MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION PLANS FOR PROPOSED

SAINT LOUIS DISTRICT WIDE

ADA IMPROVEMENTS

ST CHARLES COUNTY ST LOUIS COUNTY JEFFERSON COUNTY

MO 94

ADA UPGRADES

67

ST LOUIS COUNTY

ROUTE EE

ADA UPGRADES

INTERSTATE 70 ADA UPGRADES

ADA UPGRADES

I-44 AT I-270 ADA UPGRADES

MO 141

ADA UPGRADES

I - 55 ADA UPGRADES

ST LOUIS

NO RIGHT-OF-WAY ACOUISITION

LOCATION OF ST CHARLES COUNTY

ST LOUIS COUNTY JEFFERSON COUNTY

MO 141 (SOUTH WOODS MILL ADA UPGRADES

I-70 (NORTH OUTER ROAD)

FRANKLIŃ COUNTY

ST CHARLES COUNTY

CONVENTIONAL SYMBOLS

(USED IN PLANS	5)	
	EXISTING	NEW
BUILDINGS AND STRUCTURES GUARD RAIL GUARD CABLE CONCRETE RIGHT-OF-WAY MARKER STEEL RIGHT-OF-WAY MARKER LOCATION SURVEY MARKER	0000 0000 11 7	
UTILITIES FIBER OPTICS OVERHEAD CABLE TV UNDERGROUND CABLE TV OVERHEAD TELEPHONE UNDERGROUND TELEPHONE OVERHEAD POWER UNDERGROUND POWER SANITARY SEWER STORM SEWER GAS WATER	- FOOTVUTV OT UT OE UE SS G W	-F0 -OTV -UTV -OT -UT -OE -VE -S -SS -G
MANHOLE	SAN E)
FIRE HYDRANT	WV]
WATER VALVE	wa C)
WATER METER	D.,,)
DROP INLET	اً ا	
DITCH BLOCK	=	⊨
GROUND MOUNTED SIGN	SIGN	_
LIGHT POLE		
H-FRAME POWER POLE	PED PED	
TELEPHONE PEDESTAL FENCE CHAIN LINK WOVEN WIRE GATE POST	Δ	/ —— / ——
BENCHMARK	· (8)

NOTE: DASHED OR OPEN SYMBOLS INDICATE EXISTING FEATURES

THE EXISTENCE AND APPROXIMATE LOCATION OF UTILITY FACILITIES KNOWN TO EXIST, AS SHOWN ON THE PLANS, ARE BASED ON THE BEST INFORMATION AVAILABLE TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE. RELIANCE UPON THIS INFORMATION IS DONE AT THE RISK AND PERIL OF THE USER, AND THE COMMISSION SHALL NOT BE LIABLE FOR ANY DAMAGES THAT MAY ARISE FROM ANY ERROR IN THE INFORMATION. IT IS, THEREFORE, THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE, LOCATION AND STATUS OF ANY FACILITY. SUCH VERIFICATION INCLUDES DIRECT CONTACT WITH THE LISTED UTILITIES.

JEFFERSON

COUNTY

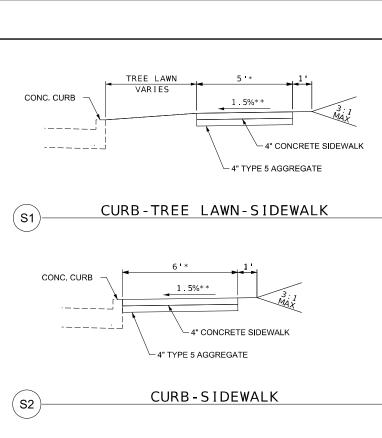
I-55 (WEST OUTER ROAD)

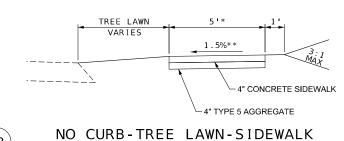
INDEX OF SHEETS

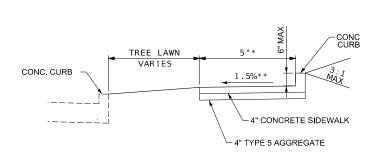
DESCRIPTION	SHEET NUMBER
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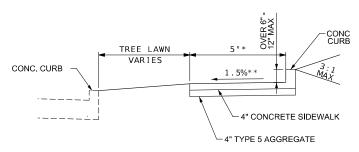




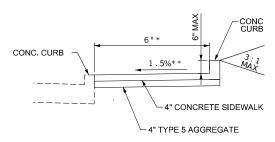


S3

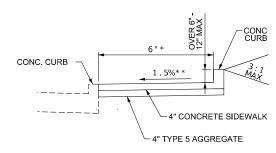




CURB-TREE LAWN-SIDEWALK-CURB S5)

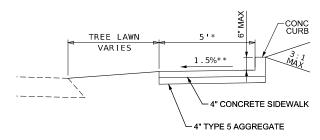


CURB-SIDEWALK-CURB S6

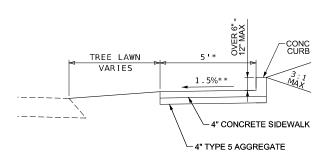


CURB-SIDEWALK-CURB

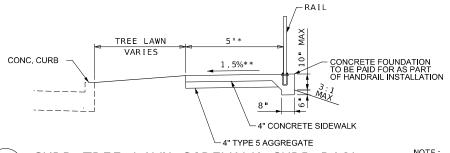
S7



NO CURB-TREE LAWN-SIDEWALK-CURB

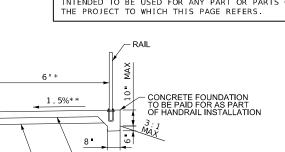


NO CURB-TREE LAWN-SIDEWALK-CURB



CURB-TREE LAWN-SIDEWALK-CURB-RAIL S10

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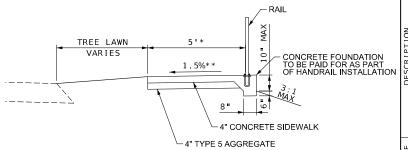
4" CONCRETE SIDEWALK

CURB-SIDEWALK-CURB-RAIL

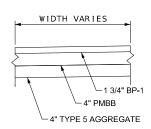
- 4" TYPE 5 AGGREGATE

CONC. CURB

S11



NO CURB-TREE LAWN-SIDEWALK-RAIL S12



ASPHALT SIDEWALK DETAIL (**-**A

PLAN SHEET LEGEND

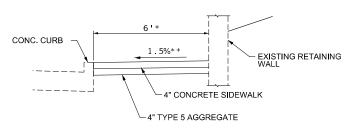
- S# SIDEWALK TYPICAL SECTION DESIGNATION
- CURB RAMP TYPICAL SECTION DESIGNATION C#
- (R# OTHER REPAIR TYPICAL SECTION DESIGNATION
- SIDEWALK TYPICAL SECTION DESIGNATION USING ASPHALT MATERIALS

- * UNLESS SHOWN OTHERWISE ON PLAN SHEETS
- ** THE NEW SIDEWALK SLOPE DIRECTION SHOULD MATCH SLOPE DIRECTION OF EXISTING SIDEWALK

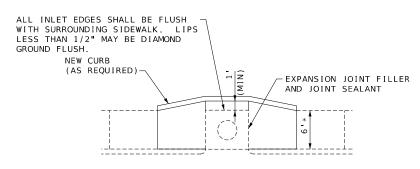
TYPICAL APPLICATIONS

SHEET 1 OF 5

THIS SHEET HAS BEEN GNED, SEALED, AND DATED **ELECTRONICALLY** 3/28/2024 /ARIES MO STL 2 VARIES J6P3510B CONTRACT ID. PROJECT NO. BRIDGE NO.

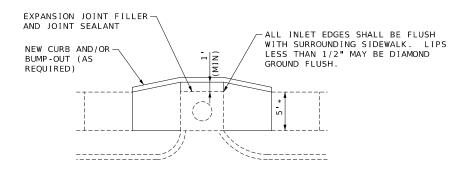


CURB-SIDEWALK-RETAINING WALL (S13)

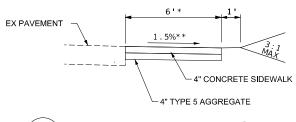


S14

SIDEWALK AT BACK OF CURB AND AT INLET



SIDEWALK WITH TREELAWN AND AT INLET S15



PAVEMENT-SIDEWALK (S16)

DISCLAIMER

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3/28/2024 VARIES MO STL 2

> VARIES J6P3510B

PROJECT NO.

BRIDGE NO.

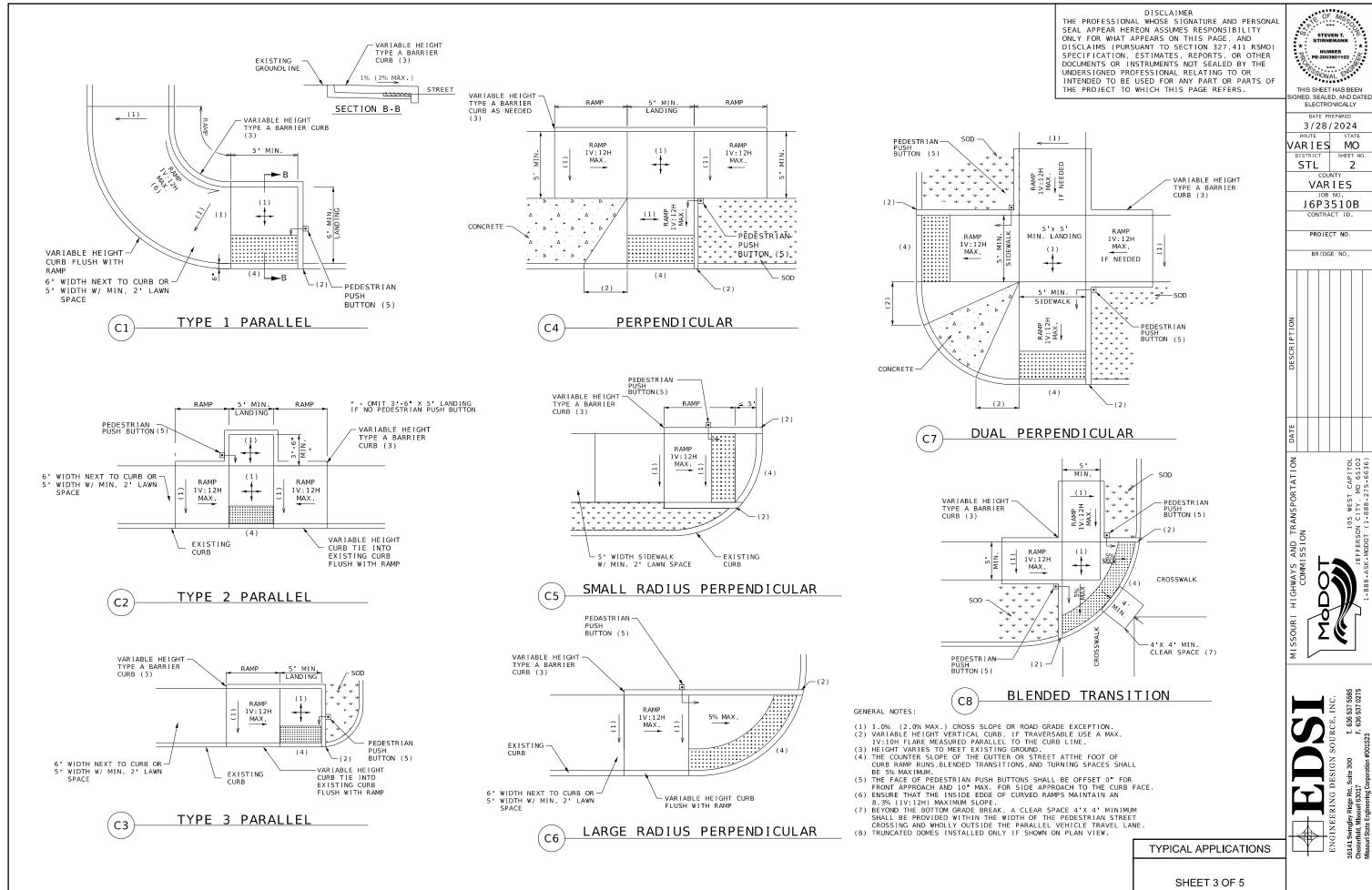
TYPICAL APPLICATIONS

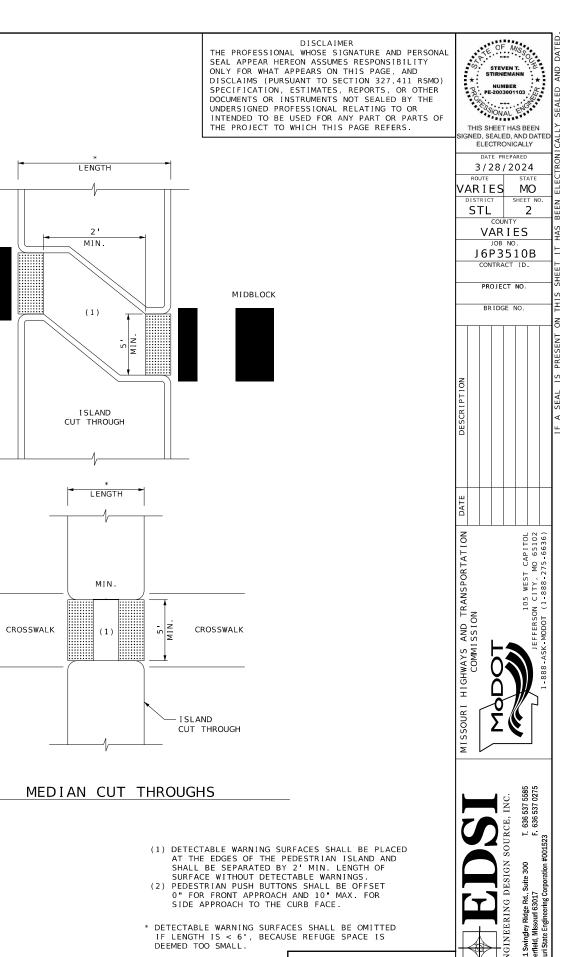
SHEET 2 OF 5

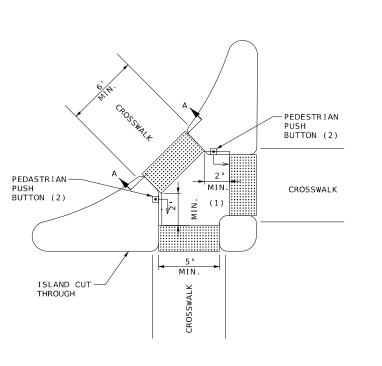
NOTE:

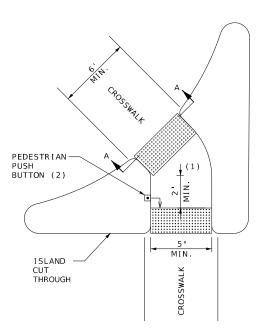
UNLESS SHOWN OTHERWISE ON PLAN SHEETS

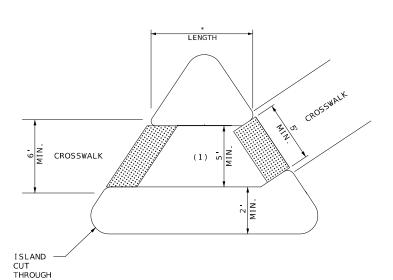
** THE NEW SIDEWALK SLOPE DIRECTION SHOULD MATCH SLOPE DIRECTION OF EXISTING SIDEWALK.

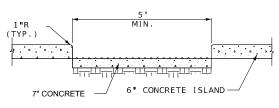












MIDBLOCK

C10

SECTION A-A ISLAND CUT THROUGH TYPICAL

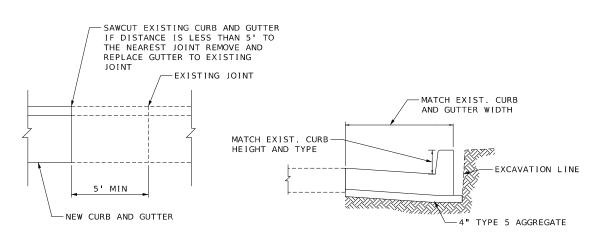
RAMP OR CUT-THROUGH DEPENDING ON ISLAND WIDTH. IF RAMPED, PROVIDE 4' MINIMUM LANDING AND SLOPE RAMPS AT 1V:12H MAX.

RAMP MUST BE CONSTRUCTED TO DRAIN TO THE OUTSIDE.

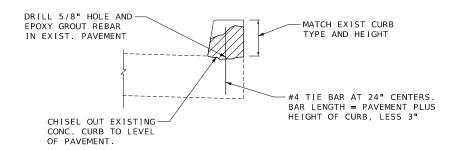
C9 ISLAND CUT THROUGHS

TYPICAL APPLICATIONS

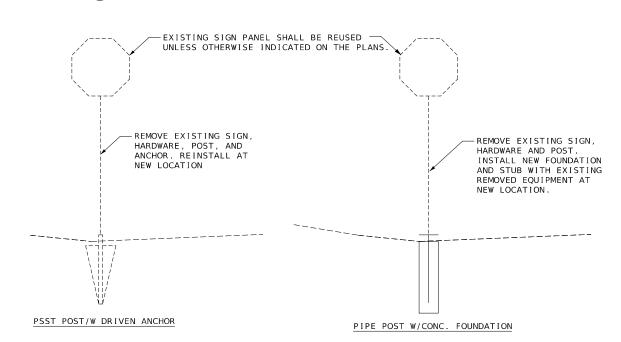
SHEET 4 OF 5



CURB AND GUTTER REPAIR (R1



INTEGRAL CURB REPAIR



EXISTING SIGN RELOCATION R3

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J6P3510B CONTRACT ID. PROJECT NO.

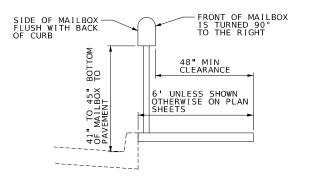
BRIDGE NO.

TYPICAL APPLICATIONS

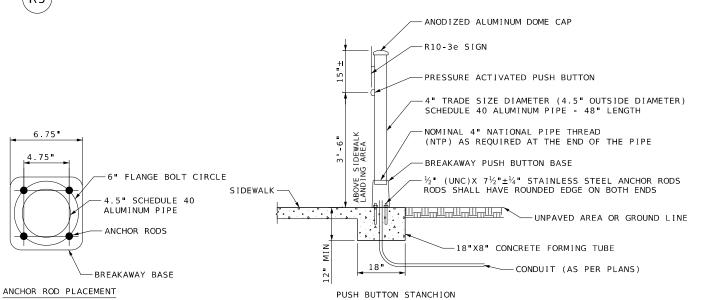
SHEET 5 OF 5

DIAMOND GRIND EXISTING CONC. SIDEWALK SLAB FLUSH WITH ADJACENT 1/2" MAX — SIDEWALK SLAB 1. IF EXISTING LIP EXCEDES 1/2"
IN HEIGHT ENTIRE SIDEWALK SLAB MUST BE REPLACED . SIDEWALK GRINDING TO BE A MINIMUM 10" WIDE FROM THE JOINT.

DIAMOND GRINDING SIDEWALK EDGE



MAILBOX RELOCATION R5



NOTES:

(R4

- THE PUSH BUTTON STANCHION FOUNDATION IS CONSTRUCTED AS PART OF THE SIDEWALK. INCREASE THE SIDEWALK THICKNESS TO 12" (MIN.) TO PROVIDE FOR THE PUSH BUTTON STANCHION FOUNDATION.
- INSTALLATION OF THE PUSH BUTTON STANCHION SHALL BE PER MANUFACTURER'S REQUIREMENTS.
- CABLES SHALL HAVE A SLIT-FIT CONNECTION IN THE PEDESTAL THAT WILL DETACH IN THE EVENT THE POLE IS STRUCK.

				SIGNS			Tester de la contrata	PERFORATED SQUARE STEEL TUBE											
							CONCRETE FOOTINGS -	2 IN. POST						2.5 IN. POST					BREAK-
		902 SI	GNAL SIGNS	TABULATED O	N D-37A SHEET		EIVIBEDDED					ANCHORS				Tanza T	ANCHCRS		AWAY
OVERALL SHEET NUMBER	SIGN NO.	INO. SIGN SIZE	STATION	HORZ CLEAR IF NOT STD	LOCATION	SIGN DTL. SHT. NO.	ITEM NO. 9031010	POST NO. 1	POST NO. 2	TOTAL ITEM NO. 90312/1A	DRIVEN 12- GA. ITEM NO 9031273A	DRIVEN 7-GA. ITFM NO 9031273A	CONCRETE 7-GA. ITFM NO 9(31274	POST NO.	POST NO. 2	ITEM NO.	DRIVEN 7-GA. ITFM NO 9031281A	7-GA. 1TFM NO 9031285	ITEM NO. 9031241
						110.	CY	LF	LF	LF	EA	EA	EA	LF	LF	LF	EA	EA	EA
6	1	48x48	13+78 RT		INTERSTATE 55	1	0.36	- 1	7										
6	2	48x48	13+81 LT		INTERSTATE 55		0.36				1				1/				
9	1	48x48	13+59 LT		W OUTER RD		0.36									1			
9	2	48x48	13+59 RT		W OUTER RD		0.36	- 1	-	17.7	1 = 1					-			4 == =
9	3	48x48	16+24 RT		W OUTER RD		0.36												
9	4	48x48	16+33 LT		W OUTER RD		0.36												
10	5	ASSEMBLY	230+37 RT		W OUTER RD		0.36				1 7								
10	6	ASSEMBLY	231+22		W OUTER RD						1 2			1625		17	1		
10	7	48x48	231+32 LT		W OUTER RD		0.36												
10	8	48x48	232+83 LT		W OUTER RD		0.36	_ 1										1	II.
10	9	48x48	232+76 RT	7-	W OUTER RD	7	0.36												
23	3	ASSEMBLY	3+58 LT		1-70	-	0.36								-				
					SUBTOTAL		3.96	0	0	0	0	0	0	1625	0	17	1	0	0
					TOTAL		4.0	N/A	N/A	0	0	0	0	N/A	N/A	17	1	0	0

MOBILIZA	ATION (LS)
TOTAL	1

TRAFFIC CO	NTROL (LS)
TOTAL	1

				PAVEMENT	,		
OVERALL SHEET NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	TYPE 5 AGGREGATE FOR BASE (4 IN THICK)	CONCRETE PAVEMENT (10 IN. NON-REINFORCED, 15 FT JOINTS)	
					(SQYD)	(SQYD)	
14	182+49	183+32	WOODSON RD	RT	1418	141.8	SIDE STREET
15	185+51	186+38	WOODSON RD	RT	1766	176.6	SIDE STREET
15	189+25	190+09	WOODSON RD	LT	1218	121.8	SIDE STREET
				SUBTOTAL	4401	440.1	
32	11+05		S OLD HWY 94	LT	278	278	SIDE STREET
				SUBTOTALS	2780	278	
				PAVEMENT SUBTOTAL	+718.1		*SEE SUBTOTAL SCHEDULI FOR FINAL PAY ITEM TOTAL
				TOTAL		718.1	

			STA	NDARD SIGN AS	SEMBLIES						SIGN SUMMARY						
					ТҮРЕ							-	-	17			
	ER				W11-2	M16-7PL	51-1	W16-9P						SIZE, TYPE & SQUARE FIET			
SIGN NUMBER	OVERALL SHEET NUMBER	STATION	LOCATION	ROADWAY	PEDESTRIAN SYNBOL 36" X 36"	45 DEGREE DIRECTONAL ARROW (LEF.) 30" X 18"	SCHOOL 36" X 36"	AHEAD 30" X :8"	ER OF B	acn	STANDARD SIGN OR SPECIAL SIGN NUMBER	SIGN DETAIL SHEET NO.	NO. EACH	SIZE (SQFT)	FLAT EIFET FLUORESCENT YELLOW GREEN ITEM NO. 903-50.69A(SQFT		
6	10	231+22	69' RT	W OUTER	1	1				7	W11-2		1	9.00	9.00		
1	29	969+76	307' RT	1-44			1	1		_	W16-7PL		5	3.75	18.75		
2	29	970+08	218' RT	1-44		1	1			_	S1-1		6	9.00	54		
3	29	970+23	149' RT	1-44		1	1			_	W16-9P		2	3.75	7.5		
4	29	971+31	190' LT	1-44		1	1										
5	29	971+86	241' LT	1-44		1	1										
6	29	972+52	185' LT	1-44			1	1	= =						11-		
_		-		TOTAL	1	5	6	2	0	0				TOTAL	90		

NUMBER
3. PE-2003001103
SONAL
THIS SHEET HAS BEEN
IGNED, SEALED, AND DATED
ELECTRONICALLY

3/28/2024 VARIES MO

VARIES

PROJECT NO. BRIDGE NO.

J6P3510B

3

STL

		REMOVE AND	REPLACE PULL BO	DX .	
OVERALL SHEET NUMBER	STATION	ROADWAY	LOCATION	REMOVE AND REPLACE CONC. PULL BOX W/PREFORMED CLASS 2 PULL BOX	REMARKS
				(EA)	
17	170+60	I-70 (McKelvey)	RIGHT	1	
17	173+03	I-70 (McKelvey)	RIGHT	1	
			SUBTOTALS	2	
			TOTAL	2	

		neino vi	AND RELOCATE EXISTING	Choone in	CONT. STOR	
OVERALL SHEET NUMBER	SIGN NO.	STATION	ROADWAY	LOCATION	REMOVE AND RELOCATE EXISTING GROUND MOUNT SIGN	REMARKS
				y	(EA)	
6	1	13+78	IMPERIAL MAIN	RT	1	R1-2
6	2	13+81	IMPERIAL MAIN	LT	1	R1-2
				SUBTOTAL	2	
9	1	13+59	IMPERIAL MAIN	LT	1	R1-2
9	2	13+59	IMPERIAL MAIN	RT	1	R1-2
9	3	16+24	IMPERIAL MAIN	LT	1	R1-2
9	4	16+33	IMPERIAL MAIN	RT	1	R1-2
10	5	230+37	W OUTER RD	RT	1	R10-15R
10	7	231+32	W OUTER RD	LT	1	R1-2
10	8	232+83	W OUTER RD	LT	1	R1-2
10	9	232+76	W OUTER RD	RT	1	R1-2
				SUBTOTAL	8	
23	1	1+44	JENNINGS STATION RD	RT	1	R1-1
23	2	3+58	JENNINGS STATION RD	LT	2	DUAL MOUNTED SIGN
				SUBTOTAL	1	
				TOTAL	11	

T	ADJUST TO GRADE													
OVERALL SHEET NUMBER	STATION	ROADWAY	LOCATION	ADJUST PULL BOX TO GRADE	ADJUST WATER VALVE TO GRADE	REMARKS								
				(EA)	(EA)									
17	169+46	I-70 (McKelvey)	RIGHT		1	valve								
19	529+47	I-70 (N Hanley)	RIGHT	1										
			SUBTOTALS	1	1									
32	11+39	MO 94 S OLD 94	LT		1									
			SUBTOTALS	0	1									
			TOTAL	1	2									

		100	DRIVEWAYS			
OVERALL SHEET NUMBER	CENTERLINE STATION	ROADWAY	LOCATION	TYPE 5 AGGREGATE FOR BASE (4 IN. THICK)	PAVED APPROACH 8 IN.	REMARKS
				(SQYD)	(SQYD)	
13	172+41	WOODSON RE	RT	129.8	129.8	
13	173+01	WOODSON RE	LT	22.4	22.4	
13	173+21	WOODSON RE	RT	126.1	125.1	
14	183+04	WOODSON RC	LT	22.4	22.4	,
15	189+77	WOODSON RE	RT	63.9	63.9	
15	190+62	WOODSON RE	RT	27.9	27.9	
			SUBTOTALS	392.6	392.6	
			DRIVEWAYS SUBTOTAL	*392.6		*SEE SUBTOTAL SCHEDULE FOR FINAL PAY ITEM TOTAL
		S 10	TOTAL		392.6	

SCHEDULE OF QUANTITIES

SHEET 1 OF 10

								SIDEWALK						
PLAN SHEET		i i					SUGGESTED ADA	ing.	SIDEWALK EDGE	CONCRETE CURB	CONCRETE SIDEWALK, 4	CONCRETE	TYPE 5 AGGREGATE	
NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	SIDEWALK LENGTH		RAMP TYPE	TRUNCATED DOMES		RAMP	IN.	SIDEWALK, 7 IN.	FOR BASE (4 IN. THICK)	REMARKS
-		* 14.44		-	(FT)	(FT)		(SF)	(LF)	(EA)	(SQYD)	(SQYD)	(SQYD)	
6	13+05	13+11	IMPERIAL MAIN	RT			C2			1	1			I-55 NB OFF RAMP SWC; LANDING ONLY
7	10+01 16+01	10+06 15+12	CHURCH RD	LT RT	5	5	C4	10		1	3	7	3	OZARK NWC MICHIGAN AVE SEC
	10.01	10.12	CHORCIND	- Ki				10		-				WICHIGATAVESEC
						7	SUBTOTALS	10	0	2	3	0	3	
9	12+29	12+34	IMPERIAL MAIN	RT			C2			1				W OUTER RD SWC; LANDING ONLY
9	12+30	12+35	IMPERIAL MAIN	LT			C2			1				W OUTER RD NWC; LANDING ONLY
9	12+37	12+44	IMPERIAL MAIN	LT			C2			1				W OUTER RD NWC; LANDING ONLY
9	12+51	12+58	IMPERIAL MAIN	RT			C2	h		1		1		W OUTER RD SWC; LANDING ONLY
9	16+94	17+04	IMPERIAL MAIN	LT			C1	10		1				I-55 SB OFF RAMP NEC
9	17+05	17+62	IMPERIAL MAIN	RT			C6	14		1		20	20	I-55 SB ON RAMP SEC
10	231+47 231+48	231+64 231+66	W OUTER RD W OUTER RD	RT LT			C9	30				13	20	ISLAND CUT THROUGH ISLAND CUT THROUGH
10	232+46	232+59	W OUTER RD	LT			C9	30				13	13	ISLAND CUT THROUGH
10	232+47	232+56	W OUTER RD	RT			C9	30				11	11	ISLAND CUT THROUGH
10	232+61	232+71	W OUTER RD	LT	10	6		1 = =			5		5	VOGEL RD NWC
10	232+64	232+71	W OUTER RD	RT	13	6					6		6	VOGEL RD NEC
10	232+66	232+79	W OUTER RD	LT			C2	10		i				VOGEL RD NWC
10	232+67	232+82	W OUTER RD	RT			C2	11		1				VOGEL RD NEC
10	232+78	232+84	W OUTER RD	RT	10	6			/	1 1 1 1	5		5	VOGEL RD NEC
10	232+77	232+86	W OUTER RD	LT	11	6					5		5	VOGEL RD NWC
_							State at the	1,000						
13	150.25	100.13	WOODSON RD	LT			SUBTOTALS C6	165	0	8	21	57	78	RED, STAMPED, YELLOW DOMES
12	160+26 160+68	160+43 160+85	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
12	161+24	161+39	WOODSON RD	LT			C6	1		1				RED, STAMPED, YELLOW DOMES
12	161+63	161+75	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
12	161+75	161+87	WOODSON RD	LT			C5	14		1				RED, STAMPED, YELLOW DOMES
12	162+46	162+58	WOODSON RD	LT			C5	12.		1				RED, STAMPED, YELLOW DOMES
12	162+72	162+90	WOODSON RD	RT		1	C2			1				RED, STAMPED, YELLOW DOMES
12	163+17	163+40	WOODSON RD	LT		A =	C6			1				RED, STAMPED, YELLOW DOMES
12	163+36	163+58	WOODSON RD	RT	n — — — i		C6	.11		1				RED, STAMPED, YELLOW DOMES
12	163+80	163+98	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
13	167+11	167.29	WOODSON RD	RT			C2	16		1				RED, STAMPED, YELLOW DOMES
13	172+02 172+66	172+17 172+81	WOODSON RD WOODSON RD	RT RT			C6 C5			1				RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES
13	172+80	172+91	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
13	173+10	173+21	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
13	172+81	172+96	WOODSON RD	RT			C5			1				RED, STAMPED, YELLOW DOMES
13	173+46	173+61	WOODSON RD	RT			C6	12 2		1				RED, STAMPED, YELLOW DOMES
13	173+94	174+08	WOODSON RD	RT	p 1	/	C6			1				RED, STAMPED, YELLOW DOMES
13	174+54	174+68	WOODSON RD	RT	_ == :		C6			1				RED, STAMPED, YELLOW DOMES
14	175+35	175+50	WOODSON RD	RT			C6			1				RED, STAMPED, YELLOW DOMES
14	175+93	176+09	WOODSON RD	RT			C6			1				RED, STAMPED, YELLOW DOMES
14	176+28	176+42	WOODSON RD	RT			C6			1				RED, STAMPED, YELLOW DOMES
14	176+62 177+04	176+76 177+18	WOODSON RD WOODSON RD	RT			C6			1				RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES
14	177+04	177+18	WOODSON RD	RT		,	C6			1				RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES
14	177+77	177+92	WOODSON RD	RT			C6			1			-	RED, STAMPED, YELLOW DOMES
14	179+23	179+49	WOODSON RD	RT		1	C6	34		1				RED, STAMPED, YELLOW DOMES
14	179+90	180+16	WOODSON RD	RT			C6	34		1				RED, STAMPED, YELLOW DOMES
14	182+44	182+67	WOODSON RD	RT			C6	34		1				RED, STAMPED, YELLOW DOMES
14	183+14	183+36	WOODSON RD	RT			C6	34		1			J	RED, STAMPED, YELLOW DOMES
14	182+83	182+94	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
14	183+13	183+24	WOODSON RD	LT			C6	des di		1				RED, STAMPED, YELLOW DOMES
14	183+52	183+63	WOODSON RD	LT			C6	A		1				RED, STAMPED, YELLOW DOMES
14	183+82	183+93	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
15	185+46	185+70	WOODSON RD	RT	4		C6	35		1				RED, STAMPED, YELLOW DOMES
15	186+20	186+43	WOODSON RD	RT LT			C6	35		1				RED, STAMPED, YELLOW DOMES
15	187+07 187+69	187+20 187+82	WOODSON RD WOODSON RD	RT			C6			1				RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES
15	187+71	187+84	WOODSON RD	LT			C6			1				RED, STAMPED, YELLOW DOMES
15	189+20	189+46	WOODSON RD	LT			C6	36		1				RED, STAMPED, YELLOW DOMES
15	189+23	189+49	WOODSON RD	RT	26	6					16.67		16.67	RED, STAMPED, YELLOW DOMES
				RT			C6			1	_			RED, STAMPED, YELLOW DOMES



3/28/2024 VARIES MO STL

VARIES J6P3510B

PROJECT NO.

BRIDGE NO.

SCHEDULE OF QUANTITIES

SHEET 2 OF 10

	SIDEWALK E	GRINDING (LF) 0 5.0 10.0 6.0 6.0 6.0	NOCRETE CURB S RAMP (EA) 1 1 1 1 1 1 1 1 1 1 1 1 1	CONCRETE SIDEWALK, 4 IN. (SQYD) 26.3 43.0 55.4 12.8 12.6 4.4 10.8 11.3 9.1 2.8 10.0 8.3	CONCRETE SIDEWALK, 7 IN. (SQYD)	TYPE 5 AGGREGATE FOR BASE (4 IN. THICK) (SQYD) 26.30 42.97 55.44 12.78 12.56 4.44 10.78 4.33 9.11 2.78 14.11 10 8.33 17 26 4	RED, STAMPED, YELLOW DOMES
NUMBER PROMETATION TO STATION ROMOVAY CANTON SIDEWALK INFORM SIDEWALK WIDTH RAMPPINE RUMCATED	0 5.0 10.0 6.0 6.0	GRINDING (LF) 0 5.0 10.0 6.0 6.0 6.0	1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	1N. (SQYD) 26.3 43.0 55.4 12.8 12.6 4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	SIDEWALK, 7 IN. (SQYD)	600 BASE (4 IN. THICK) (SQYD) 26.30 42.97 55.44 12.78 12.56 4.44 10.78 4.33 9.11 2.78 14.11 10 8.33	RED, STAMPED, YELLOW DOMES
15	5.0 10.0	5.0 10.0	1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	(SQYD) 26.3 43.0 55.4 12.8 12.6 4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	(SQYD)	(SQYD) 2630 42.97 55.44 12.78 12.56 4.44 10.78 4.33 9.11 10 8.33 17 26 4	RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES
15	5.0	6.0 6.0 6.0	1 1 1 1 1 1 1 1 1 1	43.0 55.4 12.8 12.6 4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14,11	42.97 55.44 12.78 12.56 4.44 10.78 4.33 9.11 2.78 14.11 10 8.33	RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES
15	5.0	6.0 6.0 6.0	1 1 1 1 1 1 1 1 1 1	43.0 55.4 12.8 12.6 4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14,11	42.97 55.44 12.78 12.56 4.44 10.78 4.33 9.11 2.78 14.11 10 8.33	RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES
15	5.0	6.0 6.0 6.0	1 1 1 1 1 1 1	43.0 55.4 12.8 12.6 4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14,11	42.97 55.44 12.78 12.56 4.44 10.78 4.33 9.11 2.78 14.11 10 8.33	RED, STAMPED, YELLOW DOMES RED, STAMPED, YELLOW DOMES
15	5.0	6.0 6.0 6.0	1 1 1 1 1 1 1	12.8 12.6 4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14,11	55,44 12,78 12,56 4,44 10,78 4,33 9,11 2,78 14,11 10 8,33	RED, STAMPED, YELLOW DOMES
17	5.0	6.0 6.0 6.0	1 1 1 1 1 1	12.8 12.6 4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14,11	55,44 12,78 12,56 4,44 10,78 4,33 9,11 2,78 14,11 10 8,33	
17	5.0	6.0 6.0 6.0	1 1 1 1 1 1	12.8 12.6 4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14,11	55,44 12,78 12,56 4,44 10,78 4,33 9,11 2,78 14,11 10 8,33	ISLAND CUTTHROUGH
17	5.0	6.0 6.0 6.0	1 1 1 1 1 1	12.8 12.6 4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14,11	55,44 12,78 12,56 4,44 10,78 4,33 9,11 2,78 14,11 10 8,33	ISLAND CUTTHROUGH
17	6.0	6.0	1 1 1 1 1	12.8 12.6 4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14.11	12.78 12.56 4.44 10.78 4.33 9.11 2.78 14.11 10 8.33	ISLAND CUTTHROUGH
17	6.0	6.0	1 1 1 1 1	12.6 4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14.11	12.56 4.44 10.78 4.33 9.11 2.78 14.11 10 8.33	ISLAND CUTTHROUGH
17 172+11 172+20 1-70 LT 9 5	6.0	6.0	1 1 1 1 1	4.4 10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14.11	4.44 10.78 4.33 9.11 2.78 14.11 10 8.33	ISLAND CUTTHROUGH
17	6.0	6.0	1 1 1 1 1	10.8 4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14.11	10.78 4.33 9.11 2.78 14.11 10 8.33 17 26 4	ISLAND CUTTHROUGH
17	6.0	6.0	1 1 1 1 1	4.3 9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14.11	4.33 9.11 2.78 14.11 10 8.33	ISLAND CUTTHROUGH
17	6.0	6.0	1 1 1 1 1	9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14.11	9.11 2.78 14.11 10 8.33 17 26 4	ISLAND CUTTHROUGH
18	6.0	6.0	1 1 1	9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14.11	9.11 2.78 14.11 10 8.33 17 26 4	ISLAND CUTTHROUGH
19	6.0	6.0	1 1 1	9.1 2.8 10.0 8.3 17.3 26.0 4.0 8.7	14.11	9.11 2.78 14.11 10 8.33 17 26 4	ISLAND CUTTHROUGH
19	6.0	6.0	1 1 1	10.0 8.3 17.3 26.0 4.0 8.7	14.11	2.78 14.11 10 8.33 17 26 4	ISLAND CUTTHROUGH
19	6.0	6.0	1 1 1	10.0 8.3 17.3 26.0 4.0 8.7	14.11	2.78 14.11 10 8.33 17 26 4	ISLAND CUTTHROUGH
19	6.0	6.0	1 1 1	10.0 8.3 17.3 26.0 4.0 8.7	1411	14.11 10 8.33 17 26 4	ISLAND CUTTHROUGH
20	6.0	6.0	1	10.0 8.3 17.3 26.0 4.0 8.7	1411	14.11 10 8.33 17 26 4	ISLAND CUTTHROUGH
20 529+22 529+27 1-70	6.0	6.0	1	17.3 26.0 4.0 8.7	1411	10 8.33 17 26 4	ISLAND CUTTHROUGH
20	6.0	6.0	1	17.3 26.0 4.0 8.7		17 26 4	
20	6.0	6.0	1	17.3 26.0 4.0 8.7		17 26 4	
21	6.0	6.0	1	17.3 26.0 4.0 8.7		17 26 4	
21 590+67	6.0	6.0	1	26.0 4.0 8.7		26 4	
21	_	6.0	1	26.0 4.0 8.7		26 4	
23 1+35 1+60 1-70 RT 26 6 23 1+46 1+86 1-70 LT 39 6 23 1+86 1+97 1-70 LT 13 6 23 1+97 2+08 1-70 LT 15 6 C3 10.2 23 2+08 2+15 1-70 LT 15 6 C3 10.2 23 2+62 2+74 1-70 LT 15 5 C3 10 23 2+74 3+23 1-70 LT 50 5 C3 10 23 1+60 1+77 1-70 RT 15 6 C3 11.5 23 3+41 3+55 1-70 RT varies varies C9 19 23 3+42 3+47 1-70 LT 25 5 5 23 3+43 3+47 1-70 LT 25<	6.0		1	26.0 4.0 8.7		26 4	
23			1	26.0 4.0 8.7		26 4	
23			1	8.7		4	:
23 1+97 2+08 1-70 LT 13 6 C3 10.2 23 2+08 2+15 1-70 LT 15 6 C3 10.2 23 2+62 2+74 1-70 LT 15 5 C3 10 23 2+74 3+23 1-70 LT 50 5 5 23 1+60 1+77 1-70 RT 15 6 C3 11.5 23 3+41 3+55 1-70 RT varies varies C9 19 23 0+52 0+58 1-70 LT 8 2 2 23 3+23 3+47 1-70 LT 25 5 5 23 3+47 3+57 1-70 LT 25 5 30 22 12+40 12+76 1-70 LT C9 30 22 12+48 12+68 1-70 R			1	8.7			
23 2+08 2+15 I-70 LT 15 6 C3 10.2 23 2+62 2+74 I-70 LT 15 5 C3 10 23 2+74 3+23 I-70 LT 50 5 23 1+60 1+77 I-70 RT 15 6 C3 11.5 23 3+41 3+55 I-70 RT varies varies C9 19 23 0+52 0+58 I-70 LT 8 2 23 3+43 3+47 I-70 LT 25 5 23 3+47 3+57 I-70 LT C3 10 22 12+40 12+76 I-70 RT C9 30 22 12+48 12+68 I-70 RT C9 30 22 13+24 13+43 I-70 RT			1				
23 2+62 2+74 1-70 LT 15 5 C3 10 23 2+74 3+23 1-70 LT 50 5 23 1+60 1+77 1-70 RT 15 6 C3 11.5 23 3+41 3+55 1-70 RT varies varies C9 19 23 0+52 0+58 1-70 LT 8 2 23 3+23 3+47 1-70 LT 25 5 23 3+47 3+57 1-70 LT C3 10 22 12+40 12+76 1-70 RT C9 30 22 12+48 12+68 1-70 RT C9 30 22 13+24 13+43 1-70 RT C9 30 25 10+54 11+98 SWOODS MILL LT 149 6<			1	27.0			
23 2+74 3+23 I-70 LT 50 5 23 1+60 1+77 I-70 RT 15 6 C3 11.5 23 3+41 3+55 I-70 RT varies varies C9 19 23 0+52 0+58 I-70 LT 8 2 2 23 3+23 3+47 I-70 LT 25 5 5 23 3+47 3+57 I-70 LT C3 10 22 12+40 12+76 I-70 LT C9 30 22 12+48 12+68 I-70 RT C9 30 22 13+24 13+43 I-70 RT C9 30 22 13+24 13+43 I-70 RT C9 30 25 10+54 SWOODS MILL LT 149 6 6 25 10+54 13+58 SWOODS				27.0			
23 3+41 3+55 1-70 RT varies varies C9 19 23 0+52 0+58 1-70 LT 8 2 23 3+23 3+47 1-70 LT 25 5 23 3+47 3+57 1-70 LT 25 5 22 12+40 12+76 1-70 LT C9 30 22 12+48 12+68 1-70 RT C9 30 22 13+24 13+43 1-70 RT C9 30 25 10+54 11+98 \$WOODS MILL LT C3 12 25 10+54 13+58 \$WOODS MILL LT 149 6 25 10+54 13+58 \$WOODS MILL LT 5 6 26 119 27 12+34 12+39 \$WOODS MILL LT 5 6 28 119 29 19 20 20 21 20 21				4/.0		28	-
23			1				
23 3+23 3+47 1-70 LT 25 5 5					5	5	
23 3+47 3+57 1-70 LT		11 11			2	2	100
12+40 12+76 1-70 LT C9 30		10 100		13.1		13.11	
22 12+48 12+68 1-70 RT C9 30 22 13+24 13+43 1-70 RT C9 30 SUBTOTAL 227.7 25 10+47 10+54 \$ WOODS MILL LT C3 12 25 10+54 11+98 \$ WOODS MILL LT 149 6 25 10+54 13+58 \$ WOODS MILL LT 5 6 25 14+34 14+39 \$ WOODS MILL LT 5 6			1				
22 13+24 13+43 1-70 RT C9 30 SUBTOTAL 227.7 25 10+47 10+54 SWOODS MILL LT C3 12 25 10+54 11+98 SWOODS MILL LT 149 6 25 10+54 13+58 SWOODS MILL LT 5 6 25 14+34 14+39 SWOODS MILL LT 5 6					32.33	32.33	
SUBTOTAL 227.7					15.78	15.78	
25 10+47 10+54 SWOODSMILL LT C3 12 25 10+54 11+98 SWOODSMILL LT 149 6 25 10+54 13+58 SWOODSMILL LT 5 6 25 14+34 14+39 SWOODSMILL LT 5 6	-				15.22	15.22	1
25 10+47 10+54 SWOODSMILL LT C3 12 25 10+54 11+98 SWOODSMILL LT 149 6 25 10+54 13+58 SWOODSMILL LT 5 6 25 14+34 14+39 SWOODSMILL LT 5 6	33.0	33.0	8	227.4	84.04	311.49	
25 10+54 11+98 S WOODS MILL LT 149 6 25 10+54 13+58 S WOODS MILL LT 5 6 25 14+34 14+39 S WOODS MILL LT 5 6	33.0		1	-4.74	54.54	32.73	WEST CURB RAMP
25 10+54 13+58 S WOODS MILL LT 5 6 25 14+34 14+39 S WOODS MILL LT 5 6				99.0		99	WEST SIDEWALK
25 14+34 14+39 SWOODSMILL LT 5 6				3.0		3	WEST SIDEWALK PANEL
				3.0	1.1	3	WEST SIDEWALK PANEL
			1				WEST CURB RAMP
SUBTOTALS 24	0	0	2	105.0	0	105	To the second se
27 10+35 10+50 CRAIGWOLD RD LT C6 29		-1-1	1				
27 10+50 10+69 CRAIGWOLD RD LT VARIES VARIES				11.7		11.7	
27 13+81 13+96 CRAIGWOLD RD LT VARIES VARIES				8.1		8.1	1. =
27 13+96 14+01 CRAIGWOLD RD LT C6 10			1				
					-		
SUBTOTALS 39	0	0	2.0	19.8		19.8	
29 970+04 970+11 I-44 RT 7 5				4.2		4.2	
29 970+06 970+15 I-44 RT C4 10			1				
29 971+22 971+34 I-44 LT 13 6 C9 14					7.4	7.4	
29 971+40 971+49 1-44 LT 14 6 C9 15 30 1051+65 1051+80 1-44 LT C1 11			1		7.7	7.7	17
30 1051+65 1051+80 1-44 LT C1 11 30 1051+75 1051+81 1-44 LT 8 6			1	3.6		3.6	
30 105145 1051481 1-44 LT 8 6				0.0		3.0	
30 1052+25 1052+25 1-44 LI 9 4 30 1051+72 1051+77 1-44 RT 6 5				4.0		4.0	



3/28/2024 VARIES MO SHEET NO. STL

VARIES

J6P3510B

PROJECT NO.

BRIDGE NO.

SCHEDULE OF QUANTITIES

SHEET 3 OF 10

							SIDI	EWALK (CONTINUED)						
PLAN SHEET NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	SIDEWALK LENGTH	SIDEWALK WIDTH	SUGGESTED ADA RAMP TYPE	TRUNCATED DOMES	SIDEWALK EDGE GRINDING	CONCRETE CURB	CONCRETE SIDEWALK, 4 IN.	CONCRETE SIDEWALK, 7 IN.	TYPE 5 AGGREGATE FOR BASE (4 IN. THICK)	REMARKS
		5 = 1 ⁶			(FT)	(FT)		(SF)	(LF)	(EA)	(SQYD)	(SQYD)	(SQYD)	
30	1052+23	1052+29	1-41	RT	VARIES	VARIES		X			3.4		3.4	
30	1052+24		1-41	RT					5.0					
			_				SUBTOTAL	50.0	5.0	2.0	18.6	15.1	33.7	
32	10+62	10+77	OLD HWY 94	LT			C4	17		1				HOLLOWBROOK DR SWC
32	11+32	11+44	OLD HWY 95	ц			C4	12		1				HOLLOWBROOK DR NWC
							SUBTOTAL	29	0	2	0	0	0	
34	41+88	42+00	N OUTER RD	LT			C1	11	, =	1				SHADY SPRINGS LN NWC
							SUBTOTAL	11	0	1	0	0	0	
							DECORATIVE SUBTOTALS	292		45	43.0			RED, STAMPED, YELLOW DOMES
							SUBTOTAL	*848				*156.2	*593.9	*SEE SUBTOTAL SCHEDULE FOR FINA PAY ITEM TOTAL
							TOTALS		38.0	27	394.8			

		SUBTOTALS	<u> </u>				
APPLICATION	TRUNCATED DOMES	TYPE 5 AGGREGATE FOR BASE (4 IN THICK)	CONCRETE SIDEWALK, 7 IN				
	(SF)	(SQYD)	(SQYD)				
ISLAND & MEDIAN	*30.0	*29.2	*9.7				
PAVEMENT		*718.1					
DRIVEWAYS		*392.6					
SIDEWALK	*848	*593.9	*156.2				
TOTAL	878	1734	165.9				

VEDALL SHEET					LINEAD CDADING	
NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	LINEAR GRADING, CLASS 2 MODIFIED, STA	REMARKS
6	13+05	13+11	1-55	RT	0.10	I-55 NB OFF RAMP SWC RAMP
7	10+01	10+06	1-55	LT	0.50	OZARK NWC SIDEWALK
7	16+01	16+12	1-55	RT	0.10	MICHIGAN AVE SEC RAMP
				SUBTOTALS	0.70	
9	12+29	12+35	1-55 W. OUTER RD	RT	0.06	W OUTER RD SWC LANDING
9	12+30	12+35	I-55 W. OUTER RD	LT	0.07	W OUTER RD NWC LANDING
9	12+37	12+44	1-55 W. OUTER RD	LT	0.08	W OUTER RD NWC LANDING
9	12+51	12+58	1-55 W. OUTER RD	RT	0.07	W OUTER RD SWC LANDING
9	16+94	17+04	IMPERIAL MAIN	LT	0.06	I-55 SB OFF RAMP
9	17+05	17+62	IMPERIAL MAIN	RT	0.06	I-55 SB ON RAMP
10	232+61	232+86	1-55 V/, OUTER RD	ĹT	0.37	VOGEL NWC
10	232+64	232+84	I-55 W. OUTER RD	RT	0.37	VOGEL NEC
30 at 1			1	SUBTOTALS	1.40	
12	160+26	160+43	WOODSONRD	LT	0.18	
12	160+68	160+85	WOODSONRD	LT	0.18	
12	161+24	161+39	WOODSONRD	LT	0.15	1 1 1 0
12	161+63	161+87	WOODSONRD	LT	0.25	
12	162+46	162+58	WOODSONRD	LT	0.13	
12	162+72	162+90	WOODSONRD	RT	0.15	
12	163+17	163+40	WOODSONRD	LT	0.22	
12	163+36	163+58	WODDSONRD	RI	0.22	
12	163+80	163+98	WODDSONRD	LT	0.18	
13	167+11	167+29	WODDSONRD	RT	0.16	
13 13	172+02 172+80	173+61 173+21	WODDSON RD WODDSON RD	RT LT	1.73 0.48	
13	173+94	174+08	WODDSONRD	RT	0.15	
13	174+54	174+68	WOODSONRD	RT	0.15	
14	175+35	175+50	WOODSONRD	RT	0.14	
14	175+93	176+09	WOODSONRD	RT	0.15	
14	176+28	176+42	WOODSON RD WOODSON RD	RT	0.15	
14	176+62 177+04	176+76 177+18	WODDSONRD	RT RT	0.15 0.15	
14	177+31	177+46	WOODSONRD	RT	0.15	
14	177+77	177+92	WOODSONRD	RT	0.15	PACE TO THE PACE T
14	179+23	179+49	WOODSONRD	RT	0.24	
14	179+90	180+16	WOODSONRD	RT	0.24	
14	182+44	183+37	WODDSONRD	RT	1.17	t o
14	182+83	183+24	WODDSONRD	LT	0.41	
14	183+52	183+63	WODDSONRD	LT	0.11	
14	183+82	183+93	WOODSONRD	LT	0.11	
15	185+46	186+43	WOODSONRD	RT	1.37	
15	187+07	187+20	WODDSONRD	LT	0.13	13
15	187+69	187+82	WOODSONRD	RT	0.13	
15	187+71	187+84	WOODSONRD	LT	0.13	
15	189+20	190+14	WOODSONRD	LT	1.10	
	189+23	190+78	WOODSONRD	RT	1.51	

SUBTOTAL

VERALL SHEET NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	LINEAR GRADING, CLASS 2 MODIFIED, STA	REMARKS
17	169+29	169+83	1-70 & McKelvey	RT	0.75	
17	170+03	170+28	1-70 & McKelvey	RT	0.42	
17	170+54	170+65	1-70 & McKelvey	RT	0.27	
19	529+43	529+49	1-70 & N Hanley	RT	0.15	
19	529+53	529+59	1-70 & N Hanley	RT	0.13	
19	530+30	530+35	1-70 & N Hanley	RT	0.05	
20	529+22	-	1-70 & N Hanley	LT	0.10	
20	529+23		1-70 & N Hanley	LT	0.15	
20	530+16		1-70 & N Hanley	LT	0.15	
22	12+40	12+76	Jennings Sta Rd	LT	0.60	
23	3+23	3+57	Jennings Sta Rd	LT	0.33	
				SUBTOTAL	3.10	
25	10+47	11+98	S WOODS MILL	LT	1.60	WEST SIDEWALK AND CURB RAMP
25	13+54	13+61	S WOODS MILL	LT	0.10	WEST SIDEWALK
25	14+34	14+39	S WOODS MILL	LT	0.10	WEST SIDEWALK
25	15+46	15+53	S WOODS MILL	LT	0.10	WEST CURB RAMP
				SUBTOTAL	1.90	
27	10+35	10+69	CRAGWOLD RD	LT	0.32	
27	13+81	14+01	CRAGWOLD RD	LT	0.20	
				CURTOTAL	0.52	
				SUBTOTAL		
29	970+08	970+15	1-44	RT	0.26	
30	1051+65	1051+75	1-44	LT	0.16	
30	1051+72		1-44	RT	0.06	
30	1052+29		(-44	RT	0.00	
30	1052+29		1-44	LT	0.09	
					1	
_			1	SUBTOTAL	0.66	
32	10+62	10+77	S OLD HWY 94	LT	0.10	HOLLOWBROOK DR SWC RAMP
32	11+05		S OLD HWY 94		0.30	
32	11+32	11+44	S OLD HWY 94	LT	0.20	HOLLOWBROOK DR NWC RAMP
				SUBTOTAL	0.60	
34	41+88	42+00	SHADY SPRINGS	LT	0.10	SHADY SPRINGS NWC RAMP
	7 1 1 1 m			SUBTOTAL	0.10	
				2-7-00:05		

EARTHWORK (CONTINUED)

DISCLAIMER
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PE-2003001103 SONAL SONA

3/28/2024 VARIES MO SHEET NO. STL 3

VARIES J6P3510B

PROJECT NO.

BRIDGE NO.

SCHEDULE OF QUANTITIES

			TEMPOR	ARY EROSION CON	TROL			
OVERALL SHEET NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	CURB INLET CHECK	SEDIMENT REMOVAL	SILT FENCE	REMARKS
6	23+02	23+13	IMPERIAL MAIN	RT	(EA)	(CUYD) 0.2	(LF) 21	
7	10+07	25,15	CHURCH	LT	1	1		
7	16+16		CHURCH	RT	1	1		
						- 55		
				SUBTOTALS	2	2.2	21	
9	12+26	12+38	IMPERIAL MAIN	RT		0.3	30	
9	12+27	12+46	IMPERIAL MAIN	LT		0.3	26	
9	12+48	12+59	IMPERIAL MAIN	RT		0.2	23	
10	232+59	232+90	W OUTER RD W OUTER RD	LT RT	1	0.5	47 46	
10	232+65	232+87	WOOTERNO	KI.		0.5	46	
				SUBTOTALS	0	1.8	172	
12	160+25		WOODSON RD	LT	1	1.0	1/2	
12	160+26	160+43	WOODSON RD	LT	1	0.2	18	
12	160+68	160+85	WOODSON RD	LT		0.2	18	
12	161+24	161+39	WOODSON RD	LT		0.2	15	
12	161+63	161+87	WOODSON RD	LT		0.3	25	
12	162+46	162+58	WOODSON RD	LT		0.1	13	
12	162+72	162+90	WOODSON RD	RT		0.2	15	
12	163+17	163+40	WOODSON RD	LT	100	0.2	22	
12	163+36	163+58	WOODSON RD	RT		0.2	22	
12	163+80	163+98	WOODSON RD	LT		0.2	18	
13	16/+11	167+29	WOODSON RD	RT	. .	0.2	16	
13	172+02 172+02	173+61	WOODSON RD	RT	1	1.7	173	
13	172+02	1/3701	WOODSON RD	LT	1	1.7	1/3	
13	172+80		WOODSON RD	LT	1			
13	172+80	173+21	WOODSON RD	LT		0.5	48	
13	172+83		WOODSON RD	RT	1			
13	173+33		WOODSON RD	LT	1			
13	173+59		WOODSON RD	RT	1			
13	173+94	174+08	WOODSON RD	RT		0.2	15	
13	174+54	174+68	WOODSON RD	RT	4	0.2	15	
14	175+35	175+50	WOODSON RD	RT		0.1	14	
14	175+93	176+09	WOODSON RD	RT		0.2	15	
14	176+28	176+42	WOODSON RD	RT		0.2	15	
14	176+62	176+76	WOODSON RD	RT		0.2	15	
14	177+04	177+18	WOODSON RD	RT		0.2	15	
14	177+31	177+46	WOODSON RD	RT		0.2	15	
14 14	177+77	177+92 179+49	WOODSON RD WOODSON RD	RT		0.2	15 24	
14	179+23 179+90	180+16	WOODSON RD	RT		0.2	24	_
14	182+44	183+37	WOODSON RD	RT		1.2	117	
14	182+83	183+24	WOODSON RD	LT		0.4	41	
14	183+28	103124	WOODSON RD	LT	1	0.4	7.	
14	183+52	183+63	WOODSON RD	LT		0.1	11	
14	183+82	183+93	WOODSON RD	LT	11 - 4 11	0.1	11	
14	184+86		WOODSON RD	LT	1			
15	185+18		WOODSON RD	LT	1			
15	185+46	186+43	WOODSON RD	RT		1.4	137	
15	185+64		WOODSON RD	LT	1			
15	185+92		WOODSON RD	LT	1			
15	186+42		WOODSON RD	RT	1			
15	187+07	187+20	WOODSON RD	LT		0.1	13	
15	187+09	407.00	WOODSON RD	LT	1		42	
15	187+69	187+82	WOODSON RD WOODSON RD	RT LI		0.1	13	
15 15	187+71 187+89	187+84	WOODSON RD	LT	1	0.1	13	
15	189+01		WOODSON RD	LT	1			
15	189+20	189+46	WOODSON RD	LT		0.2	24	
15	189+23	190+78	WOODSON RD	RT	San Sand	1.5	151	53.00
15	189+36		WOODSON RD	RT	1			
15	189+43		WOODSON RD	LT	1			
15	189+91		WOODSON RD	LT	1			
15	189+89	190+14	WOODSON RD	LT		0.2	23	
15	190+15		WOODSON RD	LT	1			
15	191+16		WOODSON RD	RT	1			
							2,00	
17	100.20	160.00	1.70 9 54-4-1	SUBTOTALS	21	11.4	1139	
17	169+29 170+54	169+83	I-70 & McKelvey		1	0.8	75	
17	170+54		I-70 & McKelvey I-70 & McKelvey		1			
17	173+12		1-70 & McKelvey		1			
18	268+87		1-70 & Fee Fee		1			
19	529+42		1-70 & N Hanley	RT		0.2	15	
19	529+53		1-70 & N Hanley	RT		0.1	13	
19	529+57		I-70 & N Hanley	RT	1			
19	530+32	_ = = =	I-70 & N Hanley	RT	1			
19	530+36		I-70 & N Hanley	RT		0.1	5	
20	529+21		I-70 & N Hanley	LT		0.1	10	
20	529+23		I-70 & N Hanley	LT		0.2	15	
20	529+34		I-70 & N Hanley	LT	1			
20	529+49		I-70 & N Hanley	LT				
20	530+17		1-70 & N Hanley	LT		0.2	15	
22	12+03		Jennings Sta Rd	LT	1			
22	12+38	12.75	Jennings Sta Rd	LT	1	0.5	F2	
22	12+40	12+76	Jennings Sta Rd	LT	1	0.6	59	
22	12+62	13+48	Jennings Sta Rd	RT		0.7	72	

			TEMPORARY ER	OSION CONTROL (CONTINUED)			
OVERALL SHEET NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	CURB INLET CHECK	SEDIMENT REMOVAL	SILT FENCE	REMARKS
					(EA)	(CLYD)	(LF)	
23	3+23	3+56	Jennings Sta Rd	LT		0.3	33	
				SUBTOTALS	7	0.6	148	
25	10+47	12+00	S WOODS MILL	LT		1.6	160	
25	13+55	13+64	S WOODS MILL	LT		0.1	14	
25	14+31	14+42	S WOODS MILL	LT		0.1	14	
25	15+43	15+53	S WOODS MILL	LT		0.1	11.5	
				SUBTOTALS	0	1.9	199.5	
30	1052+23		1-44	RT	1			
				SUBTOTALS	1	0	0	
32	10+59	10+74	HOLLOWBROOK	LT	1	0.3	27	
32	11+37	11+47	HOLLOWBROOK	LT		0.3	31	
				SUBTOTALS	0	1	58	
				TOTALS	31	19	1738	

					REMOVAL OF IN	/IPROVEMENTS					
		STATION	STATION		LENGTH	WIDTH	AREA	AREA			
SHEET	ROAJWAY	(FROM)	(TO)	LOCATION	(FEET)	(FEET)	(SF)	(SY)	EACH	DESCRIPTION OF REMOVAL	
45	MC 141	95+94.00		RT					1	PEDESTRIAN PUSH BUTTON	
45	MC 141	95+93.51	98+01.21	RT/LT	430					CABLE, 16 AWG 2 CONDUCTOR	
7	CHURCH	10+01.00	10+06.00	LT				2.8		SIDEWALK	
7	CHURCH	16+01.00	16+12.00	RT				9		CURB RAMP	
10	W OLTER RD	231+47.00	231+64.00	RT			_	19.8	-	PARTIAL REMOVAL OF MEDIAN STRIP	
10	W OLTER RD	231+48.00	231+66.00	LT		1 = 1		13		PARTIAL REMOVAL OF MEDIAN STRIP	
10	W OLTER RD	232+41.93	232+55.82	LT					9	YIELD LINE TRIANGLES	
10	W OLTER RD	232+46.00	232+59.00	LT				13		PARTIAL REMOVAL OF MEDIAN STRIP	
10	W OLTER RD	232+47.00	232+56.00	RT				11		PARTIAL REMOVAL OF MEDIAN STRIP	
10	W OLTER RD	232+72.03	232+82.94	RT					9	YIELD LINE TRIANGLES	
47	W OLTER RD	231+55.79	231+65.78	LT	5					1" CONDUIT	
47	W OLTER RD	231+55.79	232+01.74	LT	1520					CABLE, 14 AWG 1 CONDUCTOR	
47	W OLTER RD	231+60.00		RT		. 4			2	PEDESTRIAN PUSH BUTTON AND SIGN	
47	W OLTER RD	231+60.00		LT					2	PEDESTRIAN PUSH BUTTON AND SIGN	
47	W OLTER RD	231+99.36	232+52.50	RT	1072					CABLE, 14 AWG 1 CONDUCTOR	
47	W OLTER RD	232+46.76	232+51.60	RT	5					1" CONDUIT	
47	W OLTER RD	232+49.67	232+49.90	LT	10				1 - 1 =	1"CONDUIT	
47	W OLTER RD	232+51.60	233+07.38	LT	1520					CABLE, 14 AWG 1 CONDUCTOR	
47	W OLTER RD	232+55.00		LT		7			2	PEDESTRIAN PUSH BUTTON AND SIGN	
47	W OLTER RD	232+56.00		RT		T.			2	PEDESTRIAN PUSH BUTTON AND SIGN	
VARIES	WOODSON	VARIES	VARIES	LEFT			7		18	CURBRAMPS	
VARIES	WOODSON	VARIES	VARIES	RIGHT					27	CURBRAMPS	
13	WOODSON	172+41		RIGHT	VARIES	WARIES	1168	129.8		PAVED APPROACH (DRIVE)	
13	WOODSON	173+01		LEFT	VARIES	WARIES	202	22.4		PAVED APPROACH (DRIVE)	
13	WOODSON	173+22		RIGHT	VARIES	VARIES	1135	126.1		PAVED APPROACH (DRIVE)	
14	WOODSON	182+49	183+26	RIGHT	VARIES	VARIES	1276	141.8		PAVED APPROACH (BOBB AVE)	
14	WOODSON	183+04	4 = +4-1	LEFT	VARIES	WARIES	202	22.4		PAVED APPROACH (DRIVE)	
15	WOODSON	185+51	186+38	RIGHT	VARIES	WARIES	1589	176.6		PAVED APPROACH (KATHYN DR)	
15	WOODSON	189+23	189+49	RIGHT	26	6	150	15.7		SIDEWALK	
15	WOODSON	189+25	190+09	LEFT	VARIES	WARIES	1096	121.8		PAVED APPROACH (HAROLD DR)	
15	WOODSON	189+77		RIGHT	VARIES	VARIES	575	63.9		PAVED APPROACH (DRIVE)	
15	WOODSON	190+06	190+46	RIGHT	39	6	237	26.3	1 111	SIDEWALK	
15	WOODSON	190+62		RIGHT	VARIES	VARIES	251	27.9		PAVED APPROACH (DRIVE)	
	70 1-70	-					parts 1				
17	1-70	169+29	169+83	RIGHT	94	VARIES	499	55.4		SIDEWALK	
17	1-70	170+03	170+19	RIGHT	23	5	115	12.8		SIDEWALK	
17	1-70	170+52	170+63	RIGHT	21	WARIES	113	12.6		SIDEWALK	
17	1-70	172+11	172+20	LEFT	9	5	40	4.4		SIDEWALK	
17	1-70	172+94	173+09	LEFT	20	5	97	10.8		SIDEWALK	
17	1-70	170+14	170+28	RIGHT					1	CURBRAMP	
17	1-70	170+52	170+65	RIGHT					1	CURBRAMP	
18	1-70	268+77	268+86	RIGHT	6	VARIES	39	4.3		SIDEWALK	
19	1-70	529+43	529+49	RT	15	5.5	82	9.1		SIDEWALK	
		500.50	529+59	RT					1	CURBRAMP	
19	1-70	529+53 530+30	530+35	RT	5	5	25	2.8	*	SIDEWALK	



THIS SHEET HAS BEEN IGNED, SEALED, AND DATED ELECTRONICALLY

3/28/2024 VARIES MO SHEET NO. STL 3

> VARIES J6P3510B

PROJECT NO.

BRIDGE NO.

SCHEDULE OF QUANTITIES

SHEET 5 OF 10

					REMOVAL OF IN	PRCVEMENTS				
_		STATION	STATION		LENGTH	WIDTH	AREA	AREA		
HEET	ROADWAY	(FROM)	(TO)	LOCATION	(FEET)	(FEET)	(SF)	(SY)	EACH	DESCRIPTION OF REMOVAL
20	1-70	529+22	529+27	LT		- 4			1	CURBFAMP
20	1-70	529+23	529+30	LT	15	6	90	10.0		SIDEWALK
20	1-70	530+11	530+16	LT	15	5	75	8.3		SIDEWALK
22	1-70	12+40	12+76	LT			291	32.3		ISLAND CUTTHROUGH
22	1-70	12+48	12+68	RT			142	15.8		ISLAND CUTTHROUGH
22	1-70	13+24	13+43	RT			137	15.2		ISLAND CUTTHROUGH
23	1-70	0+95	0+8	LT	13.1	6	78.6	8.7		SIDEWALK
23	1-70	0+8	0+15	LT	15	6	90	10.0		CURB RAMP
23	1-70	0+62	0+74	LT	15	5	75	8.3		CURB RAMP
23	1-70	0+74	0+23	LT	50	5	250	27.8		SIDEWALK
23	1-70	0+57	0+77	RT	15	6	90	10.0		CURB RAMP
23	1-70	0+41	0+55	RT	varies	varies	43.4	4.8		ISLAND CUTTHROUGH
23	1-70	0+52	0+58	LT	8	2	16	1.8		ASPHALT
23	1-70	1+35	1+35	RT						STOP 3IGN
23	1-70	3+23	3+47	LT	25	5	118	13.1		SIDEWALK
23	1-70	3+47	3+57	LT					1	CURBFAMP
53	1-70	12+63		RT					2	PUSHBLTTONS
53	1-70	12+65		LT					2	PUSHBUTTONS
53	1-70	13+26		RT					2	PUSHBLTTONS
53	1-70	13+30		LT					2	PUSHBLTTONS
25	S WOODS MILL	10+47.00	10+54.00	LT		_		5.8		CURBFAMP
25	S WOODS MILL	10+54.00	11+98.00	LT				99	+ + +	SIDEWALK
25	S WOODS MILL	10+54.00	13+58.00	LT				3		SIDEWALK
25	S WOODS MILL	14+34.00	14+39.00	LT				3		SIDEWALK
25	S WOODS MILL	15+46.00	15+53.00	LT				6		CURBFAMP
27	CRAIGWOLD RD	10+35	10+50	LT			1		1	CURBFAMP
27	CRAIGWOLD RD	10+50	10+69	LT	VARIES	VARIES	105	11.7		SIDEWALK
27	CRAIGWOLD RD	13+81	13+96	LT	VARIES	VARIES	73	8.1		SIDEWALK
27	CRAIGWOLD RD	13+96	14+01	LT					1	CURBFAMP
29	I-44 AT ELM	970+06	970+15	RT					1	CURBFAMP
29	I-44 AT ELM	970+04	970+11	RT	8	5	38	4.2		SIDEWALK
29	I-44 AT ELM	VARIES	VARIES	LT					2	ISLAND CURBRAMPS
30	I-44 AT MURDOCH	1051+65	1051+80	LT		2:1			1	CURBFAMP
30	I-44 AT MURDOCH	1051+75	1051+81	LT	8	5	32	3.6		SIDEWALK
30	I-44 AT MURDOCH	1052+25	1052+29	LT	13	4	36	4.0		SIDEWALK
30	I-44 AT MURDOCH	1051+72	1051+77	RT	6	5	30	3.3		SIDEWALK
30	I-44 AT MURDOCH	1052+23	1052+29	RT	VARIES	VARIES	31	3.4		SIDEWALK
56	I-44 AT ELM	970+50	-	RT & LT		* -			6	PUSHBLTTONS
56	I-44 AT ELM	970+50		RT & LT					4	PEDESTRIAN SIGNAL HEADS
59	I-44 AT MURDOCH	1051+65		LT					2	PUSHBUTTONS
-					T			lane.	1	
32	S OLD HWY 94	10+02.00	11+58.00	LT				278		CONCRETE PAVEMENT
32	S OLD HWY94	10+02.00	10+02.00	LT	86			10		CURB RAMP
32	S OLD HWY94	10+66.00	11+11.00	LT	16.4			10	-	CURB RAMP
32	S OLD HWY94	11+02.00 11+30.00	11+58.00	LT	16.4	-			-	CURB
32	S OLD HWY94 S OLD HWY94	11+30.00	11+58.00	LT	42			9.2		SIDEWALK
32	S OLD HWY94	11+31.00	11+43.00	LT				7		CURB RAMP
34	3 OLD AW1 94	11+30.00	111-40.00	LI				,		CONDINAINE
34	N OUTER RD	41+82.00	42+00.00	LT		- 4	143.1	15.9		CURB RAMP
34										
52	N OUTER RD	41+83.00		LT					1	PEDESTRIAN PUSH BUTTON AND SIGN

NO DIRECT PAYMENT FOR SAWCUTS, SAWCUTS ARE NOT LISTED BUT ARE REQUIRED FOR ALL REMOVALS ADJACENT TO AN ASPHALT OR CONCRETE SURFACE TO REMAIN IN PLACE.

ANY ITEMS FOR REMOVAL NOT LISTED ABOVE BUT REQUIRED FOR THE COMPLETION OF THE PROJECT SHALL BE INCLUDED IN THE LUMP SUM COST FOR REMOVAL OF IMPROVEMENTS.

1 LS

TOTAL

ROMARS FROM STATION TO STATION BANGWAY LOCATION FROM STATION GSCRO) FROM STATION FROM STATION GSCRO) FROM STATION FRO				SODDING		, m	
6 13-06 13-12 MPRENAL MANN IT 1.7 1.7 1.9 1.9 1.9 1.9 1.9 1.9 1.9 1.9 1.9 1.9	OVERALL SHEET NUMBER	FROM STATION	TO STATION		LOCATION	FESCUE SODDING	REMARKS
19-01							
The color							
Substitute							
S	7	16+01	16+12	CHURCH	RT	8.6	
3 12-29 12-32 13-32 100-0000, reverse 3 4 1 1 1 1 1 1 1 1 1					CURTOTAL	15	
9 12-597 22-44	c	12,20	12422	IMPERIAL MAIN			
9 12-25							
9 12-51 12-58 INFERNAMON IT 2.3 3.3 9 16-94 17-06 INFERNAMON IT 3.3 3.3 9 17-06 INFERNAMON IT 3.3 3.3 9 17-06 INFERNAMON IT 10.3 17-17 INFERNAMON IT 10.3 INFERN							
9 16-94 17-96 17-96 1 10-717 1 IMPERIAL MANN 1 T 3.3 1 10-716 17-717 1 IMPERIAL MANN 1 T 10.3 1 10-716 17-717 1 IMPERIAL MANN 1 T 10.3 1 10-716 17-717 1 IMPERIAL MANN 1 T 10.3 1 10-716 17-717 1 IMPERIAL MANN 1 T 10.3 1 10-716 17-717 1 IMPERIAL MANN 1 T 10.3 1 10-716 17-717 1 IMPERIAL MANN 1 T 10.3 1 10-716 17-717 1 IMPERIAL MANN 1 T 10.3 1 10-716 17-717 1 IMPERIAL MANN 1 T 10.3 1 10-716 17-717 1 IMPERIAL MANN 1 T 10.3 1 10-716 1 IMPERIAL MANN 1 T 10.3 1 10-716 1 IMPERIAL MANN 1 T 10.3 1 IMPERIAL MANN 1 T 10.3 1 IMPERIAL MANN 1 IMPERIAL							
9 17-05 17-17 IMPERIAL MANN RT 4.2 10 232-46 1 232-46 WOUTER RD IT 10.3 10 232-46 1 232-46 WOUTER RD RT 10.3 10 232-46 1 232-46 WOUTER RD RT 10.3 11 10.3 12 160-65 150-43 WOODSONERD IT 4.0 11 5.6 11				IMPERIAL MAIN			
10					RT	4.2	
10	10			W OUTER RD	LT	10.3	
12					RT		
12							
12					SUBTOTAL	37	
12	12	160+26	160+43	WOODSON RD	LT	4.0	
12	12	160+68	160+85	WOODSON RD	LT	4.0	
12	12	161+24	161+39	WOODSON RD	LT	3.3	
12	12	161+63	161+87	WOODSON RD	LT	5.6	
12	12	162+46	162+58	WOODSON RD	LT	2.9	
12	12	162+72	162+90	WOODSON RD	RT	3.3	
12	12	163+17	163+40	WOODSON RD	LT	4.9	
15741		163+36	163+58				
13			163+98				
13	13	167+11	167+29				
13		1					
14	13	173+94	174+08				
14	13	174+54	174+68				
14		1					
14	14	175+93	176+09	WOODSON RD	RT	3.3	
14	14	176+28	176+42				
14	14	176+62	176+76				
14	14	177+04	177+18				
14	14	177+31	177+46		RT	3.3	
14		177+77	177+92		RT		
14	14	179+23	179+49	WOODSON RD	RT	5.3	
14							
14							
14							
15			100000000000000000000000000000000000000				
15	11/970-0		and Antonio and				
15							
15	7/1/05						
15							
15							
15							
SUBTOTAL 215 169+83 I-70 & McKelvey RT 25 17 170+03 170+28 I-70 & McKelvey RT 14 17 170+54 170+65 I-70 & McKelvey RT 9 19 529+43 I-70 & N Hanley RT 5 19 529+33 I-70 & N Hanley RT 4 19 529+33 I-70 & N Hanley RT 4 19 529+33 I-70 & N Hanley RT 4 19 530+35 I-70 & N Hanley RT 2 20 529+22 I-70 & N Hanley IT 3 3 20 529+23 I-70 & N Hanley IT 5 5 20 530+16 I-70 & N Hanley IT 5 5 20 530+16 I-70 & N Hanley IT 5 5 20 212+40 12+76 Jennings Sta Rd IT 20 23 3+23 3+57 Jennings Sta Rd IT 11 11 11 11 12 12 14+34 I+3+9 S WOODS MILL IT 52 14+34 I4+39 S WOODS MILL IT I.7 1.7							
17	15	189+89	190+14	WOODSON RD	LI	5.1	
17				- 1	CURTOTAL	215	
17	17	150.20	160.02	1.70.9 MaKaluau			
17 170+54 170+65 1-70 & McKelvey RT 9 19 529+43 1-70 & N Hanley RT 5 19 529+53 1-70 & N Hanley RT 2 20 529+22 1-70 & N Hanley LT 3 20 529+23 1-70 & N Hanley LT 5 20 529+23 1-70 & N Hanley LT 5 21 12-40 12-76 Jennings Sta Rd LT 20 22 12+40 12-76 Jennings Sta Rd LT 20 23 3+23 3+57 Jennings Sta Rd LT 11 25 10+47 11+98 SWOODS MILL LT 52 25 13+54 13+61 SWOODS MILL LT 1.7 25 14+34 14+39 SWOODS MILL LT 1.7 25 15+46 15+53 SWOODS MILL LT 2.5 27 10+35 10+69 CRAGWOLD LT 11 27 13+81 14+01 CRAGWOLD LT 7 29 970+08 970+15 I-44 & ELM RT 8 32 10+63 10+76 SOLD HWY 94 LT 7.8 32 11+31 11+44 SOLD HWY 94 LT 7.8 34 41+81 42+00 I-70 N OUTER RD LT 10.4 34 41+81 42+00 I-70 N OUTER RD LT 10.4 35 UBITOTAL 19 34 41+81 42+00 I-70 N OUTER RD LT 10.4					2.5		
19							
19			170403				
19	15/76	1 10000		CONTRACTOR CONTRACTOR CONTRACTOR	300		
1-70 & N Hanley	1000		-		-		
1-70 & N Hanley							
1-70 & N Hanley							
12+40							
23 3+23 3+57 Jennings Sta Rd LT 11		+	12+76				
SUBTOTAL 103							
25							
25					SUBTOTAL	103	
25 13+54 13+61 S WOODS MILL LT 1.7 25 14+34 14+39 S WOODS MILL LT 1.7 25 15+46 15+53 S WOODS MILL LT 2.5 SUBTOTAL 58 27 10+35 10+69 CRAGWOLD LT 11 27 13+81 14+01 CRAGWOLD LT 7 SUBTOTAL 18 29 970+08 970+15 I-44 & ELM RT 8 32 10+63 10+76 S OLD HWY 94 LT 7.8 32 11+31 11+44 S OLD HWY 94 LT 11.1 SUBTOTAL 19 34 41+81 42+00 I-70 N OUTER RD LT 10.4 SUBTOTAL 19	25	10+47	11+98	S WOODS MILL			
25							
25		-					
SUBTOTAL 58 10+69 CRAGWOLD LT 11 11 12 13+81 14+01 CRAGWOLD LT 7 7 13+81 14+01 CRAGWOLD LT 7 7 14-4 & ELM RT 8 14-44 & ELM RT 8 14-44 & ELM RT 8 14-44 14-44 14-44 15 14-44 17 17-8 11-44 11							
27							
27					SUBTOTAL	58	
27 13+81 14+01 CRAGWOLD LT 7	27	10+35	10+69	CRAGWOLD			
SUBTOTAL 18			A STATE OF THE STA				
29 970+08 970+15 I-44 & ELM RT 8 SUBTOTAL 8 32 10+63 10+76 SOLD HWY94 LT 7.8 32 11+31 11+44 SOLD HWY94 LT 11.1 SUBTOTAL 19 34 41+81 42+00 I-70 N OUTER RD LT 10.4 SUBTOTAL 11							
SUBTOTAL 8		7 7		·	SUBTOTAL	18	
32 10+63 10+76 S OLD HWY94 LT 7.8 32 11+31 11+44 S OLD HWY94 LT 11.1 SUBTOTAL 19 34 41+81 42+00 I-70 N OUTER RD LT 10.4 SUBTOTAL 11	29	970+08	970+15	I-44 & ELM	RT	8	
32 10+63 10+76 S OLD HWY94 LT 7.8 32 11+31 11+44 S OLD HWY94 LT 11.1 SUBTOTAL 19 34 41+81 42+00 I-70 N OUTER RD LT 10.4 SUBTOTAL 11				- 4 E T			
32 10+63 10+76 S OLD HWY94 LT 7.8 32 11+31 11+44 S OLD HWY94 LT 11.1 SUBTOTAL 19 34 41+81 42+00 I-70 N OUTER RD LT 10.4 SUBTOTAL 11				4	SUBTOTAL	8	
32 11+31 11+44 S OLD HWY 94 LT 11.1 SUBTOTAL 19 34 41+81 42+00 I-70 N OUTER RD LT 10.4 SUBTOTAL 11	32	10+63	10+76	S OLD HWY 94		7.8	
SUBTOTAL 19 34 41+81 42+00 I-70 NOUTERRD LT 10.4							
34 41+81 42+00 1-70 N OUTER RD LT 10.4 SUBTOTAL 11							
34 41+81 42+00 I-70 N OUTER RD LT 10.4 SUBTOTAL 11				ja	SUBTOTAL	19	
SUBTOTAL 11	34	41+81	42+00	I-70 N OUTER RD			
TOTAL A9A				_ /-	SUBTOTAL	11	
TOTAL 484				= -1	TOTAL	484	

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3/28/2024 VARIES MO STL 3

> VARIES J6P3510B

PROJECT NO.

BRIDGE NO.

SCHEDULE OF QUANTITIES

SHEET 6 OF 10

PAVEMENT MARKING	PAVEMENT MARKING QUANTITIES											
6 IN. WHITE HIGH	PREFORMED	PREFORMED	PREFORMED THERMOPLASTIC PAVEMENT MARKING, 12 IN WHITE, YIELD LINE	PAVEMENT	TEMPORARY							
BUILD WATERBORNE	THERMOPLASTIC	THERMOPLASTIC		MARKING	PAVEMENT							
PAVEMENT MARKING	PAVEMENT MARKING,	PAVEMENT MARKINS, 30		REMOVAL	MARKING PAIN							

					PAVEMENT MARKING	QUANTITIES				
OVERALL SHEET NUMBER	BASELINE	FROM STATIC ^{IN}	TO STATION	LOCATION	6 IN. WHITE HIGH BUILD WATERBORNE PAVEMENT MARKING PAINT, TYPE L BEADS	PREFORMED THERMOPLASTIC PAVEMENT MARKING, 24 IN. WHITE	PREFORMED THERMOPLASTIC PAVEMENT MARKING, 30 IN WHITE MIDBLCCK	PREFORMED THERMOPLASTIC PAVEMENT MARKING, 12 IN WHITE, YIELD LINE TRIANGLES	PAVEMENT MARKING REMOVAL	TEMPORARY PAINSMENT MARKING PAINT*
			1		(LF)	(LF)	(EA)	(EA)	(LF)	(LS)
6	IMPERIAL MAIN	13+08,00	13+46.00	LT	39			1	1	17
6	IMPERIAL MAIN	13+13,00	13+45.00	LT	33			11		
6	IMPERIAL MAIN	13+16.00	13+32.00	RT	18				H ar	
6	IMPERIAL MAIN	13+18,00	13+34.00	RT	18					
6	IMPERIAL MAIN	13+21.00	13+34.00	RT	1	15				
6	IMPERIAL MAIN	13+50.00	13+63.00	LT	22					
6	IMPERIAL MAIN	13+54.00	13+68.00	LT	22					
6	IMPERIAL MAIN	13+54.00	13+70.00	RT	21					
6	IMPERIAL MAIN	13+56.JU	13+57.00	кі				9		
6	IMPERIAL MAIN	13+58,00	13+74.00	RT	21					
6	IMPERIAL MAIN	13+62,00	13+71.00	LT				9		
					1					
		_		SUBTOTAL	194	15	0	18	0	1
9	IMPERIAL MAIN	12+30.00	12+30.00	LT/RT	51					
9	IMPERIAL MAIN	12+36,00	12+36.00	LT/RT	57					+
9	IMPERIAL MAIN	12+36.00	12+45.00	RT	10					+
9	IMPERIAL MAIN	12+36.00	12+44.00	LT	10				,	
9	IMPERIAL MAIN	12+51,00	13+11.00	LT	64					+
9		12+51,30	13+11.00	LT						+
	IMPERIAL MAIN				61					+
9	IMPERIAL MAIN	12+62,00	13+15.00	RT	50				-	-
9	IMPERIAL MAIN	12+66,00	13+16.00	RT	47					
9	IMPERIAL MAIN	13+24.00	13+24.00	LT/RT	67					-
9	IMPERIAL MAIN	13+27,00	13+46.00	RT	23					
9	IMPERIAL MAIN	13+28.00	13+42.00	LŢ	20	1				
9	IMPERIAL MAIN	13+30,00	13+30.00	LT/RT	67					
9	IMPERIAL MAIN	13+30.00	13+43.00	RT				9		
9	IMPERIAL MAIN	13+31.00	13+50.00	RT	24		-			
9	IMPERIAL MAIN	13132,00	13 (46.00	LT	21					
9	IMPERIAL MAIN	13+37.00	13+49.00	LT				11		
9	IMPERIAL MAIN	16+34.00	16+45.00	RT		4		9		
9	IMPERIAL MAIN	16+37,00	16+51.00	RT	19					
9	IMPERIAL MAIN	16+37.00	16+51.00	LT	19					
9	IMPERIAL MAIN	16+41,00	16+55.00	RT	20					
9	IMPERIAL MAIN	16+42.00	16+55.00	LT	19					
9	IMPERIAL MAIN	16+45.30	16+55.00	LT	1		-	9		
9	IMPERIAL MAIN	16+70.00	17+05.00	RT	36	·				
9	IMPERIAL MAIN	16+70.00	17+12.00	RT	42	16				
9	IMPERIAL MAIN	16+71.30	16+89.00	LT		18				
9	IMPERIAL MAIN	16+72.00	16+94.00	LT	23					
9	IMPERIAL MAIN	16+73.00	16+95.00	ĹT.	23					
9	IMPERIAL MAIN	16+75,00	16+76.00	RT	11					
9	IMPERIAL MAIN	17+05,00	17+16.00	RT	13					
10	W OUTER RD	231+30.00	231+47.00	RT	21					†
10	W OUTER RD	231+33.00	231+50.00	RT	21					
10	W OUTER RD	231+34.00	231+47.00	LT	18					+
10	W OUTER RD	231+38.00	231+51.00	LT	19					+
10	W OUTER RD	231+31.00	231+51.00	LT	1			9		+
10	W OUTER RD	231+41.00	231.31.00					,		+

231+42.00 231+43.00

W OUTER RD

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3/28/2024 VARIES MO SHEET NO. 3 STL

> VARIES J6P3510B

PROJECT NO.

BRIDGE NO.

SCHEDULE OF QUANTITIES

* SEE JSP NN

OVERALL SHEET	BASELI NE	FROM	TO STATION	LOCATION	6 IN. WHITE HIGH BUILD WATERBORNE	PREFORMED THERMOPLASTIC	PREFORMED THERMOFLASTIC	PREFORMED THERMOPLASTIC PAVEMENT MARKING, 12	PAVEMENT MARKING	TEMPORARY PAVEMENT
NUMBER	BASELINE	STATION	TOSTATION	LOCATION	PAVEMENT MARKING PAINT, TYPE L BEADS	PAVEMENT MARKING, 24 IN. WHITE	PAVEMÉNT MARKING, 30 IN WHITE MIDBLOCK	IN WHITE, YIELD LINE TRIANGLES	REMOVAL	MARKING PAINT
					(LF)	(LF)	(EA)	(EA)	(LF)	(LS)
10	W OUTER RD	231+47.00	231+52.00	LT/RT	56					
10	W OUTER RD	231+54.00	231+58.00	LT/RT RT	56 83					
10	W OUTER RD	231+64.00	232+47.00	RT	83					
10	W OUTER RD	231+66.00	232+46.00	LT	80				_	
10	W OUTER RD	231+66.00	232+46.00	LT	80					
10	W OUTER RD	231+70.00	231+94.00	LT		24				
10	W OUTER RD	231+94.00	232+05.00	LT		11				
10	W OUTER RD	231+95.00	232+19.00	RT		24				<u> </u>
10	W OUTER RD	232+19.00	232+43.00	RT		24				-
10	W OUTER RD	232+48.00 232+53.00	232+49.00	LT/RT	56			9		
10	W OUTER RD W OUTER RD	232+53.00	232+65.00 232+70.00	RT RT	21			9		
10	W OUTER RD	232+54.00	232+55.00	LT/RT	56					
10	W OUTER RD	232+55.00	232+69.00	LT	21	-				†
10	W OUTER RD	232+56.00	232+74.00	RT	21					
10	W OUTER RD	232+59.00	232+73.00	LT	22					
10	W OUTER RD	232+59.00	232+59.00	LT		13				
10	W OUTER RD	232+63.00	232+75.00	LT				11		
10	W OUTER RD	232+70.00	232+70.00	LT		12				<u> </u>
				SUPTOTALS	1511	150	0	67	0	1
19	I-70 @ N HANLEY	529+51	529+60	SUBTOTALS CROSSWALK, EB I-70 OFFRAMP	1511	150	10	67		
19	I-70 @ N HANLEY	529+58	529+90	RT, CROSSWALK, SB N HANLEY			7			+
19	I-70 @ N HANLEY	530+1 ³	530+36	RT, CROSSWALK, NB N HANLEY			5			
19	I-70 @ N HANLEY	530+41	530+66	CROSSWALK, EB I-70 ONRAMP			9			
20	I-70 @ N HANLEY	529+0	529+16	LT, CROSSWALK, WB I-70 ONRAMP			4		66	
20	I-70 @ N HANLEY	530+C ⁹	530+16	LT, CROSSWALK, WB I-70 OFFRAMP			13		201	
22	JENNINGS STATION RD	12+12	12+35	LT, WB I-70 OFFRAMP			6		99	<u> </u>
22	JENNINGS STATION RD	12+2)	12+37	LT, WB I-70 OFFRAMP				12		-
22	JENNINGS STATION RD	12+22	12+38	RT, EB JENNINGS XING			4	12		
22	JENNINGS STATION RD JENNINGS STATION RD	12+3 ² 12+4 ²	12+50 12+44	RT, EB JENNINGS XING RT, JENNINGS STATION ROAD		24	4		60 144	+
22	JENNINGS STATION RD	12+52	12+63	LT & RT, JENNINGS STATION RD		24	13		184.5	+
22	JENNINGS STATION RD	12+69	13+21	RT, JENNINGS XING			10		144	
22	JENNINGS STATION RD	12+75	12+94	LT, WB I-70 OFF RAMP		ZZ			132	
22	JENNINGS STATION RD	12+77	13+22	LT, WB 1-70 ON AND OFFRAMP			10		141	
22	JENNINGS STATION RD	12+95	13+22	RT, JENNING XING		24				
22	JENNINGS STATION RD	13+3l	13+38	LT & RT, JENNINGS STATION RD			13		183	
22	JENNINGS STATION RD	13+22	13+37	LT, WB I-70 ON RAMP				12		-
22	JENNINGS STATION RD JENNINGS STATION RD	13+33 13+4)	13+53 13+68	RT, WB JENNING XING RT, WB JENNING XING			6	12	75	-
22	JENNINGS STATION RD	13+43	13+58	LT, WB I-70 ON RAMP			4		51	
22	JENNINGS STATION RD	13+45	13+53	LT & RT, JENNINGS STATION RD		36			216	
23	JENNINGS STATION RD	3+53	3+61	LT, EB I-70 OFF RAMP, SB JENNINGS			4		52.5	
23	JENNINGS STATION RD	3+49	3+52	LT, NB JENNINGS STATION RD		24			144	
23	JENNINGS STATION RD	3+50	3+95	LT, SB JENNINGS STATION RD	142				154	
23	JENNINGS STATION RD	3+56	3+73	LT & RT, JENNINGS STATION RD			11		165	-
23	JENNINGS STATION RD	3+66	3+95	RT, EB 1-70 ON RAMP			6		201	
23	JENNINGS STATION RD JENNINGS STATION RD	3+86 3+87	4+49 4+35	LT, EB I-70 OFF RAMP, NB JENNINGS EB I-70 OFF RAMP		48	13		204	+
23	JENNINGS STATION RD	3+0/	4133	EB I-70 OFF RAIVIP		40			200	
				SUBTOTAL	142	178	148	48	2704	1
29	1-44	970+C ⁸	970+10	I-44 ONRAMP @ ELM	108				154.5	
29	1-44	971+06	971+22	I-44 OFFRAMP @ ELM	54	1			79.5	
29	1-44	971+49	971+83	I-44 OFFRAMP @ ELM	62				96	
30	1-44	1051+53	1051+71	I-44 ONRAMP @ MURDOCH	108				96	
			1							
				SUBTOTALS	332	0	0	0	426	1
32	S OLD HWY 94	10+81.00	11+01.00	LT		21				
						1				
				SUBTOTALS	0	21	0	0	0	1
34	N OUTER RD	42+01.00	42+34.00	LT		34				
						200				
		_	-	CHIPTOTALS		24	0			
				SUBTOTALS	0	34	0	0	0	1
				TOTAL	2179	398	148	133	3130	1*

PAVEMENT MARKING QUANTITIES (CONTINUED)

* SEE JSP NN

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3/28/2024

VARIES MO STL 3

VARIES J6P3510B

PROJECT NO.

BRIDGE NO.

SCHEDULE OF QUANTITIES

SHEET 8 OF 10

				CURB				
OVERALL SHEET NUMBER	FROM STATION	TO STATION	RCADWAY	LOCATION	INTEGRAL CUR ³ (6 IN. HEIGHT AND UNDER) TYPE A	CONCRETE CURB (6 IN. HEIGHT AND UNGER) TYPE S	REMARKS	
					(LF)	(LF)		
21	588+97	589+01	(I-70) BERMUDA	LT	12			
23	1+65		(I-70) JENNINGS STA.	RT	1	13		
				SUBTOTAL	12	13		
32	11+01	11+11	S OLD HWY 94	LT	14		HOLLOWBROOK DR MEDIAN	
				SUBTOTAL	14			
				TOTAL	26	13		

					ISLAND	& MEDIAN				
OVERALL SHEET NUMBER	FROM STATION	TO STATION	ROADWAY	LOCATION	INTERSECTION QUADRANT	TYPE 5 AGGREATE FOR BASE (4 IN. THICK)	6 IN. CONCRETE MEDIAN STRIP	CONCRETE SIDEWALK, 7	TRUNCATED DOMES	REMARKS
			4 7 7 6 7 6 1 1 1			(SQYD)	(SQYD)	(SQYD)	(SQFT)	
23	3+64	3+87	JENNINGS STATION RD	LT	SOUTHWEST	29.2	19.6	9.7	30.0	NEW MEDIAN CUT THROUGH
					SUBTOTALS	*29.2	19.6	*9.7	*30.0	*SEE SUBTOTAL SCHEDULE FOR FINAL PAY ITEM TOTAL
					TOTALS		10.6			



3/28/2024

VARIES MO SHEET NO. STL

VARIES J6P3510B

PROJECT NO.

BRIDGE NO.

SHEET 9 OF 10

SCHEDULE OF QUANTITIES

SIZE ADE	TOTAL QTY TOTAL SIG			S 1 7 F	AREA QTY TOTAL		TOTAL					EFFECTIVE: 04-01-2023
		•	SIGN		SQ. FT EACH SQ. FT.			INOM.		ITEM	TOTAL	
IGN IN SQ FT	T EACH SQ.FT. EACH SQ.FT.	- DECCRIPTION	3101				SQ.FI.		DECCRIPTION	H	1	
1 11 100/40 16 6	WARNING SIGNS	DESCRIPTION	1 = 1	T267/4/	GUIDE SIG	1/1/2			DESCRIPTION	NUMBER		DESCRIPTION
1-1L 48X48 16.0 1-1R 48X48 16.0		TURN (SYMBOL LEFT ARROW)	E05-1		3 12.00				GORE EXIT	6122008		IMPACT ATTENUATOR 46 MPH (SAND BARRELS)
		TURN (SYMBOL RIGHT ARROW)	E05-2		5 12.00				EXIT OPEN	6122009		IMPACT ATTENUATOR 50 MPH (SAND BARRELS)
1-2L 48X48 16.0		CURVE (SYMBOL LEFT ARROW)	E05 - 2a						EXIT CLOSED	6122010		IMPACT ATTENUATOR 50 MPH (SAND BARRELS)
1 - 2R 48X48 16 . 0		CURVE (SYMBOL RIGHT ARROW)			1 10.00				ROAD WORK NEXT XX MILES	6122012		IMPACT ATTENUATOR 55 MPH (SAND BARRELS)
1-3L 48X48 16.0		REVERSE TURN (SYMBOL LEFT ARROW)	GO20 - 2						END ROAD WORK	6122014		IMPACT ATTENUATOR 60 MPH (SAND BARRELS)
1 - 3R 48X48 16 . 0		REVERSE TURN (SYMBOL RIGHT ARROW)	GO20-4						PILOT CAR FOLLOW ME PILOT CAR IN USE WAIT & FOLLOW	6122017		IMPACT ATTENUATOR 65 MPH (SAND BARRELS)
I - 4L 48X48 16.0		REVERSE CURVE (SYMBOL LEFT ARROW)	GO20 - 4a							6122019		IMPACT ATTENUATOR 70 MPH (SAND BARRELS)
1-4R 48X48 16.0		REVERSE CURVE (SYMBOL RIGHT ARROW)	GO20-4a						PILOT CAR IN USE WAIT & FOLLOW	6122020		REPLACEMENT SAND BARREL
1-4bL 48X48 16.0		DOUBLE ARROW REVERSE CURVE (SYMBOL LT ARROWS)	GO20 - 5 a F						WORK ZONE (PLAQUE)	6122030		IMPACT ATTENUATOR (RELOCATION)
-4bR 48X48 16.0		DOUBLE ARROW REVERSE CURVE (SYMBOL RT ARROWS)							END DETOUR	6123000A		TRUCK OR TRAILER MOUNTED ATTENUATOR (TMA
-4cL 48X48 16.0		TRIPLE ARROW REVERSE CURVE (SYMBOL LT ARROWS)	MO4 - 9L MO4 - 9R		5 12.00				DETOUR (LEFT ARROW)	6161008	_	ADVANCED WARNING RAIL SYSTEM
1-4cR 48X48 16.0		TRIPLE ARROW REVERSE CURVE (SYMBOL RT ARROWS)			5 12.00				DETOUR (RIGHT ARROW)	6161012		BUOYS (BOATS KEEP OUT)
I-6 60X30 12.5		HORIZONTAL ARROW (SYMBOL)			2 4.00				STREET NAME (PLAQUE)	6161013		BUOYS (NO WAKE)
1-6a 72X36 18.0		HORIZ. ARROW (SYMBOL ON PERMANENT BARRICADE)	MO4 - 10L						DETOUR (ARROW LEFT)	6161014	_	SPECIAL SIGN ASSEMBLY (BOATS KEEP OUT)
-7 60X30 12.5		DOUBLE HEAD HORIZONTAL ARROW (SYMBOL)	MO4 - 10R	<u> 48X18</u>		V CIC	NC		DETOUR (ARROW RIGHT)	6161025		CHANNELIZER (TRIM LINE)
7a 72X36 18.0		DOUBLE HEAD HORIZ. ARROW (SYMBOL ON PERM. BARR.)	11	T.07/4/	REGULATOR	Y SIG	NS I			6161030		TYPE III MOVEABLE BARRICADE
I-8 18X24 3.00		CHEVRON (SYMBOL)	R1-1		3 13.25	-			STOP	6161033		DIRECTION INDICATOR BARRICADE
1-8a 30X36 7.50		CHEVRON (SYMBOL FOR DIVIDED HIGHWAYS)	R1-2		6.93	-			YIELD	6161040	3	FLASHING ARROW PANEL
3-1 48X48 16.0		STOP AHEAD (SYMBOL)	R1-2a		9.00	-			TO ONCOMING TRAFFIC (PLAQUE)	6161047		TYPE III OBJECT MARKER
3-2 48X48 16.0		YIELD AHEAD (SYMBOL)	R1-3P		2 2.50	-			ALL WAY (PLAQUE)	6161055		SEQUENTIAL FLASHING WARNING LIGHT
48X48 16.0		SIGNAL AHEAD (SYMBOL)	R2-1		3 12.00	-			SPEED LIMIT XX	6161070		TUBULAR MARKER
-4 48X48 16.0		BE PREPARED TO STOP	R3-1		3 16.00				NO RIGHT TURN (SYMBOL)	6161095		RADAR SPEED ADVISORY SYSTEM
-5 48X48 16.0		SPEED LIMIT AHEAD	R3 - 2		3 16.00				NO LEFT TURN (SYMBOL)	1		CHANGEABLE MESSAGE SIGN,
-1L 48X48 16.0		MERGE (SYMBOL FROM LEFT)	R3-3		9.00	-			NO TURNS	6161096		COMMISSION FURNISHED/RETAINED
1-1R 48X48 16.0		MERGE (SYMBOL FROM RIGHT)	R3-4		3 16.00	-			NO U-TURN (SYMBOL)	1		CHANGEABLE MESSAGE SIGN W/O COMM.
1-1aL 48X48 16.0		MERGE (ARROW SYMBOL)	R3-7L		0 6.25	-			LEFT LANE MUST TURN LEFT	61610984	4	INTERFACE - CONTRACTOR FURNISHED/RETAINE
-1aR 48X48 16.0		MERGE (ARROW SYMBOL)	R3 - 7R		0 6.25	-			RIGHT LANE MUST TURN RIGHT	1		CHANGEABLE MESSAGE SIGN WITH COMM.
-1 48X48 16.0		ROAD/BRIDGE/RAMP NARROWS	R4-1		3 12.00				DO NOT PASS	6161099		INTERFACE - CONTRACTOR FURNISHED/RETAINE
-3 48X48 16.0		ONE LANE BRIDGE	R4-2		3 12.00				PASS WITH CARE	6162000A		WORK ZONE TRAFFIC SIGNAL SYSTEM
-5 48X48 16.0		NARROW LANES	R4-8a		3 12.00				(EEP LEFT (HORIZONTAL ARROW)	6162002		TEMPORARY LONG-TERM RUMBLE STRIPS
-1 48X48 16 0		DIVIDED HIGHWAY (SYMBOL)	R4-7a		3 12.00				KEEP RIGHT (HORIZONTAL ARROW)			TEMPORARY TRAFFIC BARRIER
-2 48X48 16 0		DIVIDED HIGHWAY END (SYMBOL)	R5-1		0 6.25				DO NOT ENTER	61736000		CONTRACTOR FURNISHED/RETAINED
5-3 48X48 16.0		TWO WAY TRAFFIC (SYMBOL)	R5-1a		1 6.00			1	WRONG WAY			TEMPORARY TRAFFIC BARRIER
7-3a 30X24 5.00	0 2 10	NEXT XX MILES (PLAQUE)	R6-1L	54X18	8 6.75				ONE WAY ARROW (LEFT)	6173602E	3	CONTRACTOR FURNISHED/COMMISSION RETAINED
-1 48X48 16.0	00	BUMP	R6-1R	54X18	8 6.75				ONE WAY ARROW (RIGHT)	6174000A	\	TEMP. TRAFFIC BARRIER HEIGHT TRANSITION
3-2 48X48 16.0	00	DIP	R6-2L	24X30	5.00				ONE WAY (LEFT)	6175010A	\	RELOCATING TEMPORARY TRAFFIC BARRIER
3-3 48X48 16.0	00	PAVEMENT ENDS	R6-2R	24X30	5.00				ONE WAY (RIGHT)	<u> </u>		TEMPORARY TRAFFIC BARRIER
3-4 48X48 16.0	00	SOFT SHOULDER	R9-9	24X12	2 2.00 9 18				SIDEWALK CLOSED	6176000E	3	COMMISSION FURNISHED/RETAINED
8-5 48X48 16.0	00	SLIPPERY WHEN WET (SYMBOL)							SIDEWALK CLOSED AHEAD,			TEMP. TRAFFIC BARRIER HEIGHT TRANSITION
8-6 48X48 16.0	00	TRUCK CROSSING (WITH FLAGS)	R9-11L	24X18	3 3.00 2 6				(ARROW LEFT) CROSS HERE	6177000E	3	COMMISSION FURNISHED/RETAINED
-6c 48X48 16.0	00	TRUCK ENTRANCE							SIDEWALK CLOSED AHEAD,	6208064A	\ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \	TEMPORARY RAISED PAVEMENT MARKER
-7 36X36 9.00	0	LOOSE GRAVEL	R9-11R	24X18	3 3.00 2 6				(ARROW RIGHT) CROSS HERE	9029400		TEMPORARY TRAFFIC SIGNALS
-7a 36X36 9.00	0	FRESH OIL/LOOSE GRAVEL	R10-6	24X36	6.00				STOP HERE ON RED (45^ ARROW)	9029401		TEMPORARY TRAFFIC SIGNALS AND LIGHTING
-9 48X48 16 0	00	LOW SHOULDER	R11-2	48X30	10.00				ROAD CLOSED			
-11 48X48 16.0	00	UNEVEN LANES		T					ROAD CLOSED XX MILES AHEAD			
-12 48X48 16.0	00	NO CENTER LINE	R11-3a	60X3C	12.50				LOCAL TRAFFIC ONLY			
-15 48X48 16.0		GROOVED PAVEMENT	R11-4	60X3C	12.50				ROAD CLOSED TO THRU TRAFFIC			
-15P 30X24 5.00	0	MOTORCYCLE (PLAQUE)	CONST-3						FINE SIGN			
-17 48X48 16.0		SHOULDER DROP-OFF (SYMBOL)	CONST-3						SPEEDING/PASSING (PLATE)			
-17P 30X24 5.00		SHOULDER DROP-OFF (PLAQUE)			MISCELLAN	IEOUS	SIGNS	-		ľ		
-1 42RND 9.62		RAILROAD CROSSING	CONST-5	48X3€	5 12.00				POINT OF PRESENCE			
2-1 24X24 4.00		DOUBLE DOWN ARROW (SYMBOL)	CONST-5						POINT OF PRESENCE	1		
2-2 48X48 16.0		LOW CLEARANCE (SYMBOL)	CONST-7						RATE OUR WORK ZONE	1		
2-2X 24X18 3.00		LOW CLEARANCE (PLAQUE)	CONST-7						RATE OUR WORK ZONE	1		
2-2a 84X24 14.0		OVERHEAD LOW CLEARANCE (FEET AND INCHES)	CONST-8					1	WORK ZONE NO PHONE ZONE			
2-4 120X60 50.0		LOW CLEARANCE XX FT XX IN XX MILES AHEAD	R-10	24X18	3.00 4 12			:	SIDEWALK CLOSED USE OTHER SIDE			
2-5 120X60 50.0		WIDTH RESTRICTION XX FT XX IN XX MILES AHEAD		\top						1		
3-1 30X30 6.25		ADVISORY SPEED (PLAQUE)		\top						1		
5-2 30X24 5.00		XXX FEET (PLAQUE)	1	\top						1		
5-3 30X24 5.00		X MILE (PLAQUE)		T								
0-1 48X48 16.0		ROAD/BRIDGE/RAMP WORK AHEAD		\top						1		
)-2 48X48 16.0		DETOUR AHEAD								1		
0-3 48X48 16.0		ROAD CLOSED AHEAD		\top						1		
0-4 48X48 16.0		ONE LANE ROAD AHEAD	616-10	J. 05	TOTAL		•			-		
0-5 48X48 16 0		RIGHT/CENTER/LEFT LANE CLOSED AHEAD			ON SIGNS 362	1						
0-5a 48X48 16.0		2 RIGHT/CENTER/LEFT LANES CLOSED AHEAD	616-10		0.0.10	-	TOTAL					
		RIGHT/CENTER/LEFT LANE CLOSED	RELOCA		SIGNS		N/A*					
1-6a 48X48 16 O		FLAGGER (SYMBOL, WITH FLAGS)	1.2200				,, .					
0-6a 48X48 16.0 0-7a 48X48 16.0	101		1									
-7a 48X48 16.0			1									
-7a 48X48 16.0 -2 36X36 9.00	0	FRESH OIL	**!	TEO.								
7a 48X48 16.0 2 36X36 9.00 5 48X48 16.0	0	FRESH OIL SHOULDER WORK AHEAD] . *NC	OTES:								
-7a 48X48 16.0	00	FRESH OIL			TITIES ON THIS SUFFE	· ADE E	こエルルヘエに		THE ENTIRE PACKAGE.			SCHEDULE OF QUANTITIE

1. QUANTITIES ON THIS SHEET ARE ESTIMATED FOR THE ENTIRE PACKAGE. 2. TRAFFIC CONTROL ITEMS TO BE PAID FOR AS A LUMP SUM PAY ITEM.

3. NO DIRECT PAYMENT FOR RELOCATION OF TRAFFIC CONTROL DEVICES.

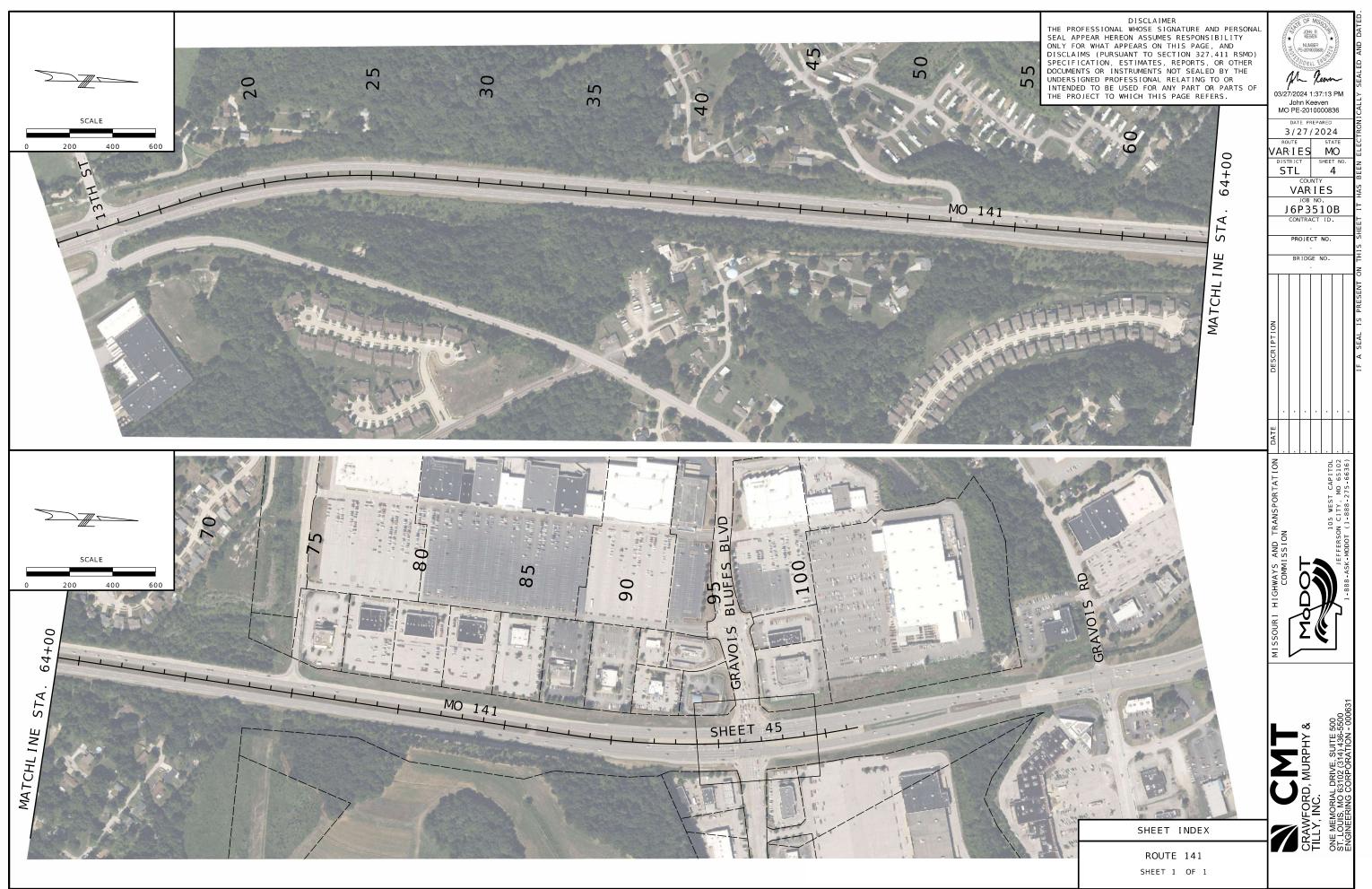
GO22-1 21X15 2.19

WET PAINT (ARROW PIVOTS)

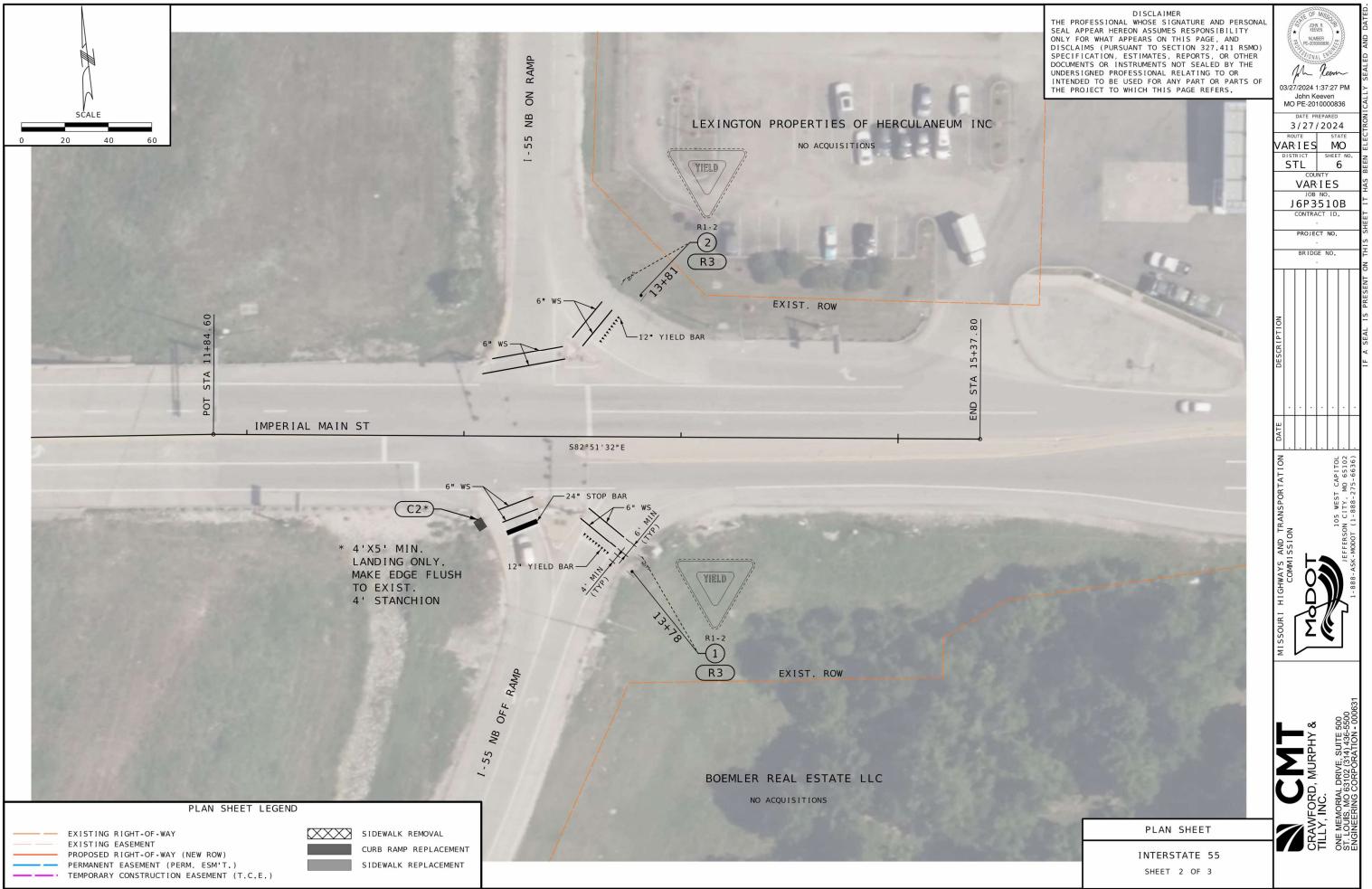
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INTRACT ID. OJECT NO. ENGINEERING DESIGN SOURCE, INC.

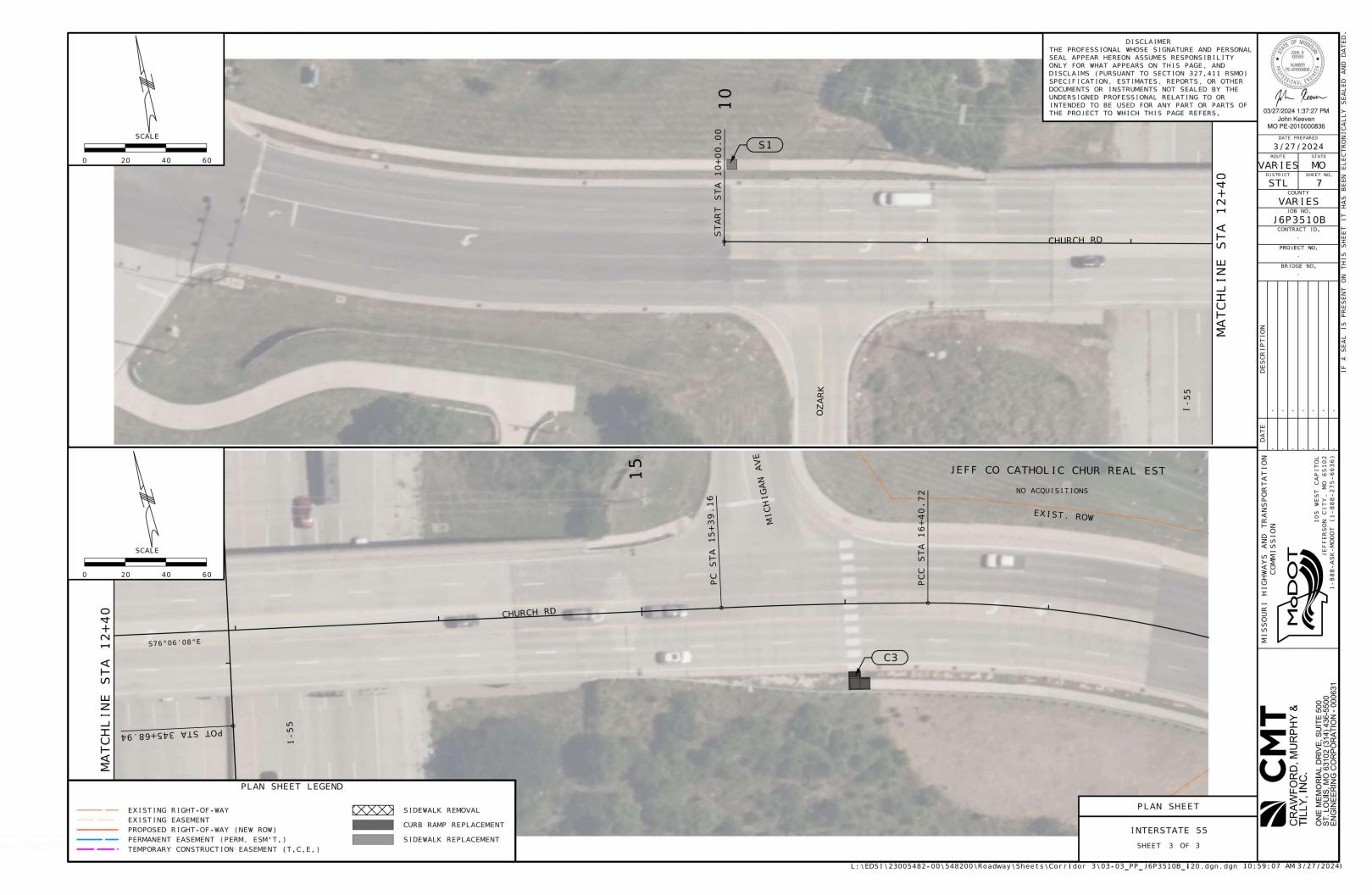
16141 Swingley Ridge Rd., Suite 300

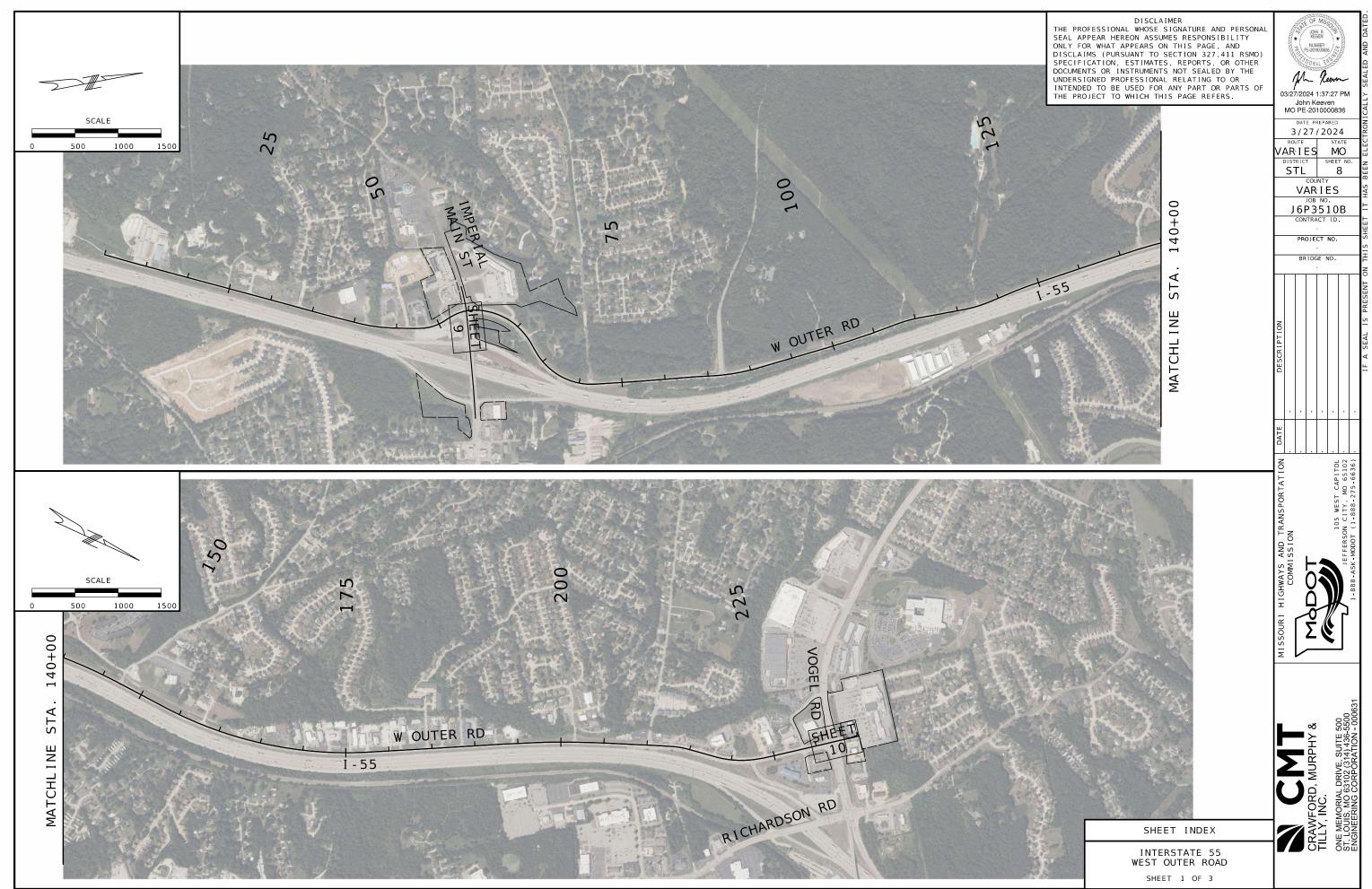
1. 636 537 6385
Chesterfledt, Missouri 63017
Rissouri State Engineering Corporation #001523 SCHEDULE OF QUANTITIES

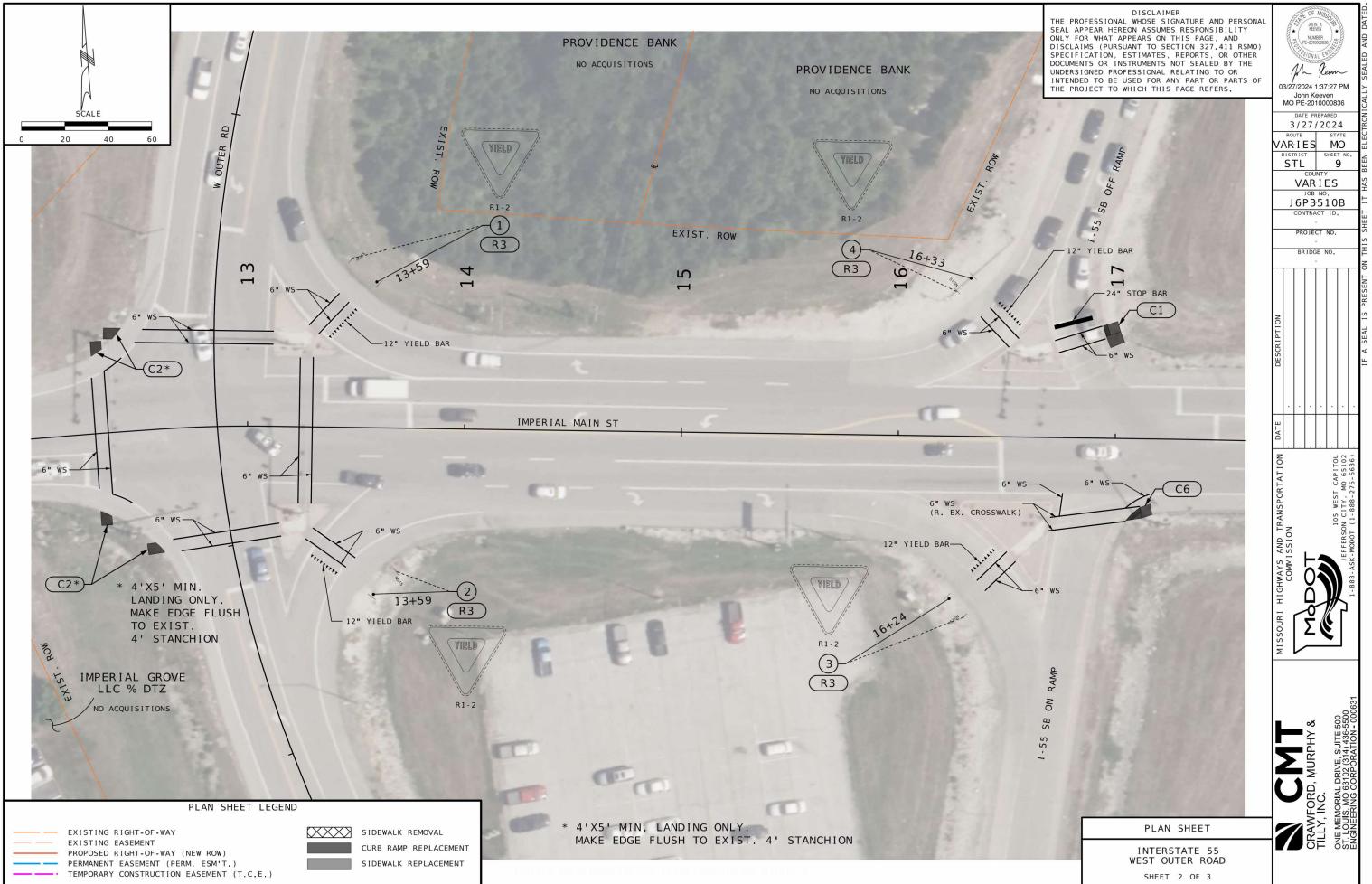


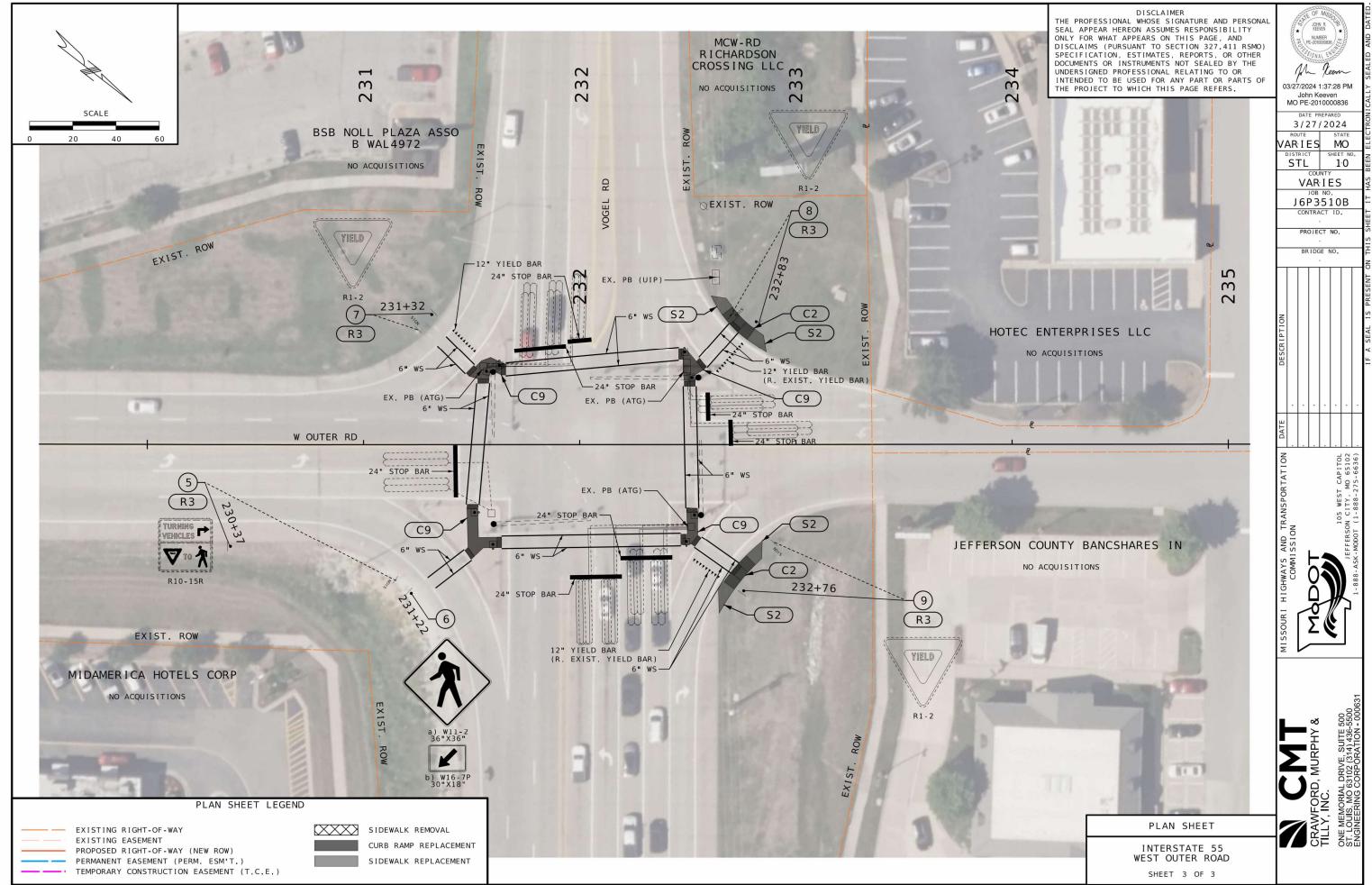


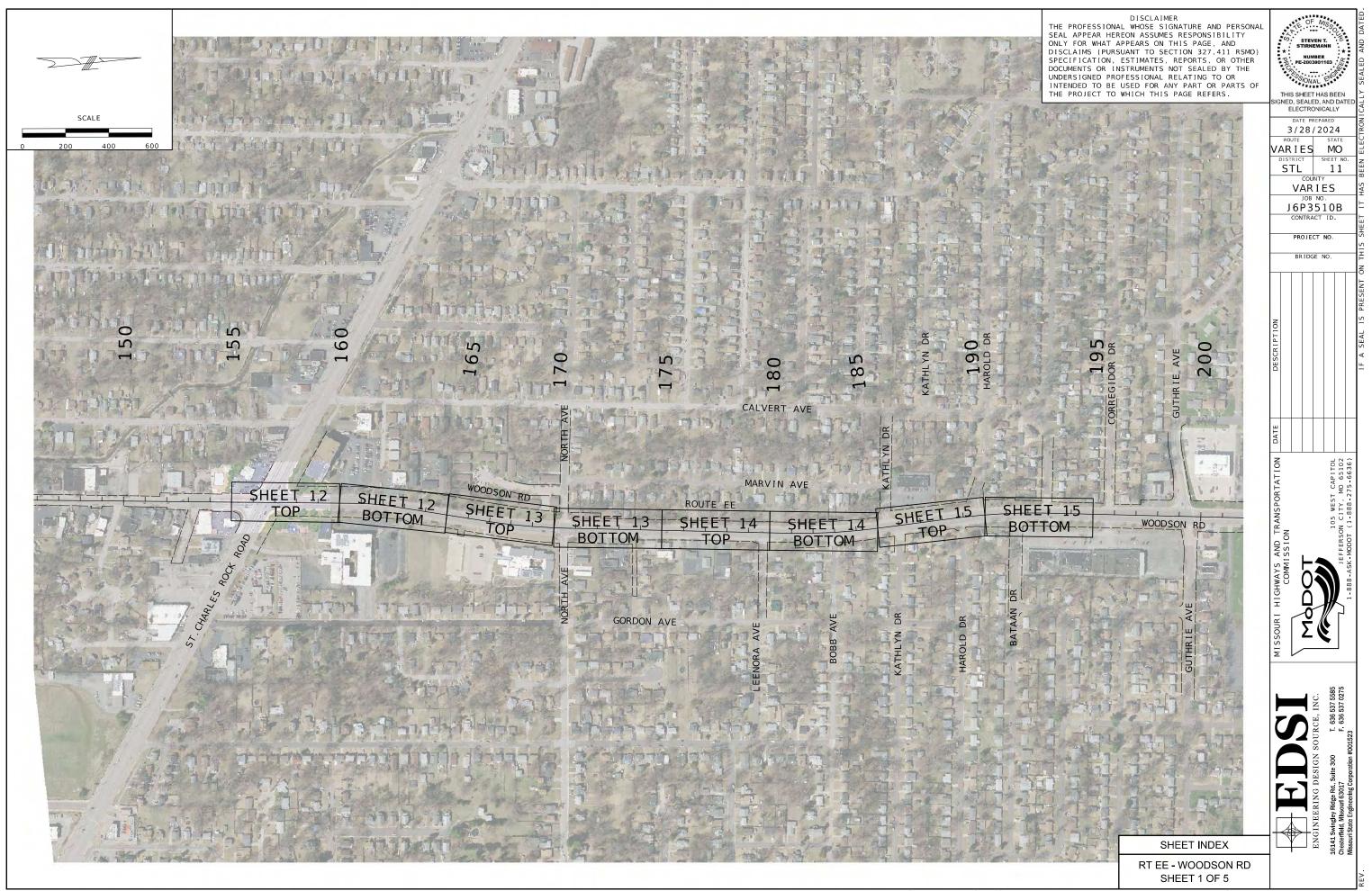


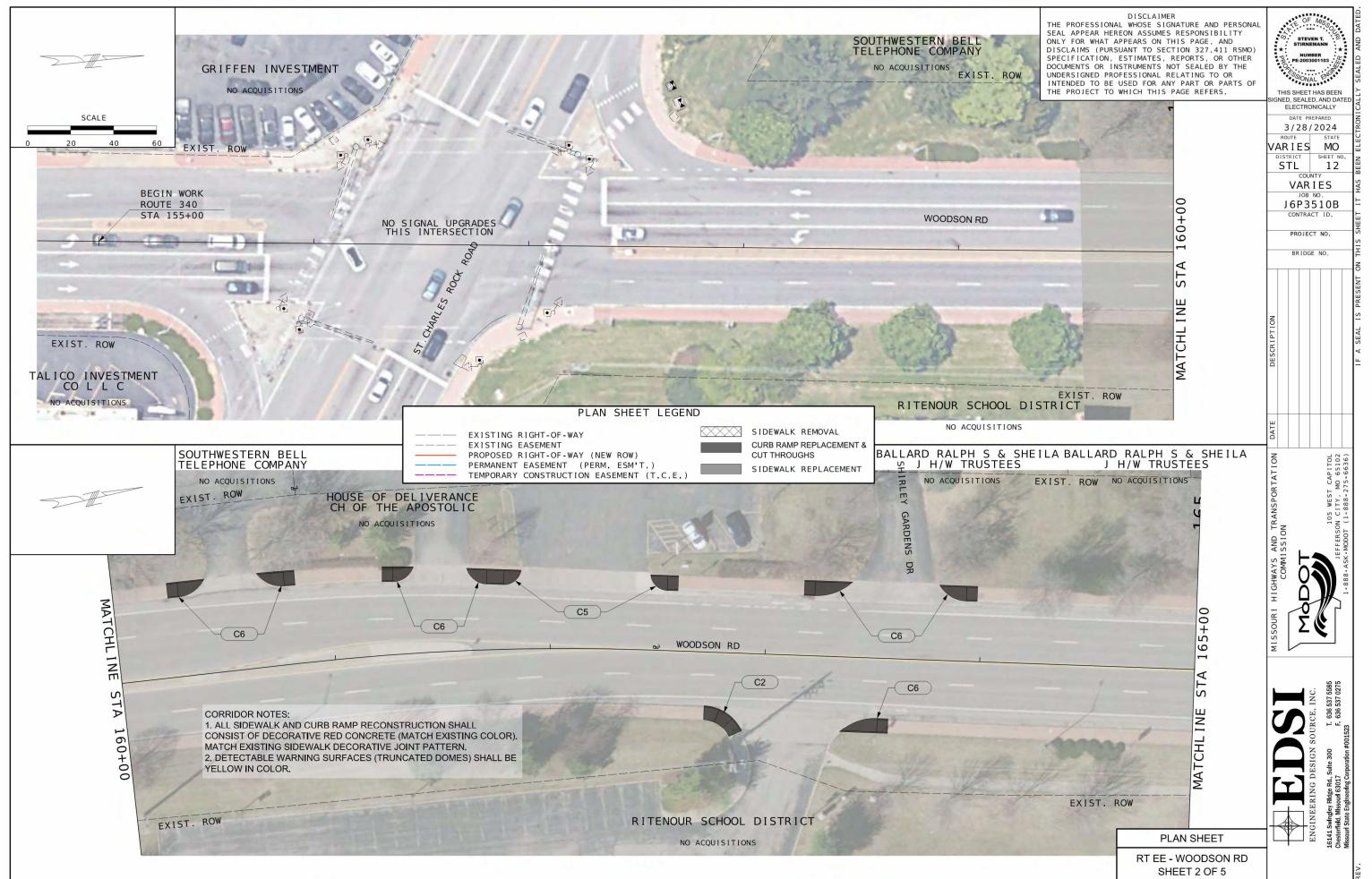


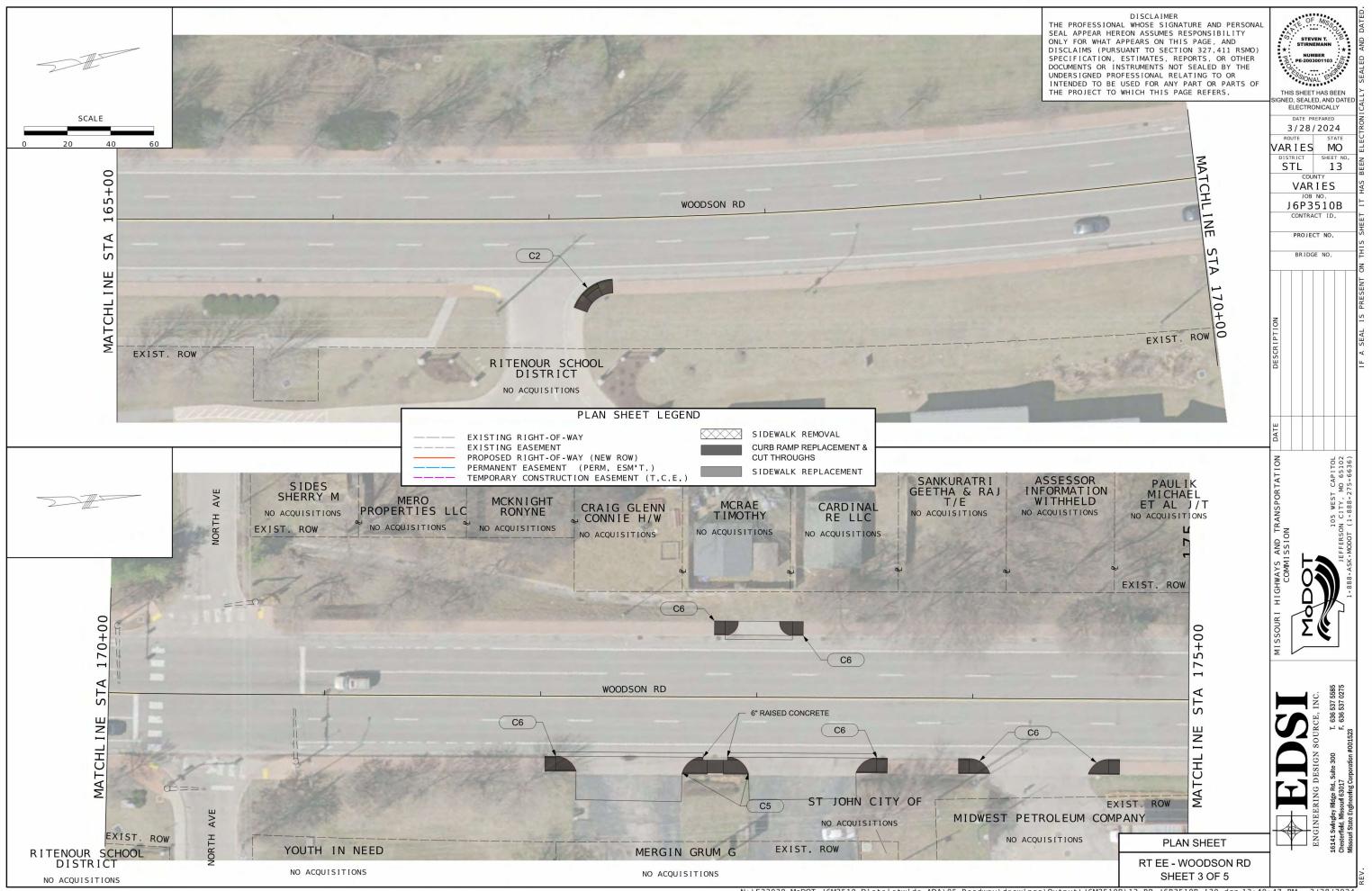


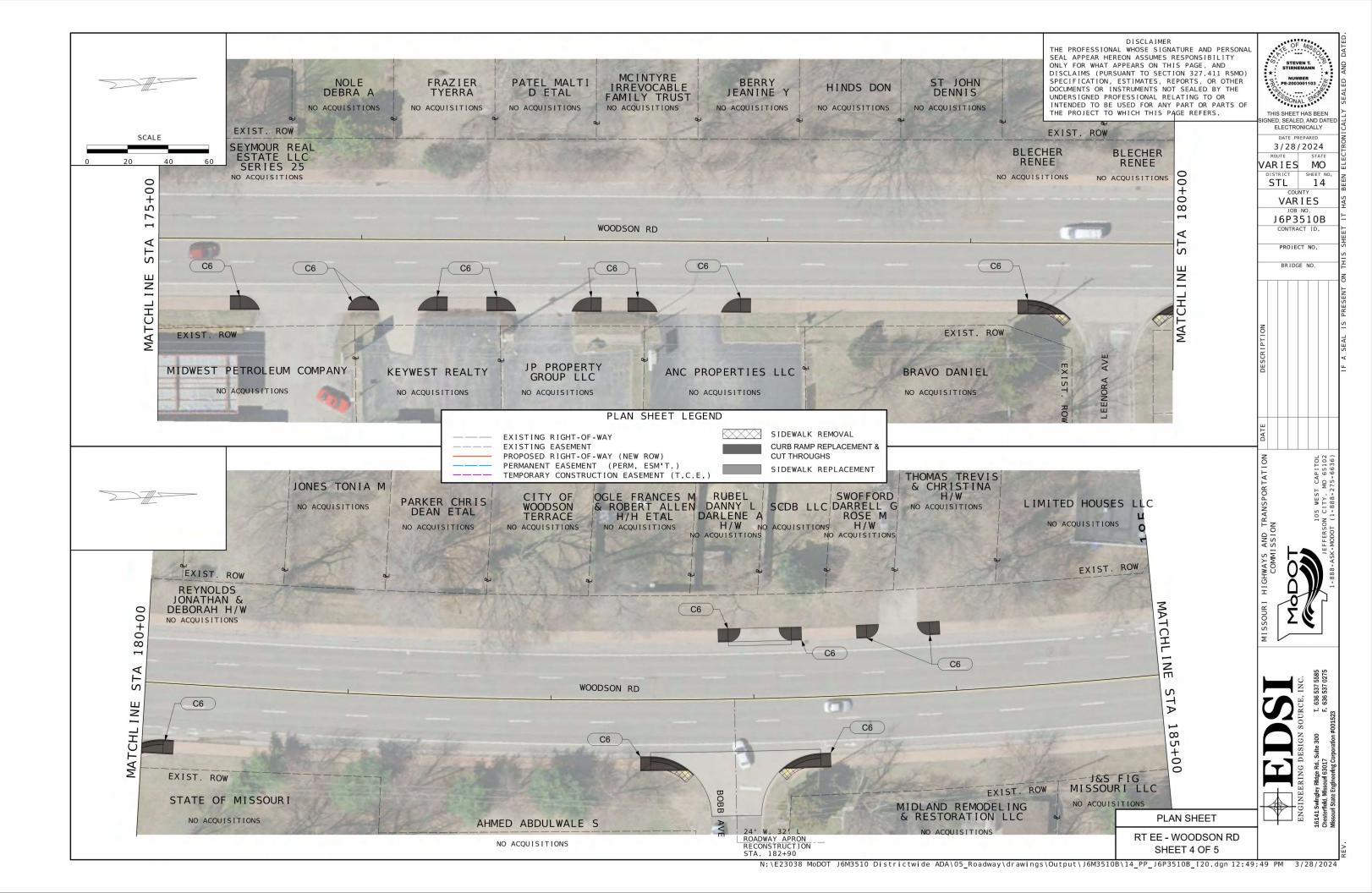


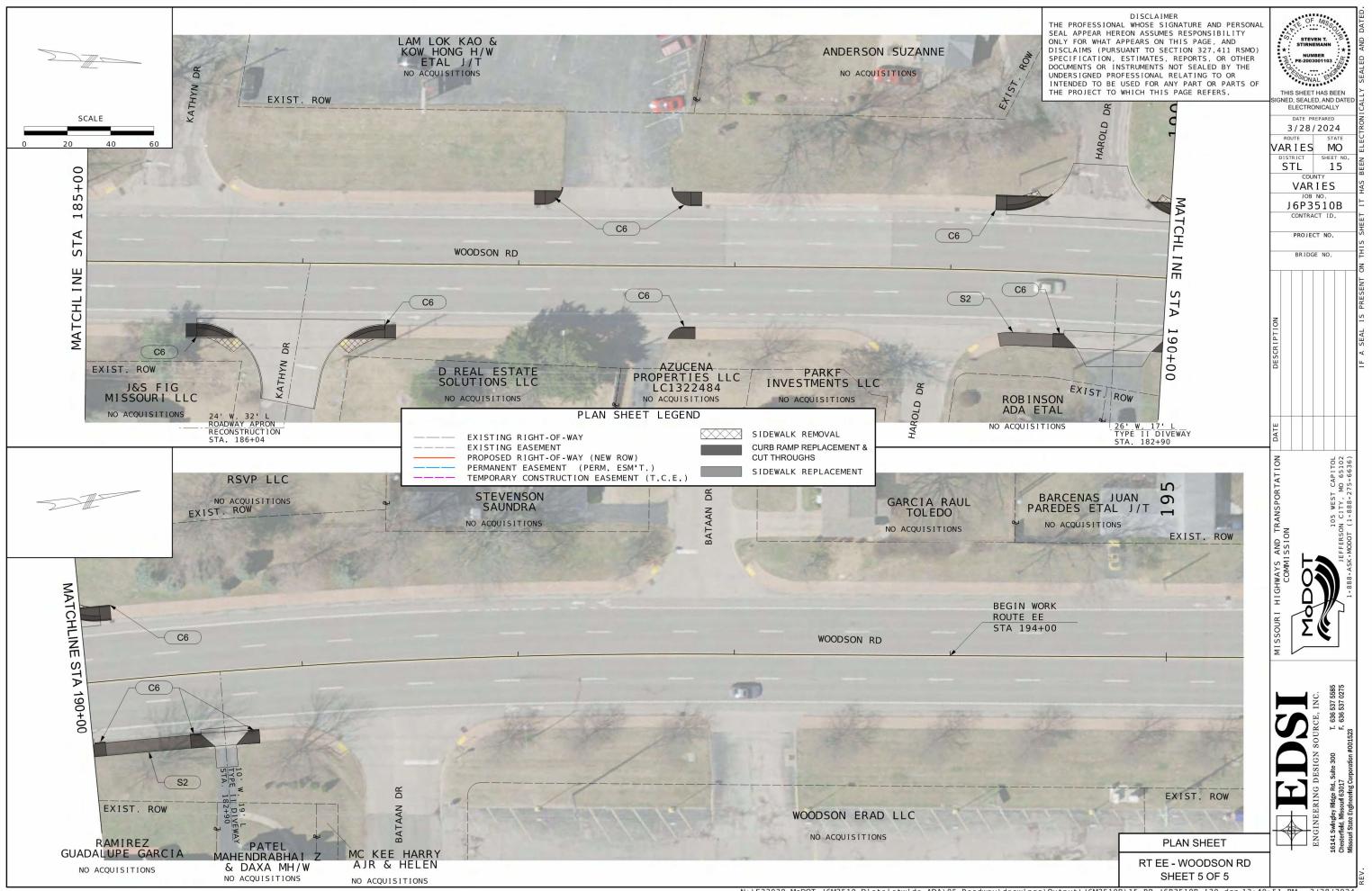




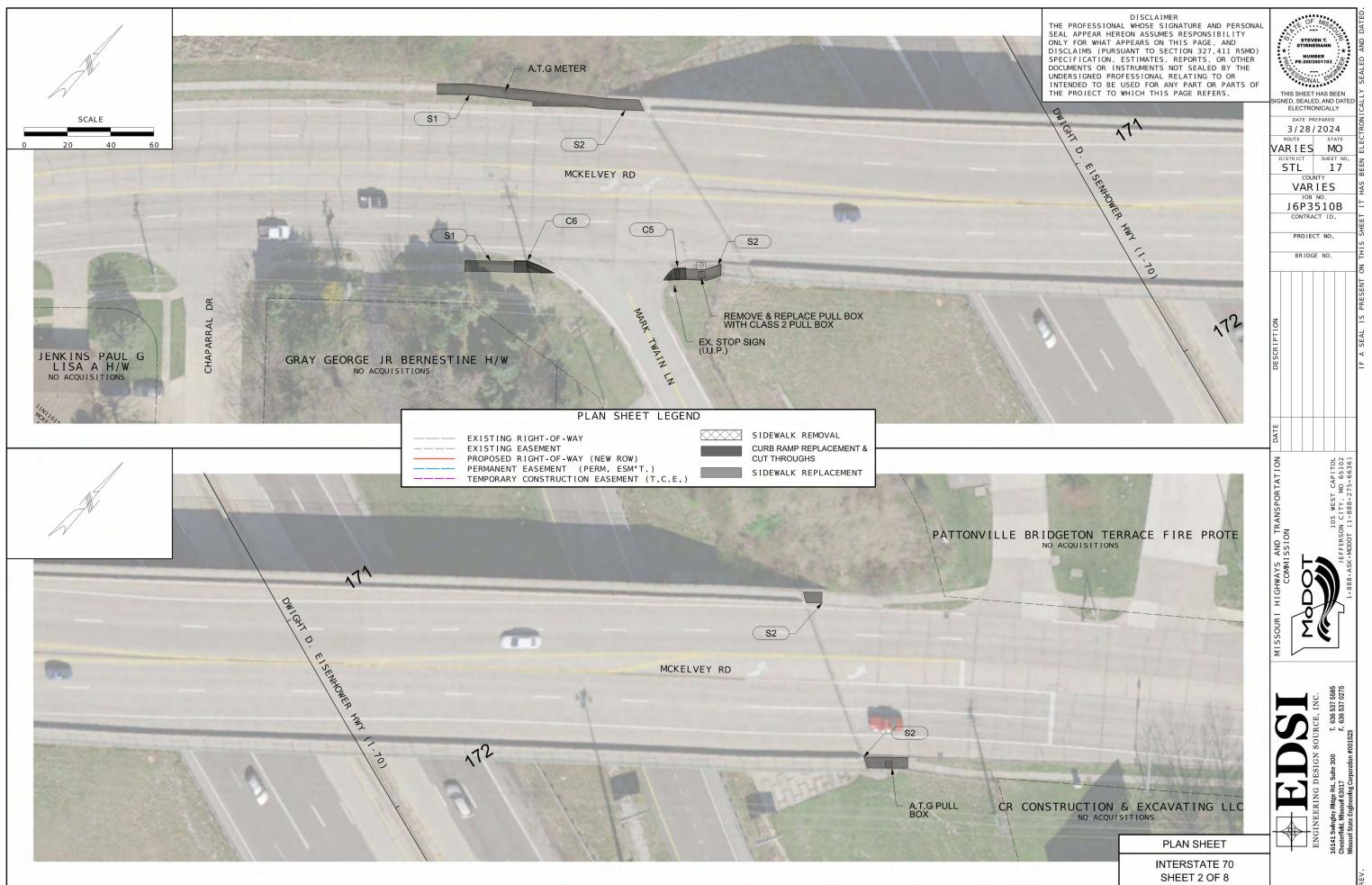


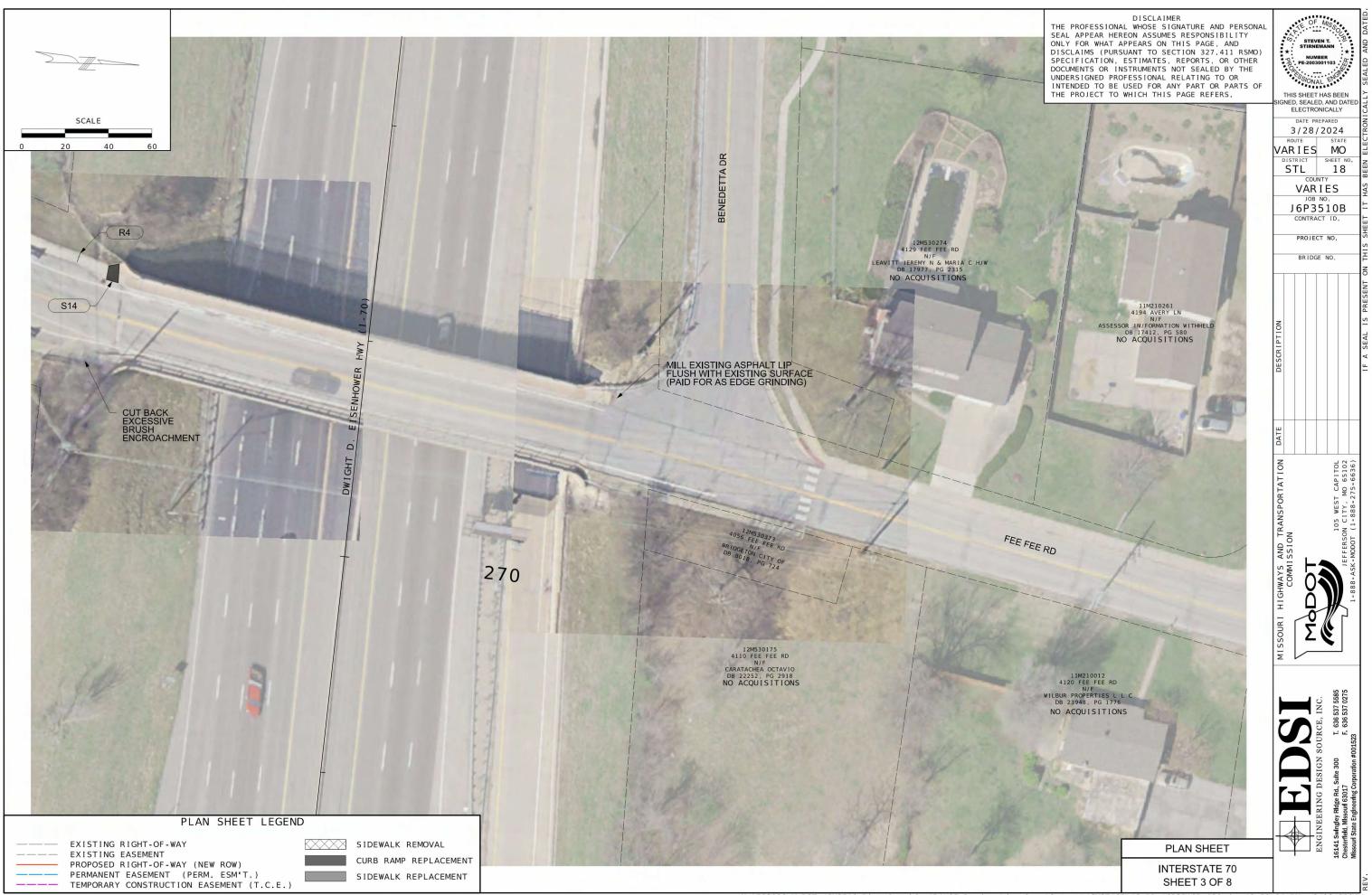


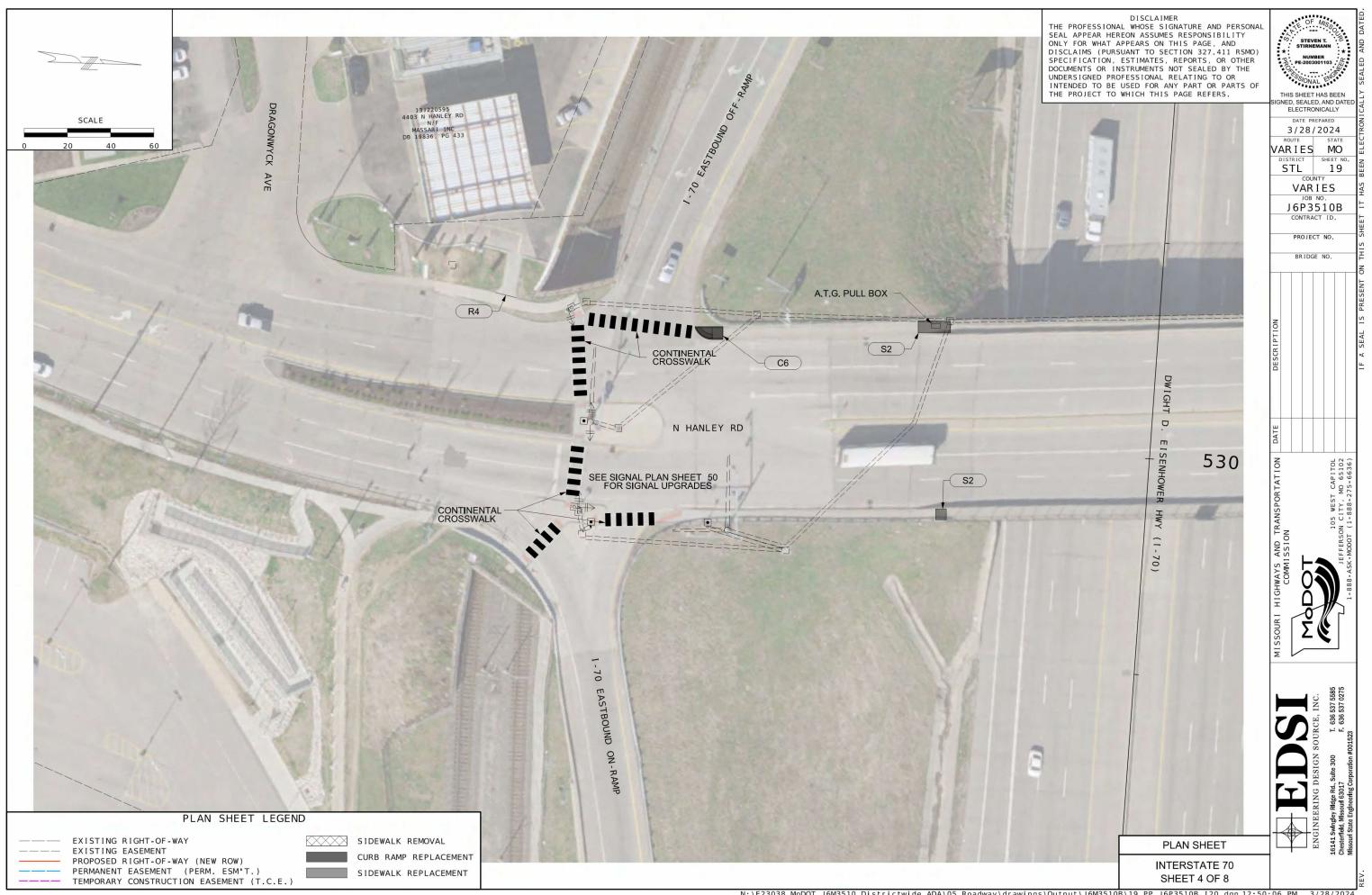


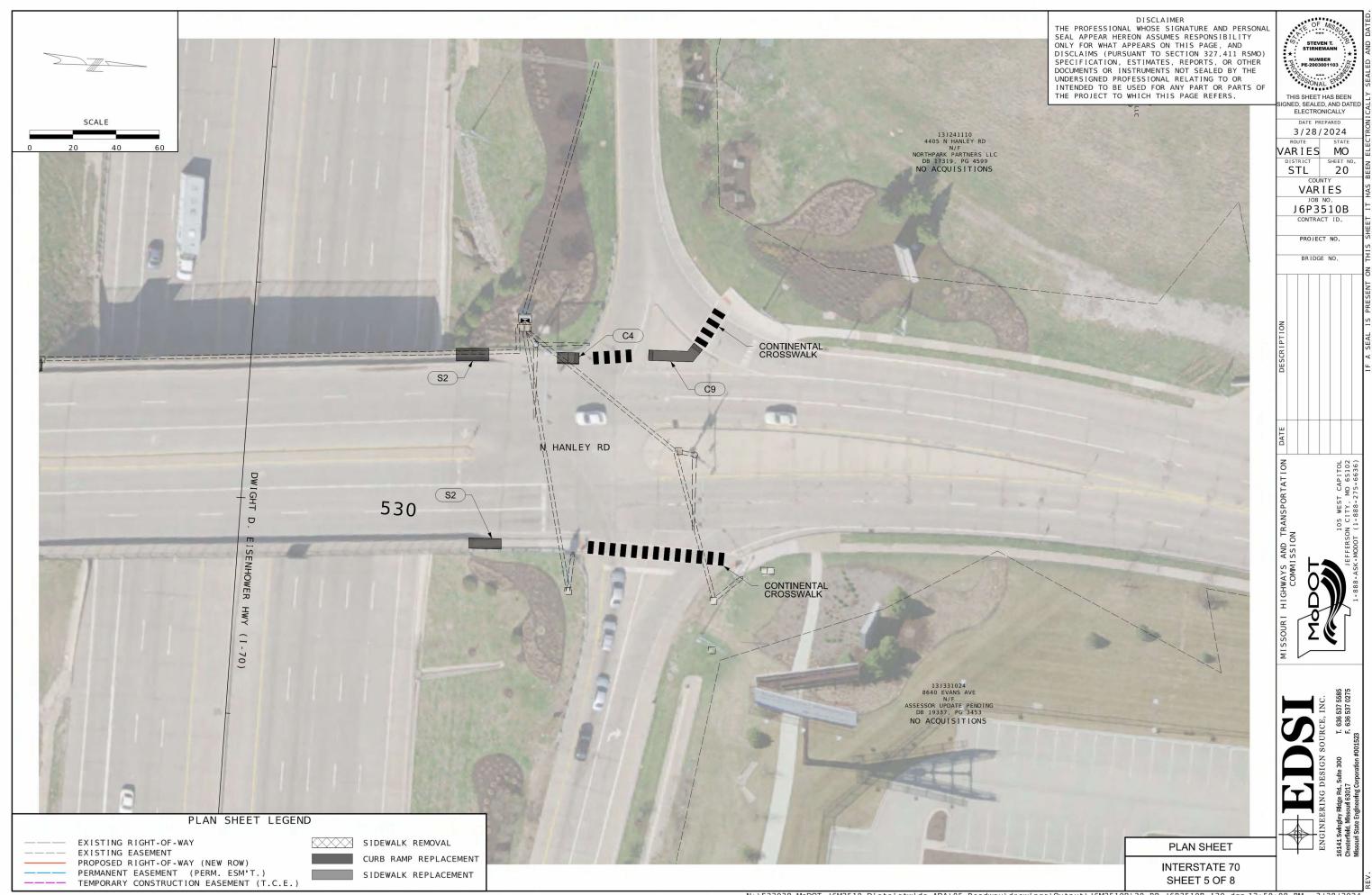


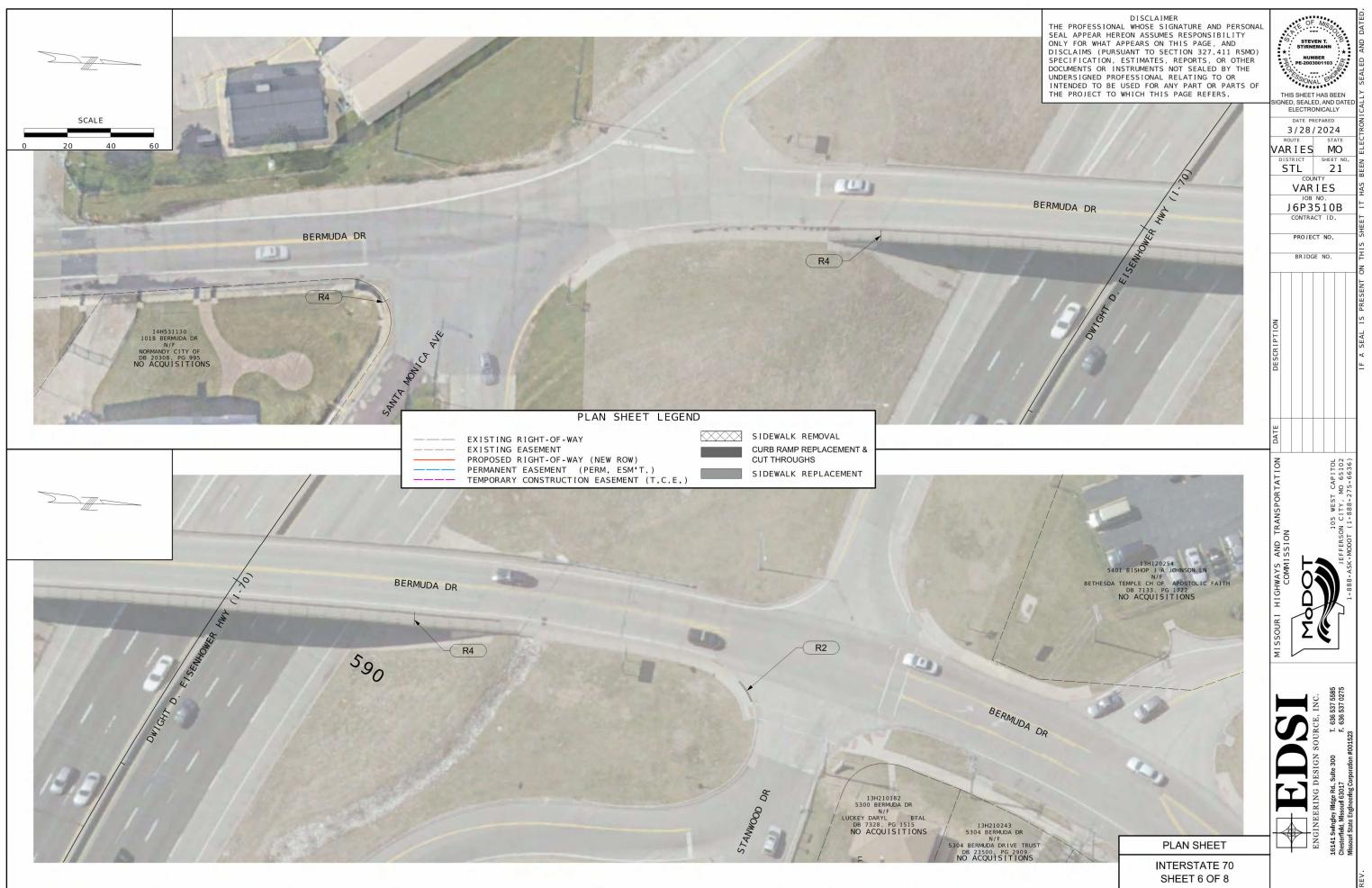


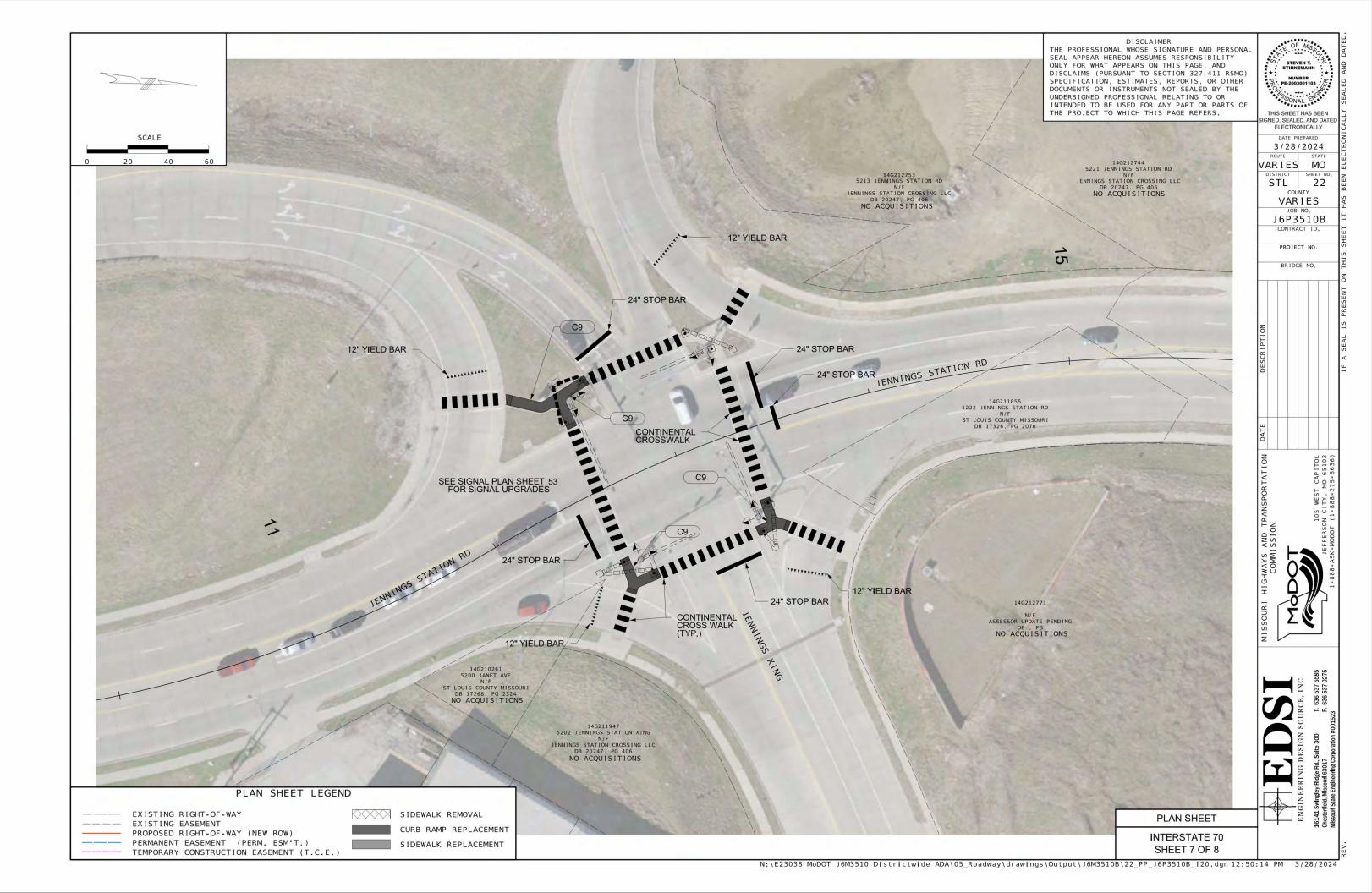


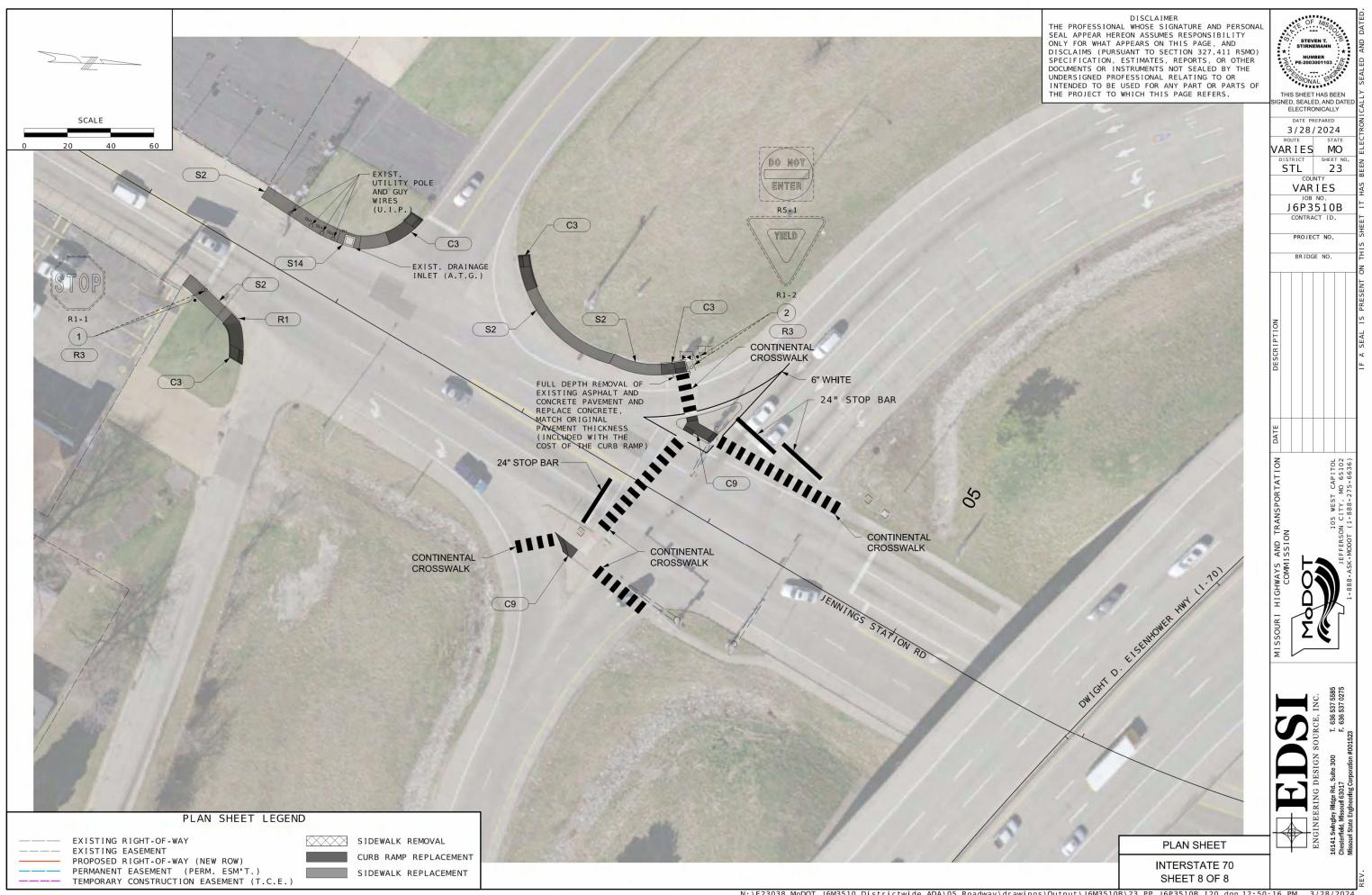


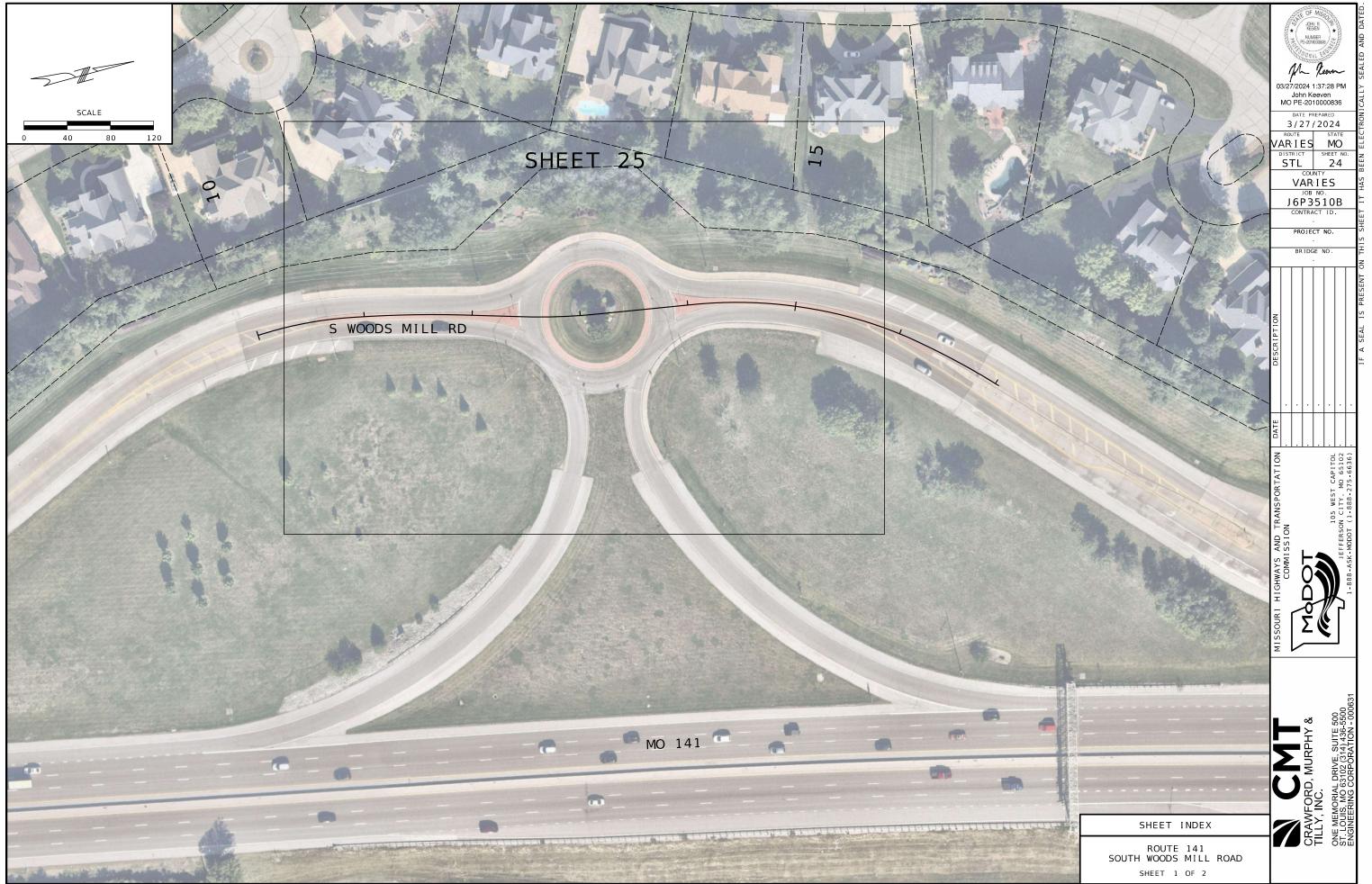


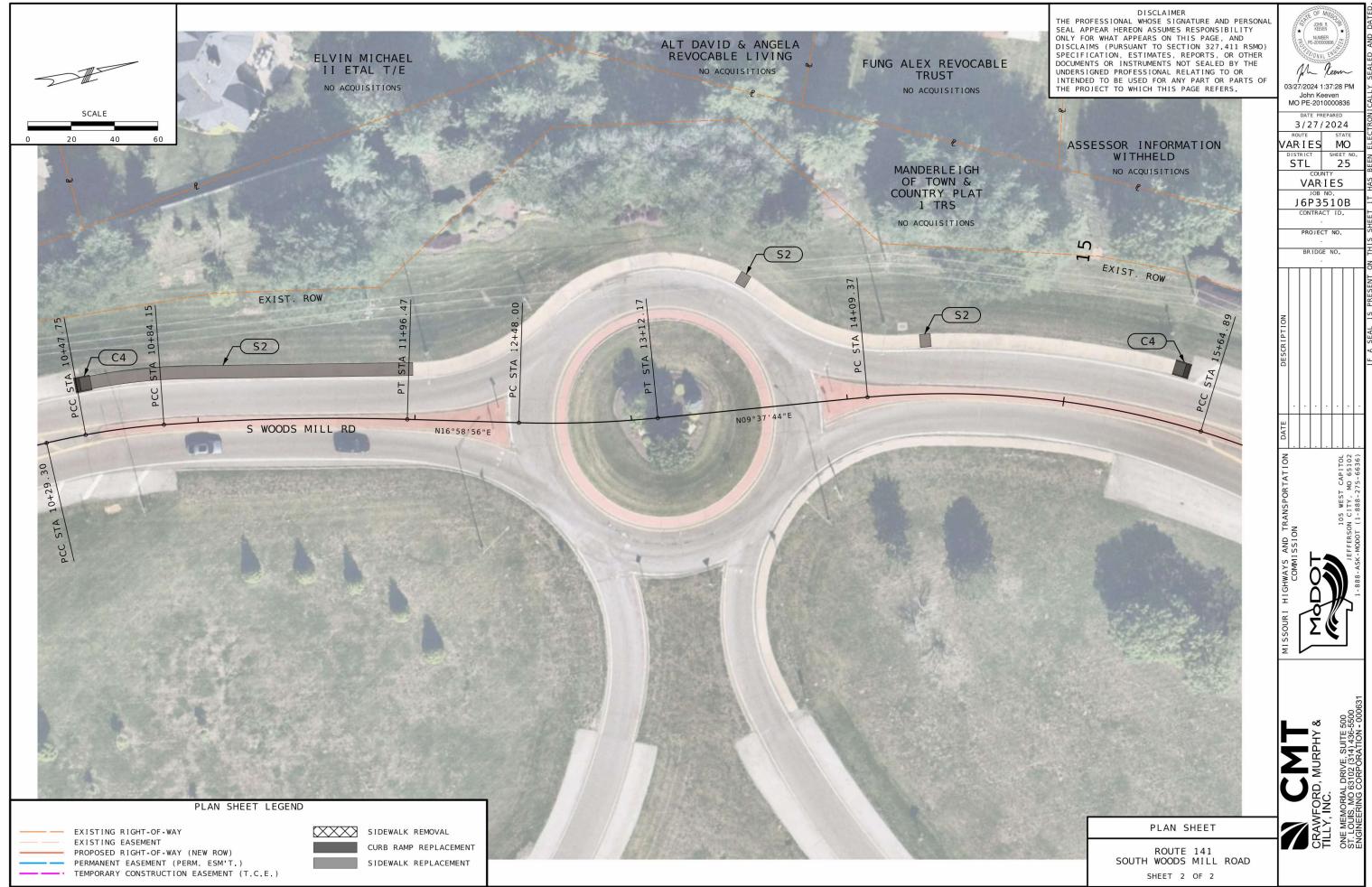


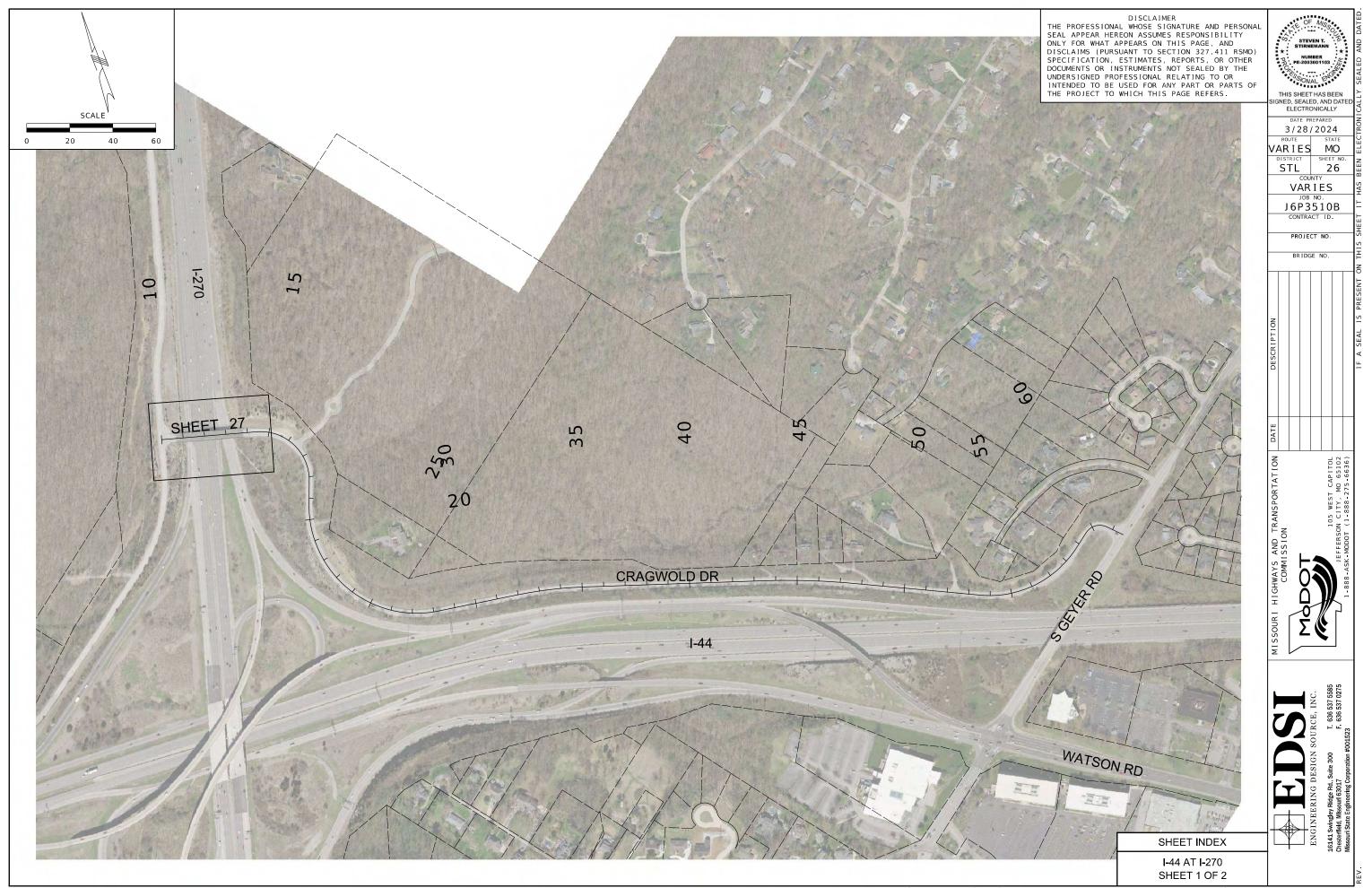


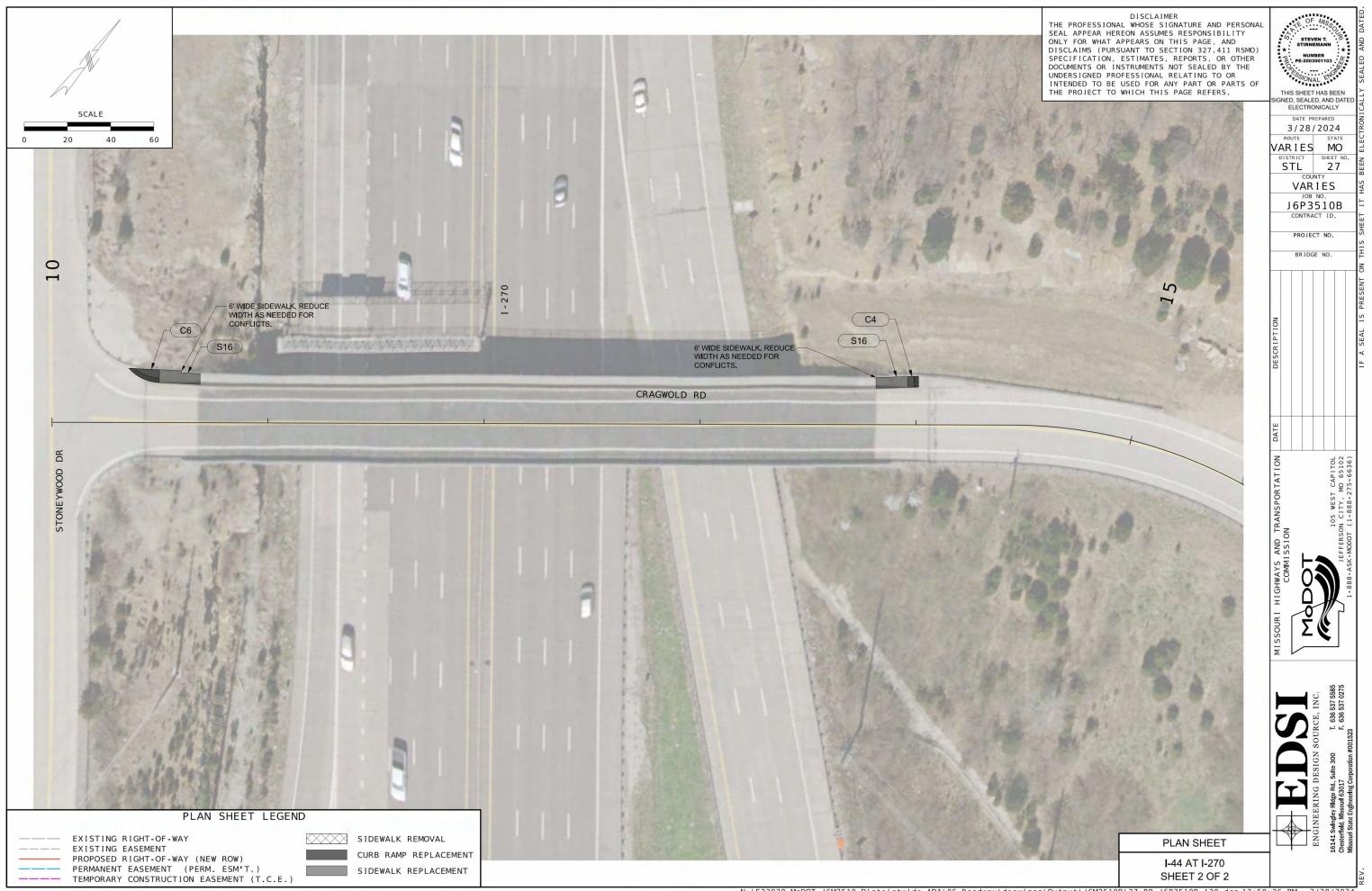




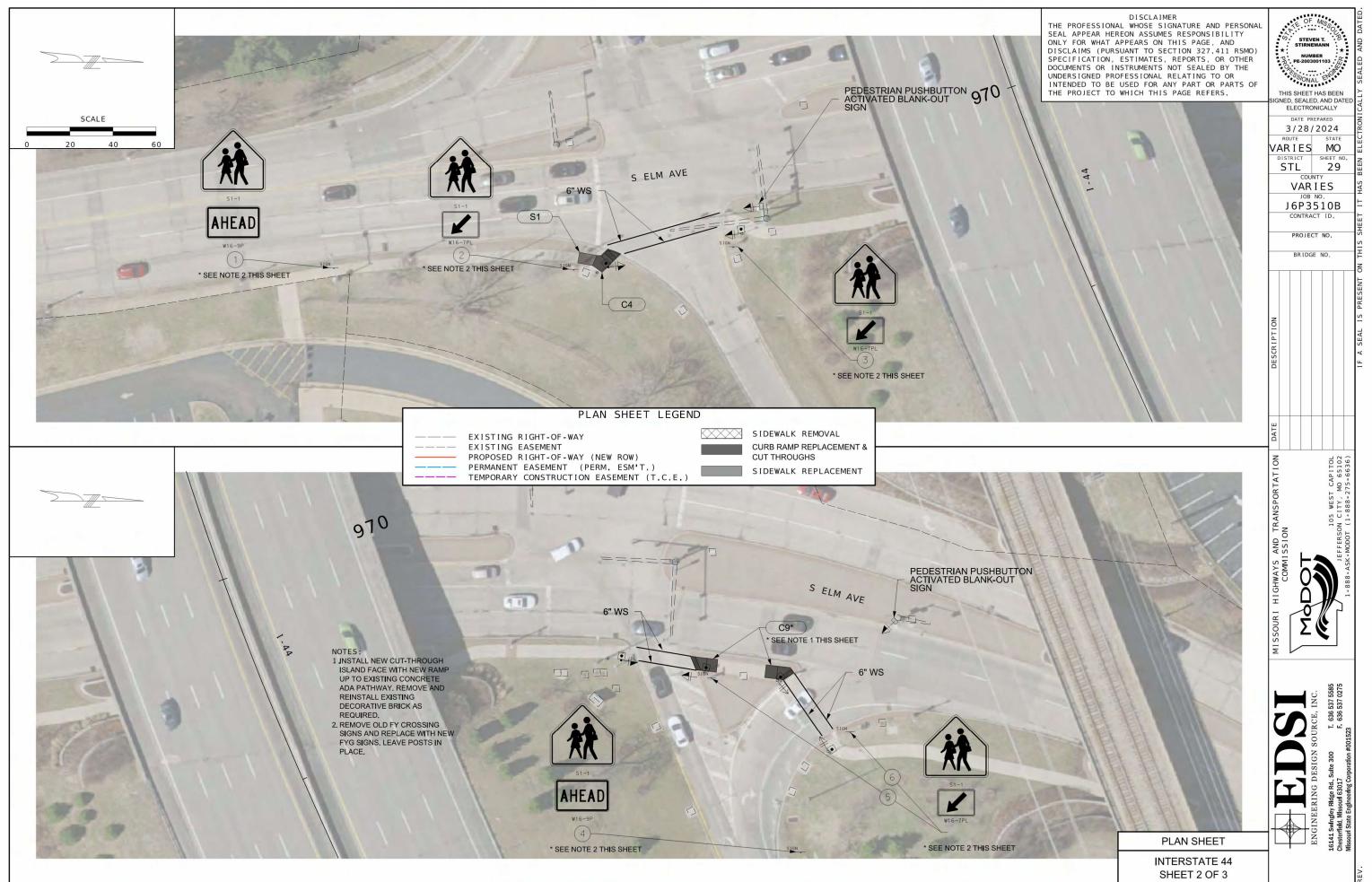


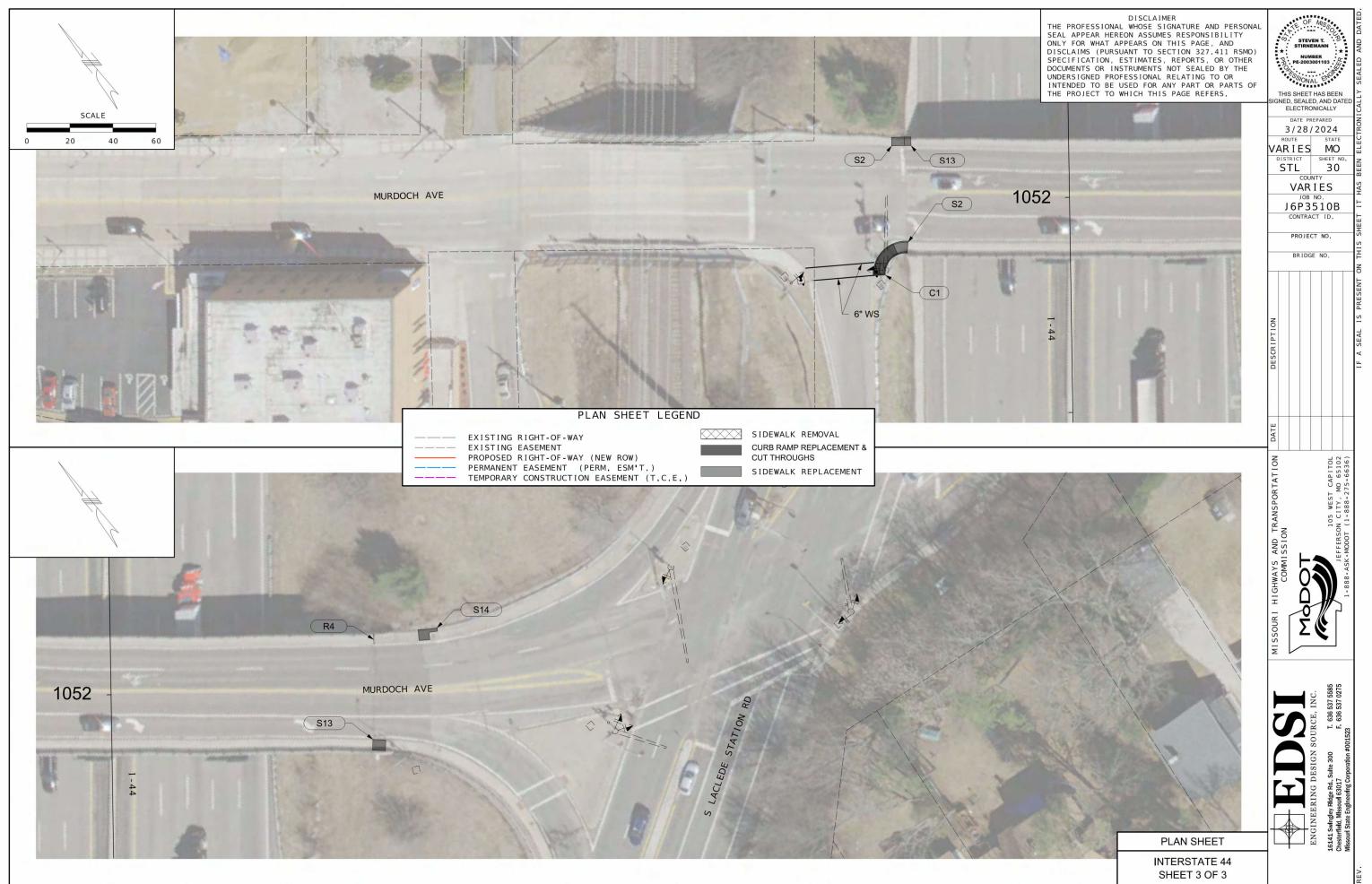


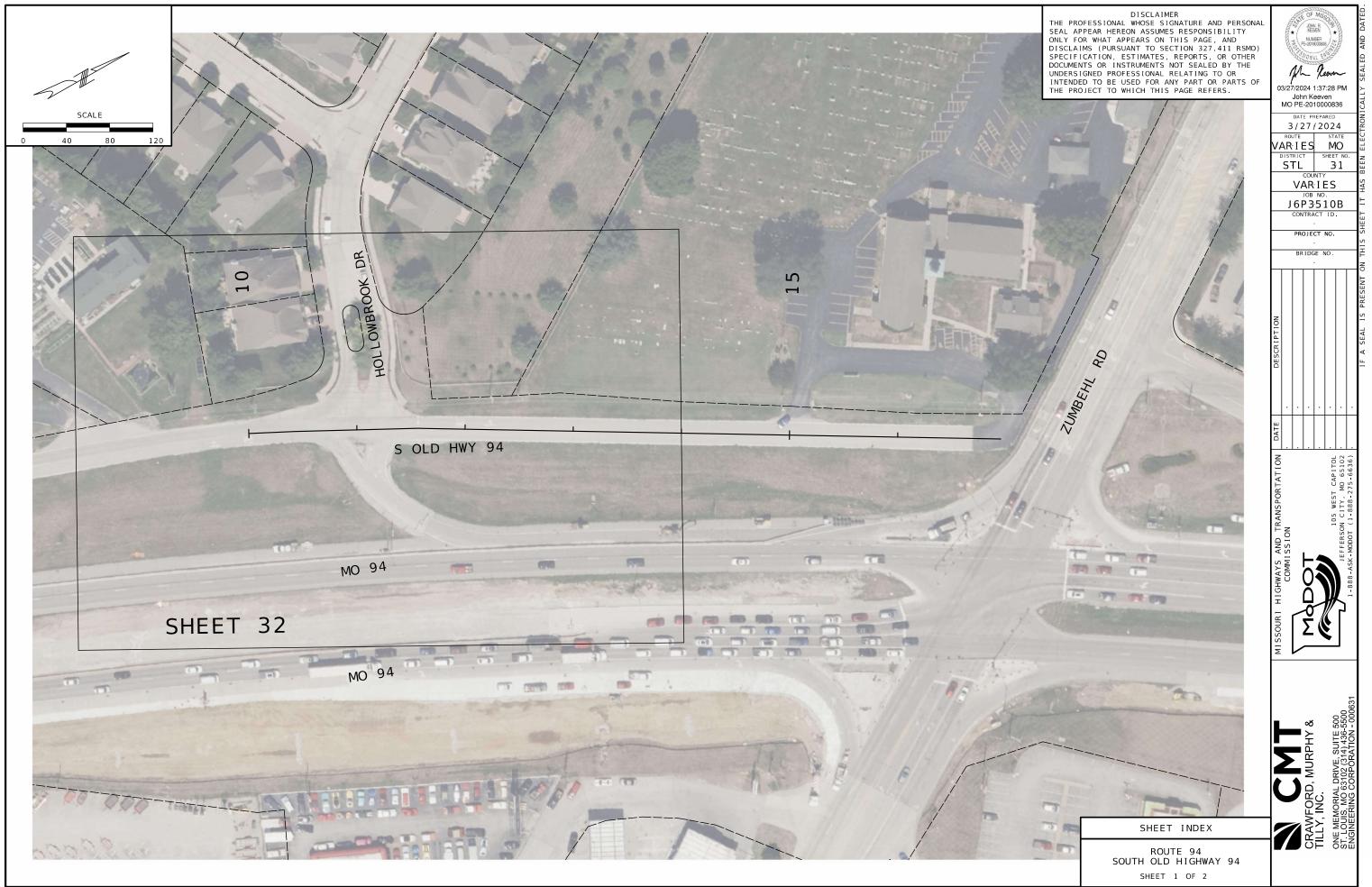


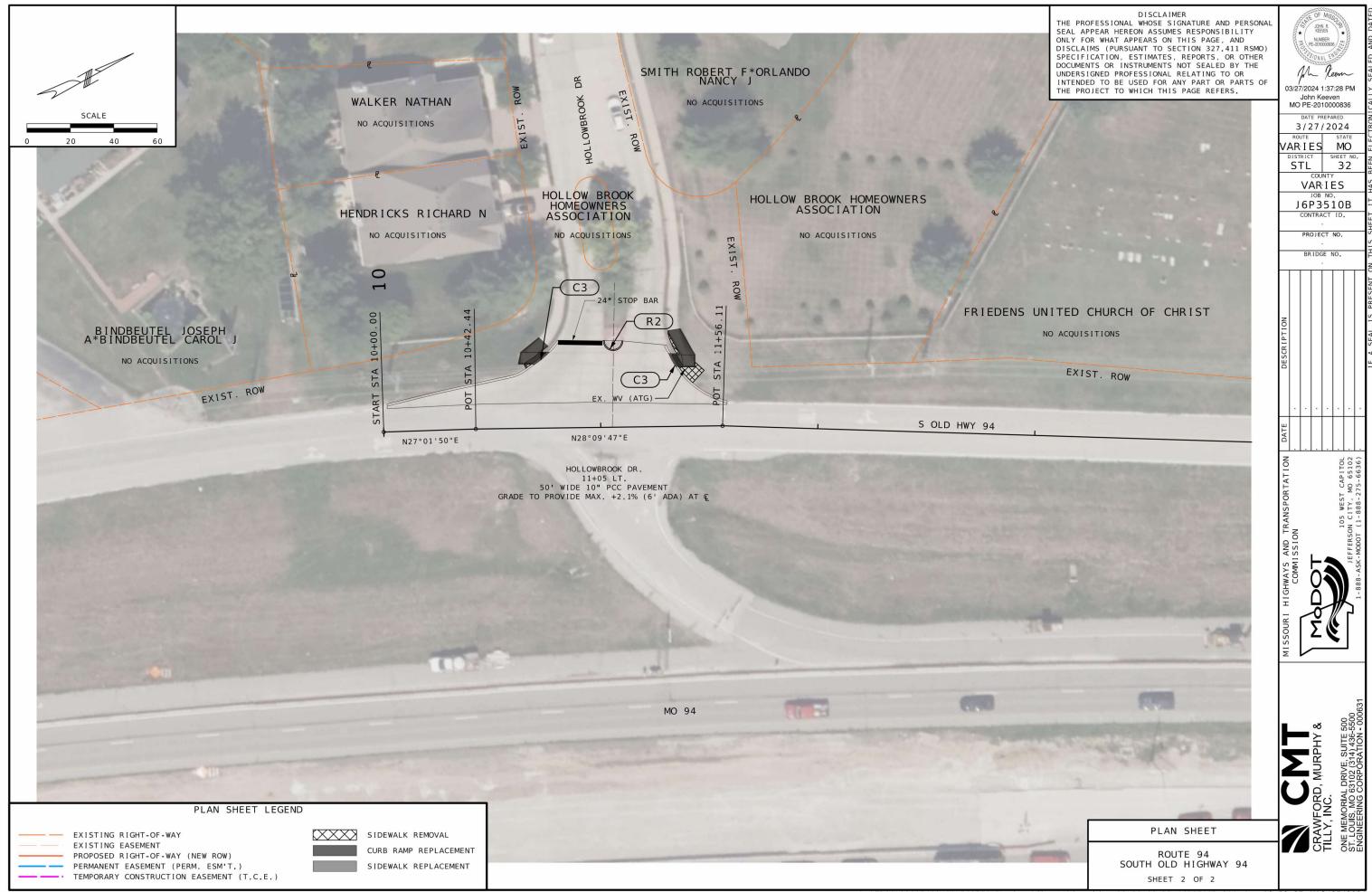


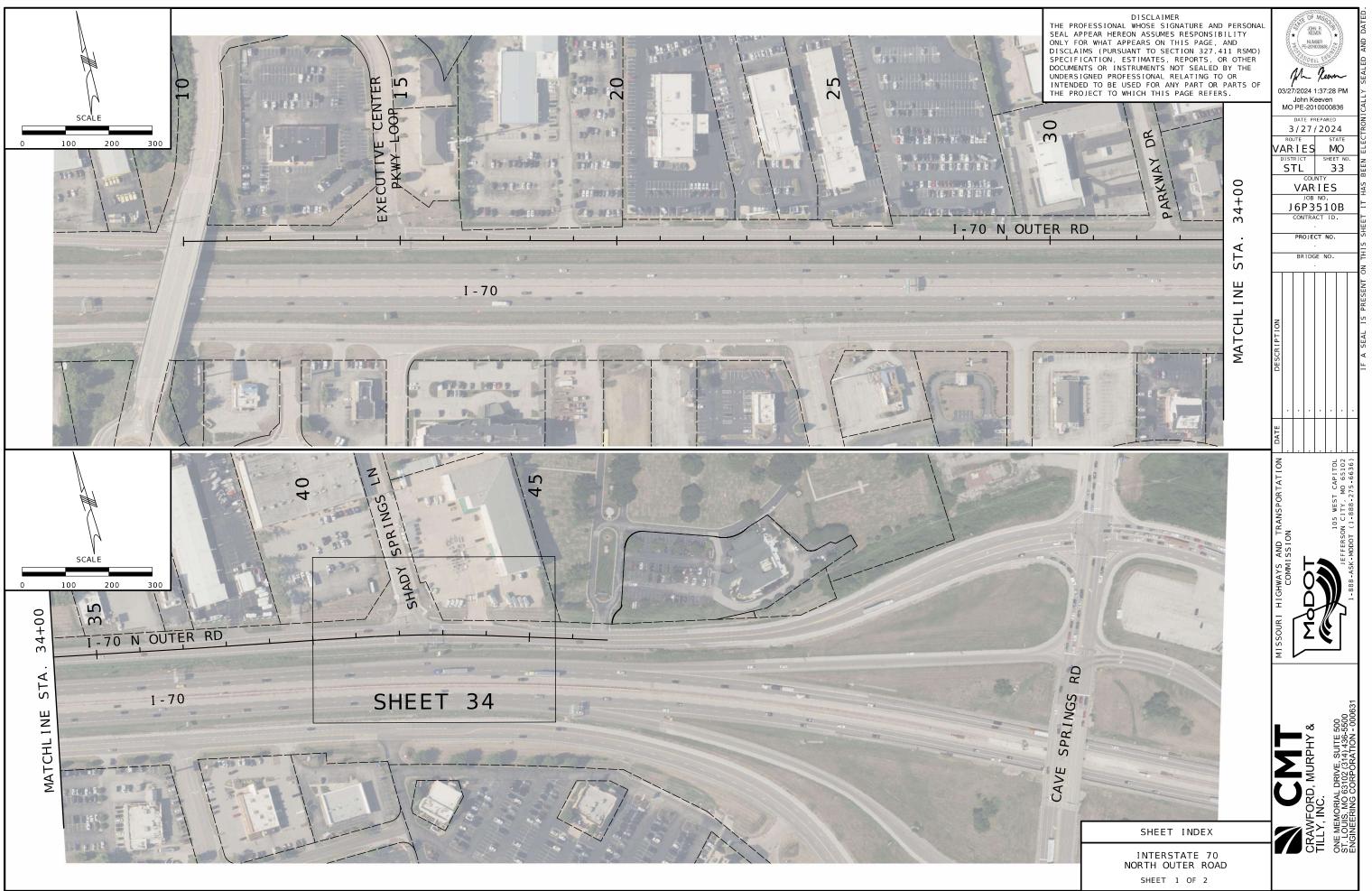


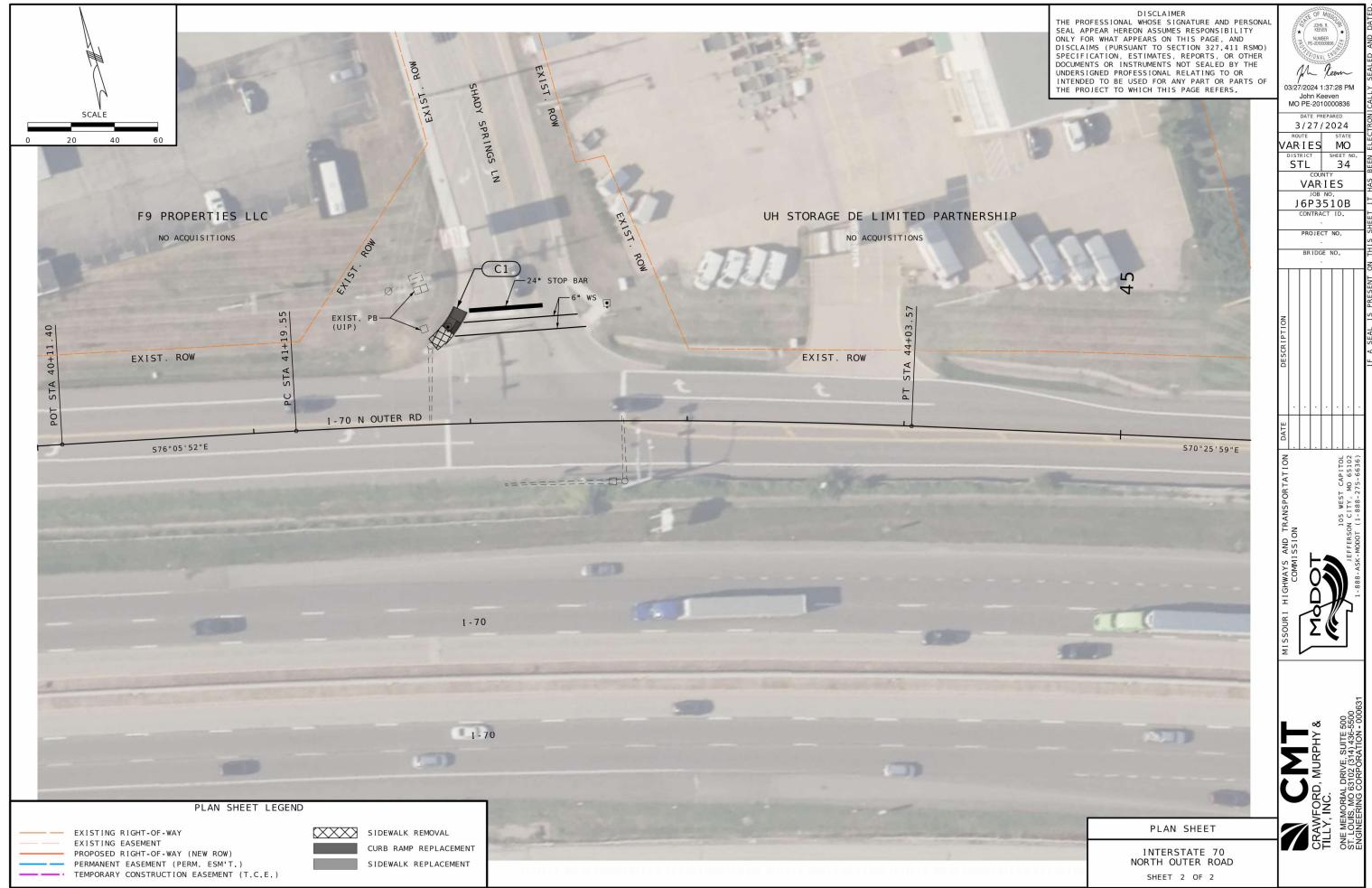












TRAFFIC CONTROL NOTES:

- TRAFFIC CONTROL SHALL BE MAINTAINED IN ACCORDANCE WITH THE CURRENT 1. EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- CONTRACTOR TO USE FLAGGING OPERATIONS AS REQUIRED FOR ANY PART OF THE TRAFFIC CONTROL SET UP OF BOTH STAGE 1 AND STAGE 2, AS WELL AS ANY OTHER INSTALLATION OF IMPROVEMENTS AS NECESSARY.
- ANY EXISTING SIGNS WHICH CONFLICT WITH THE TRAFFIC CONTROL PLAN SHALL BE 3.
- ALL SPACING AND DISTANCES ARE APPROXIMATE AND MAY BE ADJUSTED BY THE 4. ENGINEER TO FIT FIELD CONDITIONS.

- SIGNING SHOWN SHALL REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE.
 - REFER TO MODOT STANDARD DRAWING 616.10, 617.20, 619.10, AND 620.10 FOR ADDITIONAL DETAILS AND 903.03 FOR SIGN AND SIGN MOUNTING REQUIREMENTS.
- CONTRACTOR TO MAINTAIN PUBLIC ACCESS TO ALL BUSINESSES AND STREETS. NO STREET OR ENTRANCE SHALL BE COMPLETELY BLOCKED.

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THIS SHEET HAS BEEN GNED, SEALED, AND DATED **ELECTRONICALLY**

3/28/2024 VARIES | MO SHEET NO STL 35

VARIES J6P3510B CONTRACT ID.

PROJECT NO.

BRIDGE NO.

TRAFFIC CONTROL **GENERAL NOTES**

SHEET 1 OF 7

10 (OPTIONAL)	36" MIN FI	TRAFFIC CONTROL LEGEND SIGN (SINGLE SIDED) CHANNELIZER (TRIM-LINE) WORK AREA TYPE 3 BARRICADE LONGITUDINAL CHANNELIZING DEVICE FLASHING ARROW PANEL ROAD WORK	SPE NOR POS (MF 0-3 40- 50- 60-
AHEAD	SIDEWALK CLOSED USE OTHER SIDE	AHEAD ADVANCED WARNING RAIL SYS (AWRS) W020-1 13	STEM

	T				OUR OR DIVERS		
SPEED	SIGN SPAC	ING (FT.)	TAPER LENG	GTH (FT.)	OPTIONAL	CHA	NNELIZER SPACING (FT.)
NORMAL POSTED (MPH)	UNDIVIDED (S)	DIVIDED (S)	SHOULDER ¹ (T1)	LANE ² (T2)	BUFFER LENGTH (FT.) (B)	TAPERS	e ^U FFER/WORK AREAS
0-35	200	200	70		250	15	25
40-45	350	500	150		360	20	50
50-55	500	1000	185		495	50	100
60-70	SA - 1000, S AND SC		235	-	730	60	100

NOTES:

WHEN CROSSWALKS OR OTHER PEDESTRIAN FACILITIES ARE CLOSED OR RELOCATED, TEMPORARY FACILITIES SHALL BE DETECTABLE AND SHALL INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH FEATURES PRESENT IN THE EXISTING PEDESTRIAN FACILITY.

WHERE SIDEWALKS EXIST, PROVISIONS SHOULD BE MADE FOR DISABLED PERSONS.

WHERE HIGH SPEEDS ARE ANTICIPATED, A TEMPORARY TRAFFIC BARRIER AND, IF NECESSARY, A CRASH CUSHION SHOULD BE USED TO SEPARATE THE TEMPORARY SIDEWALKS FROM TRAFFIC.

ONLY THE TEMPORARY TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL TRAFFIC.

SIGNS MAY BE MOUNTED ON PORTABLE MOUNTS AT 1FT PROVIDED THEY DO NOT INTERFERE WITH PEDESTRIAN MOVEMENT OR BE OBSTRUCTED BY PARKING. OTHERWISE, SIGNS SHALL BE MOUNTED AT 7FT

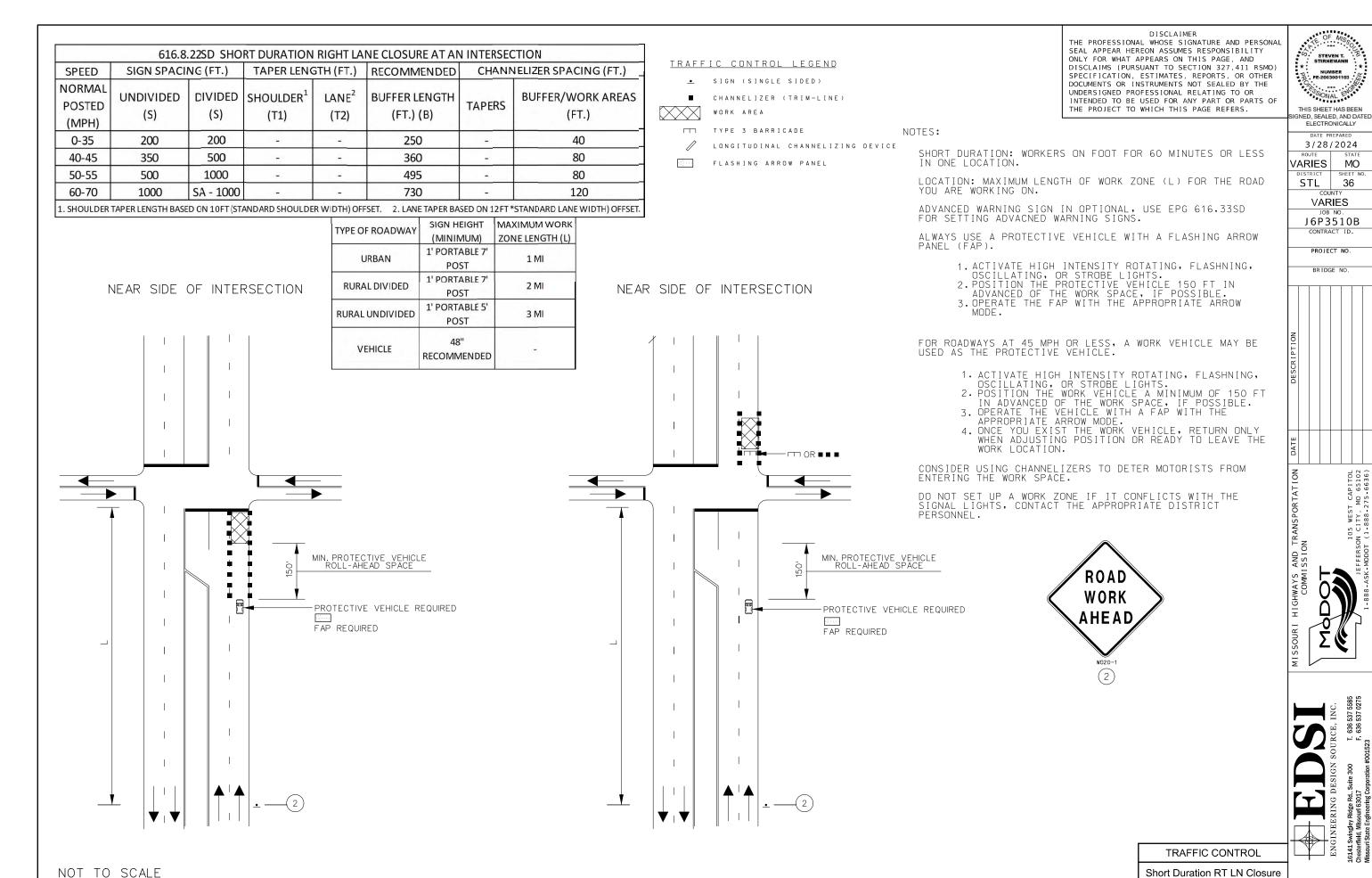
FOR HIGH SPEED FACILITIES, CHANNELIZER SPACING MAY BE REDUCED TO 1/2 SPACING NOTED IN TABLE.

OTHER APPROPRIATE SIGNS MAY BE USED IN LIEU OF SIGNS 12 OR 13.

AUDIBLE INFORMATION DEVICES SHOULD BE CONSIDERED WHERE MIDBLOCK CLOSINGS AND CHANGED SIDEWALK AREAS CAUSE INADEQUATE COMMUNICATION TO BE PROVIDED TO PEDESTIANS WHO HAVE VISUAL DISABILITIES.

FOR LONG-TERM OPERATIONS, REFER TO EPG 616.6.2.2 FLAGS AND ADVANCE WARNING RAIL SYSTEM.

NOT TO SCALE



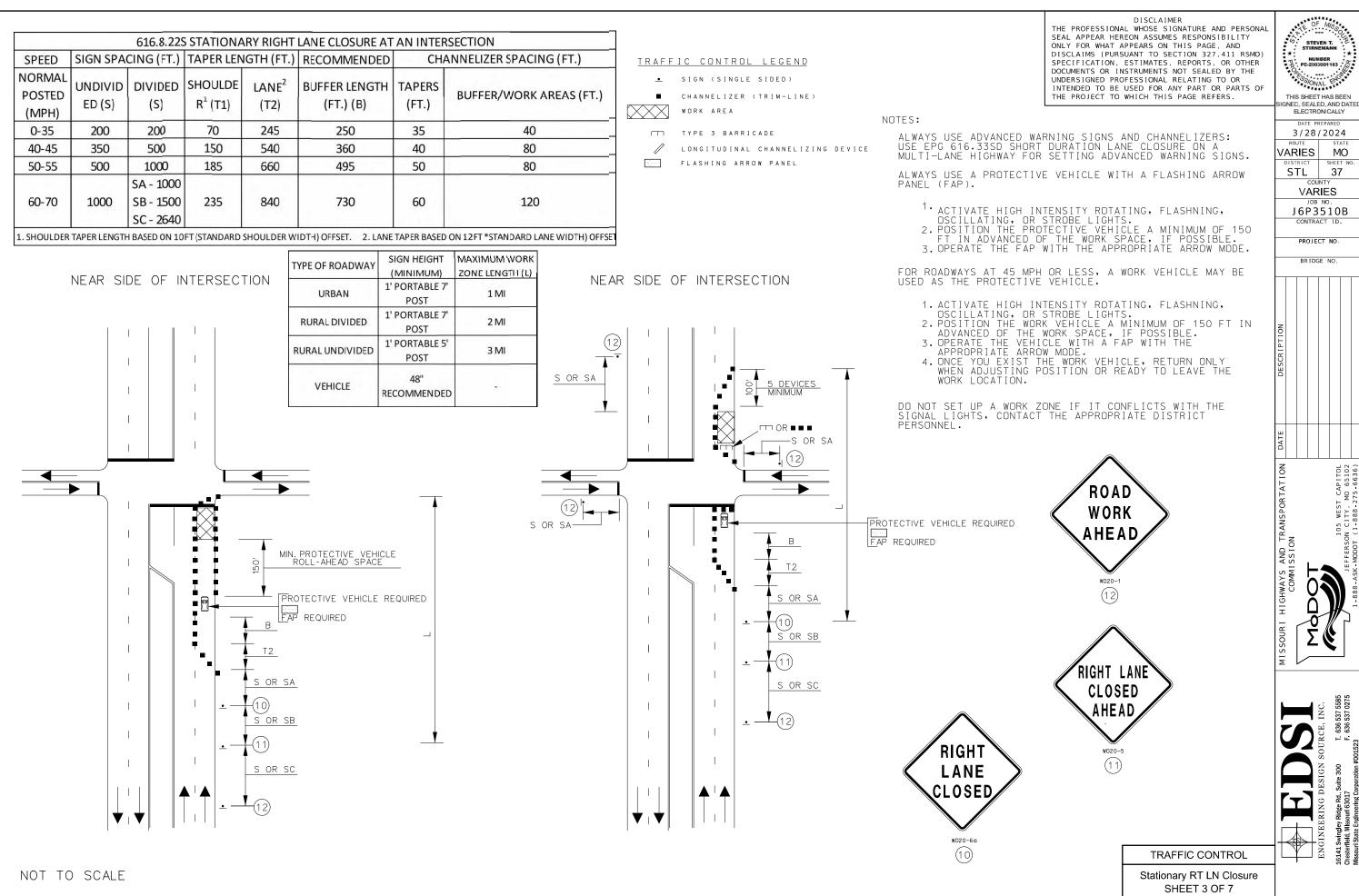
SHEET 2 OF 7

MO

36

SHEET NO

636 537 t



		6	16.8.1SD SH	ORT DURA	TION WORK BEYON	ID THE SHOULD	ER
SPEED	SIGN SPA	CING (FT.)	TAPER LEN	GTH (FT.)	RECOMMENDED	CHAN	NELIZER SPACING (FT.)
NORMAL POSTED (MPH)	UNDIVID ED (S)	DIVIDED (S)	SHOULDER 1 (T1)	LANE ² (T2)	BUFFER LENGTH (FT.) (B)	TAPERS	BUFFER/WORK AREAS
0-35	200	200		-	-	-	
40-45	350	500	- 4	- E	-	-	<u> </u>
50-55	500	1000	-	<u>-</u>		-	+
60-70	1000	1000	i i		-	1-1	<u> </u>
. SHOULDER	TAPER LENGTH	BASED ON 10	FT (STANDARD SH	IOULDER WID	TH) OFFSET. 2. LANE TAF	PER BASED ON 12FT *:	STANDARD LANE WIDTH) OFFSET.

MIN. PROTECTIVE VEHICLE ROLL-AHEAD SPACE

VEHICLE/EQUIPMENT PARKED ON SHOULDER (OPTIONAL)

TYPE OF ROADWAY	SIGN HEIGHT	MAXIMUM WORK
TIPE OF ROADWAT	(MINIMUM)	ZONE LENGTH (L)
LIDDANI	1' PORTABLE 7'	1.541
URBAN	POST	1 MI
DUDAL DIVIDED	1' PORTABLE 7'	2 1 1
RURAL DIVIDED	POST	2 MI
RURAL UNDIVIDED	1' PORTABLE 5'	3 MI
RURAL UNDIVIDED	POST	3 IVII
VEHICLE	48" RECOMMENDED	1 /2 1

TRAFFIC CONTROL LEGEND

SIGN (SINGLE SIDED)

CHANNELIZER (TRIM-LINE)

WORK AREA

TYPE 3 BARRICADE

LONGITUDINAL CHANNELIZING DEVICE

NOTES:

SHORT DURATION: DEFINED AS WORKERS ON FOOT FOR 60MIN OR LESS IN ONE LOCATION.

LOCATION: DEFINED AS THE MAXIMUM LENGTH OF WORK ZONE (L) FOR THE ROAD YOU ARE WORKING ON.

ADVANCED WARNING SIGNS MAY BE ELIMINATED IF AT LEAST ONE OF THE FOLLOWING CONDITIONS ARE MET:

1. WORK SPACE IS 15 FT OR MORE FROM THE EDGE OF ANY ROADWAY.

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DOCUMENTS OR INSTRUMENTS NOT SEALED BY THE UNDERSIGNED PROFESSIONAL RELATING TO OR INTENDED TO BE USED FOR ANY PART OR PARTS OF

- 2. WORK SPACE IS BEHIND THE CURB.
 3. WORK SPACE IS BEHIND BARRIER OR GUARDRAIL.
 4. HIGH—INTENSITY ROTATING, FLASHING, OSCILLATING,
- OR STROBE LIGHTS ARE ACTIVATED.

WHEN VEHICLES OR EQUIPMENT ARE PARKED ON THE SHOULDER:

- 1. ACTIVATED HIGH-INTENSITY ROTATING, FLASHING,
- OSCILATING, OR STROBE LIGHTS.

 2. POSITION THE PROTECTIVE VEHICLE A MINIMUM OF 150 FT IN ADVANCE OF THE WORK SPACE, IF POSSIBLE.

 3. ONCE YOU EXIST THE WORK VEHICLE, RETURN ONLY
- WHEN ADJUSTING POSITION OR READY TO LEAVE THE





SHOULDER WORK WITH MINOR TRAVELWAY ENCROACHMENT

DIVIDED AND UNDIVIDED

(11) OR (12)

DISTANCE BEYOND EDGE OF ROADWAY, REFER TO NOTES.

TRAFFIC CONTROL

Shrt Duration Work Bynd Shldr SHEET 4 OF 7

STEVEN T. STIRNEMANI THIS SHEET HAS BEEN GNED, SEALED, AND DATED

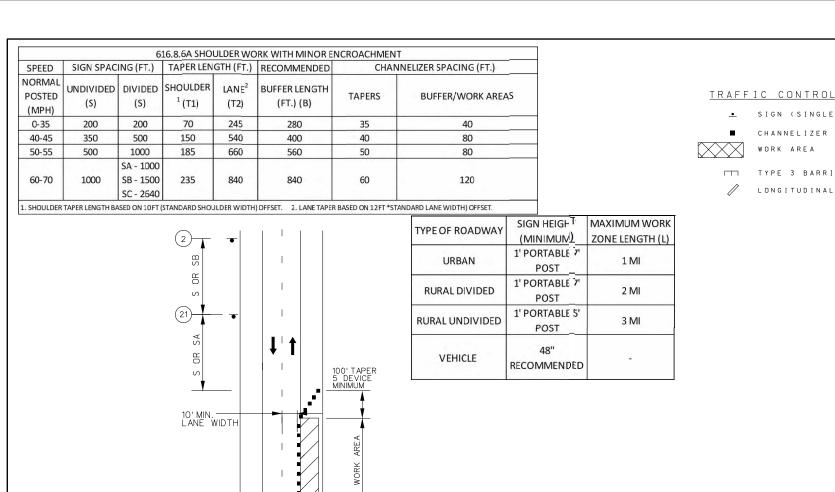
ELECTRONICALLY

3/28/2024 **VARIES** MO SHEET NO. STL 38

VARIES J6P3510B

CONTRACT ID. PROJECT NO.

BRIDGE NO.



MIN. PROTECTIVE VEHICLE ROLL-AHEAD SPACE

PROTECTIVE VEHICLE

43) (44A) (3)

SHOULDER WORK WITH MINOR TRAVELWAY ENCROACHMENT

DIVIDED AND UNDIVIDED

NOT TO SCALE

TRAFFIC CONTROL LEGEND

SIGN (SINGLE SIDED)

CHANNELIZER (TRIM-LINE)

TYPE 3 BARRICADE

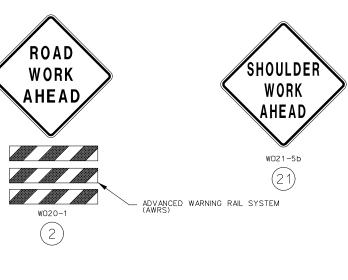
LONGITUDINAL CHANNELIZING DEVICE

ALWAYS USE ADVANCE WARNING SIGNS AND CHANNELIZERS:
1. SET ADDITIONAL WARNING SIGNS AT EACH INTERSECTION WITH ANOTHER STATE HIGHWAY WITHIN THE WORK ZONE

ALWAYS USE A PROTECTIVE VEHICLE WHEN WORKERS ARE

- 1. ACTIVATE HIGH INTENSITY ROTATING, FLASHNING,
- OSCILLATING, OR STROBE LIGHTS.

 2. POSITION THE PROTECTIVE VEHICLE 150 FT IN ADVANCED OF THE WORK SPACE, IF POSSIBLE.
- 3.NEXT XX MILES SIGN NOT REQUIRED FOR NARROW LANE SECTIONS LESS THAN ONE MILE.
- 4. A WORKER VEHICLE MAY BE USED AS A PROTECTIVE
- 5. ONCE YOU EXIT THE WORK/PROTECTIVE VEHICLE, RETURN ONLY WHEN READY TO LEAVE THE WORK





XX MILES W07-3a

(44A)

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UNDERSIGNED PROFESSIONAL RELATING TO OR INTENDED TO BE USED FOR ANY PART OR PARTS OF

THIS SHEET HAS BEEN GNED, SEALED, AND DATED

ELECTRONICALLY

3/28/2024

VARIES

J6P3510B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

MO

39

SHEET NO.

VARIES |

STL

DOCUMENTS OR INSTRUMENTS NOT SEALED BY THE

ONLY FOR WHAT APPEARS ON THIS PAGE, AND

THE PROJECT TO WHICH THIS PAGE REFERS.

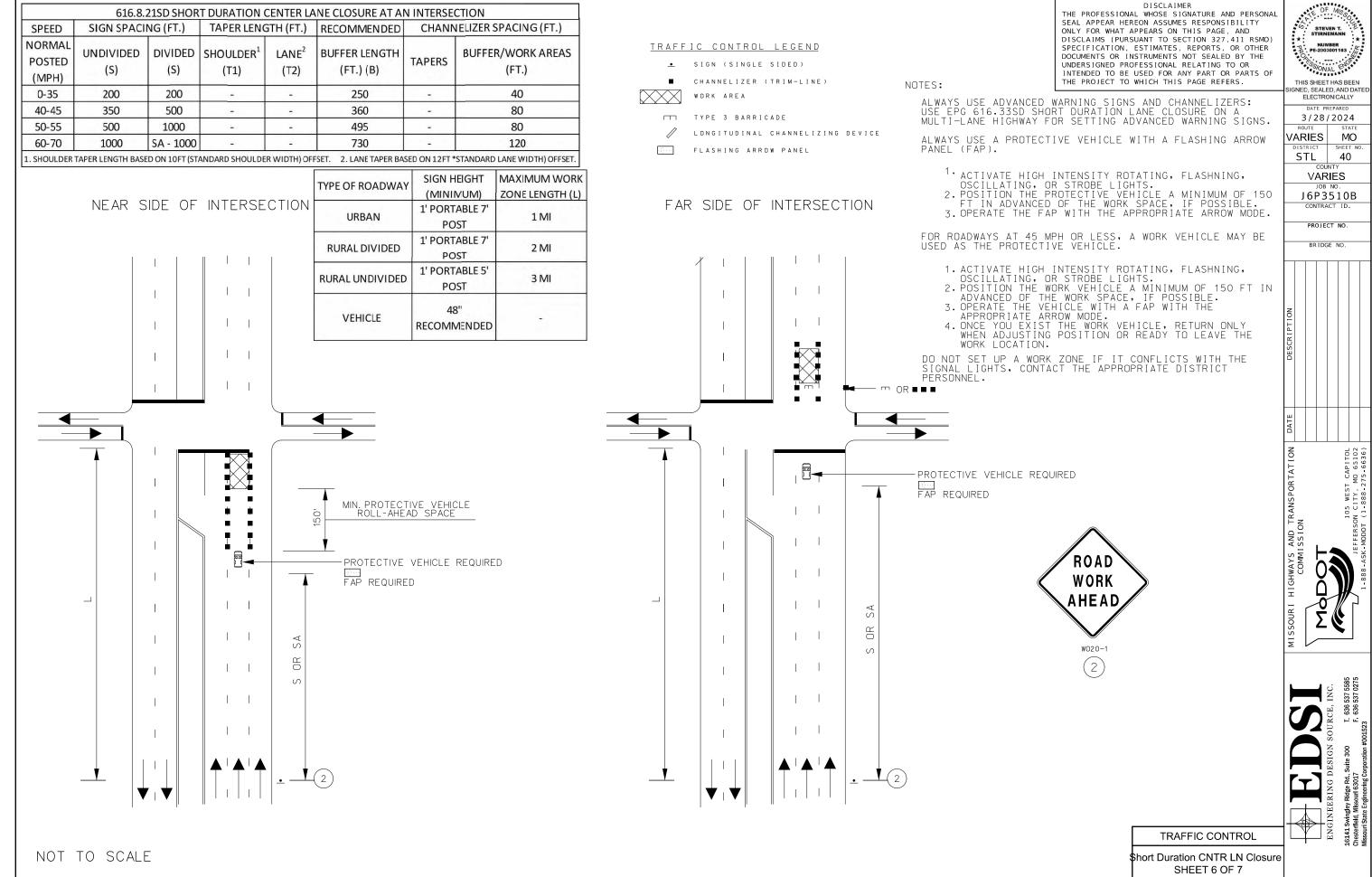
(3)

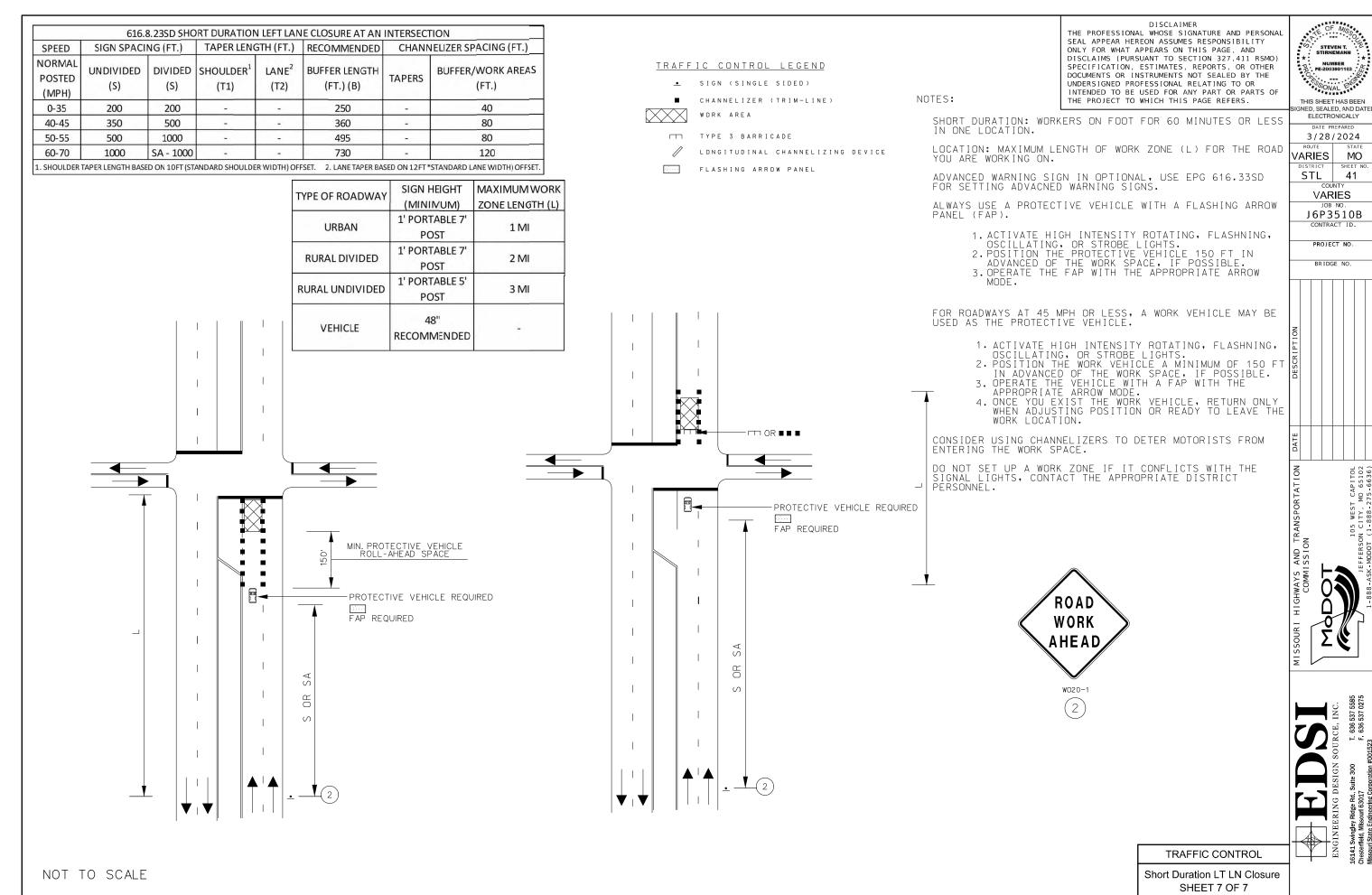
636 537 E

TRAFFIC CONTROL

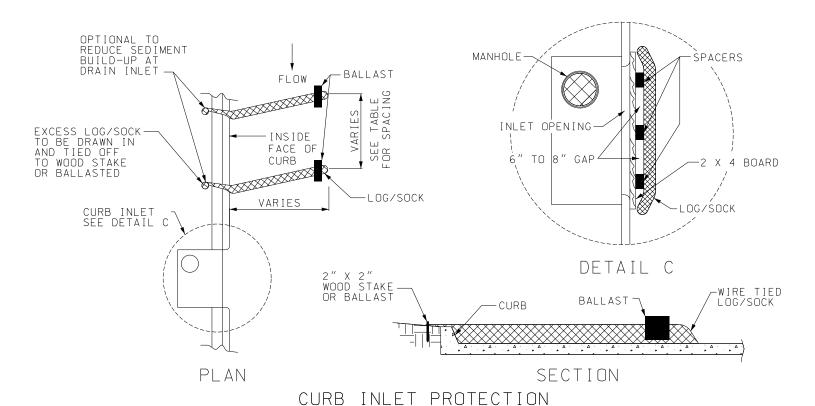
Shidr Work With Minor Encroachment \$TC5X1

SHEET 5 OF 7





TEMPORARY EROSION CONTROL MEASURES 806.10K



NOTES:

PRIOR TO PLACEMENT ALL DEBRIS, ROCK, LARGE CLODS AND WOOD VEGETATION SHALL BE CLEARED.

LOG/SOCK PLACED ON PAVEMENT SHALL BE WEIGHTED DOWN WITH GRAVEL/SAND BALLAST.

GENERAL NOTES:

OTHER PROPRIETARY INLET PROTECTION MAY BE SUBSTITUTED IN ACCORDANCE WITH SEC 806 OR AS DIRECTED BY THE ENGINEER.

DISCLAIMER

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SPACING FOR

300

150

100

75

60

50

43

38

33

30

27

25

23

21

20

19

18

17

16

15



GNED, SEALED, AND DATED ELECTRONICALLY

3/28/2024 VARIES MO STL 42

VARIES J6P3510B

CONTRACT ID. PROJECT NO.

BRIDGE NO.

EROSION CONTROL

TEMPORARY SILT FENCE SHEET 1 OF 2

ELEVATION OF TOE MUST TOP BE SAME OR LESS THAN ELEVATION OF TOP. SPACING OF DITCH CHECK MEASURED CENTER TO CENTER

EXAMPLE

DITCH CHECK SPACING FOR STANDARD HEIGHTS

9" EFF. HEIGHT 18" EFF. HEIGH

SPACING FOR

150

75

50

37

30

25

21

19

16

15

13

12

11

10

10

9

9

8

8

DITCH &

SLOPE %

0.5

1.0

1.5

2.0 2.5

3.0

3.5

4.0

4.5

5.0

5.5

6.0 6.5

7.0

7.5

8.0

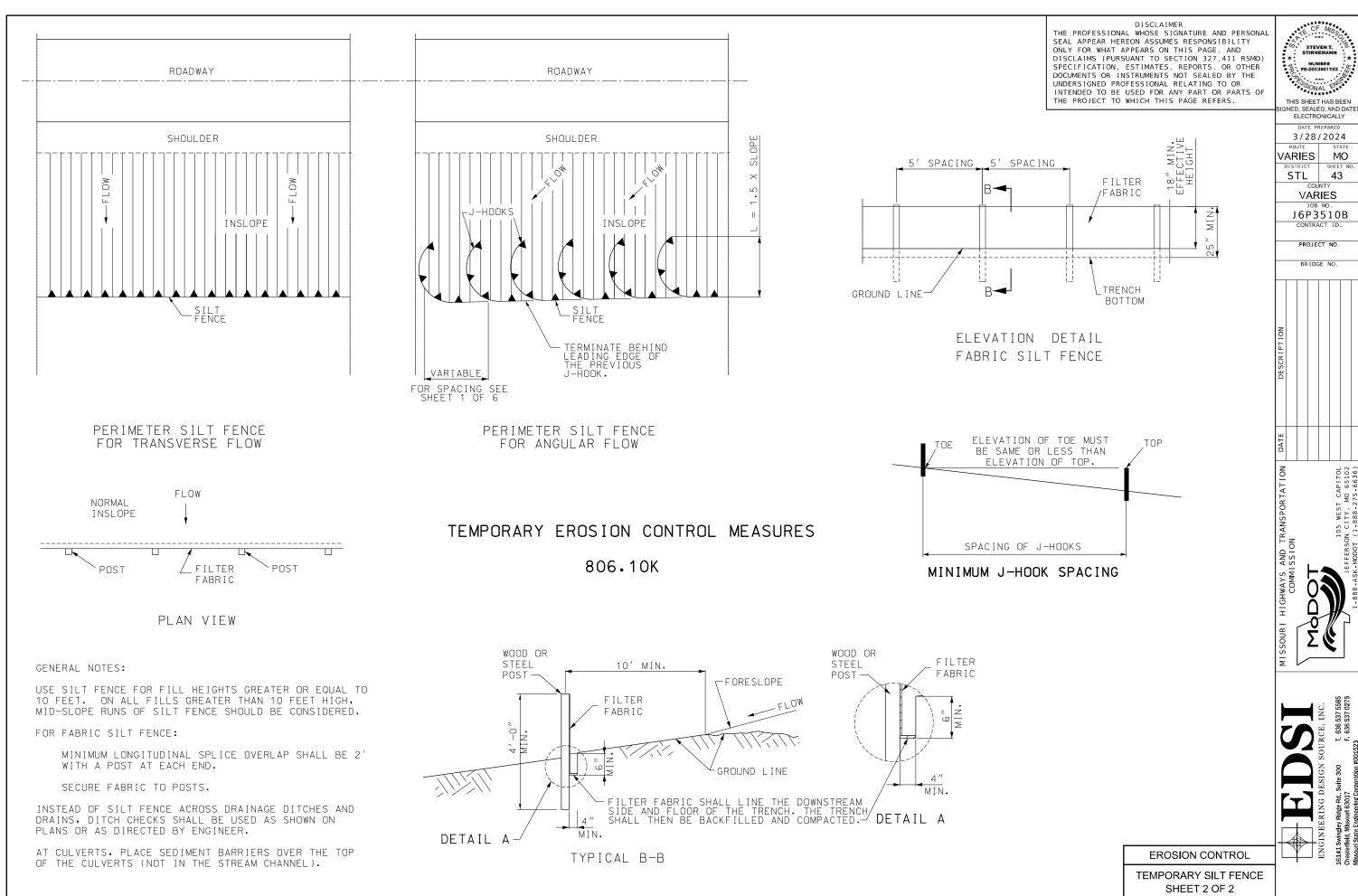
8.5

9.0

9.5

10.0

MINIMUM DITCH CHECK SPACING



PEDESTRIAN CURB RAMP PAY LIMITS

15′ S R END CURB RAMP PAY-BEGIN SIDEWALK PAY

TYPE 2 PARALLEL



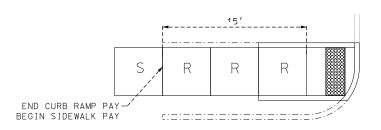
-END CURB RAMP PAY

BEGIN SIDEWALK PAY

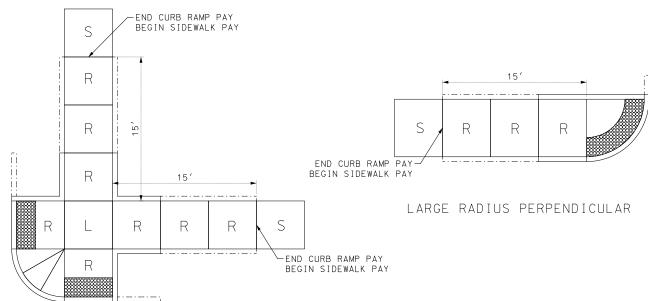
-RAMP MAY END IN CURVED OR STRAIGHT SECTION DEPENDING ON RADIUS.

S

R



SMALL RADIUS PERPENDICULAR



DUAL PERPENDICULAR PAY AS 2 EACH

Notes -

15 FEET OF WALKWAY SHALL BE INCLUDED IN CURB RAMP PAY, PER EACH, AS SHOWN.

- L LANDING OR CLEAR AREA FOR TURNING MOVEMENTS AND RESTS.
- R PAY AS RAMP
- S PAY AS SIDEWALK.

SEE JSP FOR OTHER INCIDENTALS INCLUDED IN CURB RAMP PAY. SEE STANDARD PLANS 608 SERIES FOR DIMENTIONS, GRADES, AND OTHER REQUIREMENTS. CONTRACTOR WILL HAVE OPTION TO CONSTRUCT RAMP WITH FLARE OR CURB FOR CERTAIN RAMPS ABOVE.

THE PROFESSIONAL WHOSE SIGNATURE AND PERSONAL SEAL APPEAR HEREON ASSUMES RESPONSIBILITY ONLY FOR WHAT APPEARS ON THIS PAGE, AND DISCLAIMS (PURSUANT TO SECTION 327.411 RSMO) SPECIFICATION, ESTIMATES, REPORTS, OR OTHER DOCUMENTS OR INSTRUMENTS NOT SEALED BY THE UNDERSIGNED PROFESSIONAL RELATING TO OR INTENDED TO BE USED FOR ANY PART OR PARTS OF THE PROJECT TO WHICH THIS PAGE REFERS.

END CURB RAMP PAY 7

R

BEGIN SIDEWALK PAY/

15′

END CURB RAMP PAY-

R

BEGIN SIDEWALK PAY



THIS SHEET HAS BEEN GNED, SEALED, AND DATED ELECTRONICALLY

3/28/2024 VARIES MO STL 44 VARIES

CONTRACT ID.

J6P3510B

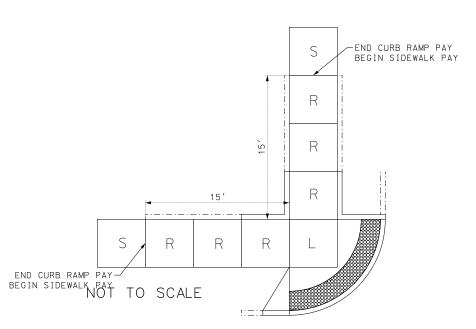
PROJECT NO.

BRIDGE NO.

COMPOUND PERPENDICULAR

R

TYPE 3 PARALLEL



S

S

END CURB RAMP PAY-

BEGIN SIDEWALK PAY

END CURB RAMP PAY-BEGIN SIDEWALK PAY

R

R

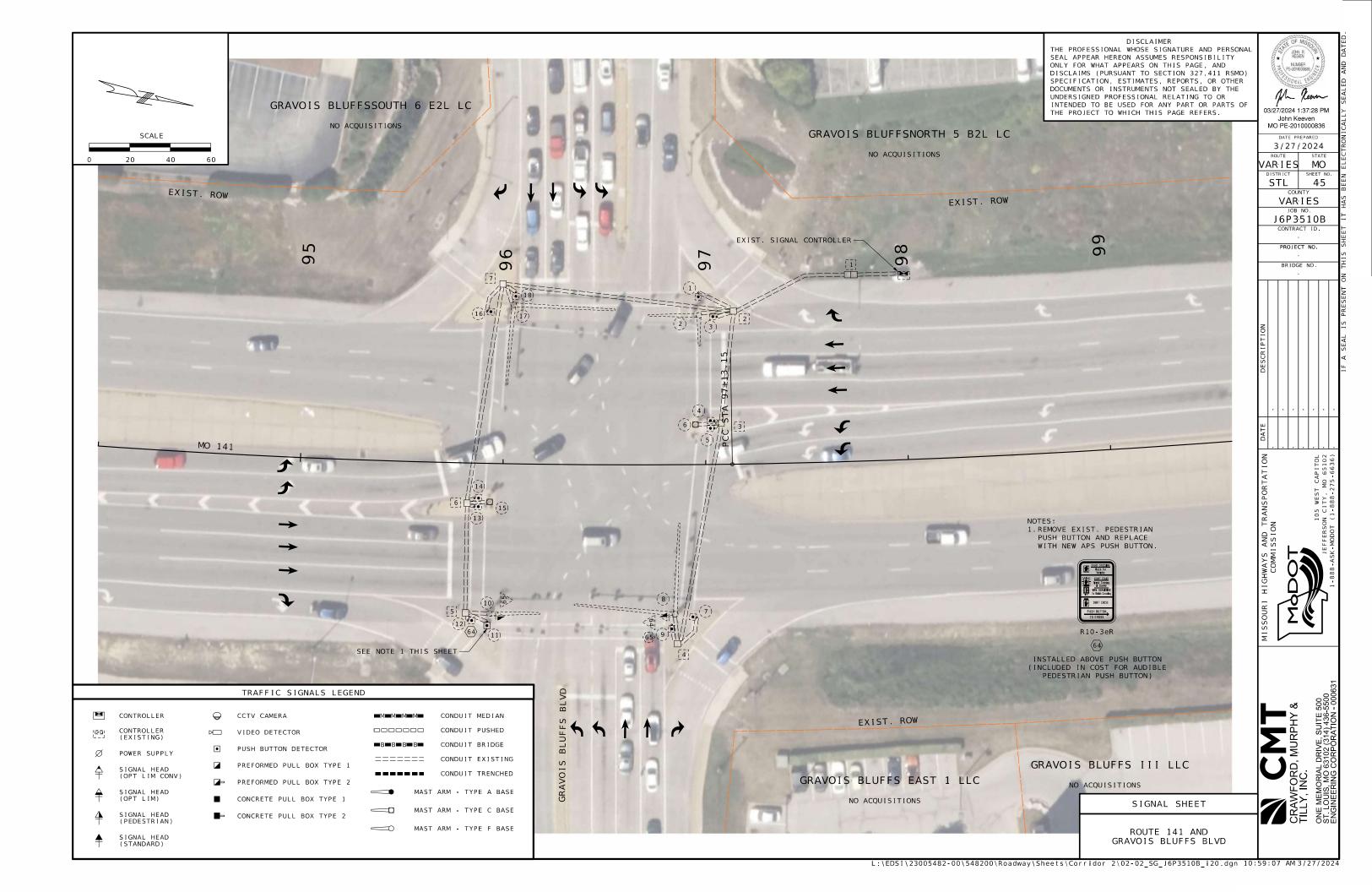
15′

R

BLENDED TRANSITION PAY AS 2 EACH

SPECIAL SHEETS CURB RAMP PAY LIMITS SHEET 1 OF 1

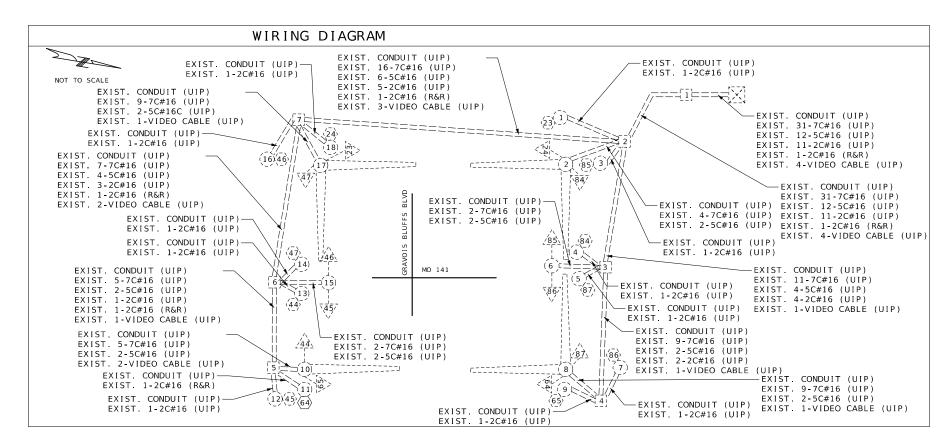
N:\E23038 MoDOT J6M3510 Districtwide ADA\05_Roadway\drawings\Output\J6M3510B\44_SS_01_J6P3510_I5.dgn 12:50:56 PM 3/28/2024

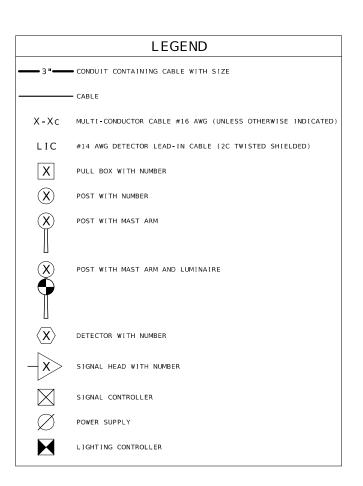


	C	CONDUIT (WITH #1	4 T	RAC	ER	WIRE)											CABLE						
	CENTER LOOP DET.	TRENCH	PUSHED		MEDIA	ίN	ON STRUCTURE				CENTER	POWER			CONTI	ROL		CLOSED LOOP INTERCONNECT		INAIRE	DET	ECTOR	FIBER	
FROM TO	CENTER 1	2" 3" 4"	2" 3" 4	." 2	, 311	4"	2" 3"	REMARKS	FROM		CENTER		3-	#16 5-	#16	7- #16			CONTROL	POLE & BRACKET	1c #14	2c #14	SINGLE-MULTI-	REMARKS
	DISTANCE 1 2						_ ,				DISTANCE		2 c	#16 50	#16	/C #16	3C #16	3 PAIR #16	2c #12	1c #10	IN DUCT	CABLE	SINGLE - MULTI - MODE	
										(10)	378'		42	25'										PUSH BUTTON 64
SUB	TOTALS								S	UBT	OTALS		42	25'										
_	OTALS									TOT	ΓALS		43	30'										

		SIGN	IAL SIG	NS		*** USE D3-1 FOR ONE LINE. USE D3-1B FOR TWO
QUANTITY	MUTCD S I GN	SIZE	AREA	SIGN TOTAL AREA	SIGNAL SIGN MOUNTING HARDWARE	
00	NUMBER			ITEM NO. 90208.33	ITEM NO. 90208-34	
		INCHES	SQ FT	SQ FT	EA.	
	R10-10L	24 X 30	5.0			
	R10-10R	24 X 30	5.0			
	R3-5L	30 X 36	7.5			
	R3-5R	30 X 36	7.5			
	R3-5A	30 X 36	7.5			
	R3-6L	30 X 36	7.5			
	R3-6R	30 X 36	7.5			
	R3 - 2	24 X 24	4.0			
	R3-1	24 X 24	4.0			
_	R3-3	24 X 24	4.0			
	D3-1 ***	VAR. X 18				
-	D3-1B *** R10-3ER	VAR. X 18 9 X 15	0.9	(0.0)	(1)	
1			5.0	(0.9)	(1)	
-	R10-11A R10-13	24 X 30 36 X 24	6.0			
	R10-13	30 X 24	7.5			
	K10-2/A	30 X 30	7.5			
						NOTE:
						PAYMENT FOR SIGN R10-3E AND SIGNAL
						MOUNTING HARDWARE ARE INCLUDED IN COST FOR
						AUDIBLE PEDESTRIAN PUSH BUTTON.
			SUBTOTAL	(0.9)	(1)	
			TOTAL	(1.0)	(1)	

1 FOR ONE LINE. USE D3-1B FOR TWO LINE.		ACCESS	IBLE PUSH	BUTTONS			
				TYPE	=		
	DETECTOR NUMBER	APPROACH	PUSH	I NI	DUCTION LOOP	(2)	VID50
			BUTTON	STANDARD	DELAY/ EXTEND *	CALL UNIT *	VIDEO
	64	NB MO 141	1				
		TOTAL	1				





MO 141 / GRAVOIS BLUFFS BLVD INTERSECTION

D-37B/C/D

03/27/2024 1:37:28 PM John Keeven MO PE-2010000836

3/27/2024 ARIES MO

VAR·I ES

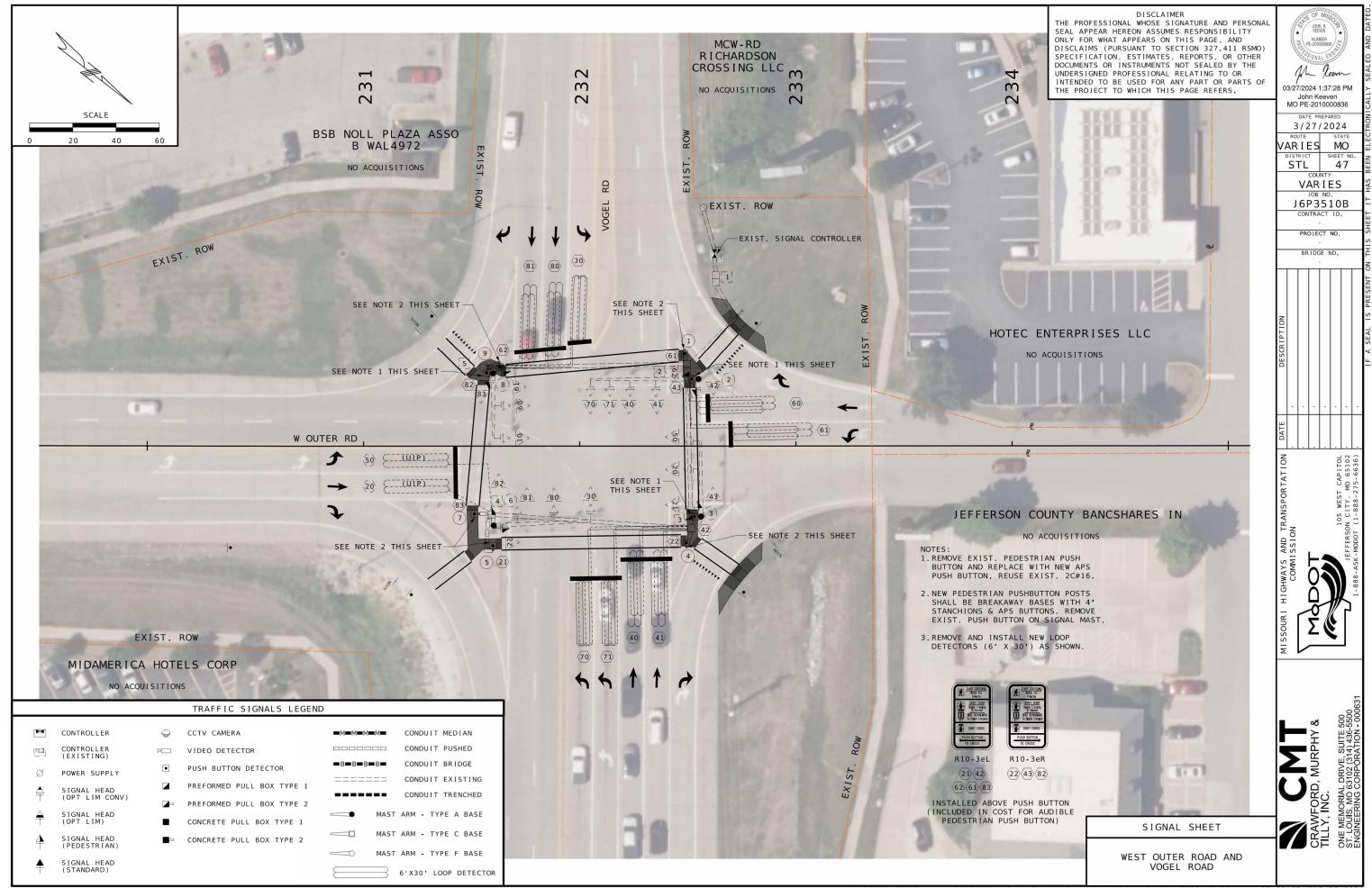
J6P3510B

PROJECT NO. BRIDGE NO

STL

SHEET NO

46



1	<u> </u>	L	OCAT I OI	V				ВА	SE					PUL	L BC	X															I	POS	Т											
MBER	NOMB					Α		F		{ C	**	PRE	FORM	ED C	ONCRE	m	ND A TAI	OST		-		-	ГҮРЕ	C	& C	L								Т	YPE	В 8	k BL						LUMI	NAIR
NUN T	Ă AP	PROACH	STATION	OFFSET						5	RETE YDS	55 1	S 3	5 5	TYPE TYPE		DRAΙN ΓΥΡΕ*	NO -	POST				SIG			SIGN						L	EFT	ARM	E			RIC	SHT A	RM K		JDED	BRA	CKET RM*
POST					9.5	10	11 12	1001019	11		\simeq	CLAS	CLAS	CLAS	BLE,	UBLE,	ΙΙ	ANCHI	<u> </u>	ARI	M LEN	STH	SPA	LING	5	PACIN	NG	ARM	LENG	TH		I GNA 'AC I N		S	SIGN		S SF	I GNAL	à	S I SPAG	GN CING	INCLI		FF
-	<u> </u>									5	Ŭ Ū				DOUBL	JOG ,	АВС	1 -	8 15	Α	АА	Α	ВС	D D) В	C D	D' E	K	E K	E K	F	G H	H'	F	G F	H,	LI	M N	N' I	_ М	N N	1.	4 0 a	10
			CONTROLLER	(AS REQ'D)																																								
1		OUTER RD	232+48	44 6 LT						1	0.07							1																							\perp			
4		OUTER RD	232+49	46 8 RT						1	0.07							1																							\perp			
5		OUTER RD	231+60	48 2 RT				\rightarrow		1	0.07							1										\perp			\perp								_		-			
7		OUTER RD	231+53	31 2 RT				\perp		1	0.07							1																										
9	W	OUTER RD	231+63	38 6 LT						1	0.07				\perp		$\sqcup \sqcup$	1				\perp						\perp			\perp		_						_		-			
_								\rightarrow	\rightarrow	\perp			_	_	\rightarrow		\vdash		_	-	_	+	_					+	\rightarrow		+			\vdash					_		\vdash		\perp	
								-							_								_								-	_			_	_			\rightarrow	_	\leftarrow			
	_			<u> </u>						_		\vdash	+	_	-	_	$\forall \forall$	+	_		_	 	XXX	XX	$\times \times \times$	$\forall \forall \times$	$\frac{1}{2}$	+	+	+		. XX.	$\frac{1}{2}$		XXX	$\frac{1}{\sqrt{\lambda}}$		لحجلا	$\frac{1}{2}$	\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\		$\frac{1}{2}$	XXX	
								5	UBTO	IAL	0.35					\triangleright	XXX	5					$\times\!\!\times\!\!\times$	$\times\!\!\times\!\!\times$	XXX	$\times\!\!\times\!\!\times$					\bowtie	XX	$\times\!\!\times\!\!\!\times$	XX	$\times\!\!\times\!\!\times$	XXX	$\!$	$\!$	∞	$\!$	⋘	$\langle \rangle \langle \rangle \langle \rangle$	$\times\!\!\times\!\!\!\times$	$\times\!\!\times\!\!\times$
									TO	TAL	0.4					$\overline{}$	XX	5					XX	XX	XX	XX	\bowtie				\bowtie	$\times\!\!\!\times\!\!\!\times$	XX	XX	XXX	$\!$	XXX	XXX	⋘	XXX	$X\!X\!X$	XXX	XXX	$\times\!\!\times\!\!\times$

EFFECTIVE: 07/01/2018

NOTE:

SIGNAL AND SIGN SPACING SHOWN IS APPROXIMATE. CONTRACTOR SHALL ADJUST AS NECESSARY.

REMARKS

ALL LENGTHS AND SPACINGS ARE IN FEET UNLESS OTHERWISE INDICATED.

* ITEMS FOR WHICH SEPARATE PAYMENT WILL NOT BE MADE.

** SEE STANDARD PLANS 902.10 AND 902.30 FOR CONCRETE REQUIREMENTS ON BASES. NO DIRECT PAY FOR REQUIRED BASE MATERIAL ASSOCIATED WITH 4 PEDESTRIAN PUSH BUTTON STANCHION. SEE JSP FOR DETAILS.

*** USE D3-16 FOR ONE LINE. USE D3-18 FOR TWO LINE.

*** USE D3-16 FOR ONE LINE. USE D3-18 FOR TWO LINE.

SIGNAL STRUCTURES WHICH WILL EXCEED THE DIMENSION LIMITS SHOWN ON STANDARD PLAN SHEETS IN SECTION 902 AND ANY OTHER INSTALLATION WHERE THE DETAILS OF CONSTRUCTION ARE NOT FURNISHED IN THE CONTRACT PLANS, SHALL BE DESIGNED BY A PROFESSIONAL ENGINEER IN ACCORDANCE WITH THE 2001 AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4TH EDITION, AND LATEST INTERIMS. THE STRUCTURE SHALL BE DESIGNED AS IMPORTANCE CATEGORY I FOR FATIGUE WITH A 50-YEAR DESIGN LIFE. THE CONTRACTOR SHALL SUBMIT A SET OF SHOP DETAIL DRAWINGS INCLUDING WELD PROCEDURE SPECIFICATIONS AND DESIGN COMPUTATIONS FOR MODOT RECORDS AND REFERENCE. THE SUBMITTED DRAWINGS AND CALCULATIONS SHALL BE SIGNED AND SEALED IN ACCORDANCE WITH THE LAWS RELATING TO ARCHITECTS AND PROFESSIONAL ENGINEERS (CHAPTER 327, RSMO.), AND SHALL INCLUDE A TITLE BLOCK OR SUMMARY SHEET WHICH LISTS AND CERTIFIES THAT THE PRODUCT MEETS ALL OF THE SPECIFIED DESIGN CRITERIA.

		(COND	UIT	(W	ΙΤΗ	#14	1 TR	RACE	ER \	WIRE	Ē)								CABLE					
	CENTER	LOOP DET.	TRE	NCH		PUSH	HED	ME	EDIAN	N .	Of STRUC	I TURE				CENTER	POWER	CONTROL		CLOSED LOOP INTERCONNEC	1	INAIRE	DETECTOR FI	BER	
FROM TO	D TO CENTER DISTAN	3 1" I	2 3	4"		2" 3"	. 4"	2"	3"	4"	2" 3		REMARKS	FROM	то	TO CENTER DISTANCE		2c #16 5c #16 7c #	16 3c #16		CONTROL	POLE & BRACKET	1c #14 2c #14 SINGLE MODE	- MULTI-	REMARKS
	DISTAN															DISTANCE		20 110 30 110 70 11	10 50 %10	, 5 17th #10	2c #12	1c #10	IN DUCT CABLE MODE	MODE	
[2] (1	7'					8'	•							[x]	1	70'		104'							PUSH BUTTON 61
[3] 4	10'					11	•							12.2	4	148'		192'							PUSH BUTTON 22
[4] 5	21'					22	1							1×3	(5)	251'		307'							PUSH BUTTON 21
[3] 7	6'					7 '	1							123	7	236'		291'							PUSH BUTTON 83
[5] 9	8'					9 '								[2]	9	165'		210'							PUSH BUTTON 62
[2] FO	C 10'	20'											2-1" CONDUIT RUNS	[2]	$\langle x \rangle$	VAR							690'		LOOP DETECTORS 10,60
[3] FO	C 5'	20'											4-1" CONDUIT RUNS	[3]	⟨xx⟩	VAR							1072'		LOOP DETECTORS 70,71
[5] FO	C 10'	30'										-	3-1" CONDUIT RUNS	[5]	$\langle x x \rangle$	VAR							1520'		LOOP DETECTORS 30,80,81
SUBT	OTALS	70'				57	7 1							S	UBT	OTALS		1104'					3282'		
TC	TALS	70'				57	, ,								TO	TALS		1110'					3290'		

CONTRACTOR SHALL USE EXISTING WIRE FOR HOOK-UP OF NEW PUSH BUTTONS 42, 43, AND 82.

w	OUTER	RD /	VOGEL	RD
	INT	ERSEC	TION	

NUMBER PE_2010000838

03/27/2024 1:37:28 PM John Keeven MO PE-2010000836

3/27/2024 /AR·IES MO

> VARIES J6P3510B CONTRACT ID.

> > PROJECT NO.

BRIDGE NO

STL

SHEET NO

4.8

D-37A/B

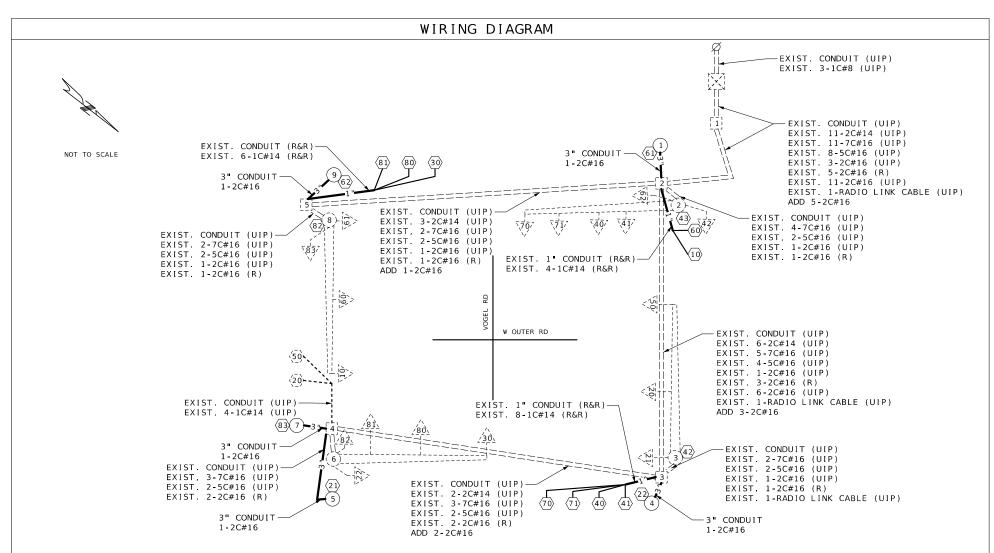
	ACCESSIE	BLE PUSH	BUTTONS			
			TYPI			
DETECTOR NUMBER	APPROACH	PUSH	I NI	DUCTION LOOP (2)	
		BUTTON	STANDARD	DELAY/ EXTEND *	CALL UNIT *	VIDEC
21	NB W OUTER RD	1				
22	SB W OUTER RD	1				
42	WB VOGEL RD	1				
43	EB VOGEL RD	1				
61	SB W OUTER RD	1				
62	NB W OUTER RD	1				
82	EB VOGEL RD	1				
83	WB VOGEL RD	1				
*						
+						
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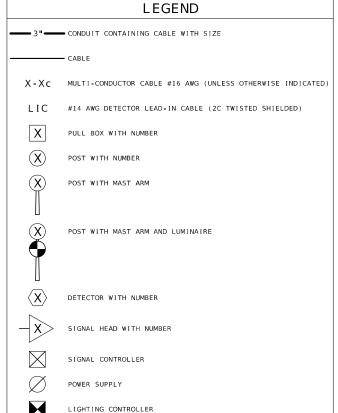
		SIGN	IAL SIGI	NS	
QUANTITY	MUTCD SIGN	SIZE	AREA	SIGN TOTAL AREA	SIGNAL SIGN MOUNTING HARDWARE
/no	NUMBER			ITEM NO 90208 33	ITEM NO. 90208.34
		INCHES	SQ FT	SQ FT	EA.
	R10-10L	24 X 30	5.0		
	R10-10R	24 X 30	5.0		
	R3-5L	30 X 36	7.5		
	R3-5R	30 X 36	7.5		
	R3-5A	30 X 36	7.5		
	R3-6L	30 X 36	7.5		
	R3-6R	30 X 36	7.5		
	R3-2	24 X 24	4.0		
	R3-1	24 X 24	4.0		
	R3 - 3	24 X 24	4.0		
	D3-1 ***	VAR. X 18			
_	D3-1B ***	VAR. X 18		(4.5)	(=)
5 3	R10-3EL R10-3ER	9 X 15 9 X 15	0.9	(4.5)	(5)
3	R10-3ER	9 X 15 24 X 30	5.0	(2.7)	(3)
	R10-11A	36 X 24	6.0		
	R10-13	30 X 24	7.5		
	K10-27A	30 X 30	7.5		
			SUBTOTAL	(7.2)	(8)
			TOTAL	(8.0)	(8)

*** USE D3-1 FOR ONE LINE. USE D3-1B FOR TWO LINE.

NOTE:

PAYMENT FOR SIGN R10-3E AND SIGNAL MOUNTING HARDWARE ARE INCLUDED IN COST FOR AUDIBLE PEDESTRIAN PUSH BUTTON.





W OUTER RD / VOGEL RD
INTERSECTION

D-37C/D

03/27/2024 1:37:28 PM John Keeven MO PE-2010000836

3/27/2024 ROUTE STATE /AR·IES MO

JOB NO.
J 6 P 3 5 1 0 B
CONTRACT ID.

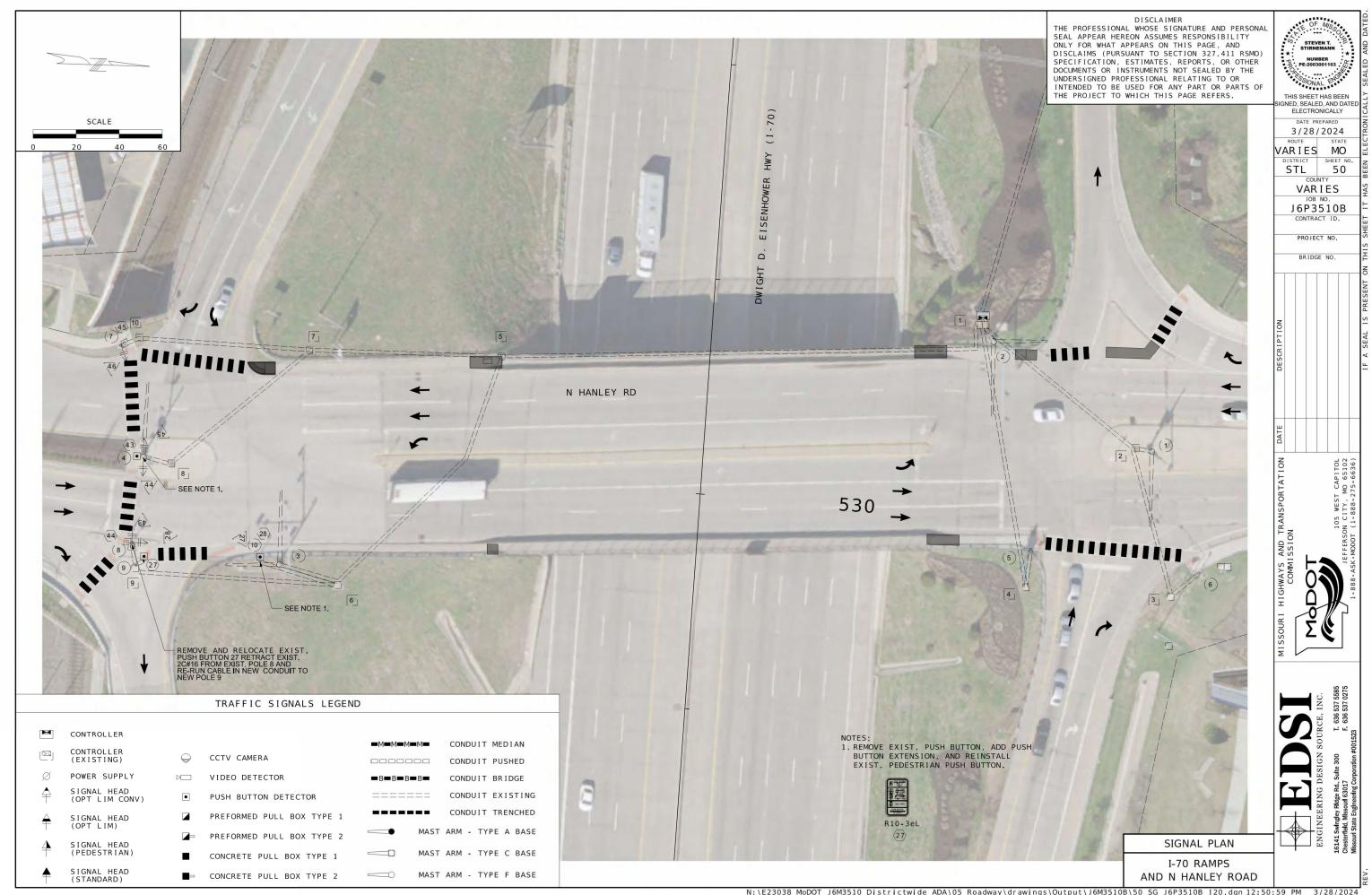
PROJECT NO.

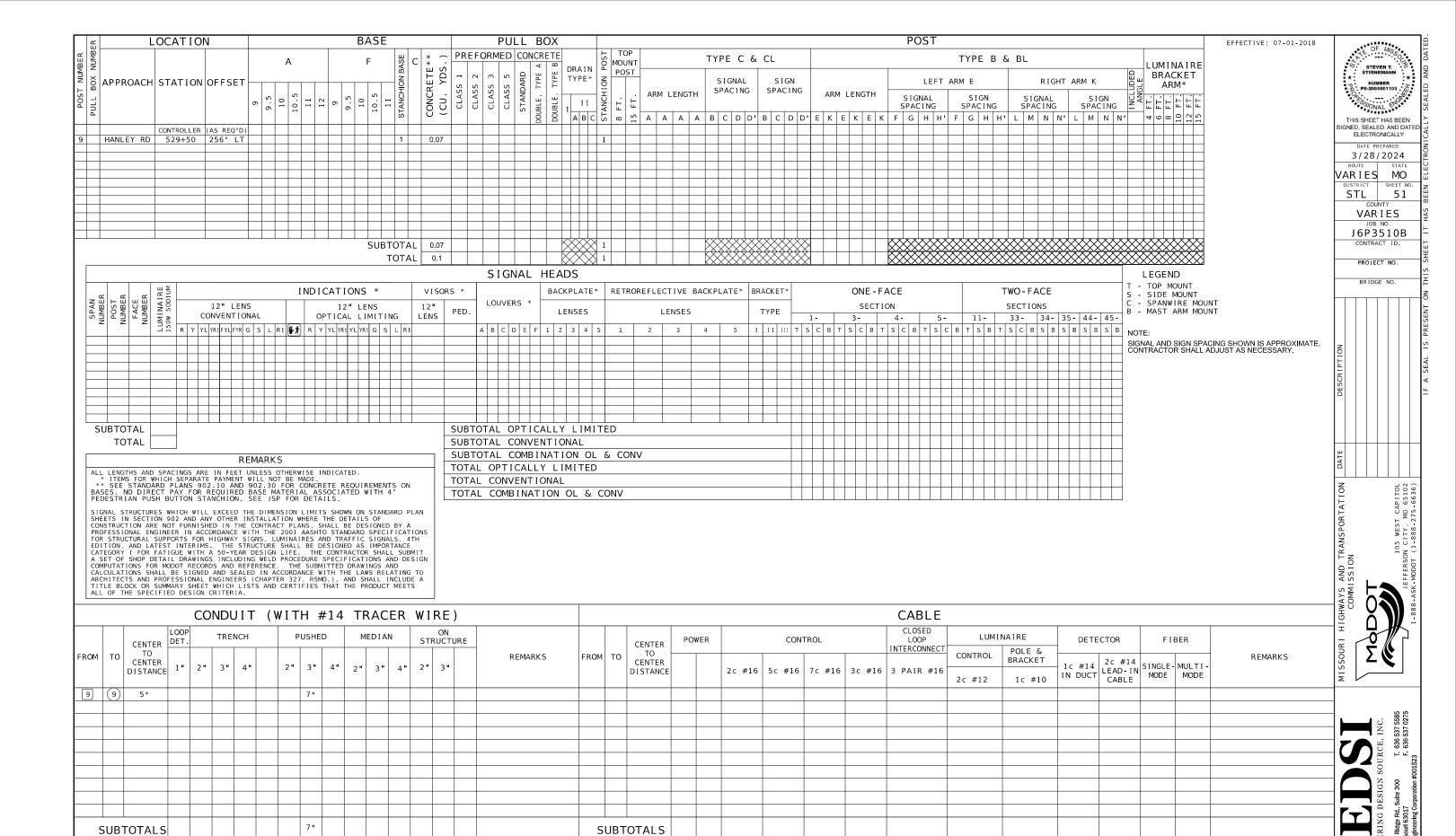
BRIDGE NO.

STL

SHEET NO

4.9





TOTALS

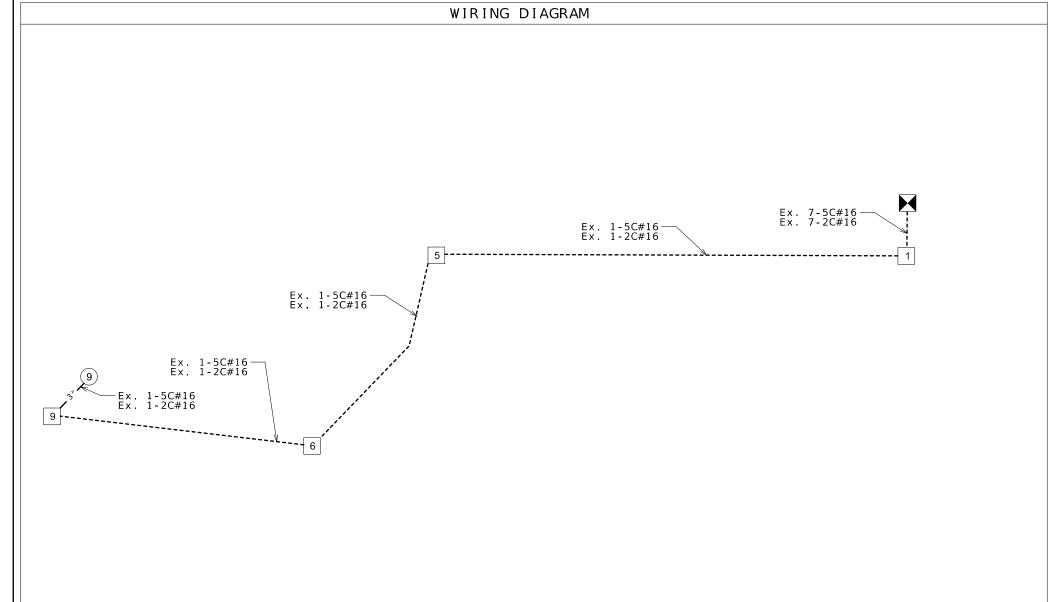
7*

TOTALS

I-70 RAMPS AND N HANLEY
INTERSECTION

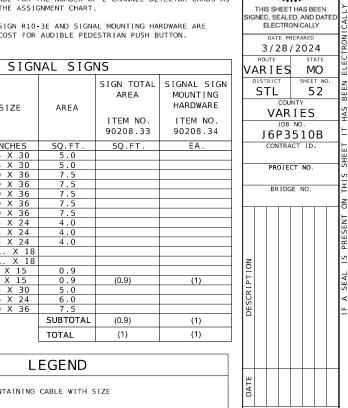
D-37A/B

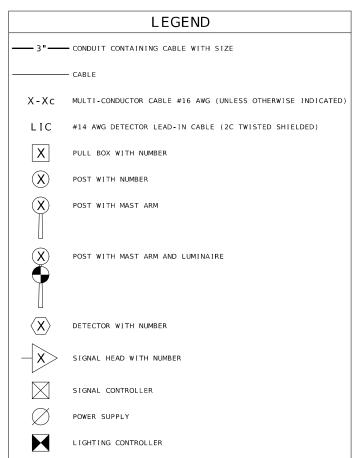
	A	CCESSIB	LE PUSH	BUTTO	NS		
				TYF	PE		
DETECTOR NUMBER	APPROACH	PUSH	EXTENSION	I	NDUCTION LOOF	P (2)	VIDEO
		BUTTON	ARM	STANDARD	DELAY/ EXTEND *	CALL UNIT *	VIDEO
28	EB I-70 ON RAMP		1				
43	N HANLEY RD		1				
	TOTAL		2				



- * ITEMS FOR WHICH SEPARATE PAYMENT WILL NOT BE MADE.
- *** USE D3-1 FOR ONE LINE. USE D3-1B FOR TWO LINE.
- (1) MoDOT "D" PLUG SHALL BE WIRED INTO ALL NEMA CONTROLLERS WITH 7C HARDWIRE INTERCONNECT.
- (2) PAYMENT IS MADE FOR THE NUMBER OF 2-CHANNEL DETECTOR CARDS AS SHOWN BELOW THE ASSIGNMENT CHART.
- (3) PAYMENT FOR SIGN R10-3E AND SIGNAL MOUNTING HARDWARE ARE INCLUDED IN COST FOR AUDIBLE PEDESTRIAN PUSH BUTTON.

		SIGN	IAL SIG	VS.	
QUANTITY	MUTCD SIGN NUMBER	SIZE	AREA	SIGN TOTAL AREA	SIGNAL SIGN MOUNTING HARDWARE
8	NOMBER			ITEM NO. 90208 33	ITEM NO. 90208.34
		INCHES	SQ.FT.	SQ FT	EA.
	R10-10L	24 X 30	5.0		
	XX 0-10R	24 X 30	5.0		
	R3-5L	30 X 36	7.5		
	R3 - 5R	30 X 36	7.5		
	R3-5A	30 X 36	7.5		
	R3-6L	30 X 36	7.5		
	R3-6R	30 X 36	7.5		
	R3-2	24 X 24	4.0		
	R3-1	24 X 24	4.0		
	R3-3	24 X 24	4.0		
	D3-1 ***	VAR. X 18			
	D3-1B ***	VAR X 18			
	R10-3ER	9 X 15	0.9		
1	R10-3EL	9 X 15	0.9	(0.9)	(1)
	R10-11A	24 X 30	5.0		
	R10-13	36 X 24	6.0		
	R10-27A	30 X 36	7.5		
		_	SUBTOTAL	(0.9)	(1)

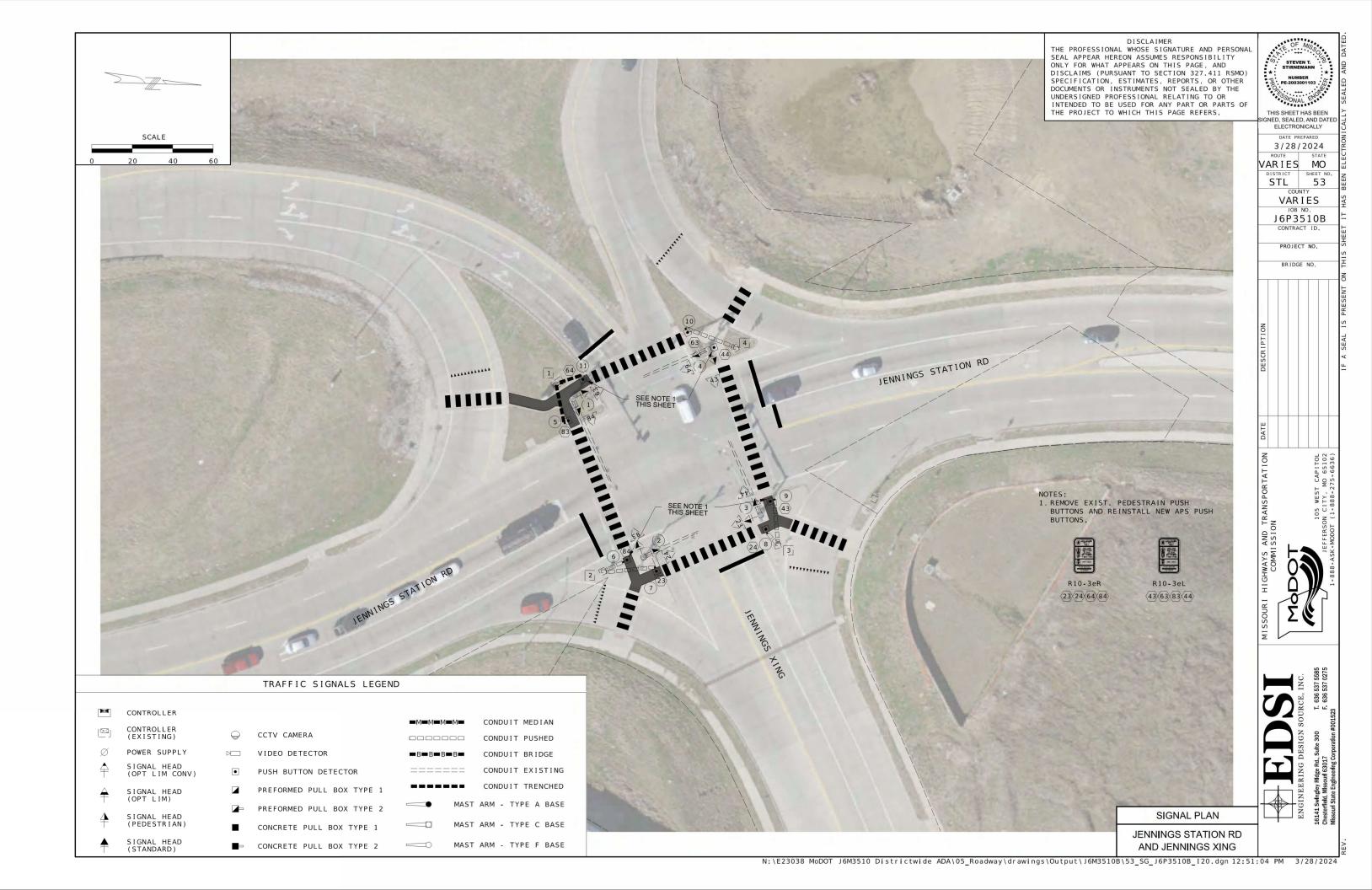


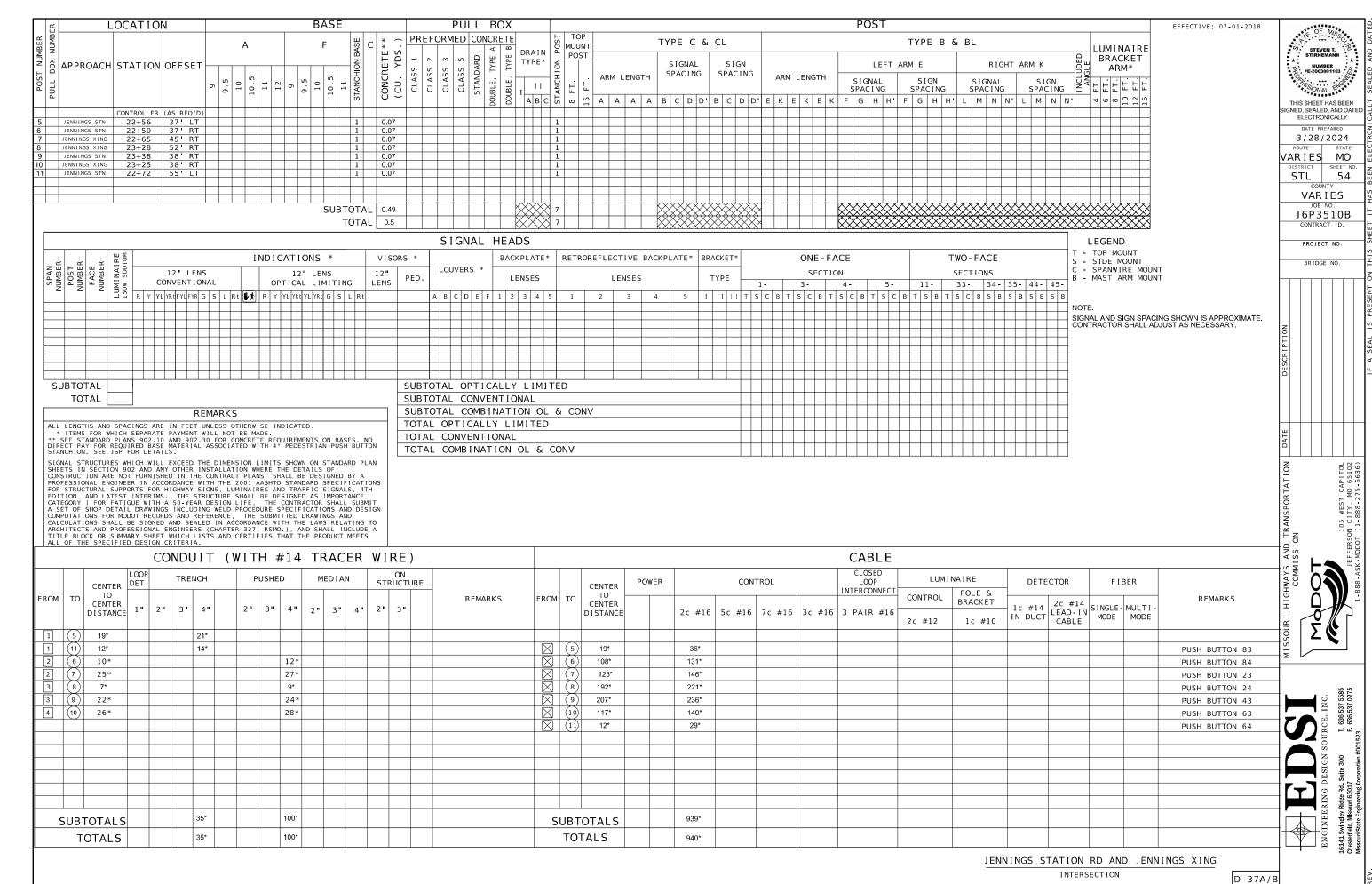




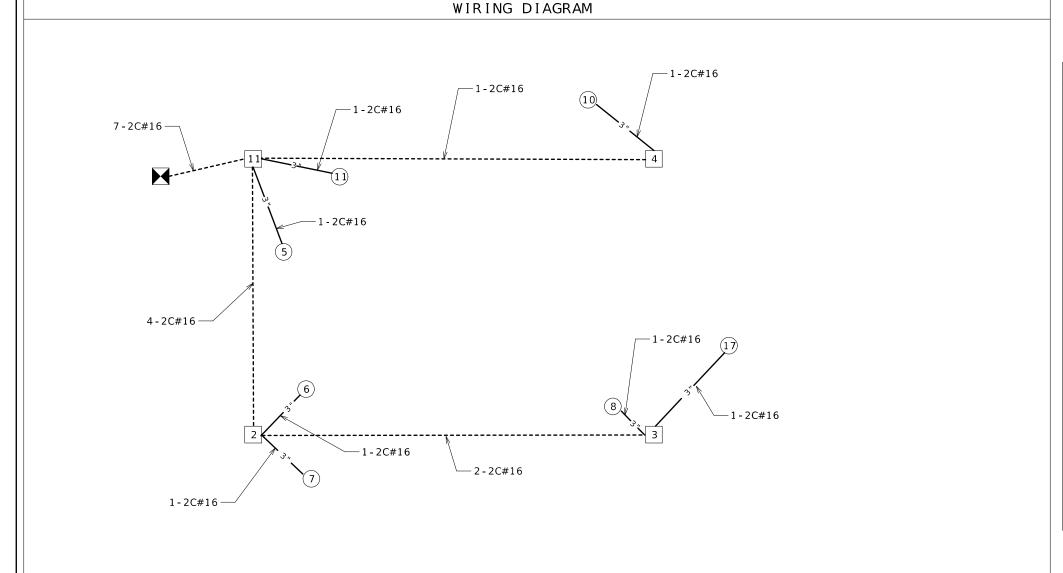
I-70 RAMPS AND N HANLEY

INTERSECTION



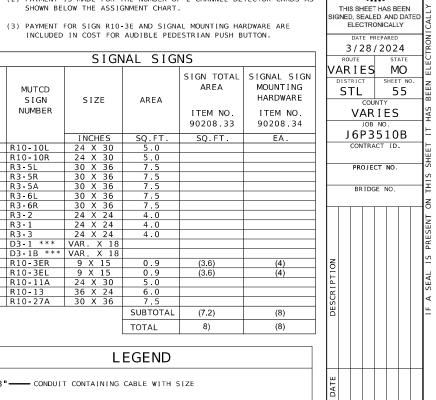


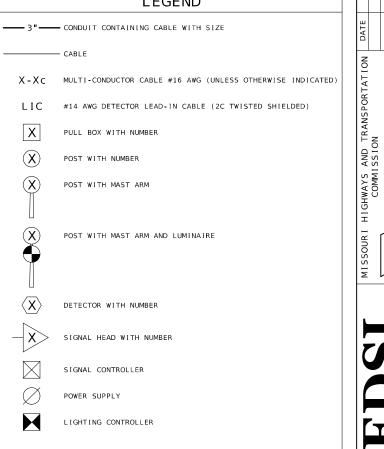
			TYPE	Ē		
DETECTOR NUMBER	APPROACH	PUSH	I NE	DUCTION LOOP ((2)	
		BUTTON	STANDARD	DELAY/ EXTEND *	CALL UNIT *	VIDEO
23	NB JENNINGS STN RD	1				
24	SB JENNINGS STN RD	1				
43	WB JENNINGS XING	1				
44	EB JENNINGS XING	1				
63	SB JENNINGS STN RD	1				
64	NB JENNINGS STN RD	1				
83	EB JENNINGS XING	11				
84	WB JENNINGS XING	1				
			+			



- * ITEMS FOR WHICH SEPARATE PAYMENT WILL NOT BE MADE.
- *** USE D3-1 FOR ONE LINE. USE D3-1B FOR TWO LINE.
- (1) MoDOT "D" PLUG SHALL BE WIRED INTO ALL NEMA CONTROLLERS WITH 7C HARDWIRE INTERCONNECT.
- (2) PAYMENT IS MADE FOR THE NUMBER OF 2-CHANNEL DETECTOR CARDS AS SHOWN BELOW THE ASSIGNMENT CHART.

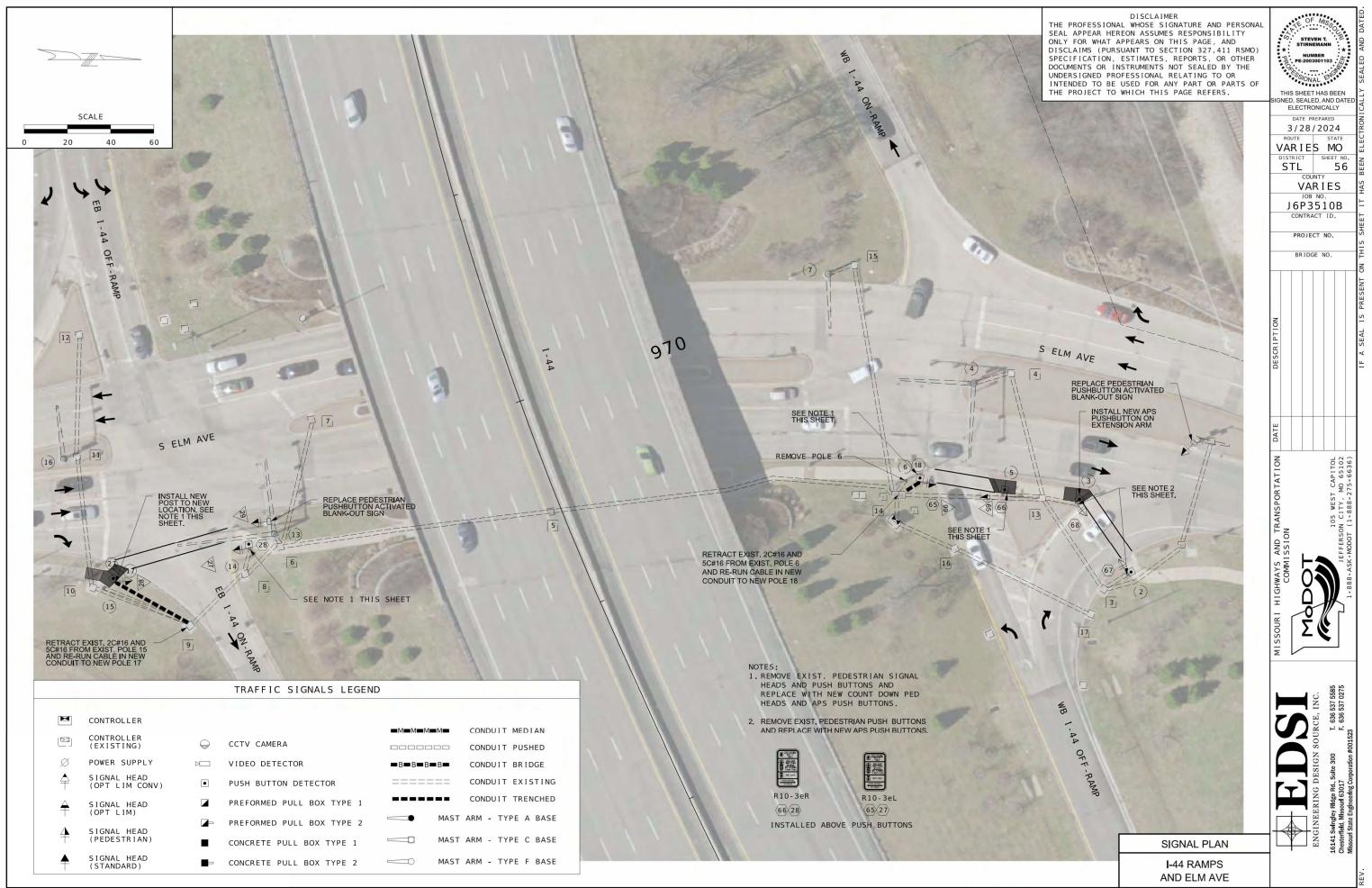
		SIGN	IAL SIGI	VS	
QUANTITY	MUTCD SIGN NUMBER	SIZE	AREA	SIGN TOTAL AREA ITEM NO. 90208.33	SIGNAL SIGN MOUNTING HARDWARE ITEM NO. 90208.34
		INCHES	SQ.FT.	SQ FT	EA.
	R10-10L	24 X 30	5.0	30.11.	LA.
	R10-10R	24 X 30	5.0		
	R3-5L	30 X 36	7.5		
	R3-5R	30 X 36	7.5		
	R3-5A	30 X 36	7.5		
	R3-6L	30 X 36	7.5		
	R3-6R	30 X 36	7.5		
	R3-2	24 X 24	4.0		
	R3-1	24 X 24	4.0		
	R3-3	24 X 24	4.0		
	D3-1 ***	VAR. X 18			
	D3-1B ***	VAR. X 18			
4	R10-3ER	9 X 15	0.9	(3.6)	(4)
4	R10-3EL	9 X 15	0.9	(3.6)	(4)
	R10-11A	24 X 30	5.0		
	R10-13	36 X 24	6.0		
	R10-27A	30 X 36	7.5		
			SUBTOTAL	(7.2)	(8)

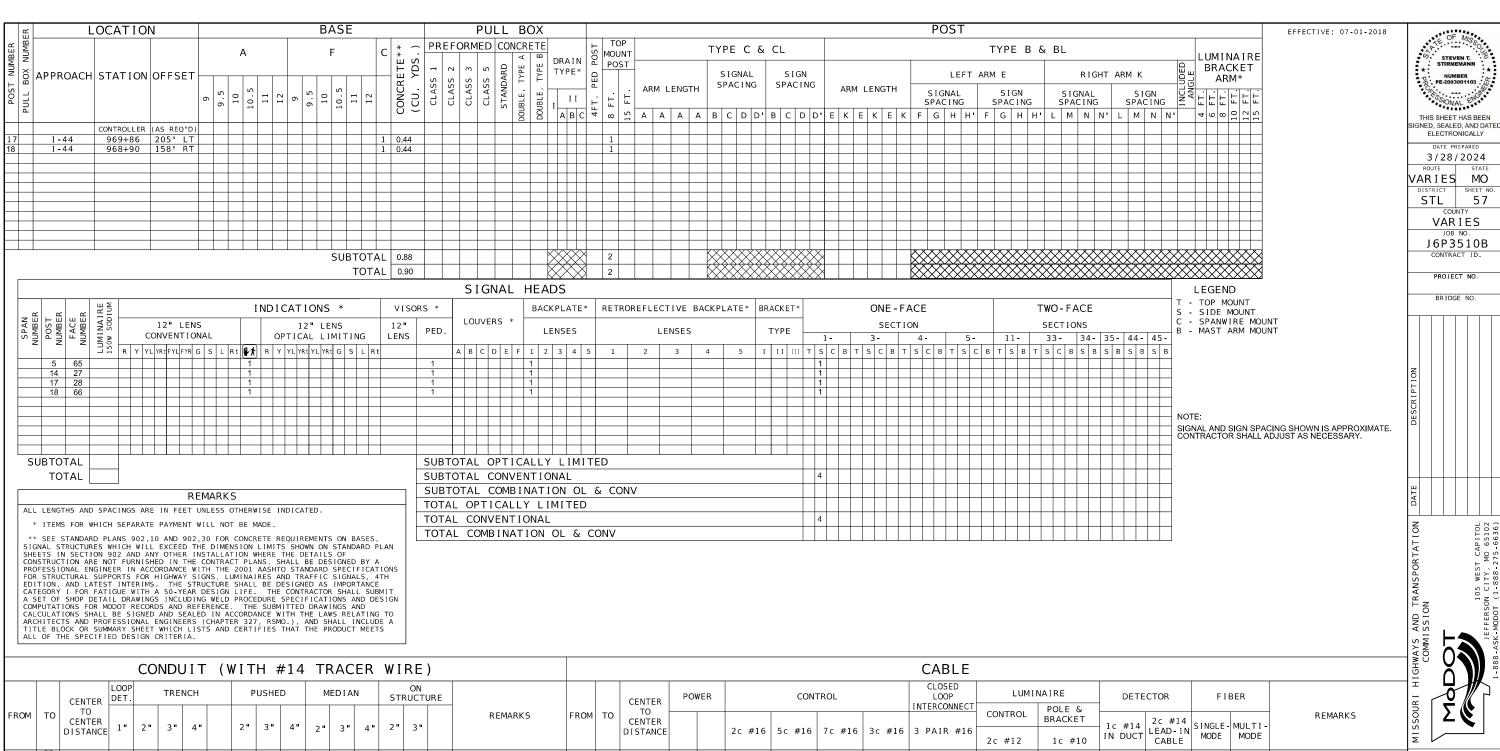




JENNINGS STN RD AND JENNINGS XING

INTERSECTION

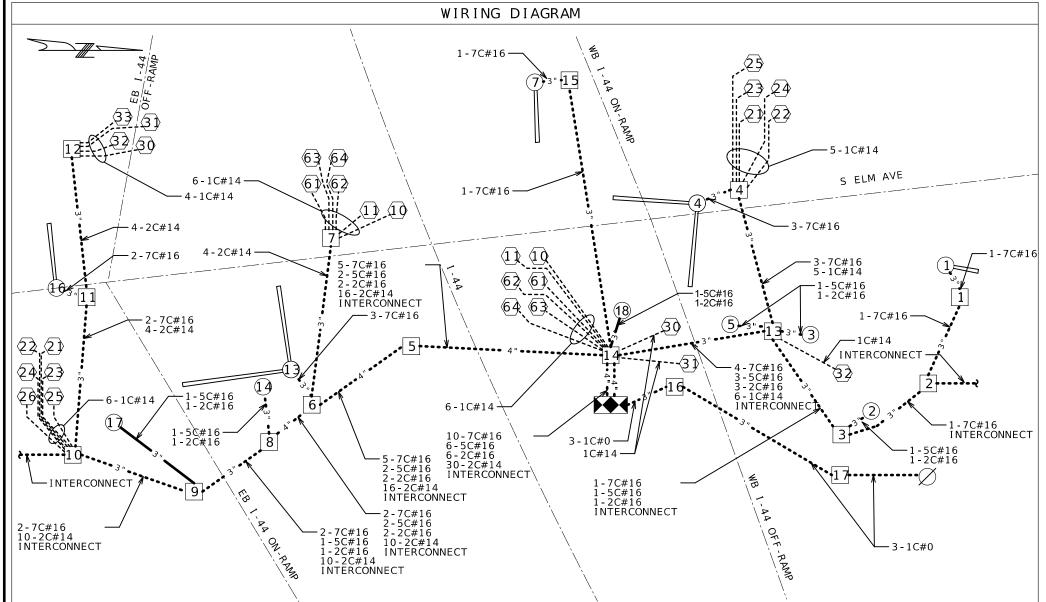




			CONDU	IT (W	ITH	l #1	4 T	RAC	CER	WIRE)									CABLE						
	CENTER	LOOI	TREN	СН	PUS	SHED	ı	MED I A	ιN	ON STRUCTURE			CENTER	POWER		CONT	ROL		CLOSED LOOP	LUMI	NAIRE	DETECTOR	FIE	BER	
ом то			2" 3"	4"	2"	3" 4	" 2'	3"	4"	2" 3"	REMARKS	FROM TO	T-0		2c #16 5	c #16	7c #16	3c #16	3 PAIR #16	CONTROL 2c #12	POLE & BRACKET	1c #14 LEAD-IN CABLE	SINGLE - MODE	MULTI - MODE	REMARKS
1 (18	40		42																	20 112	10 ,10	CABLL			
1 (18)	10		12																						
	TOTALS		54										TOTALS TALS												

S ELM ST AND I-44 RAMPS

	DETE	CTOR	SCHEDUL	. E			
				TYPE			
DETECTOR NUMBER	APPROACH	PUSH	EVTENOION	INDU	CTION LOOP (2	!)	
		BUTTON	EXTENSION ARM	STANDARD	DELAY/ EXTEND *	CALL UNIT *	VIDEO
27	EB I-44 ON-RAMP	1					
28	EB I-44 ON-RAMP	1					
65	SB I-44 OFF-RAMP	1					
66	SB I-44 OFF-RAMP	1					
67	NB I-44 OFF-RAMP	1					
68	NB I-44 OFF-RAMP	1	1				
	TOTAL	6	1				



- * ITEMS FOR WHICH SEPARATE PAYMENT WILL NOT BE MADE.
- *** USE D3-1 FOR ONE LINE. USE D3-1B FOR TWO LINE.
- (1) MoDOT "D" PLUG SHALL BE WIRED INTO ALL NEMA CONTROLLERS WITH 7C HARDWIRE INTERCONNECT.

STEVEN T. STIRNEMANN

THIS SHEET HAS BEEN GNED, SEALED, AND DATE

ELECTRONICALLY

3/28/2024 ROUTE STATE /ARIES MO

VARIES
JOB NO.
J6P3510B

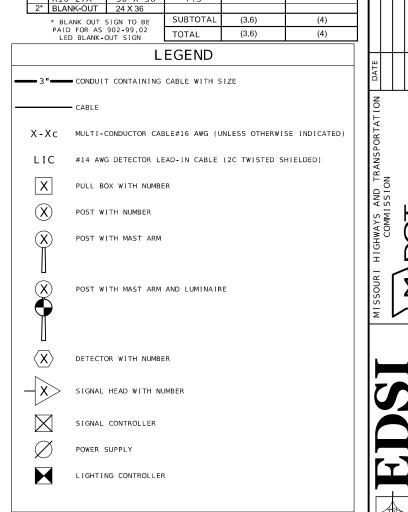
PROJECT NO

58

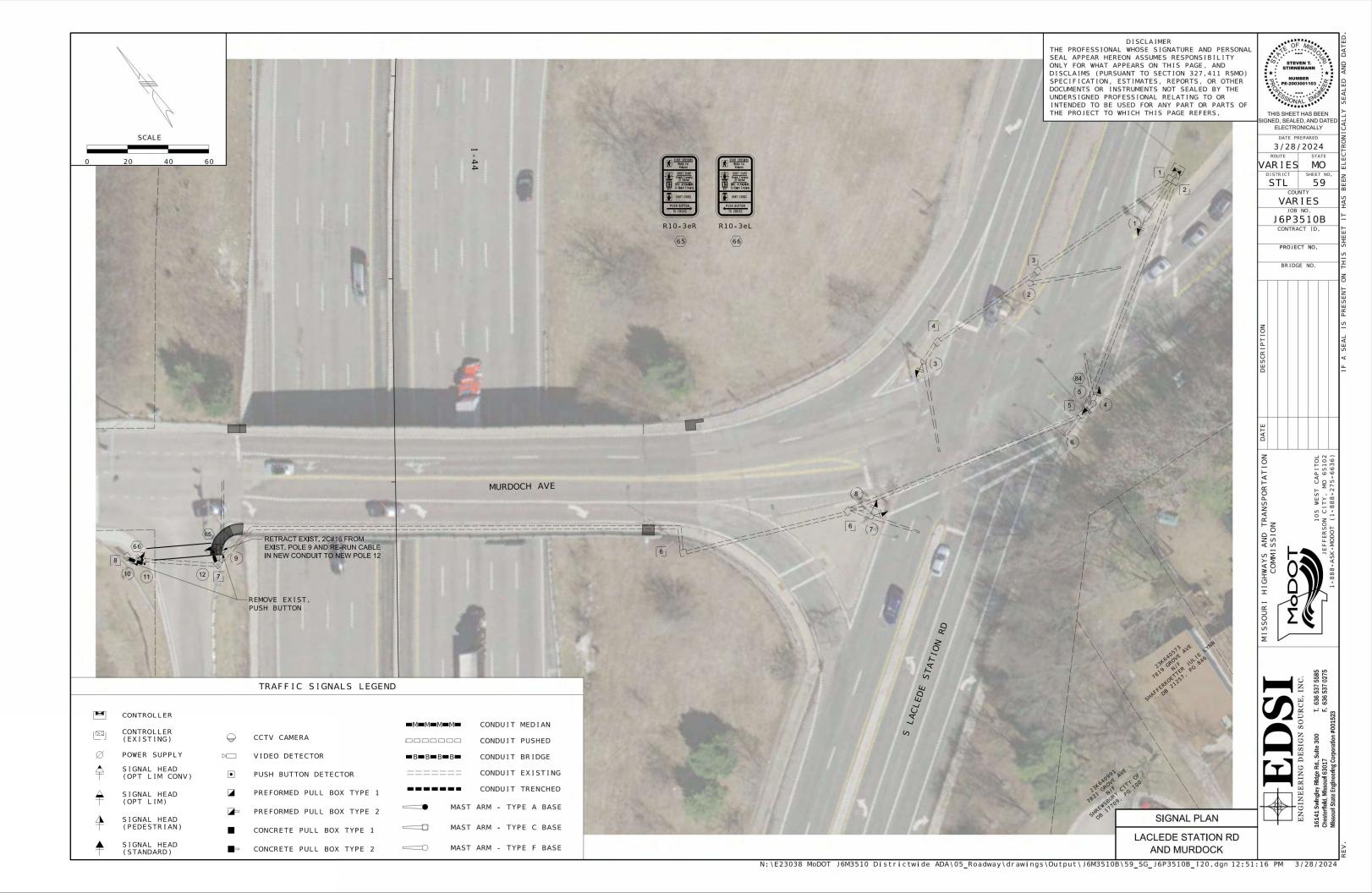
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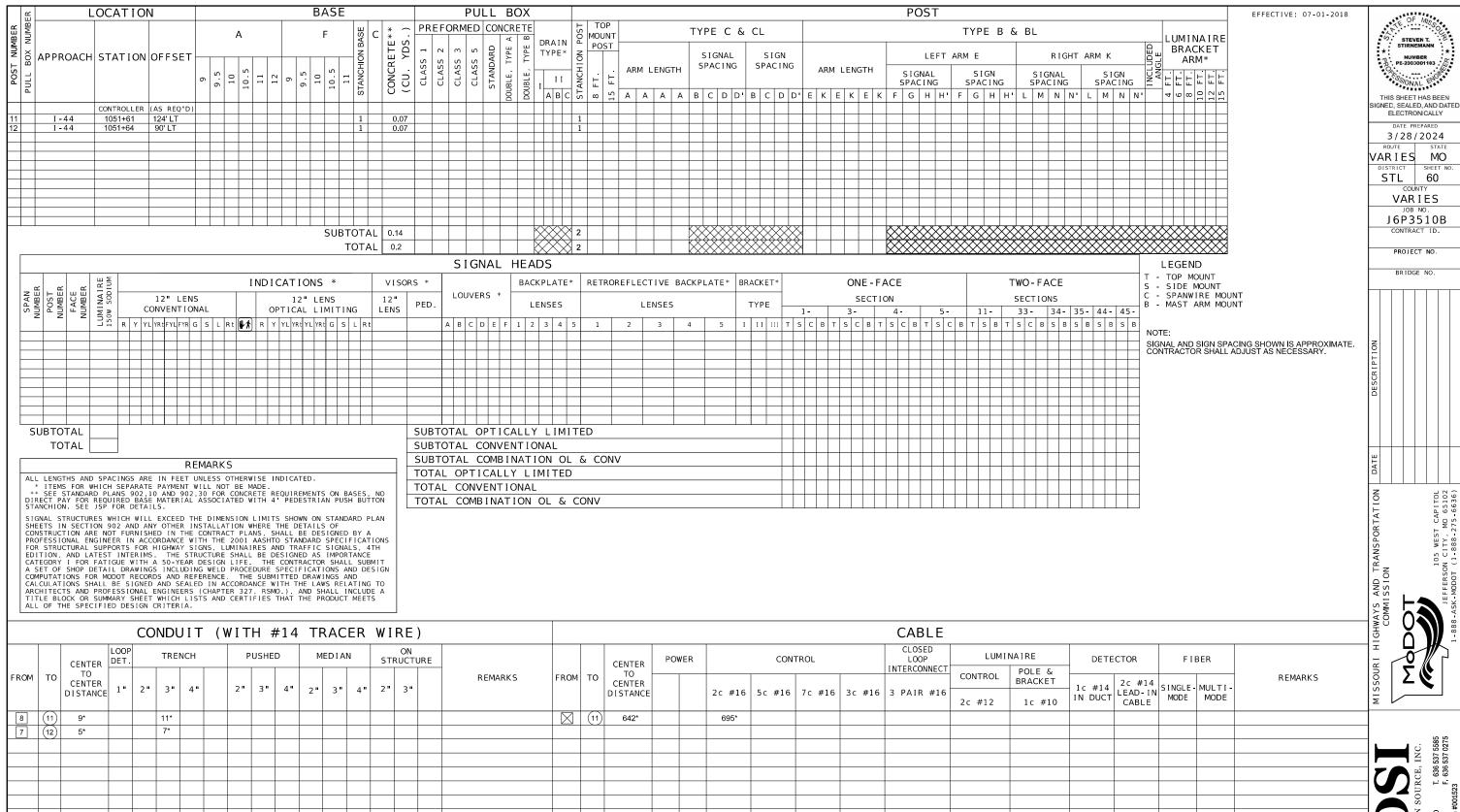
- (2) PAYMENT IS MADE FOR THE NUMBER OF 2-CHANNEL DETECTOR CARDS AS SHOWN BELOW THE ASSIGNMENT CHART.
- (3) PAYMENT FOR SIGN R10-3E AND SIGNAL MOUNTING HARDWARE ARE INCLUDED IN COST FOR AUDIBLE PEDESTRIAN PUSH BUTTON.

		SIGN	IAL SIGI	VS	
QUANTITY	MUTCD SIGN NUMBER	SIZE	AREA	SIGN TOTAL AREA ITEM NO.	SIGNAL SIGN MOUNTING HARDWARE ITEM NO.
ರ				90208.33	90208 34
		INCHES	SQ FT.	SQ.FT.	EA.
	R10-10L	24 X 30	5.0		
	R10-10R	24 X 30	5.0		
	R3-5L	30 X 36	7.5		
	R3-5R	30 X 36	7.5		
	R3-5A	30 X 36	7.5		
	R3-6L	30 X 36	7.5		
	R3-6R	30 X 36	7.5		
	R3-2	24 X 24	4.0		
	R3-1	24 X 24	4.0		
	R3-3	24 X 24	4.0		
	D3-1 ***	VAR. X 18			
	D3-1B ***	VAR. X 18			
2	R10-3ER	9 X 15	0.9	(1.8)	(2)
2	R10-3EL	9 X 15	0.9	(1.8)	(2)
	R10-11A	24 X 30	5.0		
	R10-13	36 X 24	6.0		
L_	R10-27A	30 X 36	7.5		
2*	BLANK-OUT	24 X 36			
	* BLANK OUT : PAID FOR AS		SUBTOTAL	(3.6)	(4)
	PAID FOR AS	902-99.02	ΤΩΤΔΙ	(3.6)	(4)



S ELM ST AND I-44 RAMPS
INTERSECTION





SUBTOTALS

TOTALS

18*

18*

SUBTOTALS

TOTALS

S ELM ST AND I -44 RAMPS

INTERSECTION

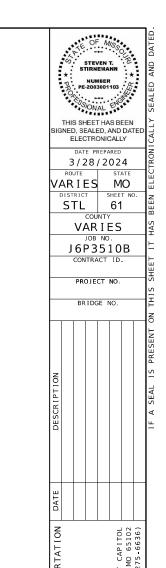
D-37A/B

N:\E23038 MoDOT J6M3510 Districtwide ADA\05_Roadway\drawings\Output\J6M3510B\60_SG_J6P3510B_11.dgn 12:51:18 PM 3/28/2024

	ACCESS	IBLE PUSH B	UTTONS			
			TYPE			
DETECTOR NUMBER	APPROACH	PUSH	I NI	DUCTION LOOP ((2)	
		BUTTON	STANDARD	DELAY/ EXTEND *	CALL UNIT *	VIDEO
65	WB I-44 ON-RAMP	1				
66	WB I-44 ON-RAMP	1				
	TOTAL	2				

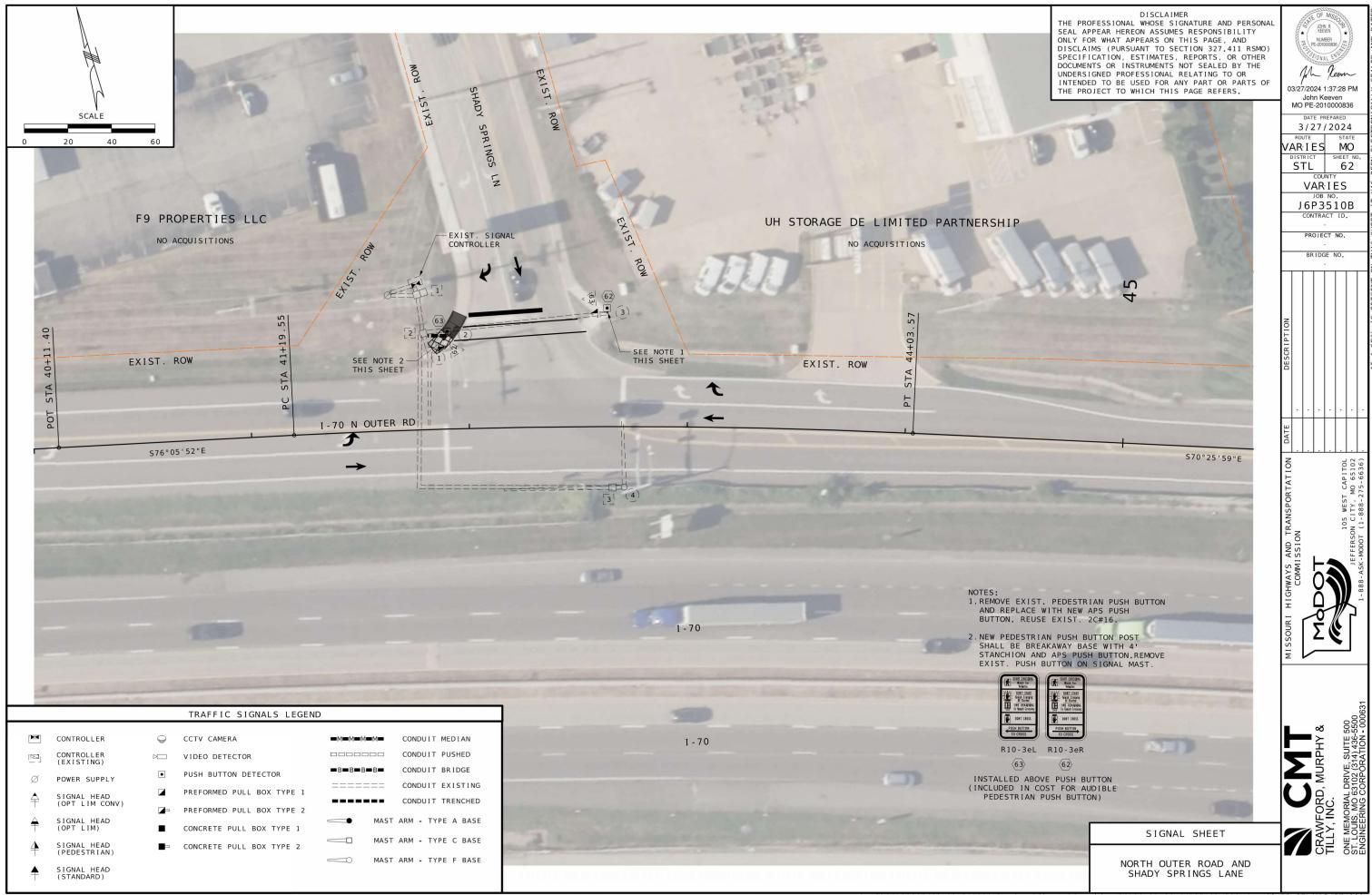
- * ITEMS FOR WHICH SEPARATE PAYMENT WILL NOT BE MADE.
- *** USE D3-1 FOR ONE LINE. USE D3-1B FOR TWO LINE.
- (1) MoDOT "D" PLUG SHALL BE WIRED INTO ALL NEMA CONTROLLERS WITH 7C HARDWIRE INTERCONNECT.
- (2) PAYMENT IS MADE FOR THE NUMBER OF 2-CHANNEL DETECTOR CARDS AS SHOWN BELOW THE ASSIGNMENT CHART.
- (3) PAYMENT FOR SIGN R10-3E AND SIGNAL MOUNTING HARDWARE ARE INCLUDED IN COST FOR AUDIBLE PEDESTRIAN PUSH BUTTON.

		SIGN	IAL SIGI	٧S	
QUANTITY	MUTCD S I GN	SIZE	AREA	SIGN TOTAL AREA	SIGNAL SIGN MOUNTING HARDWARE
√NÒ	NUMBER			ITEM NO. 90208.33	ITEM NO. 90208.34
		INCHES	SQ FT	SQ FT	EA.
	R10-10L	24 X 30	5.0		
	₹ ¥0-10R	24 XXX 30	5.0		
	R3-5L	30 X 36	7.5		
	R3-5R	30 X 36	7.5		
	R3-5A	30 X 36	7.5		
	R3-6L	30 X 36	7.5		
	R3-6R	30 X 36	7.5		
	R3-2	24 X 24	4.0		
	R3-1	24 X 24	4.0		
	R3-3	24 X 24	4.0		
	D3-1 ***	VAR. X 18			
	D3-1B ***	VAR. X 18			
1	R10-3ER	9 X 15	0.9	(0.9)	(1)
1	R10-3EL	9 X 15	0.9	(0.9)	(1)
	R10-11A	24 X 30	5.0		
	R10-13	36 X 24	6.0		
	R10-27A	30 X 36	7.5		
			SUBTOTAL	(1.8)	(2)
			TOTAL	(2)	(2)



MURDOCH AVE AND I-44 RAMPS

INTERSECTION



EFFECTIVE: 07/01/2

H H		LOCATIO	N					BAS	E				PUL	.L E	ЗОХ														P	OST												
MBER					Α			F	ASE	C *	PR	EFOR	MED	CONC		DD 4.1	 TSC		uT		T,	YPE C	& C	L								TYP	ЕВ	& BI	-					LUM	IINA	IRF
NO XO	APPROACH	STATION	OFFSET						NO B	ETE	YDS	2 6	0 0	ARD	TYPE	TYPE	* 2	POS	+			SIGNAL		SIG						LEF	T AF	RM E			R	I GHT	ARM	K	DED	BR	ACK ARM*	ET
POST LL B				6 2	0.5	: 2	6 5	10	11 ANCHI	ONCR	O. LAS	LAS	LAS	TAND	UBLE,	11	- I	<u>.</u>	<u>.</u> /	ARM LENGT	тн	SPACINO	5 S	PAC	ING	ARM LE	NGTH			SNAL CING		S I SPAC			I GNA	rr L	S	SIGN ACIN	G JUNCLI	 		
PU				6	1		6	1(STAI	8				S	DOUBL	I A B	C 7	8 4	122	AAA	А В	C D	D'B	С	D D' E	K E	K E	K F	= G		H' F	: G	н н	. r		N.	L I	M N	N.	4 F	8 10	12
		CONTROLLER	(AS REQ'D)																																							
2	1-70 N OUTE	R 41+91	42.9 LT						1	0.0	7						1																									\Box
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NOTE:

SIGNAL AND SIGN SPACING SHOWN IS APPROXIMATE. CONTRACTOR SHALL ADJUST AS NECESSARY.

REMARKS

ALL LENGTHS AND SPACINGS ARE IN FEET UNLESS OTHERWISE INDICATED.

 st ITEMS FOR WHICH SEPARATE PAYMENT WILL NOT BE MADE.

** SEE STANDARD PLANS 902.10 AND 902.30 FOR CONCRETE REQUIREMENTS ON BASES. NO DIRECT PAY FOR REQUIRED BASE MATERIAL ASSOCIATED WITH 4 PEDESTRIAN PUSH BUTTON STANCHION. SEE JSP FOR DETAILS.

*** USE D3-16 FOR ONE LINE. USE D3-18 FOR TWO LINE.

SIGNAL STRUCTURES WHICH WILL EXCEED THE DIMENSION LIMITS SHOWN ON STANDARD PLAN SHEETS IN SECTION 902 AND ANY OTHER INSTALLATION WHERE THE DETAILS OF CONSTRUCTION ARE NOT FURNISHED IN THE CONTRACT PLANS SHALL BE DESIGNED BY A PROFESSIONAL ENGINEER IN ACCORDANCE WITH THE 2001 AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4TH EDITION, AND LATEST INTERIMS, THE STRUCTURE SHALL BE DESIGNED AS IMPORTANCE CATEGORY I FOR FATIGUE WITH A 50-YEAR DESIGN LIFE. THE CONTRACTOR SHALL SUBMIT A SET OF SHOP DETAIL DRAWINGS INCLUDING WELD PROCEDURE SPECIFICATIONS AND DESIGN COMPUTATIONS FOR MODOT RECORDS AND REFERENCE. THE SUBMITTED DRAWINGS AND CALCULATIONS SHALL BE SIGNED AND SEFERENCE. THE SUBMITTED DRAWINGS AND CALCULATIONS SHALL BE SIGNED AND SEFERENCE. THE SUBMITTED DRAWINGS AND CALCULATIONS SHALL BE SIGNED AND SEFERENCE. THE SUBMITTED DRAWINGS AND THE SECONDARY SERVICES OF THE SUBMITTED DRAWINGS AND SHALL INCLUDE A TITLE BLOCK OR SUMMARY SHEET WHICH LISTS AND CERTIFIES THAT THE PRODUCT MEETS ALL OF THE SPECIFIED DESIGN CRITERIA.

		C	ONDU I T	(W I :	TH #	#14	TRA	ACE	R W	/IRE)								CABLE						
	CENTER	LOOP DET.	TRENCH		PUSHE	D	MED	DIAN		ON STRUC	I TURE			CENTER PO	OWER		CONTROL		CLOSED LOOP		NAIRE	DET	ECTOR FI	BER	
то мом	I =-		• 3• 4 •	21	3"	4"	2	211	411	211 21		REMARKS	FROM TO	TO CENTER					INTERCONNECT	CONTROL	POLE & BRACKET	1c #14	2c #14 SINGLE	MULTI-	REMARKS
	DISTANCE		3 4			1	2"	3	4	د ع				DISTANCE		2c #16	5c #16 7c #16	3c #16	3 PAIR #16	2c #12	1c #10	IN DUCT	ZC #14 LEAD IN CABLE SINGLE MODE	MODE	
2] 2	11'		12'											34'		67'									PUSH BUTTON 63
								-			+														
					+						+														
							_	+		-	+														
								+			+-														
SUB	TOTALS	i	12'										SUB	TOTALS		67'									
Т	OTALS		12'										TO	TALS		70'									

CONTRACTOR SHALL USE EXISTING WIRE FOR HOOK-UP OF NEW PUSH BUTTON 62.

I - 70 N	OUTER	RD	/	SHADY	SPRINGS	LN	
INTERSECTION							

	3/27 MO 3/27	Joh PE DAT / 2	n K -20 E PF 2 7	eev 100	en 008 RED 0 2	36 4		
	DIST			MO SHEET NO. 63				
	_	V	COU AR	NTY I	ES			
	J	61	3	5		В		
L			TRA J E			•		
\perp			IDG		10			
\perp								
DESCRIPTION	•	•	•	•	•	•		
DATE								
1 HIGHWAYS AND TRANSPORTATION	COMMISSION		FOQ4		105 WEST CAPITOL	JEFFERSON CITY, MO 65102	1-888-ASK-MODOT (1-888-275-6636)	

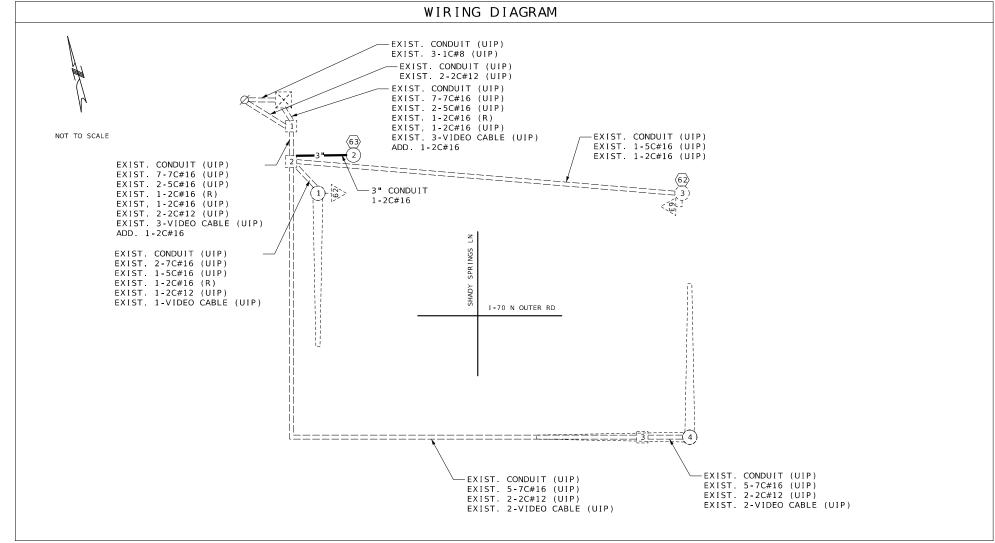
	ACCESS I	BLE PUSH	BUTTONS					
		TYPE						
DETECTOR NUMBER	APPROACH	PUSH BUTTON	INI	\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\				
			STANDARD	DELAY/ EXTEND *	CALL UNIT *	VIDEO		
62	WB I-70 N OUTER RD	1						
63	EB I-70 N OUTER RD	1						
*								
*								
4								
4								
4								
	TOTAL	2						

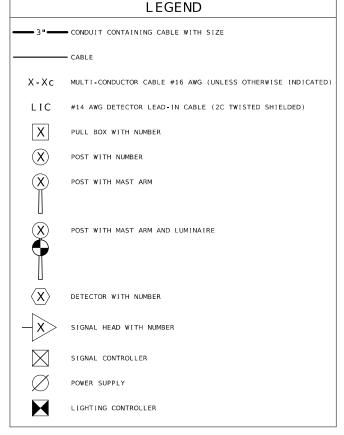
	SIGNAL SIGNS								
QUANTITY	MUTCD SIGN NUMBER	SIZE	AREA	SIGN TOTAL AREA ITEM NO. 90208.33	SIGNAL SIGN MOUNTING HARDWARE ITEM NO. 90208.34				
		THELE							
	R10-10L	INCHES	SQ FT 5 0	SQ.FT.	EA.				
	R10-10L R10-10R	24 X 30 24 X 30	5.0						
	R3-5L	30 X 36	7.5						
	R3-5R	30 X 36	7.5						
	R3-5A	30 X 36	7.5						
	R3-6L	30 X 36	7.5						
	R3-6R	30 X 36	7.5						
	R3-2	24 X 24	4.0						
	R3-1	24 X 24	4.0						
	R3-3	24 X 24	4.0						
	D3-1 ***	VAR. X 18							
	D3-1B ***	VAR. X 18							
1	R10-3EL	9 X 15	0.9	(0.9)	(1)				
1	R10-3ER	9 X 15	0.9	(0.9)	(1)				
	R10-11A	24 X 30	5.0						
	R10-13	36 X 24 30 X 36	6.0						
	R10-27A	30 X 36	7.5						
			SUBTOTAL	(1.8)	(2)				
			TOTAL	(2.0)	(2)				

*** USE D3-1 FOR ONE LINE. USE D3-1B FOR TWO LINE.

TF.

PAYMENT FOR SIGN R10-3E AND SIGNAL MOUNTING HARDWARE ARE INCLUDED IN COST FOR AUDIBLE PEDESTRIAN PUSH BUTTON.





I-70 N OUTER RD / SHADY SPRINGS LN
INTERSECTION

D-37C/D

03/27/2024 1:37:28 PM John Keeven MO PE-2010000836

3/27/2024 ROUTE STATE /AR·IES MO

VARIES
JOB NO
JOB NO

PROJECT NO.

STL

SHEET NO

6.4