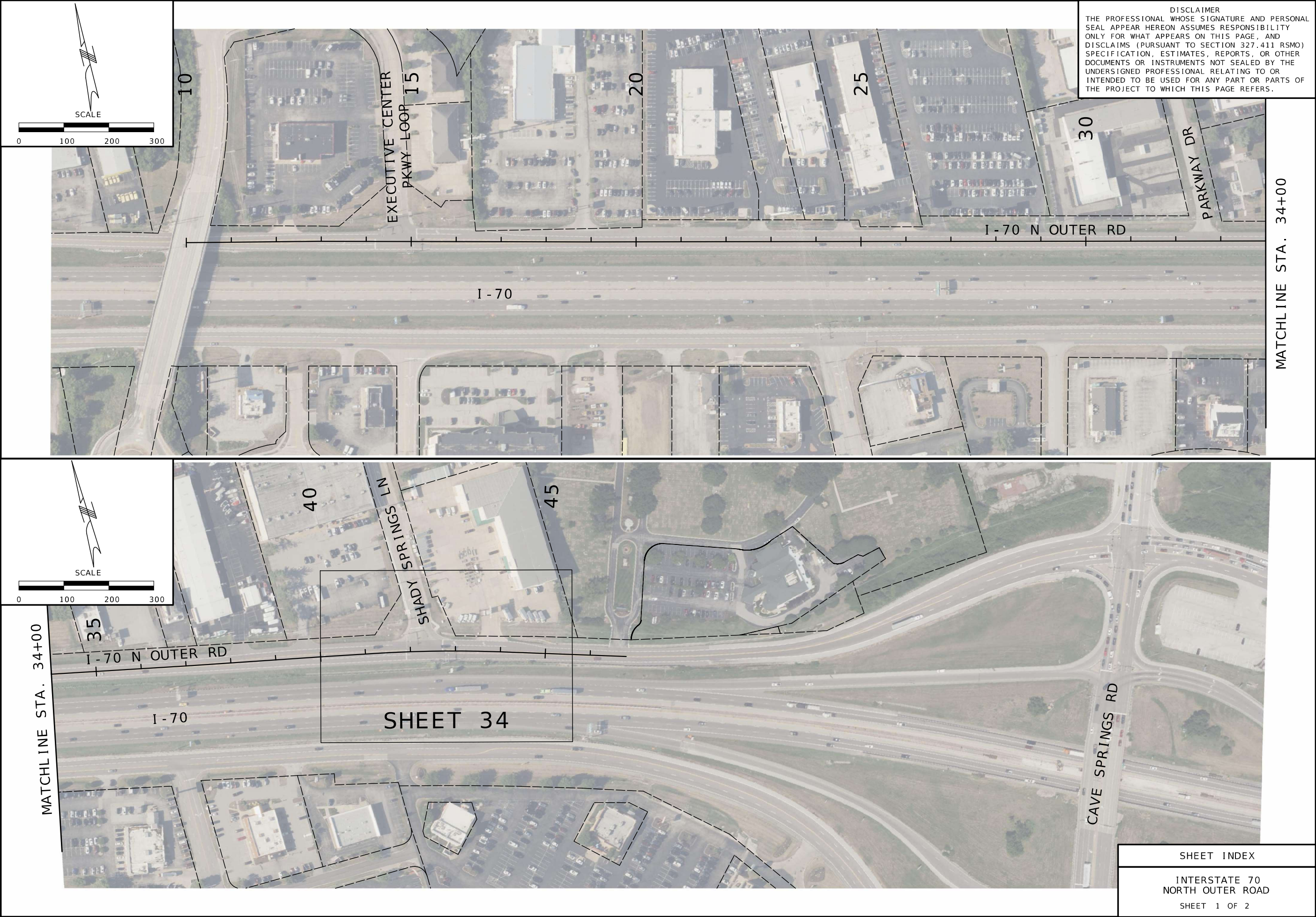
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PLAN SHEET LEGEND

	EXISTING RIGHT-OF-WAY		SIDEWALK REMOVAL
	EXISTING EASEMENT		CURB RAMP REPLACEMENT
	PROPOSED RIGHT-OF-WAY (NEW ROW)		SIDEWALK REPLACEMENT
	PERMANENT EASEMENT (PERM. ESM'T.)		
	TEMPORARY CONSTRUCTION EASEMENT (T.C.E.)		

PLAN SHEET
ROUTE 94 SOUTH OLD HIGHWAY 94 SHEET 2 OF 2



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DATE PREPARED 3/27/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 33
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

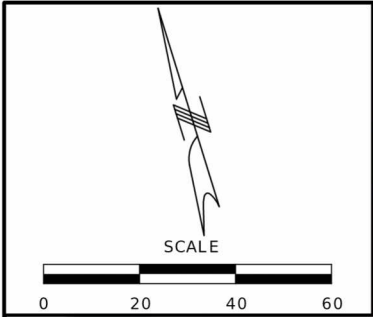


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SHEET INDEX
INTERSTATE 70 NORTH OUTER ROAD SHEET 1 OF 2



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ROUTE STATE
VARIES MO

DISTRICT SHEET NO.
STL 34

COUNTY
VARIES

JOB NO.
J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DATE	DESCRIPTION

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PLAN SHEET LEGEND			
	EXISTING RIGHT-OF-WAY		SIDEWALK REMOVAL
	EXISTING EASEMENT		CURB RAMP REPLACEMENT
	PROPOSED RIGHT-OF-WAY (NEW ROW)		SIDEWALK REPLACEMENT
	PERMANENT EASEMENT (PERM. ESM'T.)		
	TEMPORARY CONSTRUCTION EASEMENT (T.C.E.)		

PLAN SHEET
INTERSTATE 70
NORTH OUTER ROAD
SHEET 2 OF 2

TRAFFIC CONTROL NOTES:

1.

TRAFFIC CONTROL SHALL BE MAINTAINED IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
2.

CONTRACTOR TO USE FLAGGING OPERATIONS AS REQUIRED FOR ANY PART OF THE TRAFFIC CONTROL SET UP OF BOTH STAGE 1 AND STAGE 2, AS WELL AS ANY OTHER INSTALLATION OF IMPROVEMENTS AS NECESSARY.
3.

ANY EXISTING SIGNS WHICH CONFLICT WITH THE TRAFFIC CONTROL PLAN SHALL BE COVERED OR REMOVED.
4.

ALL SPACING AND DISTANCES ARE APPROXIMATE AND MAY BE ADJUSTED BY THE ENGINEER TO FIT FIELD CONDITIONS.
5.

SIGNING SHOWN SHALL REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE.
6.

REFER TO MODOT STANDARD DRAWING 616.10, 617.20, 619.10, AND 620.10 FOR ADDITIONAL DETAILS AND 903.03 FOR SIGN AND SIGN MOUNTING REQUIREMENTS.
7.

CONTRACTOR TO MAINTAIN PUBLIC ACCESS TO ALL BUSINESSES AND STREETS. NO STREET OR ENTRANCE SHALL BE COMPLETELY BLOCKED.

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STATE OF MISSOURI

STEVEN T. STIRNEMANN

NUMBER PE-2003001103

PROFESSIONAL ENGINEER

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ROUTE
VARIES

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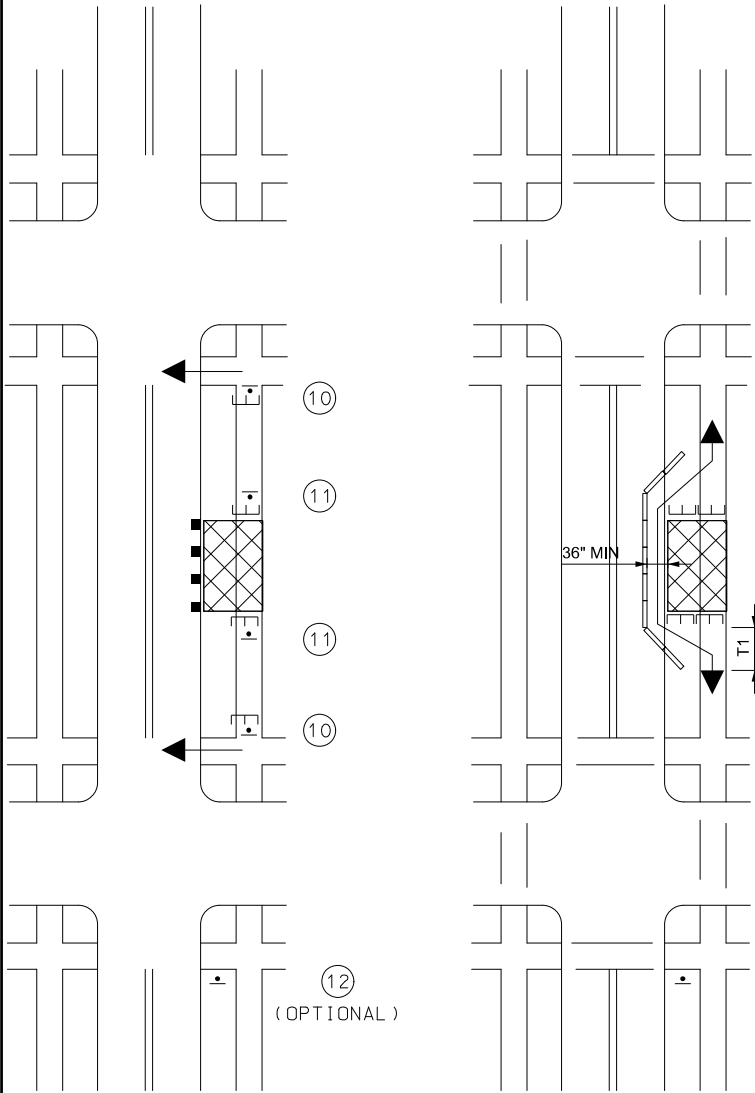
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JOB NO.
J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.



TRAFFIC CONTROL LEGEND

- SIGN (SINGLE SIDED)
- CHANNELIZER (TRIM-LINE)
- ▨

WORK AREA
- ⌌

TYPE 3 BARRICADE
- ▨

LONGITUDINAL CHANNELIZING DEVICE
- ▨

FLASHING ARROW PANEL

616.8.28 SIDEWALK DETOUR OR DIVERSION - MT							
SPEED	SIGN SPACING (FT.)		TAPER LENGTH (FT.)		OPTIONAL	CHANNELIZER SPACING (FT.)	
NORMAL POSTED (MPH)	UNDIVIDED (S)	DIVIDED (S)	SHOULDER ¹ (T1)	LANE ² (T2)	BUFFER LENGTH (FT.) (B)	TAPERS	BUFFER/WORK AREAS
0-35	200	200	70	-	250	15	25
40-45	350	500	150	-	360	20	50
50-55	500	1000	185	-	495	50	100
60-70	SA - 1000, SB - 1500, AND SC - 2640		235	-	730	60	100
1. SHOULDER TAPER LENGTH BASED ON 10FT (STANDARD SHOULDER WIDTH) OFFSET. 2. LANE TAPER BASED ON 12 FT *STANDARD LANE WIDTH) OFFSET.							

NOTES:

WHEN CROSSWALKS OR OTHER PEDESTRIAN FACILITIES ARE CLOSED OR RELOCATED, TEMPORARY FACILITIES SHALL BE DETECTABLE AND SHALL INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH FEATURES PRESENT IN THE EXISTING PEDESTRIAN FACILITY.

WHERE SIDEWALKS EXIST, PROVISIONS SHOULD BE MADE FOR DISABLED PERSONS.

WHERE HIGH SPEEDS ARE ANTICIPATED, A TEMPORARY TRAFFIC BARRIER AND, IF NECESSARY, A CRASH CUSHION SHOULD BE USED TO SEPARATE THE TEMPORARY SIDEWALKS FROM TRAFFIC.

ONLY THE TEMPORARY TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL TRAFFIC.

SIGNS MAY BE MOUNTED ON PORTABLE MOUNTS AT 1FT PROVIDED THEY DO NOT INTERFERE WITH PEDESTRIAN MOVEMENT OR BE OBSTRUCTED BY PARKING. OTHERWISE, SIGNS SHALL BE MOUNTED AT 7FT

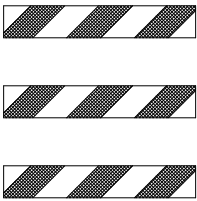
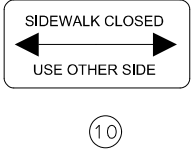
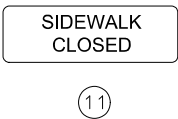
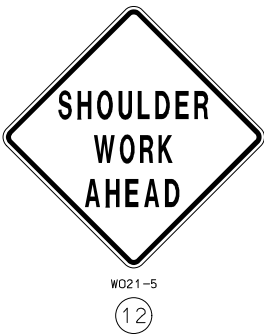
FOR HIGH SPEED FACILITIES, CHANNELIZER SPACING MAY BE REDUCED TO 1/2 SPACING NOTED IN TABLE.

OTHER APPROPRIATE SIGNS MAY BE USED IN LIEU OF SIGNS 12 OR 13.

AUDIBLE INFORMATION DEVICES SHOULD BE CONSIDERED WHERE MIDBLOCK CLOSINGS AND CHANGED SIDEWALK AREAS CAUSE INADEQUATE COMMUNICATION TO BE PROVIDED TO PEDESTIANS WHO HAVE VISUAL DISABILITIES.

FOR LONG-TERM OPERATIONS, REFER TO EPG 616.6.2.2 FLAGS AND ADVANCE WARNING RAIL SYSTEM.

NOT TO SCALE



MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

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616.8.22SD SHORT DURATION RIGHT LANE CLOSURE AT AN INTERSECTION							
SPEED	SIGN SPACING (FT.)		TAPER LENGTH (FT.)		RECOMMENDED	CHANNELIZER SPACING (FT.)	
NORMAL POSTED (MPH)	UNDIVIDED (S)	DIVIDED (S)	SHOULDER ¹ (T1)	LANE ² (T2)	BUFFER LENGTH (FT.) (B)	TAPERS	BUFFER/WORK AREAS (FT.)
0-35	200	200	-	-	250	-	40
40-45	350	500	-	-	360	-	80
50-55	500	1000	-	-	495	-	80
60-70	1000	SA - 1000	-	-	730	-	120
1. SHOULDER TAPER LENGTH BASED ON 10FT (STANDARD SHOULDER WIDTH) OFFSET. 2. LANE TAPER BASED ON 12FT *(STANDARD LANE WIDTH) OFFSET.							

TYPE OF ROADWAY	SIGN HEIGHT (MINIMUM)	MAXIMUM WORK ZONE LENGTH (L)
URBAN	1' PORTABLE 7' POST	1 MI
RURAL DIVIDED	1' PORTABLE 7' POST	2 MI
RURAL UNDIVIDED	1' PORTABLE 5' POST	3 MI
VEHICLE	48" RECOMMENDED	-

TRAFFIC CONTROL LEGEND

-  SIGN (SINGLE SIDED)
-  CHANNELIZER (TRIM-LINE)
-  WORK AREA
-  TYPE 3 BARRICADE
-  LONGITUDINAL CHANNELIZING DEVICE
-  FLASHING ARROW PANEL

NOTES:

SHORT DURATION: WORKERS ON FOOT FOR 60 MINUTES OR LESS IN ONE LOCATION.

LOCATION: MAXIMUM LENGTH OF WORK ZONE (L) FOR THE ROAD YOU ARE WORKING ON.

ADVANCED WARNING SIGN IN OPTIONAL, USE EPG 616.33SD FOR SETTING ADVACNED WARNING SIGNS.

ALWAYS USE A PROTECTIVE VEHICLE WITH A FLASHING ARROW PANEL (FAP).

1. ACTIVATE HIGH INTENSITY ROTATING, FLASHNING, OSCILLATING, OR STROBE LIGHTS.
2. POSITION THE PROTECTIVE VEHICLE 150 FT IN ADVANCED OF THE WORK SPACE, IF POSSIBLE.
3. OPERATE THE FAP WITH THE APPROPRIATE ARROW MODE.

FOR ROADWAYS AT 45 MPH OR LESS, A WORK VEHICLE MAY BE USED AS THE PROTECTIVE VEHICLE.

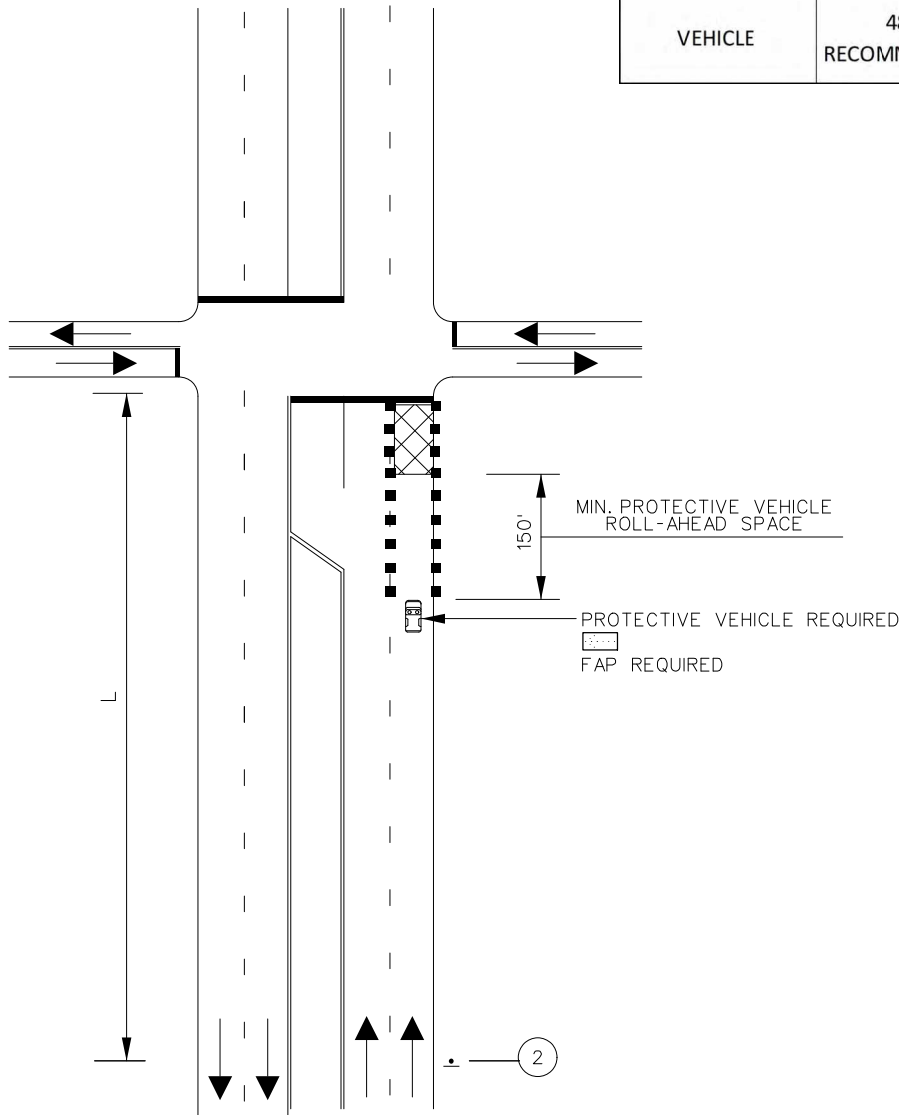
1. ACTIVATE HIGH INTENSITY ROTATING, FLASHNING, OSCILLATING, OR STROBE LIGHTS.
2. POSITION THE WORK VEHICLE A MINIMUM OF 150 FT IN ADVANCED OF THE WORK SPACE, IF POSSIBLE.
3. OPERATE THE VEHICLE WITH A FAP WITH THE APPROPRIATE ARROW MODE.
4. ONCE YOU EXIST THE WORK VEHICLE, RETURN ONLY WHEN ADJUSTING POSITION OR READY TO LEAVE THE WORK LOCATION.

CONSIDER USING CHANNELIZERS TO DETER MOTORISTS FROM ENTERING THE WORK SPACE.

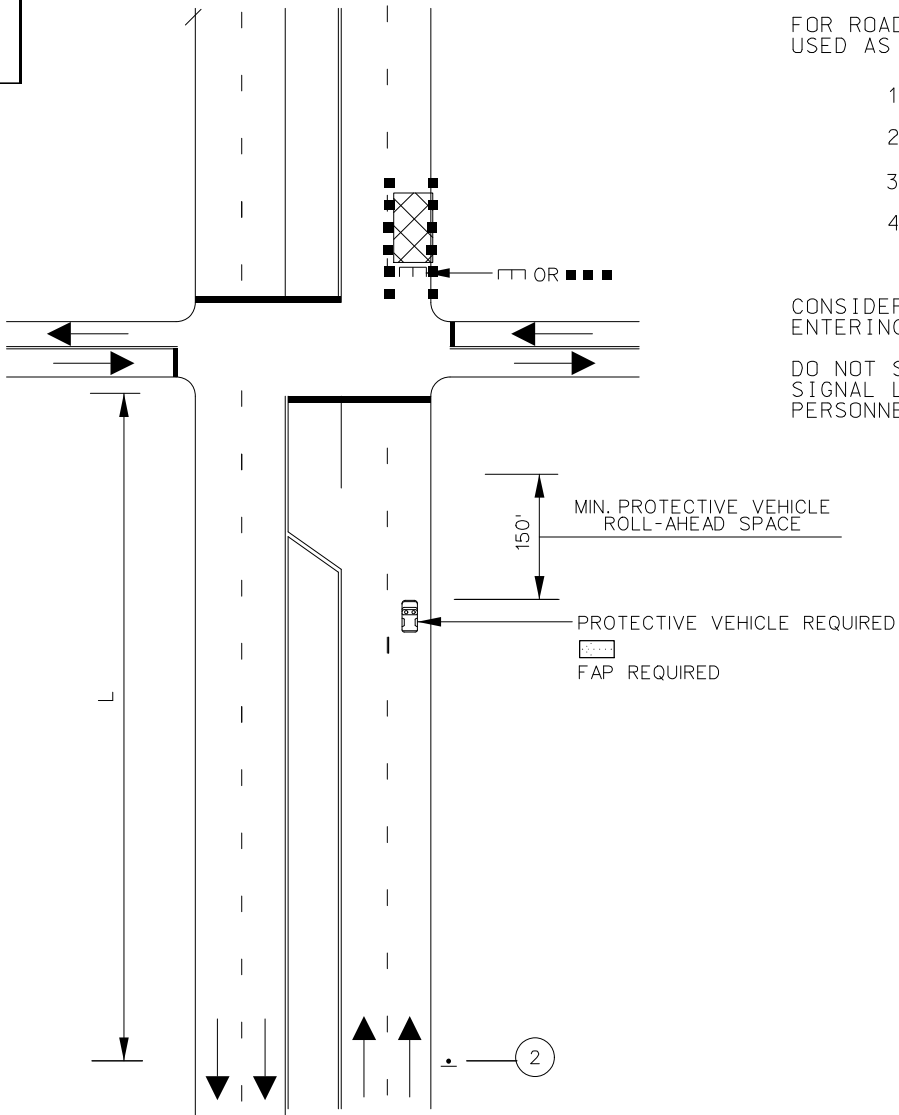
DO NOT SET UP A WORK ZONE IF IT CONFLICTS WITH THE SIGNAL LIGHTS, CONTACT THE APPROPRIATE DISTRICT PERSONNEL.



NEAR SIDE OF INTERSECTION



NEAR SIDE OF INTERSECTION



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DATE PREPARED 3/28/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 36
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION



105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

EDSI
ENGINEERING DESIGN SOURCE, INC.
16141 Swingley Ridge Rd., Suite 300
Chesterfield, Missouri 63047
Missouri State Engineering Corporation #001523
T. 636 537 5585
F. 636 537 0215

TRAFFIC CONTROL
Short Duration RT LN Closure SHEET 2 OF 7

616.8.22S STATIONARY RIGHT LANE CLOSURE AT AN INTERSECTION							
SPEED	SIGN SPACING (FT.)		TAPER LENGTH (FT.)		RECOMMENDED	CHANNELIZER SPACING (FT.)	
NORMAL POSTED (MPH)	UNDIVID ED (S)	DIVIDED (S)	SHOULDE R ¹ (T1)	LANE ² (T2)	BUFFER LENGTH (FT.) (B)	TAPERS (FT.)	BUFFER/WORK AREAS (FT.)
0-35	200	200	70	245	250	35	40
40-45	350	500	150	540	360	40	80
50-55	500	1000	185	660	495	50	80
60-70	1000	SA - 1000 SB - 1500 SC - 2640	235	840	730	60	120

1. SHOULDER TAPER LENGTH BASED ON 10FT (STANDARD SHOULDER WIDTH) OFFSET. 2. LANE TAPER BASED ON 12FT *STANDARD LANE WIDTH) OFFSET

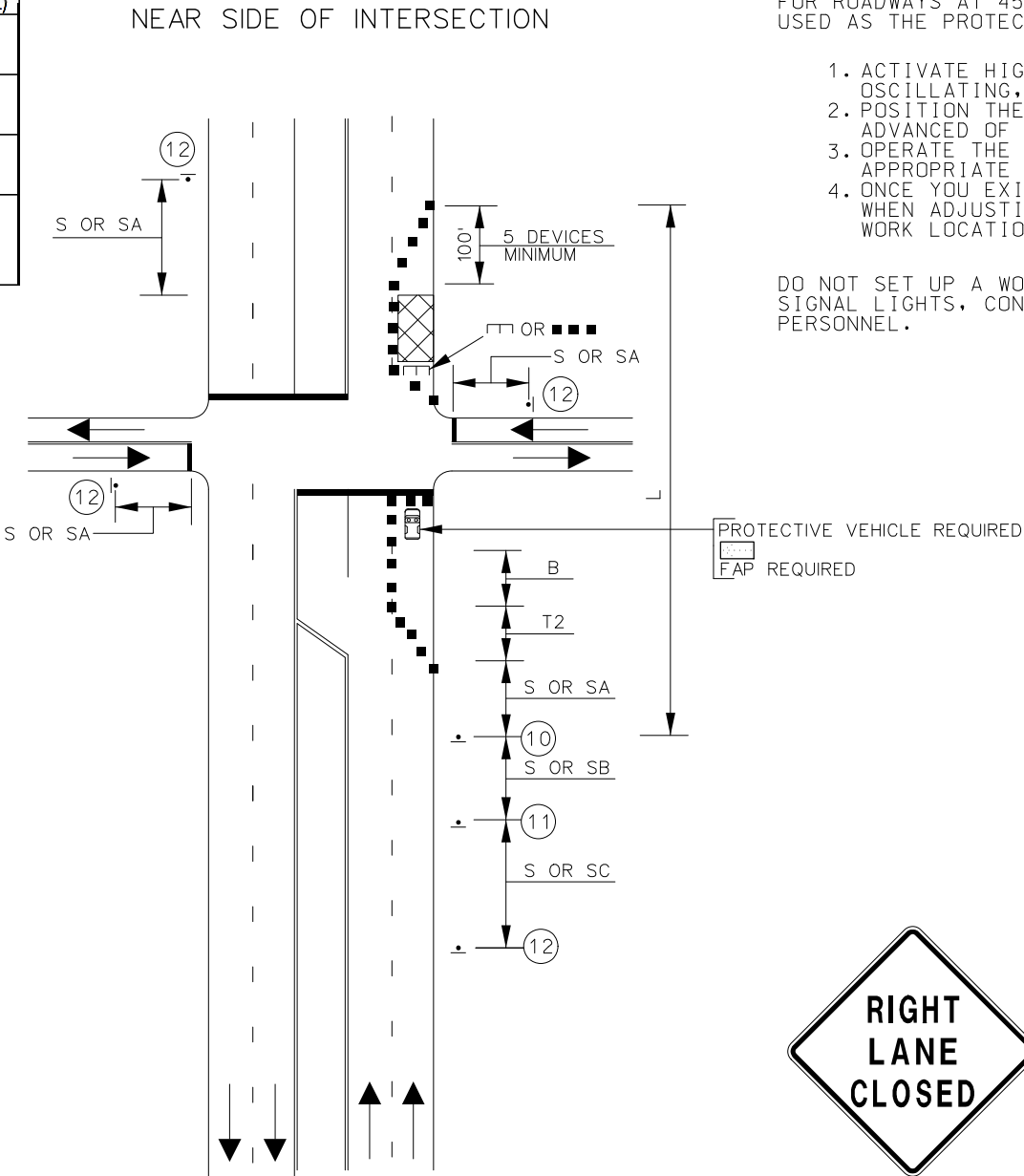
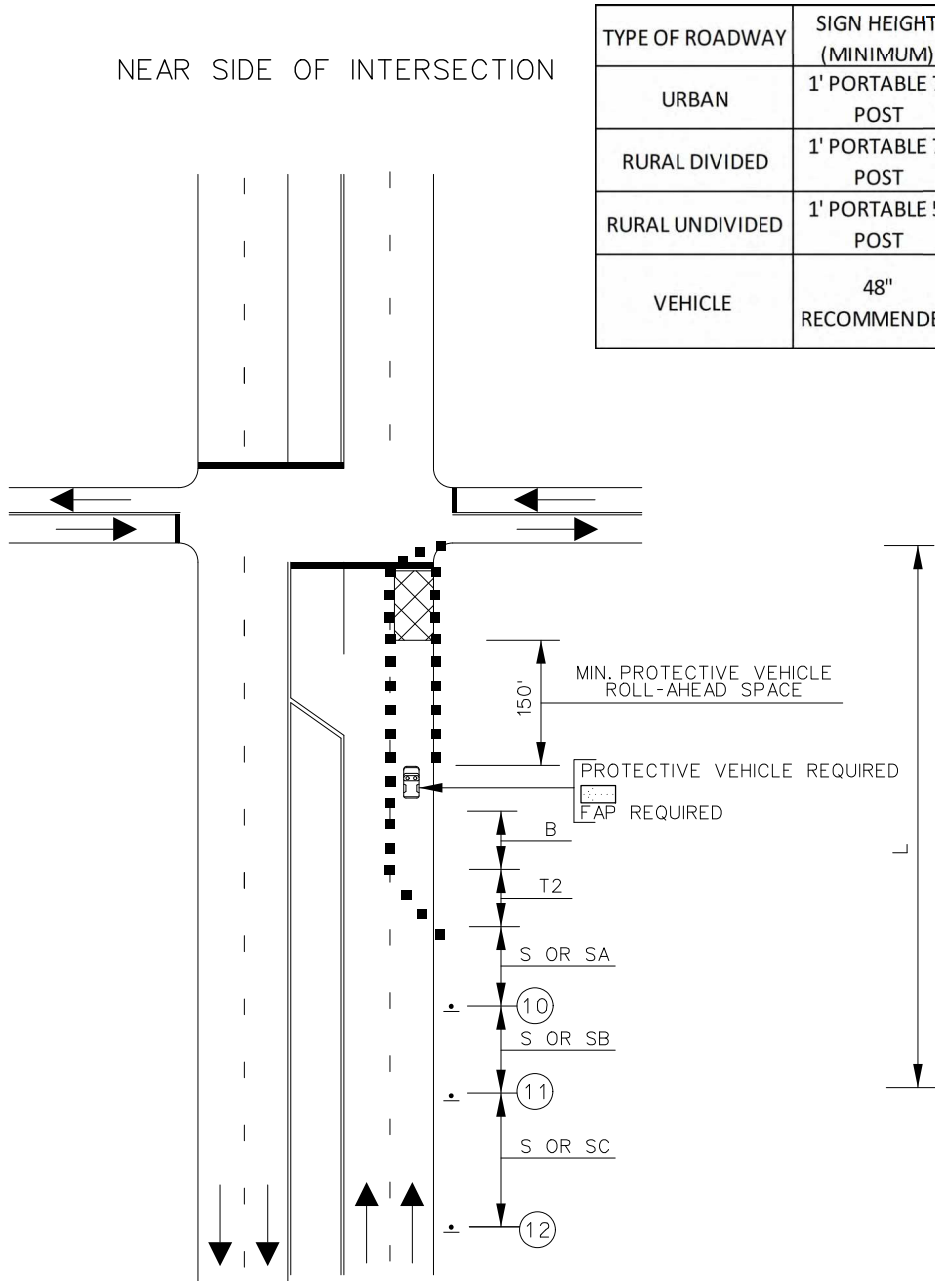
TRAFFIC CONTROL LEGEND	
	SIGN (SINGLE SIDED)
	CHANNELIZER (TRIM-LINE)
	WORK AREA
	TYPE 3 BARRICADE
	LONGITUDINAL CHANNELIZING DEVICE
	FLASHING ARROW PANEL

- NOTES:
- ALWAYS USE ADVANCED WARNING SIGNS AND CHANNELIZERS: USE EPG 616.33SD SHORT DURATION LANE CLOSURE ON A MULTI-LANE HIGHWAY FOR SETTING ADVANCED WARNING SIGNS.
- ALWAYS USE A PROTECTIVE VEHICLE WITH A FLASHING ARROW PANEL (FAP).
1. ACTIVATE HIGH INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.
 2. POSITION THE PROTECTIVE VEHICLE A MINIMUM OF 150 FT IN ADVANCED OF THE WORK SPACE, IF POSSIBLE.
 3. OPERATE THE FAP WITH THE APPROPRIATE ARROW MODE.

FOR ROADWAYS AT 45 MPH OR LESS, A WORK VEHICLE MAY BE USED AS THE PROTECTIVE VEHICLE.

1. ACTIVATE HIGH INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.
2. POSITION THE PROTECTIVE VEHICLE A MINIMUM OF 150 FT IN ADVANCED OF THE WORK SPACE, IF POSSIBLE.
3. OPERATE THE VEHICLE WITH A FAP WITH THE APPROPRIATE ARROW MODE.
4. ONCE YOU EXIST THE WORK VEHICLE, RETURN ONLY WHEN ADJUSTING POSITION OR READY TO LEAVE THE WORK LOCATION.

DO NOT SET UP A WORK ZONE IF IT CONFLICTS WITH THE SIGNAL LIGHTS, CONTACT THE APPROPRIATE DISTRICT PERSONNEL.



DISCLAIMER
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DATE PREPARED
3/28/2024

ROUTE VARIES STATE MO
DISTRICT STL SHEET NO. 37

COUNTY VARIES

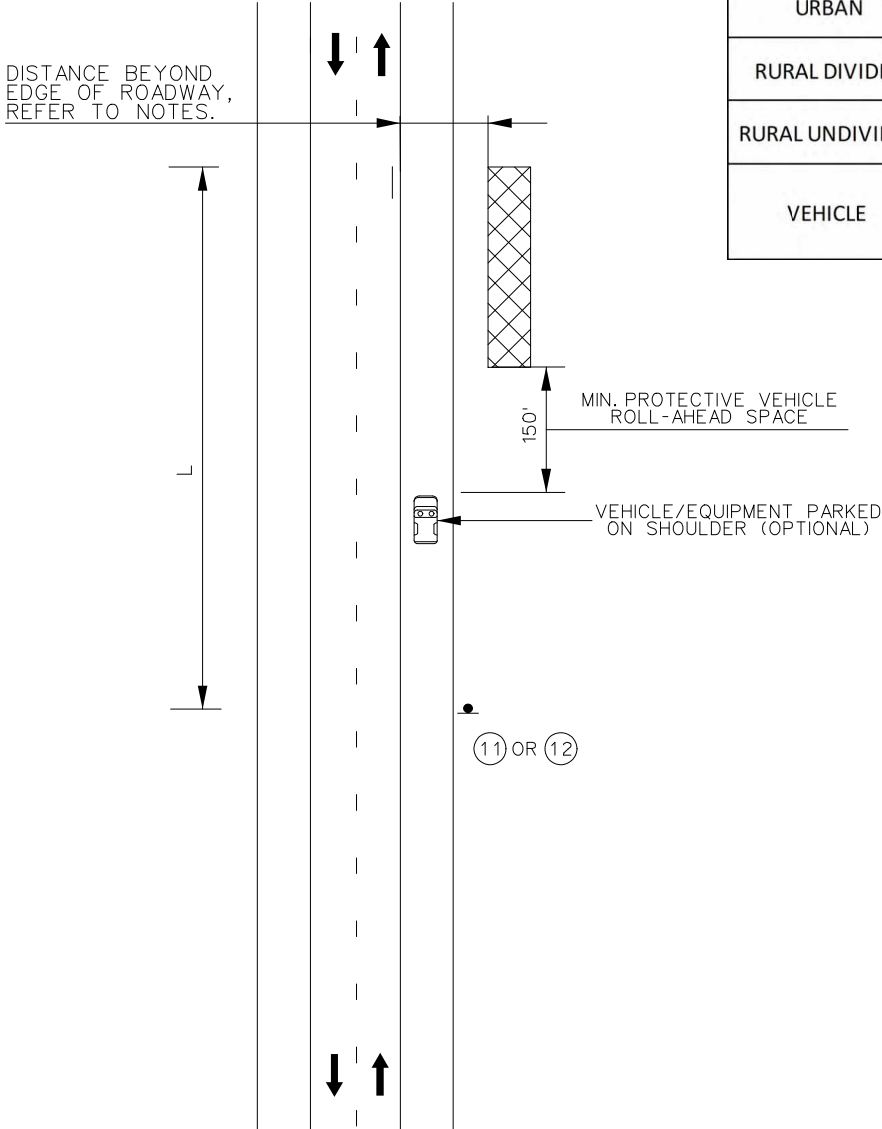
JOB NO. J6P3510B
CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION	DATE

616.8.1SD SHORT DURATION WORK BEYOND THE SHOULDER							
SPEED	SIGN SPACING (FT.)		TAPER LENGTH (FT.)		RECOMMENDED	CHANNELIZER SPACING (FT.)	
NORMAL POSTED (MPH)	UNDIVID ED (S)	DIVIDED (S)	SHOULDER ¹ (T1)	LANE ² (T2)	BUFFER LENGTH (FT.) (B)	TAPERS	BUFFER/WORK AREAS
0-35	200	200	-	-	-	-	-
40-45	350	500	-	-	-	-	-
50-55	500	1000	-	-	-	-	-
60-70	1000	1000	-	-	-	-	-
1. SHOULDER TAPER LENGTH BASED ON 10FT (STANDARD SHOULDER WIDTH) OFFSET. 2. LANE TAPER BASED ON 12FT *STANDARD LANE WIDTH) OFFSET.							



TYPE OF ROADWAY	SIGN HEIGHT (MINIMUM)	MAXIMUM WORK ZONE LENGTH (L)
URBAN	1' PORTABLE 7' POST	1 MI
RURAL DIVIDED	1' PORTABLE 7' POST	2 MI
RURAL UNDIVIDED	1' PORTABLE 5' POST	3 MI
VEHICLE	48" RECOMMENDED	-

TRAFFIC CONTROL LEGEND

- SIGN (SINGLE SIDED)
- CHANNELIZER (TRIM-LINE)
- WORK AREA
- TYPE 3 BARRICADE
- LONGITUDINAL CHANNELIZING DEVICE

NOTES:

SHORT DURATION: DEFINED AS WORKERS ON FOOT FOR 60MIN OR LESS IN ONE LOCATION.

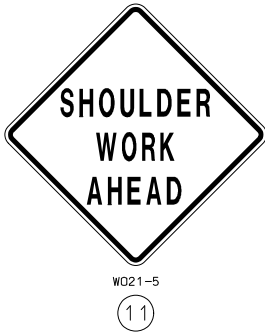
LOCATION: DEFINED AS THE MAXIMUM LENGTH OF WORK ZONE (L) FOR THE ROAD YOU ARE WORKING ON.

ADVANCED WARNING SIGNS MAY BE ELIMINATED IF AT LEAST ONE OF THE FOLLOWING CONDITIONS ARE MET:

1. WORK SPACE IS 15 FT OR MORE FROM THE EDGE OF ANY ROADWAY.
2. WORK SPACE IS BEHIND THE CURB.
3. WORK SPACE IS BEHIND BARRIER OR GUARDRAIL.
4. HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS ARE ACTIVATED.

WHEN VEHICLES OR EQUIPMENT ARE PARKED ON THE SHOULDER:

1. ACTIVATED HIGH-INTENSITY ROTATING, FLASHING, OSCILATING, OR STROBE LIGHTS.
2. POSITION THE PROTECTIVE VEHICLE A MINIMUM OF 150 FT IN ADVANCE OF THE WORK SPACE, IF POSSIBLE.
3. ONCE YOU EXIST THE WORK VEHICLE, RETURN ONLY WHEN ADJUSTING POSITION OR READY TO LEAVE THE WORK LOCATION.



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DATE PREPARED
3/28/2024

ROUTE Varies STATE MO

DISTRICT STL SHEET NO. 38

COUNTY Varies

JOB NO. J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

MoDOT

105 WEST CAPITAL JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

EDSI

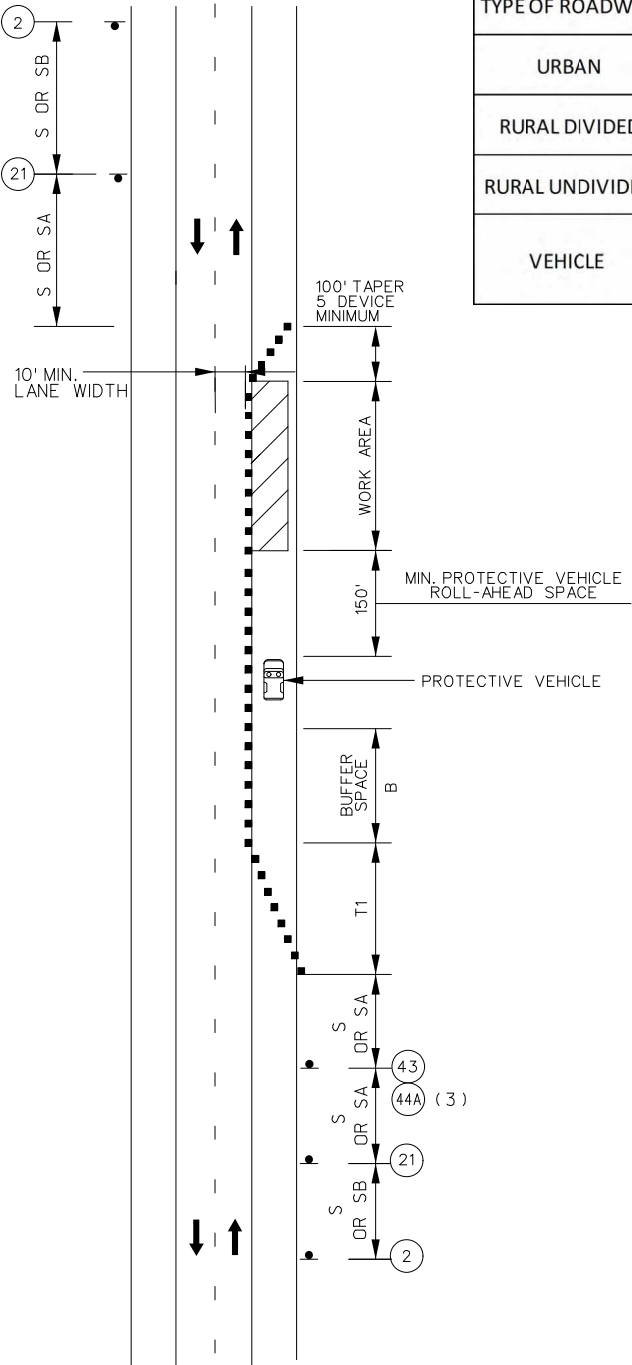
ENGINEERING DESIGN SOURCE, INC.

16141 Swingley Ridge Rd., Suite 300 Chesterfield, Missouri 63017 Missouri State Engineering Corporation #001523

T. 636 537 5585 F. 636 537 0215

616.8.6A SHOULDER WORK WITH MINOR ENCROACHMENT							
SPEED	SIGN SPACING (FT.)		TAPER LENGTH (FT.)		RECOMMENDED	CHANNELIZER SPACING (FT.)	
NORMAL POSTED (MPH)	UNDIVIDED (S)	DIVIDED (S)	SHOULDER ¹ (T1)	LANE ² (T2)	BUFFER LENGTH (FT.) (B)	TAPERS	BUFFER/WORK AREAS
0-35	200	200	70	245	280	35	40
40-45	350	500	150	540	400	40	80
50-55	500	1000	185	660	560	50	80
60-70	1000	SA - 1000 SB - 1500 SC - 2640	235	840	840	60	120

1. SHOULDER TAPER LENGTH BASED ON 10FT (STANDARD SHOULDER WIDTH) OFFSET. 2. LANE TAPER BASED ON 12FT *STANDARD LANE WIDTH) OFFSET.



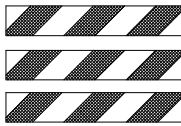
TYPE OF ROADWAY	SIGN HEIGHT (MINIMUM)	MAXIMUM WORK ZONE LENGTH (L)
URBAN	1' PORTABLE 7" POST	1 MI
RURAL DIVIDED	1' PORTABLE 7" POST	2 MI
RURAL UNDIVIDED	1' PORTABLE 5" POST	3 MI
VEHICLE	48" RECOMMENDED	-

TRAFFIC CONTROL LEGEND

- SIGN (SINGLE SIDED)
- CHANNELIZER (TRIM-LINE)
- WORK AREA
- TYPE 3 BARRICADE
- LONGITUDINAL CHANNELIZING DEVICE

NOTES:

- ALWAYS USE ADVANCE WARNING SIGNS AND CHANNELIZERS:
- SET ADDITIONAL WARNING SIGNS AT EACH INTERSECTION WITH ANOTHER STATE HIGHWAY WITHIN THE WORK ZONE
- ALWAYS USE A PROTECTIVE VEHICLE WHEN WORKERS ARE PRESENT.
- ACTIVATE HIGH INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.
 - POSITION THE PROTECTIVE VEHICLE 150 FT IN ADVANCED OF THE WORK SPACE, IF POSSIBLE.
 - NEXT XX MILES SIGN NOT REQUIRED FOR NARROW LANE SECTIONS LESS THAN ONE MILE.
 - A WORKER VEHICLE MAY BE USED AS A PROTECTIVE VEHICLE.
 - ONCE YOU EXIT THE WORK/PROTECTIVE VEHICLE, RETURN ONLY WHEN READY TO LEAVE THE WORK LOCATION.



(3)



THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY

DATE PREPARED

3/28/2024

ROUTE

VARIABLES

DISTRICT

STL

COUNTY

VARIABLES

JOB NO.

J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

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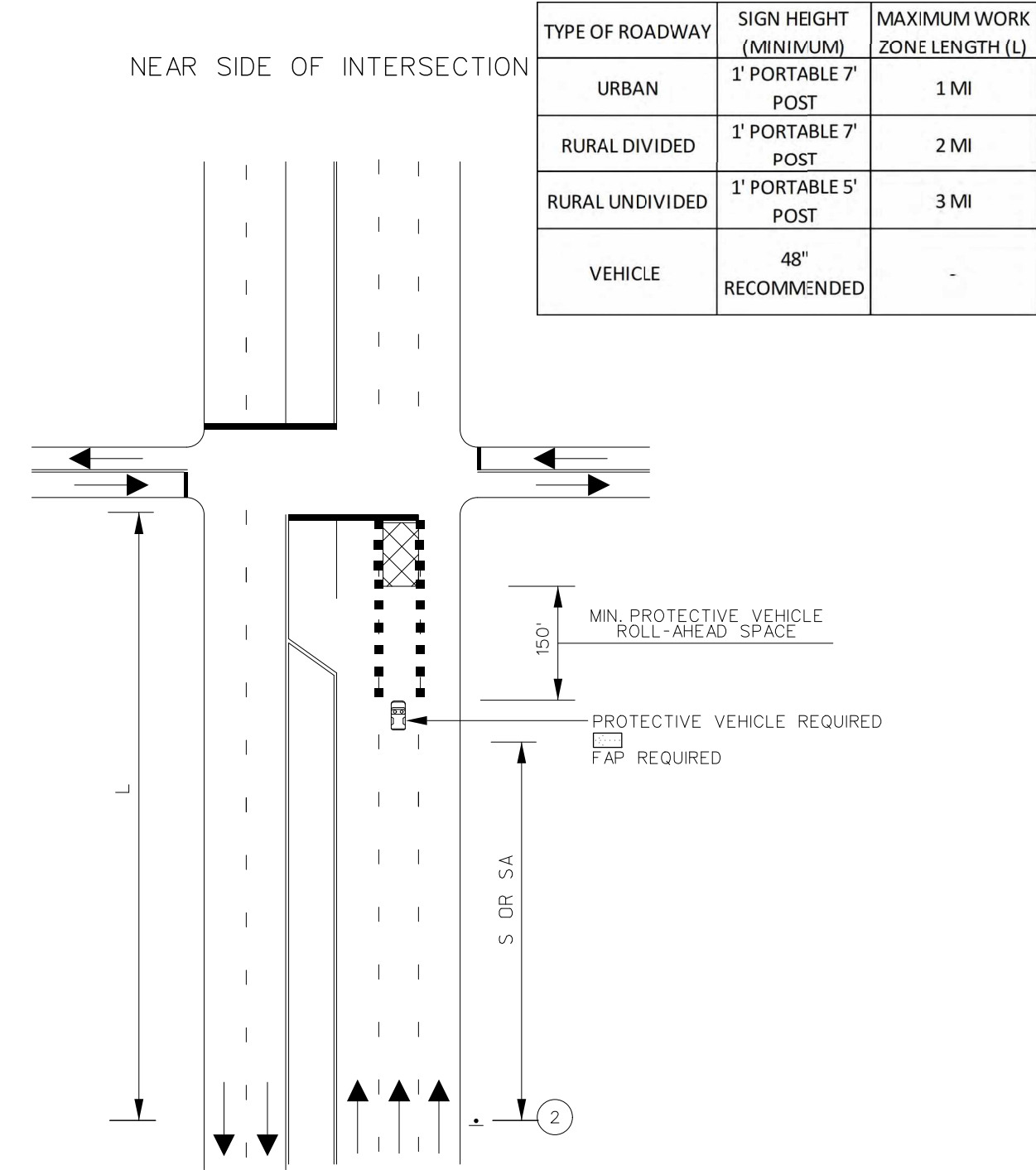
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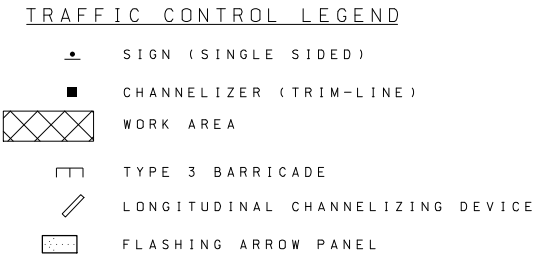
DATE

616.8.21SD SHORT DURATION CENTER LANE CLOSURE AT AN INTERSECTION							
SPEED	SIGN SPACING (FT.)		TAPER LENGTH (FT.)		RECOMMENDED	CHANNELIZER SPACING (FT.)	
NORMAL POSTED (MPH)	UNDIVIDED (S)	DIVIDED (S)	SHOULDER ¹ (T1)	LANE ² (T2)	BUFFER LENGTH (FT.) (B)	TAPERS	BUFFER/WORK AREAS (FT.)
0-35	200	200	-	-	250	-	40
40-45	350	500	-	-	360	-	80
50-55	500	1000	-	-	495	-	80
60-70	1000	SA - 1000	-	-	730	-	120

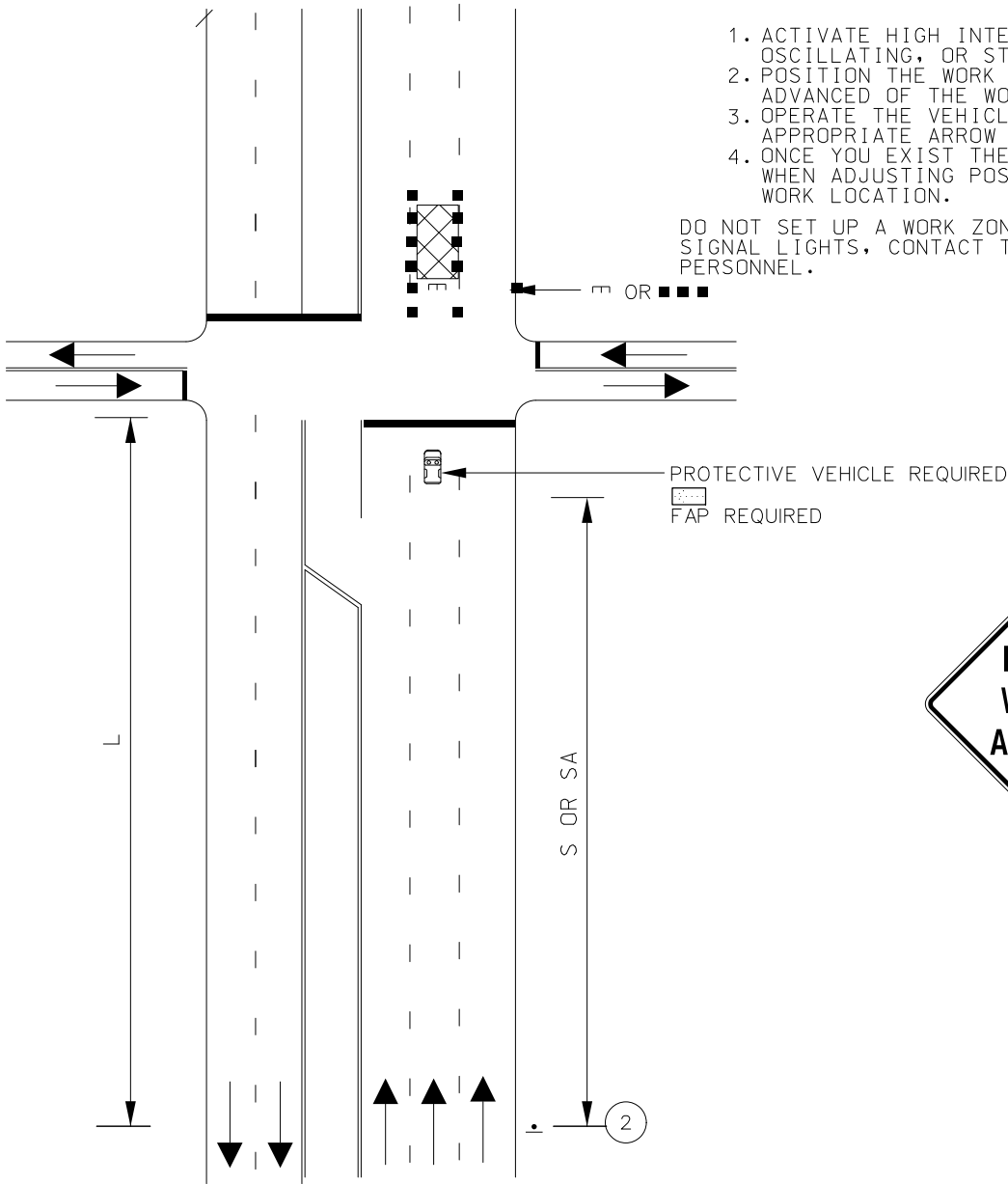
1. SHOULDER TAPER LENGTH BASED ON 10FT (STANDARD SHOULDER WIDTH) OFFSET. 2. LANE TAPER BASED ON 12FT *STANDARD LANE WIDTH) OFFSET.



NOT TO SCALE



FAR SIDE OF INTERSECTION



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THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY

DATE PREPARED 3/28/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 40
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

EDSI
ENGINEERING DESIGN SOURCE, INC.
16141 Swingley Ridge Rd., Suite 300
Chesterfield, Missouri 63017
Missouri State Engineering Corporation #001523
T. 636 537 5585
F. 636 537 0215

616.8.23SD SHORT DURATION LEFT LANE CLOSURE AT AN INTERSECTION							
SPEED	SIGN SPACING (FT.)		TAPER LENGTH (FT.)		RECOMMENDED	CHANNELIZER SPACING (FT.)	
NORMAL POSTED (MPH)	UNDIVIDED (S)	DIVIDED (S)	SHOULDER ¹ (T1)	LANE ² (T2)	BUFFER LENGTH (FT.) (B)	TAPERS	BUFFER/WORK AREAS (FT.)
0-35	200	200	-	-	250	-	40
40-45	350	500	-	-	360	-	80
50-55	500	1000	-	-	495	-	80
60-70	1000	SA - 1000	-	-	730	-	120

1. SHOULDER TAPER LENGTH BASED ON 10FT (STANDARD SHOULDER WIDTH) OFFSET. 2. LANE TAPER BASED ON 12FT *(STANDARD LANE WIDTH) OFFSET.

TYPE OF ROADWAY	SIGN HEIGHT (MINIMUM)	MAXIMUM WORK ZONE LENGTH (L)
URBAN	1' PORTABLE 7' POST	1 MI
RURAL DIVIDED	1' PORTABLE 7' POST	2 MI
RURAL UNDIVIDED	1' PORTABLE 5' POST	3 MI
VEHICLE	48" RECOMMENDED	-

TRAFFIC CONTROL LEGEND

SIGN (SINGLE SIDED)

CHANNELIZER (TRIM-LINE)

WORK AREA

TYPE 3 BARRICADE

LONGITUDINAL CHANNELIZING DEVICE

FLASHING ARROW PANEL

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THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY

DATE PREPARED 3/28/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 41
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

NOTES:

SHORT DURATION: WORKERS ON FOOT FOR 60 MINUTES OR LESS IN ONE LOCATION.

LOCATION: MAXIMUM LENGTH OF WORK ZONE (L) FOR THE ROAD YOU ARE WORKING ON.

ADVANCED WARNING SIGN IN OPTIONAL, USE EPG 616.33SD FOR SETTING ADVACNED WARNING SIGNS.

ALWAYS USE A PROTECTIVE VEHICLE WITH A FLASHING ARROW PANEL (FAP).

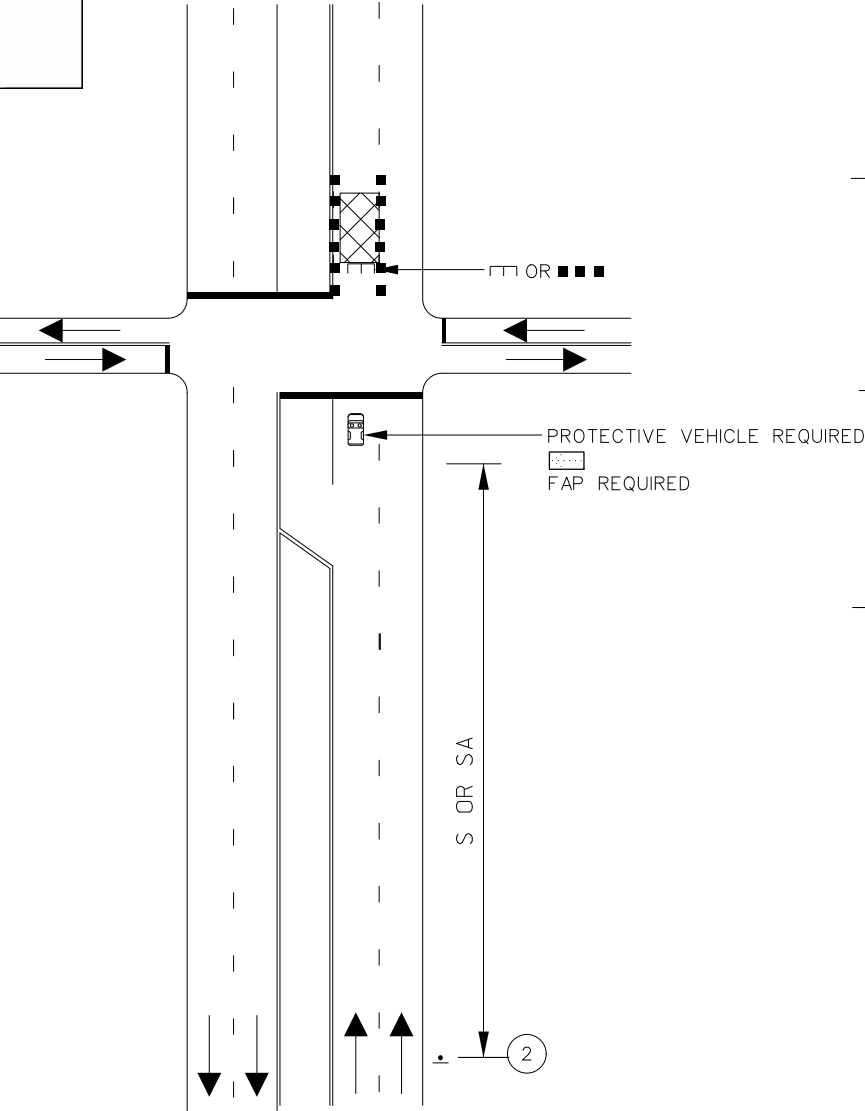
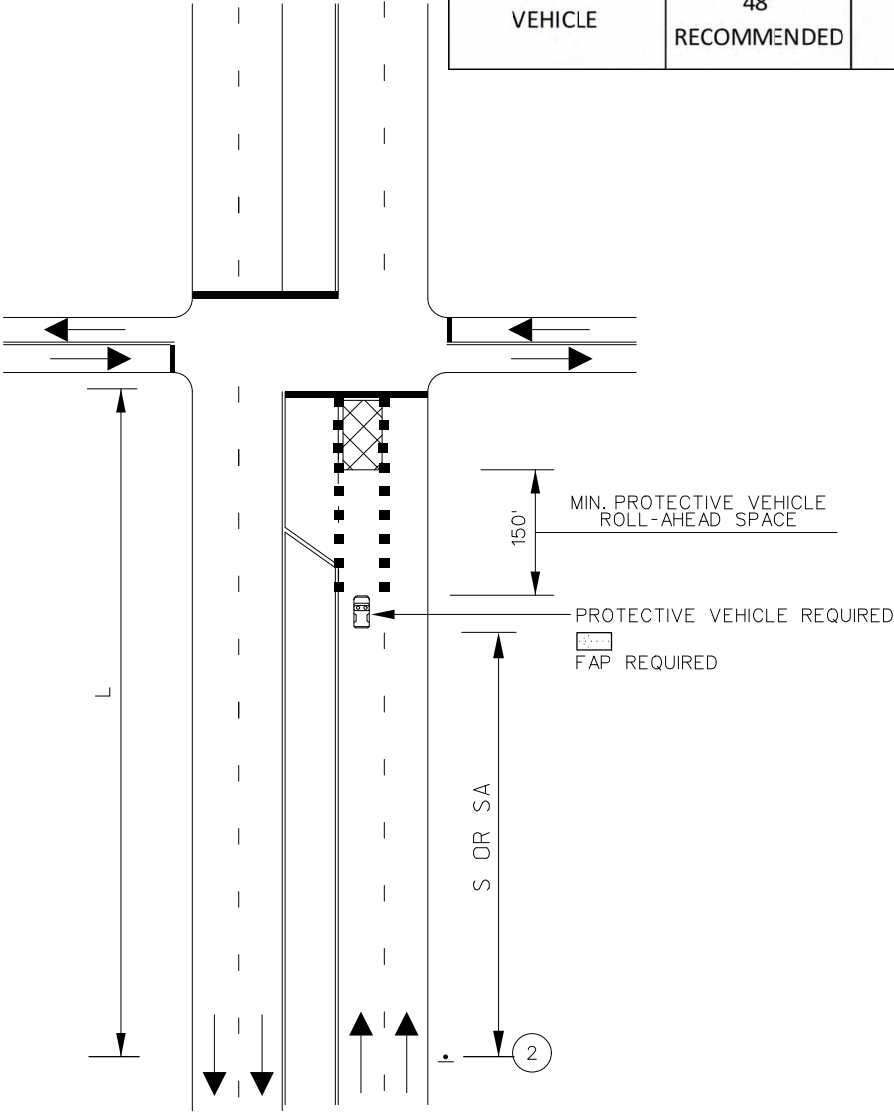
1. ACTIVATE HIGH INTENSITY ROTATING, FLASHNING, OSCILLATING, OR STROBE LIGHTS.
2. POSITION THE PROTECTIVE VEHICLE 150 FT IN ADVANCED OF THE WORK SPACE, IF POSSIBLE.
3. OPERATE THE FAP WITH THE APPROPRIATE ARROW MODE.

FOR ROADWAYS AT 45 MPH OR LESS, A WORK VEHICLE MAY BE USED AS THE PROTECTIVE VEHICLE.

1. ACTIVATE HIGH INTENSITY ROTATING, FLASHNING, OSCILLATING, OR STROBE LIGHTS.
2. POSITION THE WORK VEHICLE A MINIMUM OF 150 FT IN ADVANCED OF THE WORK SPACE, IF POSSIBLE.
3. OPERATE THE VEHICLE WITH A FAP WITH THE APPROPRIATE ARROW MODE.
4. ONCE YOU EXIST THE WORK VEHICLE, RETURN ONLY WHEN ADJUSTING POSITION OR READY TO LEAVE THE WORK LOCATION.

CONSIDER USING CHANNELIZERS TO DETER MOTORISTS FROM ENTERING THE WORK SPACE.

DO NOT SET UP A WORK ZONE IF IT CONFLICTS WITH THE SIGNAL LIGHTS, CONTACT THE APPROPRIATE DISTRICT PERSONNEL.



NOT TO SCALE

TRAFFIC CONTROL

Short Duration LT LN Closure
SHEET 7 OF 7

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

ENGINEERING DESIGN SOURCE, INC.
16141 Swingley Ridge Rd., Suite 300
Chesterfield, Missouri 63017
Missouri State Engineering Corporation #001523
T. 636 537 5585
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THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY

DATE PREPARED

3/28/2024

ROUTE

VARIES

STATE

MO

DISTRICT

STL

SHEET NO.

43

COUNTY

VARIES

JOB NO.

J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

MoDOT

105 WEST CAPITAL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

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ENGINEERING DESIGN SOURCE, INC.

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Chesterfield, Missouri 63017

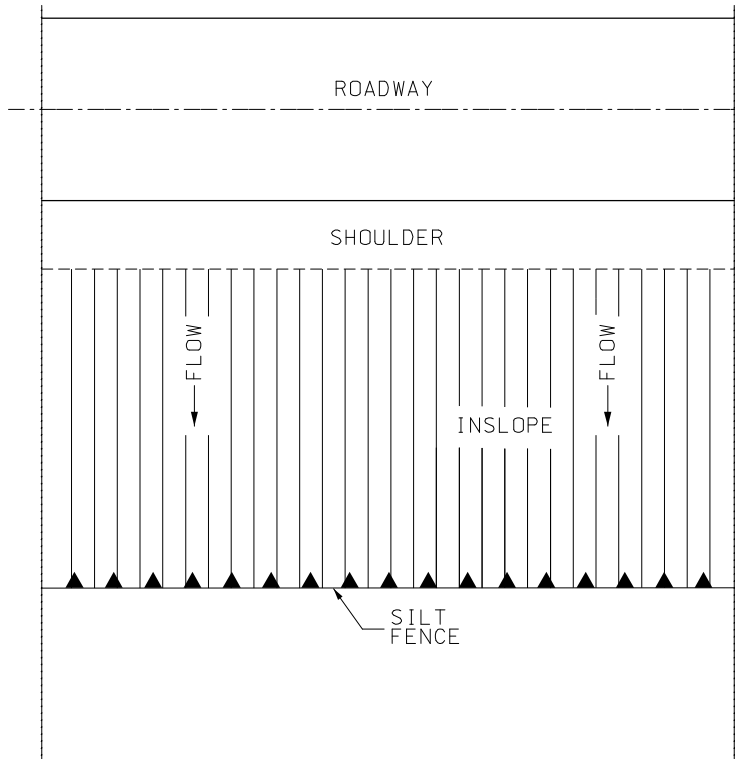
Missouri State Engineering Corporation #001523

T. 636 537 5585

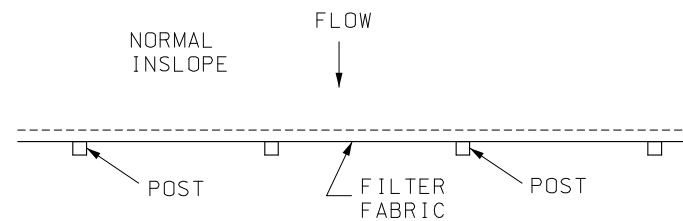
F. 636 537 0275

REV.

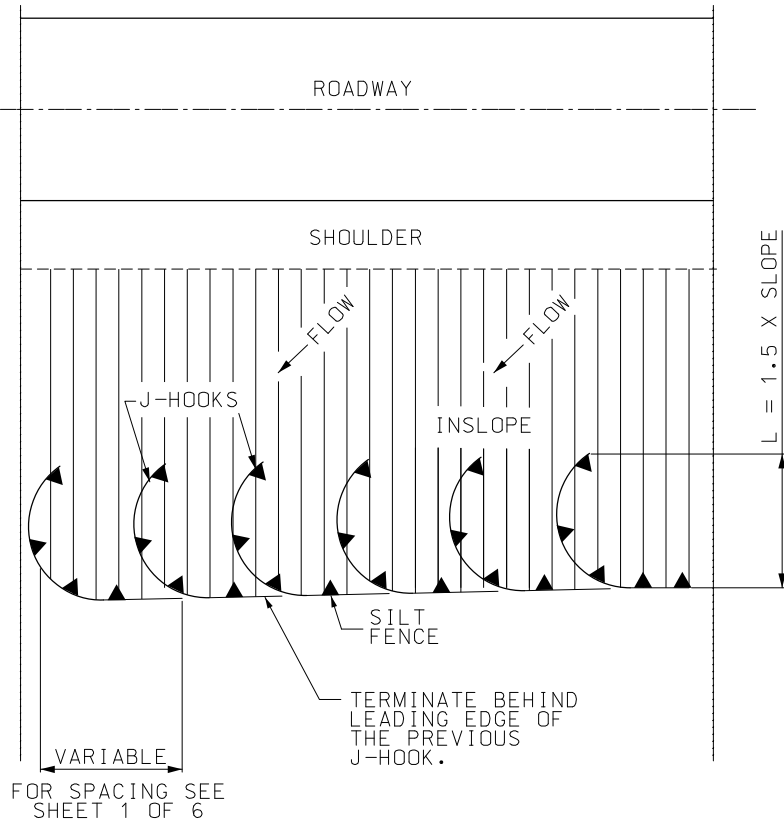
IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.



PERIMETER SILT FENCE
FOR TRANSVERSE FLOW



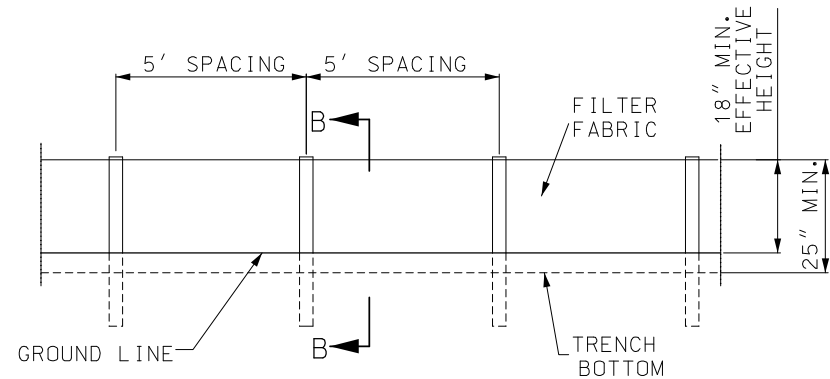
PLAN VIEW



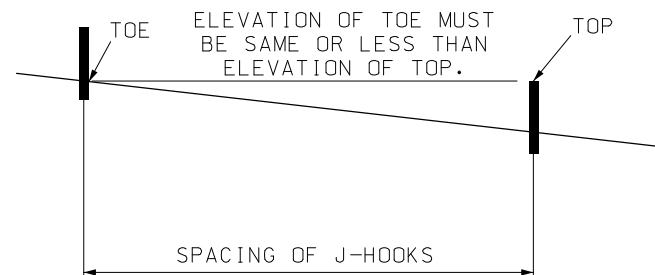
PERIMETER SILT FENCE
FOR ANGULAR FLOW

TEMPORARY EROSION CONTROL MEASURES

806.10K



ELEVATION DETAIL
FABRIC SILT FENCE



MINIMUM J-HOOK SPACING

GENERAL NOTES:

USE SILT FENCE FOR FILL HEIGHTS GREATER OR EQUAL TO 10 FEET. ON ALL FILLS GREATER THAN 10 FEET HIGH, MID-SLOPE RUNS OF SILT FENCE SHOULD BE CONSIDERED.

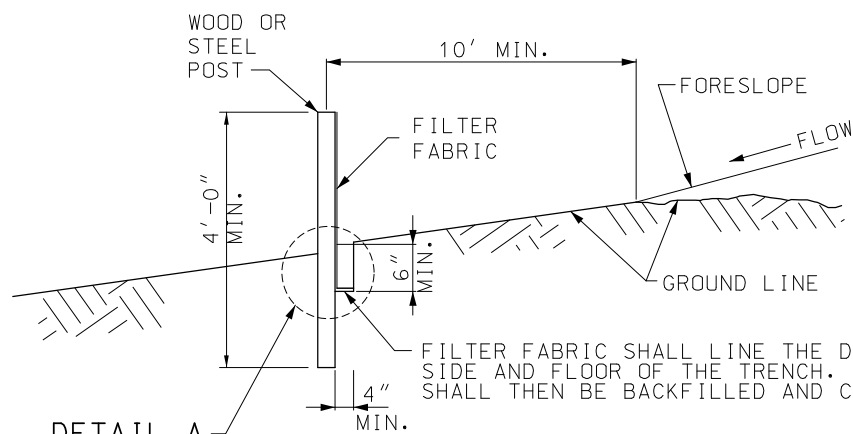
FOR FABRIC SILT FENCE:

MINIMUM LONGITUDINAL SPLICE OVERLAP SHALL BE 2' WITH A POST AT EACH END.

SECURE FABRIC TO POSTS.

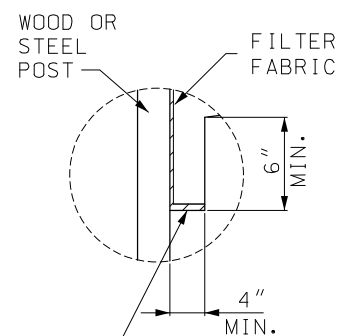
INSTEAD OF SILT FENCE ACROSS DRAINAGE DITCHES AND DRAINS, DITCH CHECKS SHALL BE USED AS SHOWN ON PLANS OR AS DIRECTED BY ENGINEER.

AT CULVERTS, PLACE SEDIMENT BARRIERS OVER THE TOP OF THE CULVERTS (NOT IN THE STREAM CHANNEL).



DETAIL A

TYPICAL B-B



DETAIL A

EROSION CONTROL

TEMPORARY SILT FENCE
SHEET 2 OF 2

PEDESTRIAN CURB RAMP PAY LIMITS

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DATE PREPARED

3/28/2024

ROUTE

VARIES

STATE

MO

DISTRICT

STL

SHEET NO.

44

COUNTY

VARIES

JOB NO.

J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITAL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

MoDOT

EDSI

ENGINEERING DESIGN SOURCE, INC.

16141 Swingley Ridge Rd., Suite 300

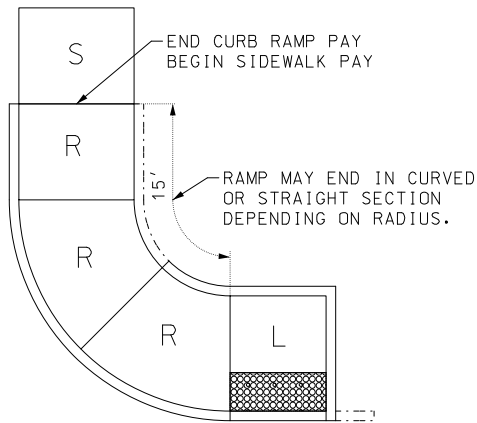
Chesterfield, Missouri 63017

Missouri State Engineering Corporation #001523

T. 636 537 5585

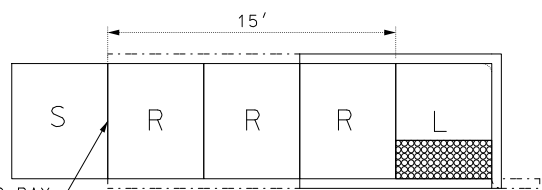
F. 636 537 0215

REV.

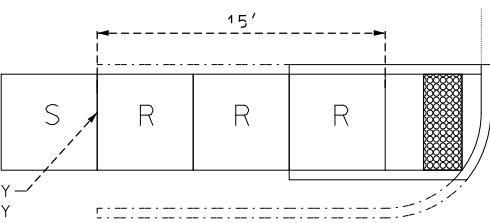


TYPE 1 PARALLEL

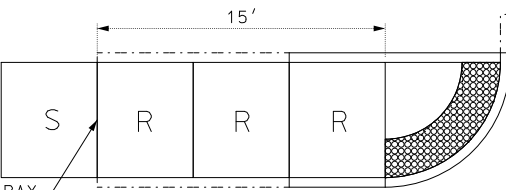
THE LENGTH OF THIS RAMP SHALL BE MEASURED ON THE INSIDE/SHORT SIDE OF THE CURVE



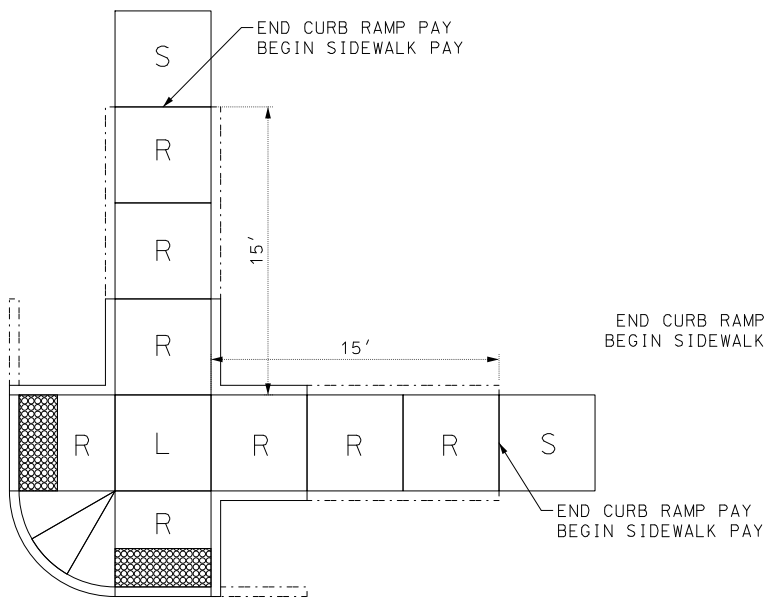
TYPE 2 PARALLEL



SMALL RADIUS PERPENDICULAR



LARGE RADIUS PERPENDICULAR

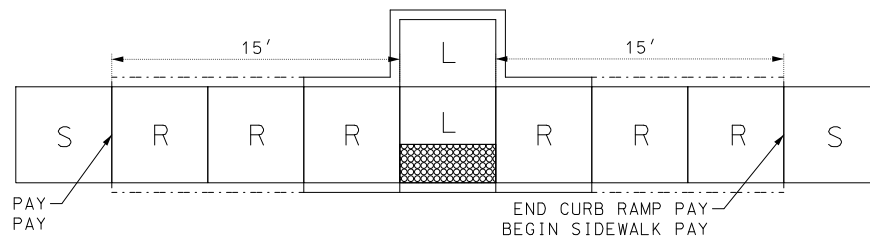


DUAL PERPENDICULAR
PAY AS 2 EACH

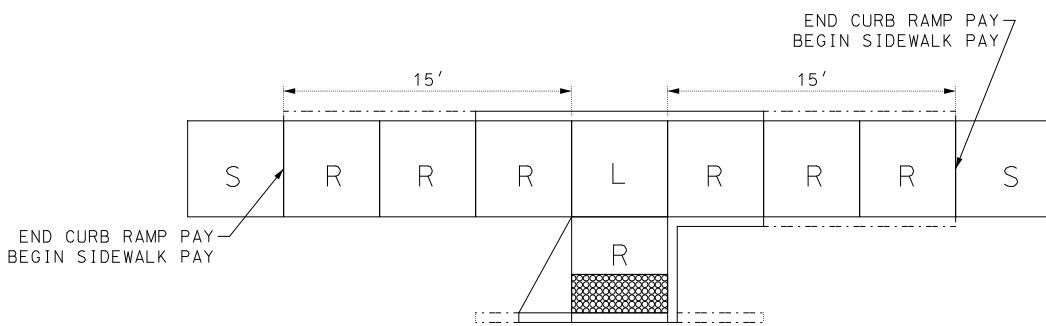
Notes -
15 FEET OF WALKWAY SHALL BE INCLUDED IN CURB RAMP PAY, PER EACH, AS SHOWN.

L - LANDING OR CLEAR AREA FOR TURNING MOVEMENTS AND RESTS.
R - PAY AS RAMP
S - PAY AS SIDEWALK.

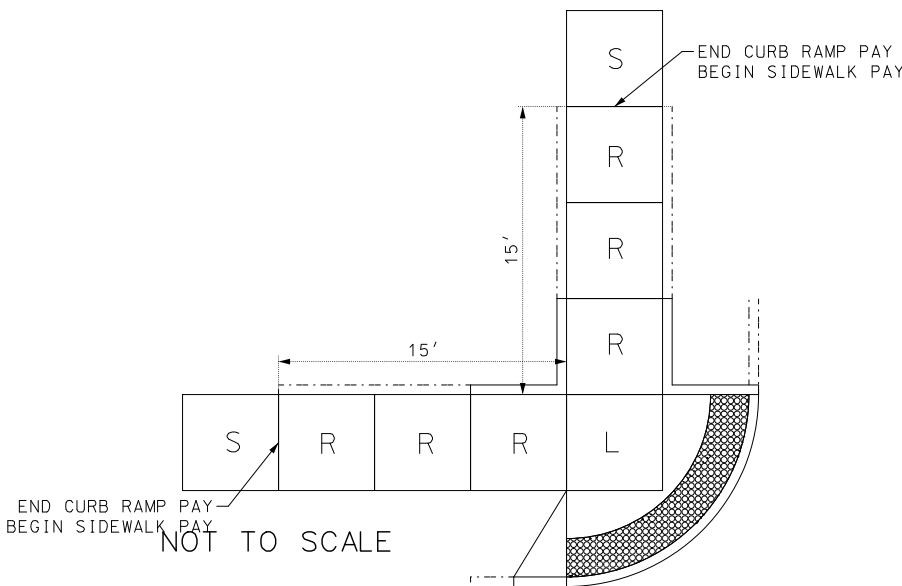
SEE JSP FOR OTHER INCIDENTALS INCLUDED IN CURB RAMP PAY.
SEE STANDARD PLANS 608 SERIES FOR DIMENSIONS, GRADES, AND OTHER REQUIREMENTS.
CONTRACTOR WILL HAVE OPTION TO CONSTRUCT RAMP WITH FLARE OR CURB FOR CERTAIN RAMPS ABOVE.



TYPE 3 PARALLEL



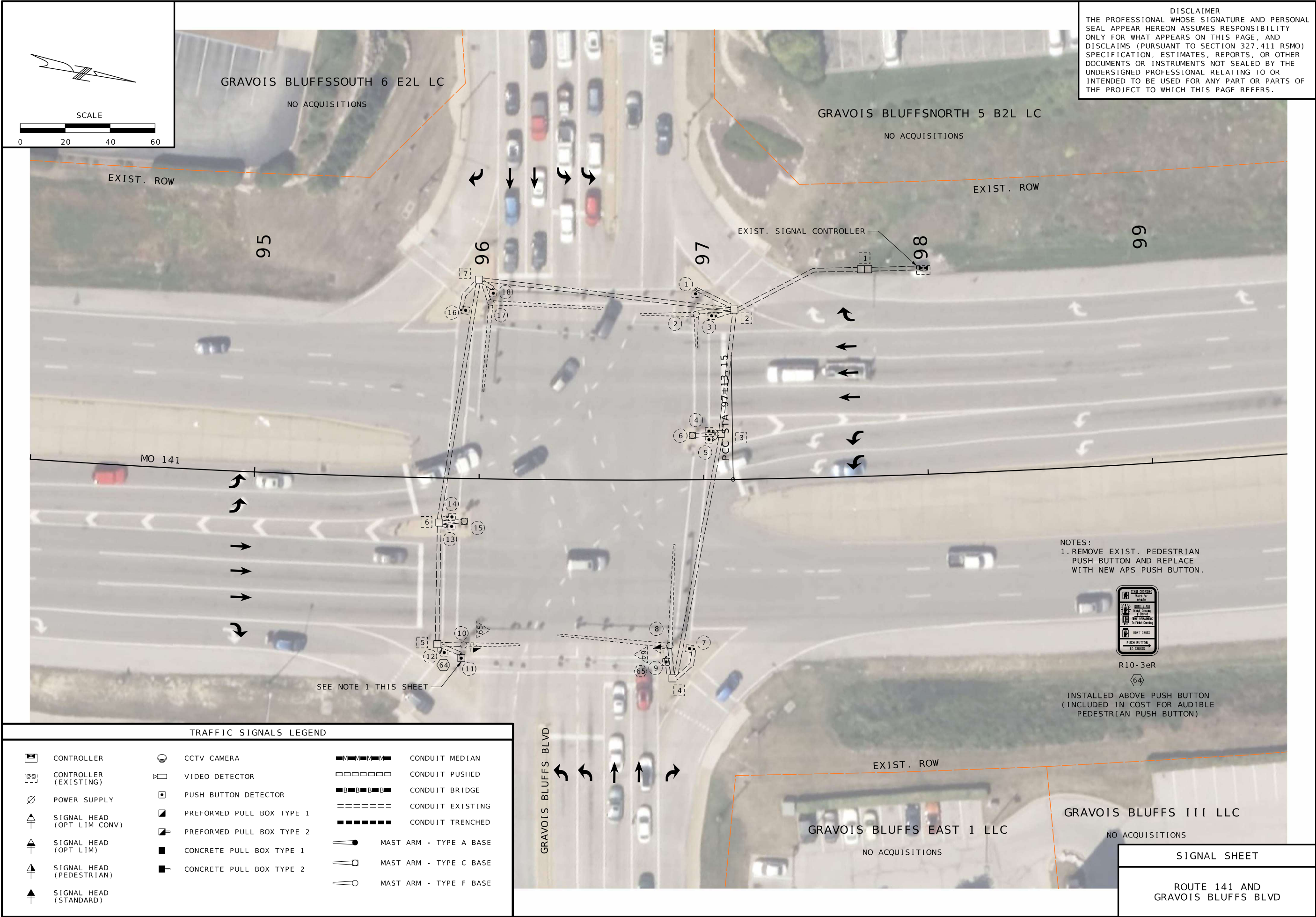
COMPOUND PERPENDICULAR


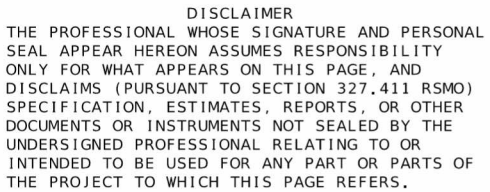


BLENDED TRANSITION
PAY AS 2 EACH

SPECIAL SHEETS

CURB RAMP PAY LIMITS
SHEET 1 OF 1





03/27/2024 1:37:28 PM
John Keaven
MO PE-2010000836

DATE PREPARED
3/27/2024

ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 47


COUNTY
VARIES

JOB NO.
J6P3510B

CONTRACT ID.

BRIDGE NO.

--	--	--	--	--	--	--	--

[illegible]

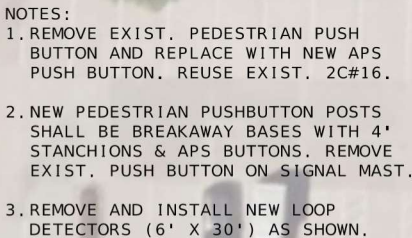
MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)
























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CRAWFORD, MURPHY &
TILLY, INC.

ONE MEMORIAL DRIVE, SUITE 500
ST. LOUIS, MO 63102 (314) 436-5500
ENGINEERING CORPORATION - 000631

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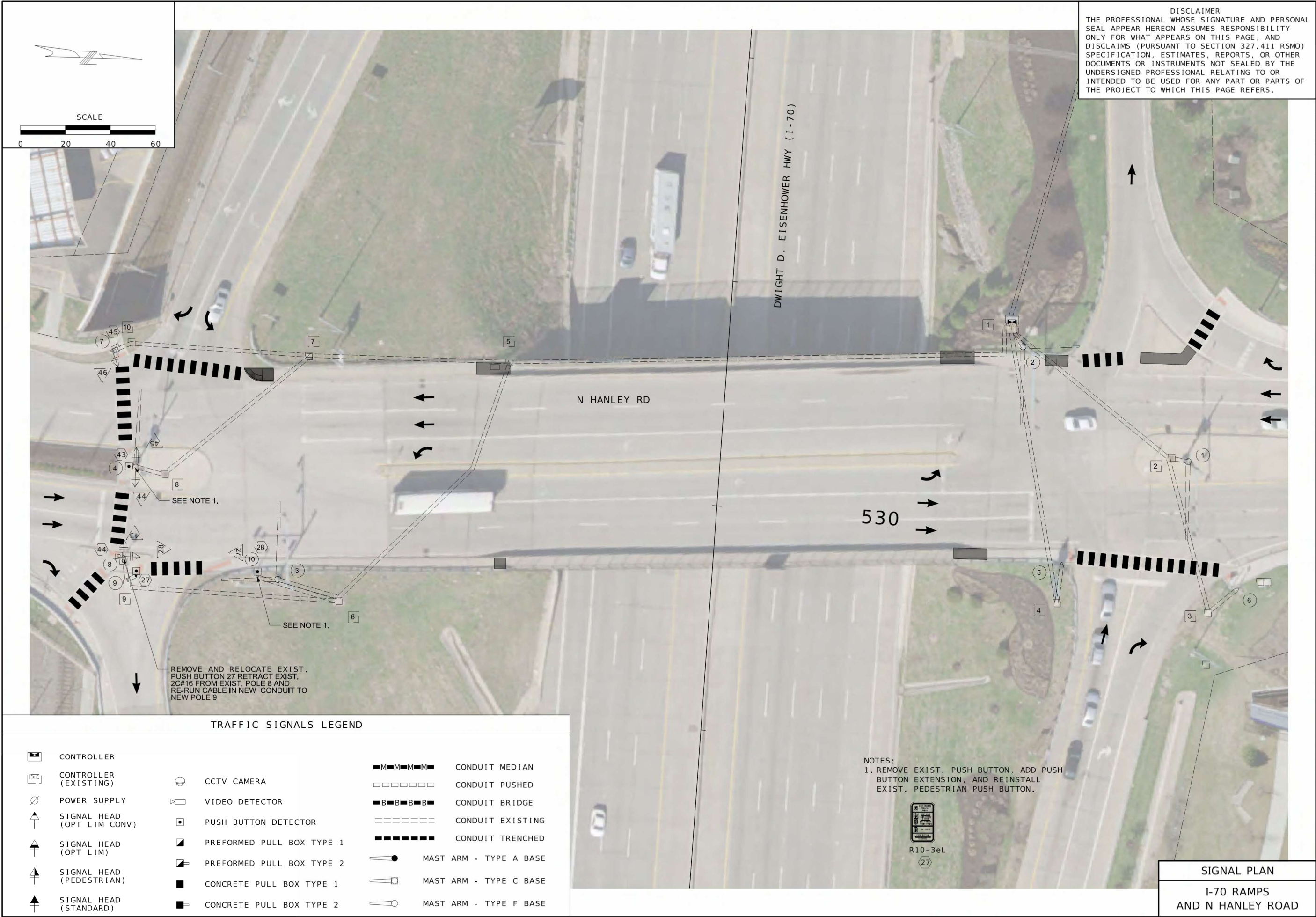


INSTALLED ABOVE PUSH BUTTON
(INCLUDED IN COST FOR AUDIBLE
PEDESTRIAN PUSH BUTTON)

	CONTROLLER		CCTV CAMERA		CONDUIT MEDIAN
	CONTROLLER (EXISTING)		VIDEO DETECTOR		CONDUIT PUSHED
	POWER SUPPLY		PUSH BUTTON DETECTOR		CONDUIT BRIDGE
	SIGNAL HEAD (OPT LIM CONV)		PREFORMED PULL BOX TYPE 1		CONDUIT EXISTING
	SIGNAL HEAD (OPT LIM)		PREFORMED PULL BOX TYPE 2		CONDUIT TRENCHED
	SIGNAL HEAD (PEDESTRIAN)		CONCRETE PULL BOX TYPE 1		MAST ARM - TYPE A BASE
	SIGNAL HEAD (STANDARD)		CONCRETE PULL BOX TYPE 2		MAST ARM - TYPE C BASE
					MAST ARM - TYPE F BASE
					6'X30' LOOP DETECTOR

SIGNAL SHEET

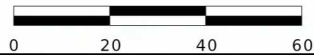
WEST OUTER ROAD AND
VOGEL ROAD



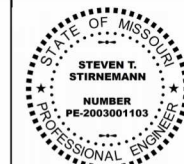
[illegible]



SCALE



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DATE PREPARED

3/28/2024

ROUTE STATE

VARIES MO

DISTRICT SHEET NO.

STL 53

COUNTY

VARIES

JOB NO.

J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

NOTES:
1. REMOVE EXIST. PEDESTRAIN PUSH BUTTONS AND REINSTALL NEW APS PUSH BUTTONS.



R10-3eR
23 24 64 84



R10-3eL
43 63 83 44

TRAFFIC SIGNALS LEGEND

	CONTROLLER		CONDUIT MEDIAN
	CONTROLLER (EXISTING)		CONDUIT PUSHED
	POWER SUPPLY		CONDUIT BRIDGE
	SIGNAL HEAD (OPT LIM CONV)		CONDUIT EXISTING
	SIGNAL HEAD (OPT LIM)		CONDUIT TRENCHED
	SIGNAL HEAD (PEDESTRIAN)		MAST ARM - TYPE A BASE
	SIGNAL HEAD (STANDARD)		MAST ARM - TYPE C BASE
	CCTV CAMERA		MAST ARM - TYPE F BASE
	VIDEO DETECTOR		
	PUSH BUTTON DETECTOR		
	PREFORMED PULL BOX TYPE 1		
	PREFORMED PULL BOX TYPE 2		
	CONCRETE PULL BOX TYPE 1		
	CONCRETE PULL BOX TYPE 2		

SIGNAL PLAN

JENNINGS STATION RD
AND JENNINGS XING

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION



105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



ENGINEERING DESIGN SOURCE, INC.
16141 Swingley Ridge Rd., Suite 300
Chesterfield, Missouri 63017
Missouri State Engineering Corporation #001523

T. 636 537 5585
F. 636 537 0215



SCALE



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DATE PREPARED

3/28/2024

ROUTE STATE

VARIES MO

DISTRICT SHEET NO.

STL 56

COUNTY

VARIES

JOB NO.

J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

MODOT

EDSI

ENGINEERING DESIGN SOURCE, INC.

16141 Swingley Ridge Rd., Suite 300

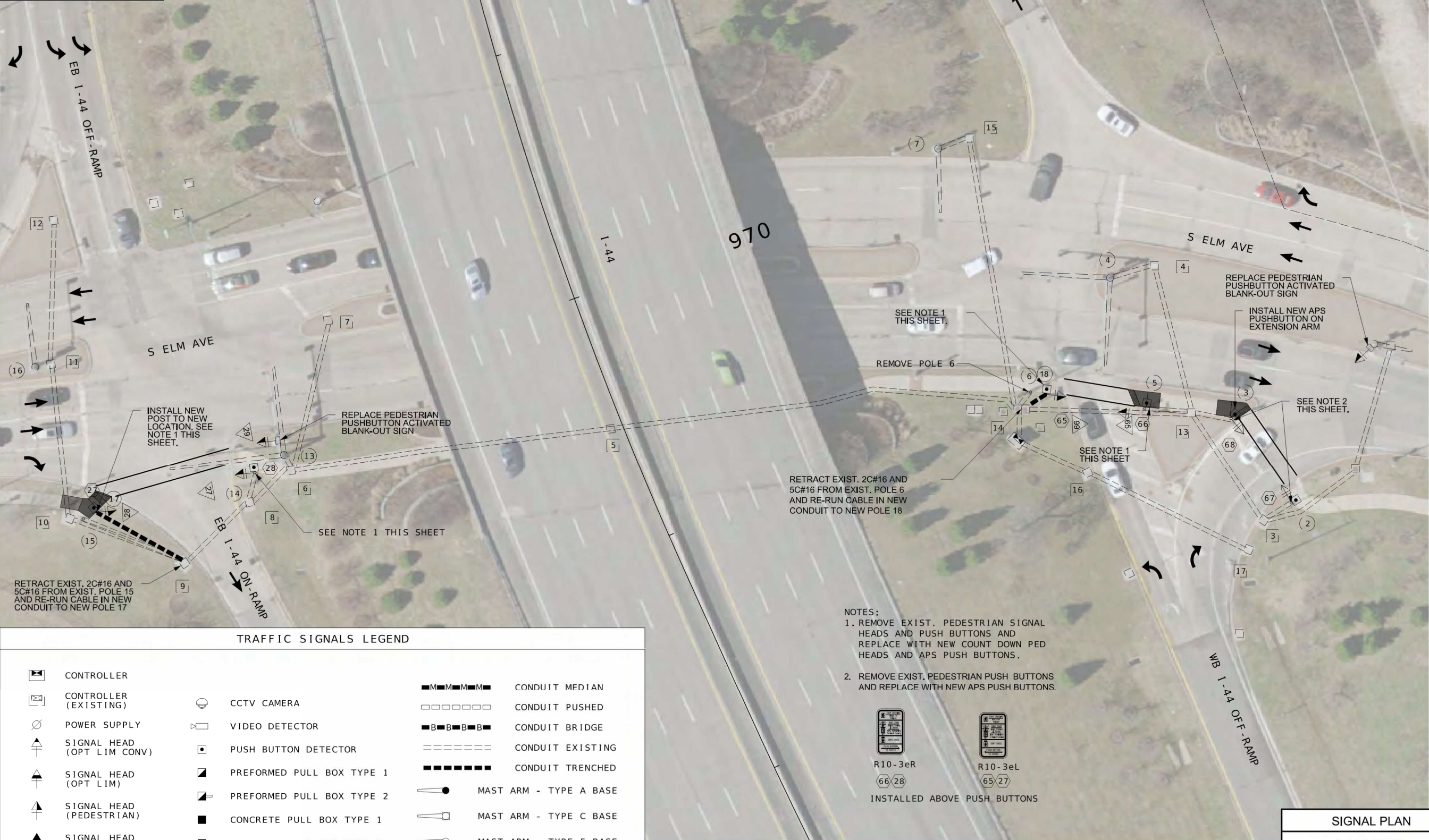
Chesterfield, Missouri 63047

Missouri State Engineering Corporation #001523

T. 636 537 5585

F. 636 537 0215

REV.



- NOTES:
1. REMOVE EXIST. PEDESTRIAN SIGNAL HEADS AND PUSH BUTTONS AND REPLACE WITH NEW COUNT DOWN PED HEADS AND APS PUSH BUTTONS.
 2. REMOVE EXIST. PEDESTRIAN PUSH BUTTONS AND REPLACE WITH NEW APS PUSH BUTTONS.



R10-3eR

66/28

INSTALLED ABOVE PUSH BUTTONS



R10-3eL

65/27

INSTALLED ABOVE PUSH BUTTONS

TRAFFIC SIGNALS LEGEND



CONTROLLER



CONTROLLER (EXISTING)



POWER SUPPLY



SIGNAL HEAD (OPT LIM CONV)



SIGNAL HEAD (OPT LIM)



SIGNAL HEAD (PEDESTRIAN)



SIGNAL HEAD (STANDARD)



CCTV CAMERA



VIDEO DETECTOR



PUSH BUTTON DETECTOR



PREFORMED PULL BOX TYPE 1



PREFORMED PULL BOX TYPE 2



CONCRETE PULL BOX TYPE 1



CONCRETE PULL BOX TYPE 2



CONDUIT MEDIAN



CONDUIT PUSHED



CONDUIT BRIDGE



CONDUIT EXISTING



CONDUIT TRENCHED



MAST ARM - TYPE A BASE



MAST ARM - TYPE C BASE



MAST ARM - TYPE F BASE

SIGNAL PLAN

I-44 RAMPS

AND ELM AVE

[illegible]



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DATE PREPARED

3/28/2024

ROUTE

VARIES

DISTRICT

STL

COUNTY

VARIES

JOB NO.

J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

MODOT

105 WEST CAPITAL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

EDSI

ENGINEERING DESIGN SOURCE, INC.

16141 Swingley Ridge Rd., Suite 300

Chesterfield, Missouri 63017

Missouri State Engineering Corporation #001523

T. 636 537 5585

F. 636 537 0215

SIGNAL PLAN

LACLEDE STATION RD AND MURDOCK

REV.

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ACCESSIBLE PUSH BUTTONS						
DETECTOR NUMBER	APPROACH	TYPE				VIDEO
		PUSH BUTTON	INDUCTION LOOP (2)			
			STANDARD	DELAY / EXTEND *	CALL UNIT *	
65	WB I-44 ON-RAMP	1				
66	WB I-44 ON-RAMP	1				
TOTAL		2				

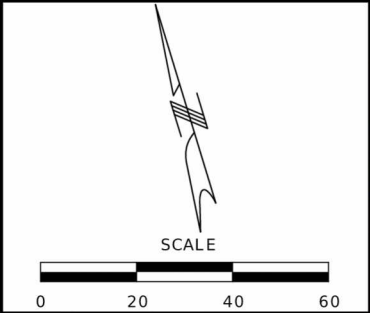
* ITEMS FOR WHICH SEPARATE PAYMENT WILL NOT BE MADE.

*** USE D3-1 FOR ONE LINE. USE D3-1B FOR TWO LINE.

- (1) MoDOT "D" PLUG SHALL BE WIRED INTO ALL NEMA CONTROLLERS WITH 7C HARDWARE INTERCONNECT.
- (2) PAYMENT IS MADE FOR THE NUMBER OF 2-CHANNEL DETECTOR CARDS AS SHOWN BELOW THE ASSIGNMENT CHART.
- (3) PAYMENT FOR SIGN R10-3E AND SIGNAL MOUNTING HARDWARE ARE INCLUDED IN COST FOR AUDIBLE PEDESTRIAN PUSH BUTTON.

SIGNAL SIGNS					
QUANTITY	MUTCD SIGN NUMBER	SIZE	AREA	SIGN TOTAL AREA	SIGNAL SIGN MOUNTING HARDWARE
		INCHES	SQ. FT.	ITEM NO. 90208.33	ITEM NO. 90208.34
				SQ. FT.	EA.
	R10-10L	24 X 30	5.0		
	R10-10R	24 X 30	5.0		
	R3-5L	30 X 36	7.5		
	R3-5R	30 X 36	7.5		
	R3-5A	30 X 36	7.5		
	R3-6L	30 X 36	7.5		
	R3-6R	30 X 36	7.5		
	R3-2	24 X 24	4.0		
	R3-1	24 X 24	4.0		
	R3-3	24 X 24	4.0		
	D3-1 ***	VAR. X 18			
	D3-1B ***	VAR. X 18			
1	R10-3ER	9 X 15	0.9	(0.9)	(1)
1	R10-3EL	9 X 15	0.9	(0.9)	(1)
	R10-11A	24 X 30	5.0		
	R10-13	36 X 24	6.0		
	R10-27A	30 X 36	7.5		
		SUBTOTAL		(1.8)	(2)
		TOTAL		(2)	(2)

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DATE PREPARED 3/28/2024	
ROUTE VARIES	STATE MO
DISTRICT STL	SHEET NO. 61
COUNTY VARIES	
JOB NO. J6P3510B	
CONTRACT I.D.	
PROJECT NO.	
BRIDGE NO.	
DESCRIPTION	
DATE	
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION	
 <p>105 WEST CAPITOL JEFFERSON CITY, MO 65102 1-888-ASK-MODOT (1-888-275-6636)</p>	
 <p>EDSI ENGINEERING DESIGN SOURCE, INC.</p> <p>16141 Swingley Ridge Rd. Suite 300 Chesapeake, Missouri 63017 Missouri State Engineering Corporation #001523</p> <p>T. 636 637 5585 F. 636 637 0215</p>	

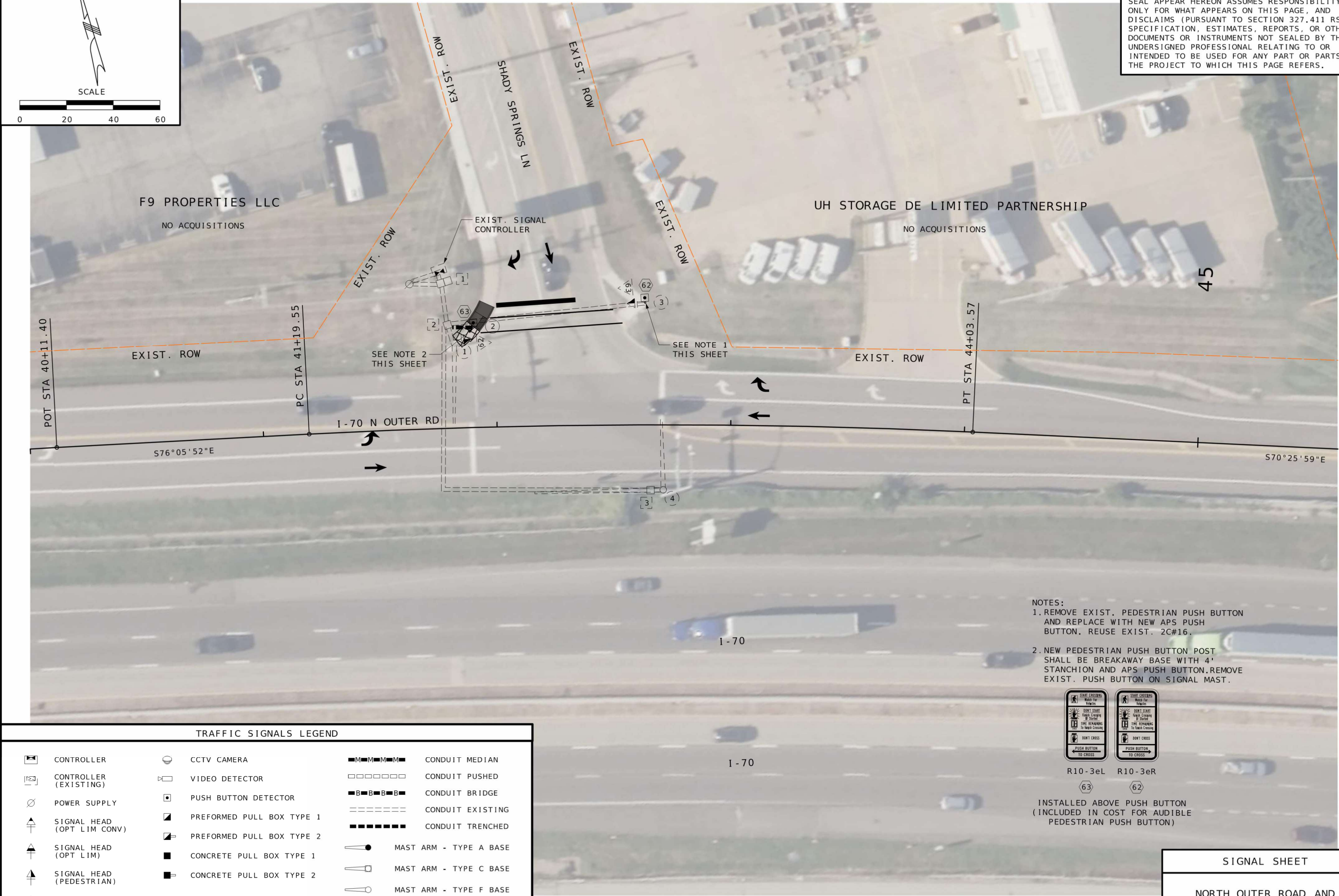


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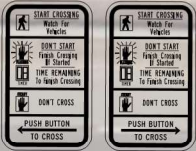
STATE OF MISSOURI
JOHN R. KEEVEN
NUMBER
PE-2010000836
PROFESSIONAL ENGINEER

03/27/2024 1:37:28 PM
John Keeven
MO PE-2010000836

DATE PREPARED		3/27/2024
ROUTE	STATE	VARIES MO
DISTRICT	SHEET NO.	STL 62
COUNTY		VARIES
JOB NO.		J6P3510B
CONTRACT ID.		
PROJECT NO.		
BRIDGE NO.		



- NOTES:
1. REMOVE EXIST. PEDESTRIAN PUSH BUTTON AND REPLACE WITH NEW APS PUSH BUTTON. REUSE EXIST. 2C#16.
 2. NEW PEDESTRIAN PUSH BUTTON POST SHALL BE BREAKAWAY BASE WITH 4' STANCHION AND APS PUSH BUTTON. REMOVE EXIST. PUSH BUTTON ON SIGNAL MAST.



INSTALLED ABOVE PUSH BUTTON
(INCLUDED IN COST FOR AUDIBLE
PEDESTRIAN PUSH BUTTON)

TRAFFIC SIGNALS LEGEND

	CONTROLLER		CCTV CAMERA		CONDUIT MEDIAN
	CONTROLLER (EXISTING)		VIDEO DETECTOR		CONDUIT PUSHED
	POWER SUPPLY		PUSH BUTTON DETECTOR		CONDUIT BRIDGE
	SIGNAL HEAD (OPT LIM CONV)		PREFORMED PULL BOX TYPE 1		CONDUIT EXISTING
	SIGNAL HEAD (OPT LIM)		PREFORMED PULL BOX TYPE 2		CONDUIT TRENCHED
	SIGNAL HEAD (PEDESTRIAN)		CONCRETE PULL BOX TYPE 1		MAST ARM - TYPE A BASE
	SIGNAL HEAD (STANDARD)		CONCRETE PULL BOX TYPE 2		MAST ARM - TYPE C BASE
					MAST ARM - TYPE F BASE

DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

CRAWFORD, MURPHY &
TILLY, INC.

ONE MEMORIAL DRIVE, SUITE 500
ST. LOUIS, MO 63102 (314) 436-5500
ENGINEERING CORPORATION - 000631

SIGNAL SHEET

NORTH OUTER ROAD AND
SHADY SPRINGS LANE

ACCESSIBLE PUSH BUTTONS						
DETECTOR NUMBER	APPROACH	TYPE				VIDEO
		PUSH BUTTON	INDUCTION LOOP (2)			
			STANDARD	DELAY / EXTEND *	CALL UNIT *	
62	WB I-70 N OUTER RD	1				
63	EB I-70 N OUTER RD	1				
-						
-						
-						
-						
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-						
-						
-						
TOTAL		2				

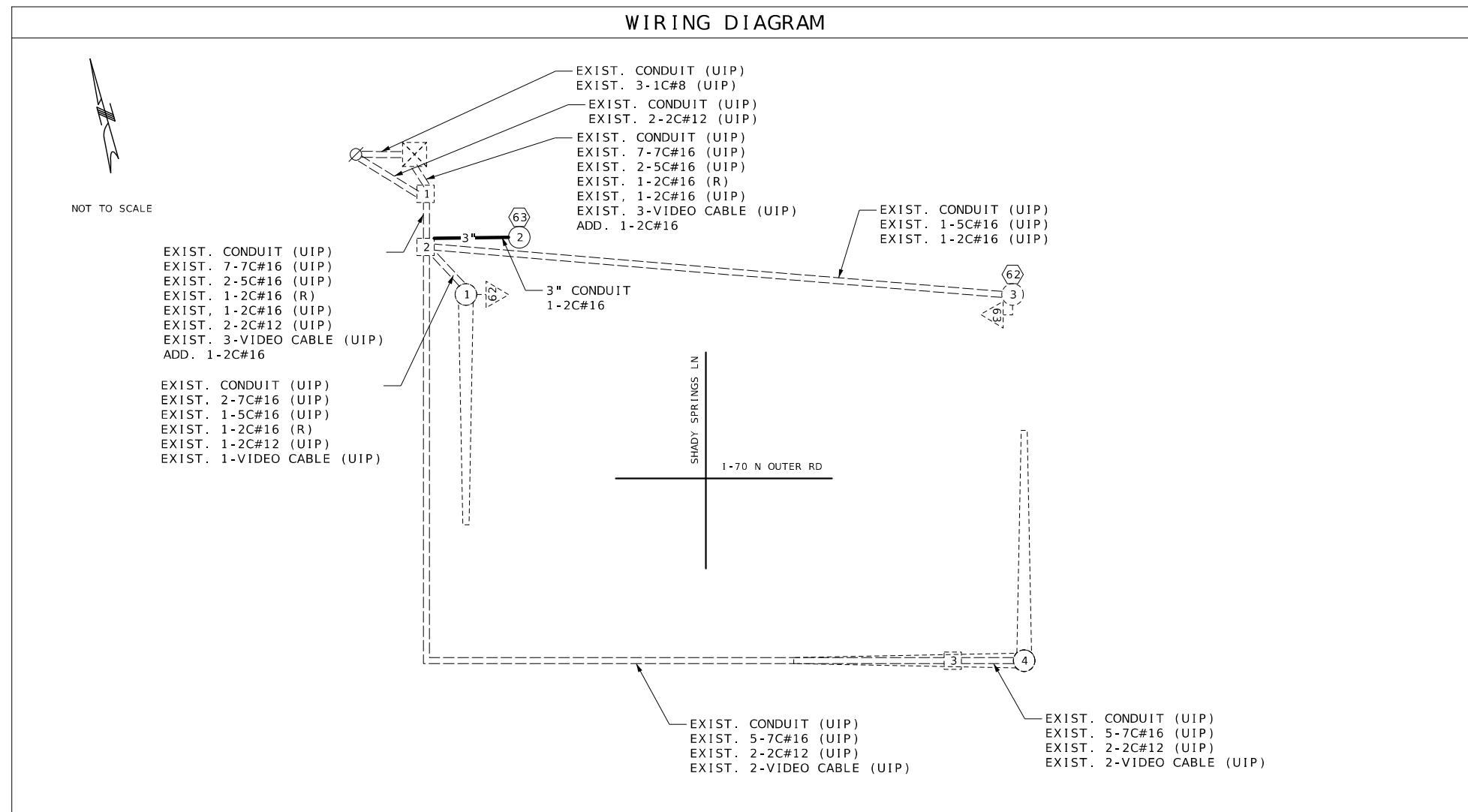
[illegible]

*** USE D3-1 FOR ONE LINE. USE D3-1B FOR TWO LINE.










NOTE :

PAYMENT FOR SIGN R10-3E AND SIGNAL
MOUNTING HARDWARE ARE INCLUDED IN COST FOR
AUDIBLE PEDESTRIAN PUSH BUTTON.

WIRING DIAGRAM



LEGEND

- | | |
|---|--|
| 3" CONDUIT CONTAINING CABLE WITH SIZE | |
| <hr/> | |
| | CABLE |
| X-Xc | MULTI-CONDUCTOR CABLE #16 AWG (UNLESS OTHERWISE INDICATED) |
| LIC | #14 AWG DETECTOR LEAD-IN CABLE (2C TWISTED SHIELDED) |
|  | PULL BOX WITH NUMBER |
|  | POST WITH NUMBER |
|  | POST WITH MAST ARM |
|  | POST WITH MAST ARM AND LUMINAIRE |
|  | DETECTOR WITH NUMBER |
|  | SIGNAL HEAD WITH NUMBER |
|  | SIGNAL CONTROLLER |
|  | POWER SUPPLY |
|  | LIGHTING CONTROLLER |

I-70 N OUTER RD / SHADY SPRINGS LN
INTERSECTION