DESIGN DESIGNATION

A.A.D.T. - 2025 = 100,840A.A.D.T. - 2045 = 111,000 D.H.V. = 12,000 V = 55 M.P.H. D = 12%

FUNCTIONAL CLASSIFICATION- INTERSTATE

NO NEW R/W REQUIRED

LOCATION OF CLAY COUNTY

CONVENTIONAL SYMBOLS

BUILDINGS AND STRUCTURES ニュニコ GUARD RAIL •••• GUARD CABLE •••• CONCRETE RIGHT-OF-WAY MARKER STEEL RIGHT-OF-WAY MARKER LOCATION SURVEY MARKER \circ UTILITIES FIBER OPTICS -FO--F0- OVERHEAD CABLE TV -OTV--OTV- UNDERGROUND CABLE TV OVERHEAD TELEPHONE -UTV-- OT--UTV- -OT- - UT -- OE -- UE -- S -UT OE UNDERGROUND TELEPHONE OVERHEAD POWER UNDERGROUND POWER SANITARY SEWER —UE— —S— STORM SEWER _SS_ —G— WATER SAN HYD WV MANHOLE FIRE HYDRANT WATER VALVE ⊕ WATER METER DROP INLET DITCH BLOCK =GROUND MOUNTED SIGN LIGHT POLE H-FRAME POWER POLE

NOTE: DASHED OR OPEN SYMBOLS INDICATE EXISTING FEATURES

Δ

 \boxtimes

TELEPHONE PEDESTAL

WOVEN WIRE

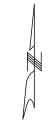
GATE POST

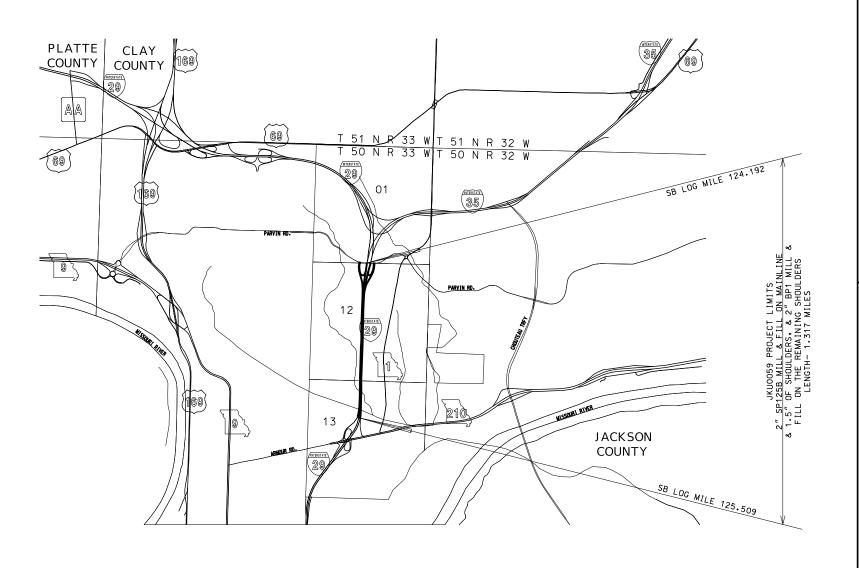
BENCHMARK

FENCE CHAIN LINK

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

PLANS FOR PROPOSED STATE HIGHWAY CLAY COUNTY





NOT TO SCALE

THE EXISTENCE AND APPROXIMATE LOCATION OF UTILITY FACILITIES KNOWN TO EXIST, AS SHOWN ON THE PLANS, ARE BASED ON THE BEST INFORMATION AVAILABLE TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE. RELIANCE UPON THIS INFORMATION IS DONE AT THE RISK AND PERIL OF THE USER, AND THE COMMISSION SHALL NOT BE LIABLE FOR ANY DAMAGES THAT MAY ARISE FROM ANY ERROR IN THE INFORMATION. IT IS, THEREFORE, THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE, LOCATION AND STATUS OF ANY FACILITY. SUCH VERIFICATION INCLUDES DIRECT CONTACT WITH THE LISTED UTILITIES.

INDEX OF SHEETS

DESCRIPTION	SHEET NUMBER
TITLE SHEET	1
TYPICAL SECTIONS (TS) (8 SHEETS)	2
QUANTITIES (QU) (3 SHEETS)	3
TRAFFIC CONTROL SHEETS (TC)	4 - 13

8/8/2024												
	2)TE				О						
		RIC C	Т	9	HEE	т NC 1	٥.					
				тти А								
		Jk	(U	ои О С	59							
		CON	TRA	CT	ID	•						
		PR	OJE	СТ	NO.							
		BR	IDC	E N	۱O.							
NC												
DESCRIPTION												
SCR												
DE												
DATE												
SPORTATION					ITO	TY, MO 65102	1303					
3TA1					QAP.	MO 6	2 2 1					
SPOI					WEST	_,	000					

CRAIG SKORSETH

NUMBER

LENGTH OF PROJECT

BEGINNING OF PROJECT SB LOG MILE 124.192 END OF PROJECT SB LOG MILE 125.509 APPARENT LENGTH 6953.76 FEET

EQUATIONS AND EXCEPTIONS:

0.00 FEET

TOTAL CORRECTIONS 0.00 FEET NET LENGTH OF PROJECT 6953.76 FEET STATE LENGTH 1.317 MILES FOR INFORMATION ONLY ESTIMATED DISTURBED ACRES

0 ACRES



NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

MAINLINE TREATMENT TO BE USED ON REMAINING SHOULDERS LESS THAN 3° .

ASPHALT FACTORS

COMBINED FACTOR

SP125B (76-22) 1.946 TONS/CY BP-1 (64-22) 1.984 TONS/CY

TACK COAT

MILLING

IRREGULARITIES: 75 TONS/MI

KC 2

COUNTY
CLAY

JOB NO.
JKU0059

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

0.10 GAL/SY



CRAIG SKORSETH

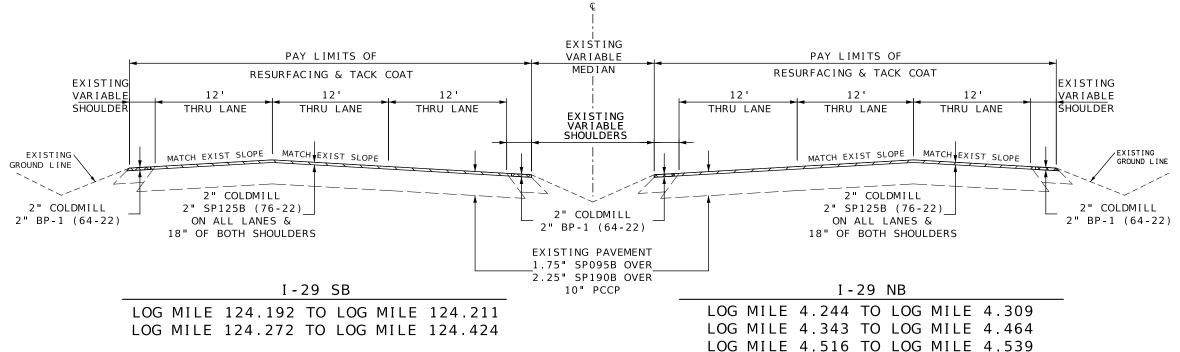
SONAL EN

Croix Shorseth

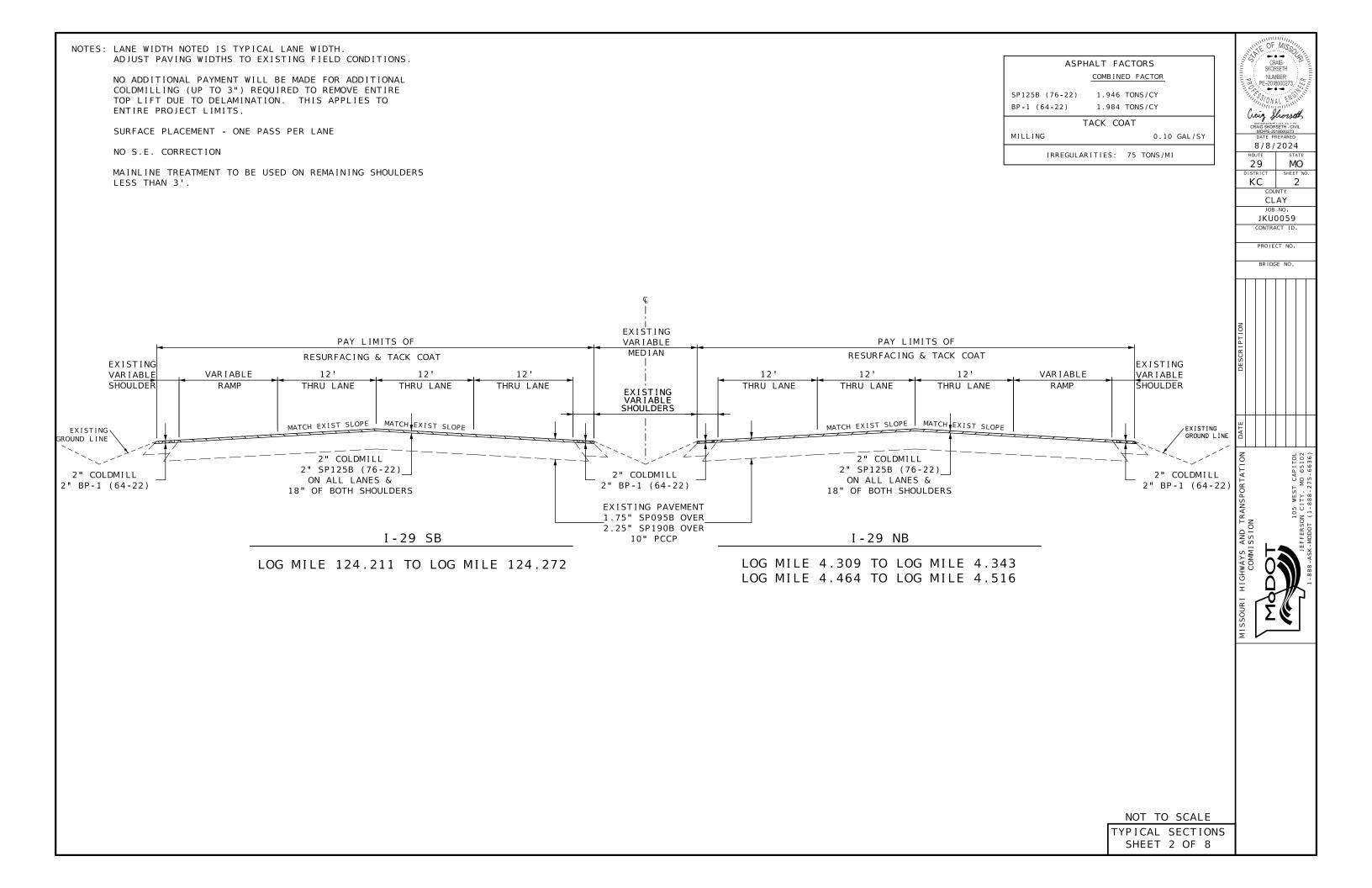
8/8/2024

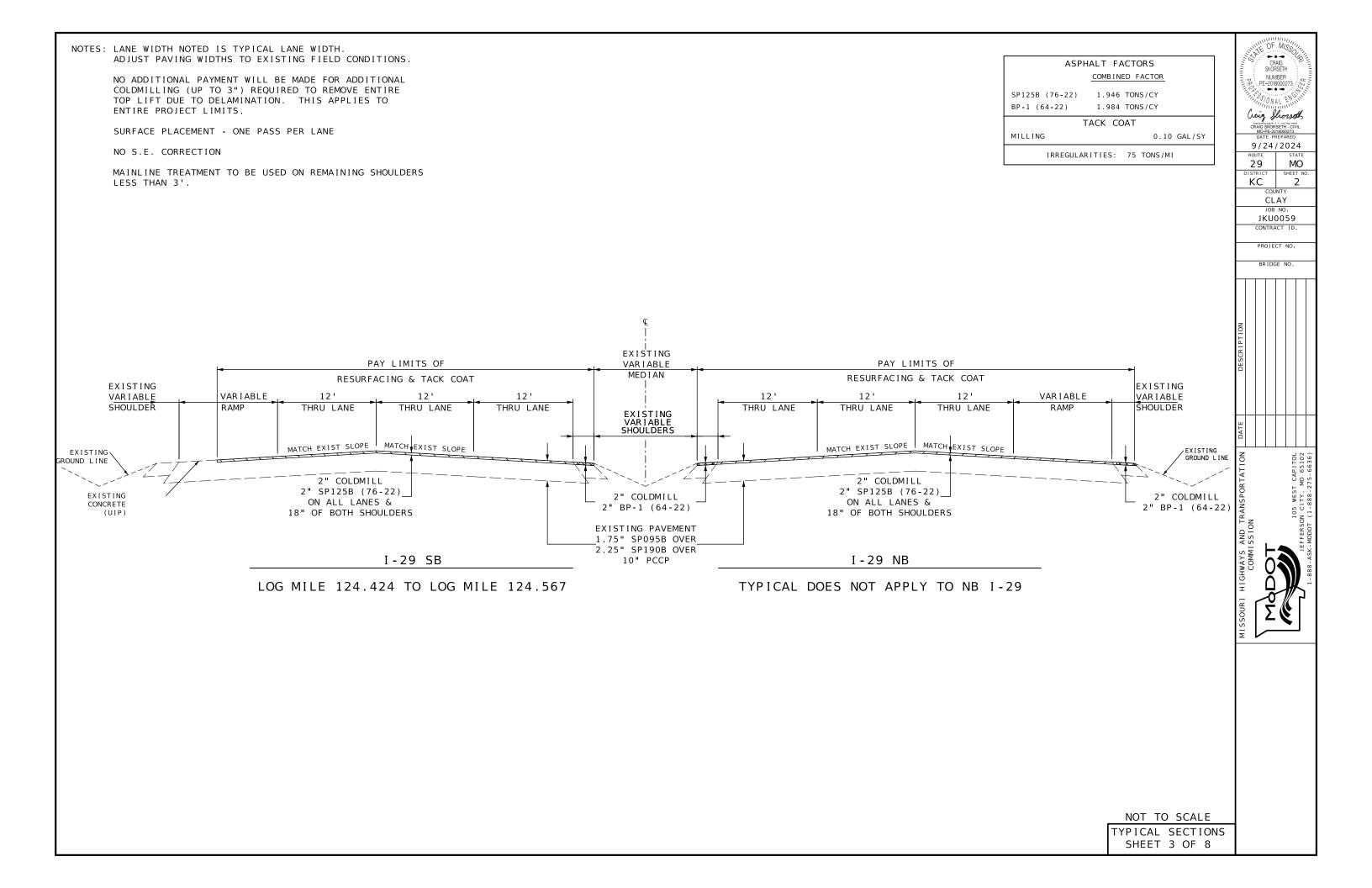
MO

29



NOT TO SCALE
TYPICAL SECTIONS
SHEET 1 OF 8





NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

MAINLINE TREATMENT TO BE USED ON REMAINING SHOULDERS LESS THAN 3° .

ASPHALT FACTORS

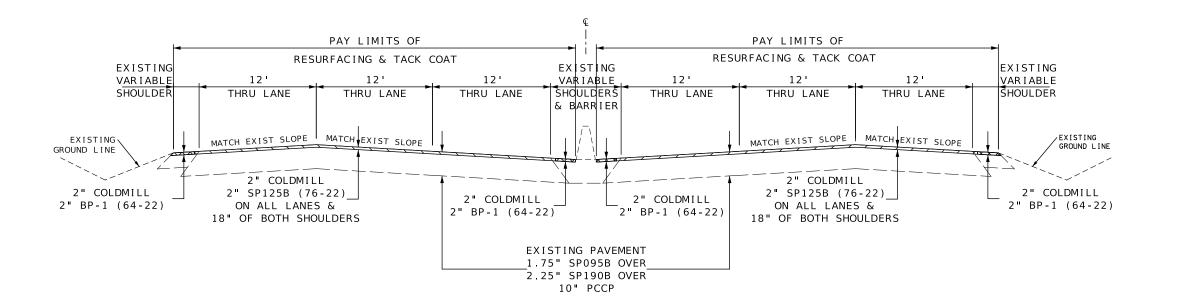
COMBINED FACTOR

SP125B (76-22) 1.946 TONS/CY BP-1 (64-22) 1.984 TONS/CY

TACK COAT

MILLING 0.10 GAL/SY

IRREGULARITIES: 75 TONS/MI



I-29 NB

LOG MILE 3.446 TO LOG MILE 4.244

I-29 SB

LOG MILE 124,567 TO LOG MILE 125,424

NOT TO SCALE
TYPICAL SECTIONS
SHEET 4 OF 8

ORAIG SKORSETH NUMBER PE-2018000273

CRAIG SKORSETH - CIVIL MO-PE-2018000273 DATE PREPARED 8/8/2024

8/8/2024

ROUTE STATE
29 MO

KC 2

COUNTY
CLAY

JKU0059

PROJECT NO.



SOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL

> NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

MAINLINE TREATMENT TO BE USED ON REMAINING SHOULDERS LESS THAN 3'.

ASPHALT FACTORS

COMBINED FACTOR

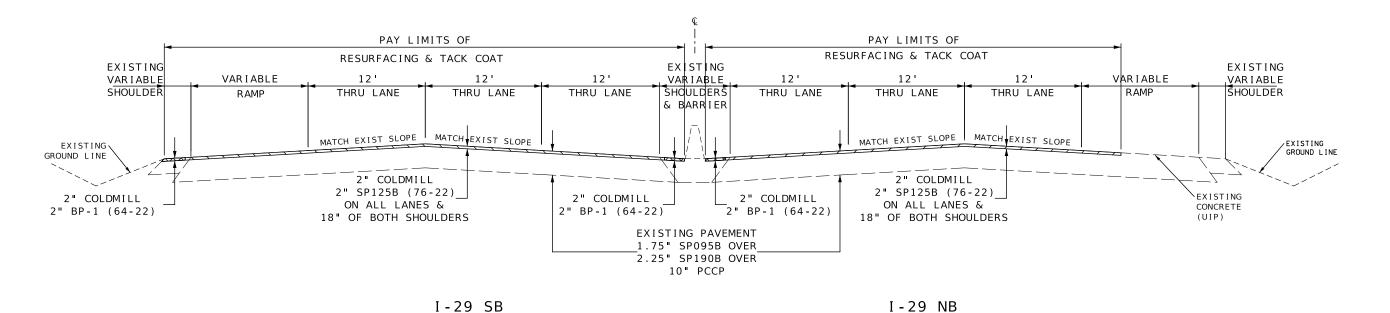
SP125B (76-22) 1.946 TONS/CY BP-1 (64-22) 1.984 TONS/CY

TACK COAT

MILLING

0.10 GAL/SY

IRREGULARITIES: 75 TONS/MI



LOG MILE 3.258 TO LOG MILE 3.446

LOG MILE 125.424 TO LOG MILE 125.462

CRAIG SKORSETH PE-2016 NUMBER Croing Shorsetts 8/8/2024 29 MO KC 2 CLAY JKU0059 CONTRACT ID.

> PROJECT NO. BRIDGE NO.

NOT TO SCALE TYPICAL SECTIONS SHEET 5 OF 8

NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

MAINLINE TREATMENT TO BE USED ON REMAINING SHOULDERS LESS THAN 3° .

ASPHALT FACTORS

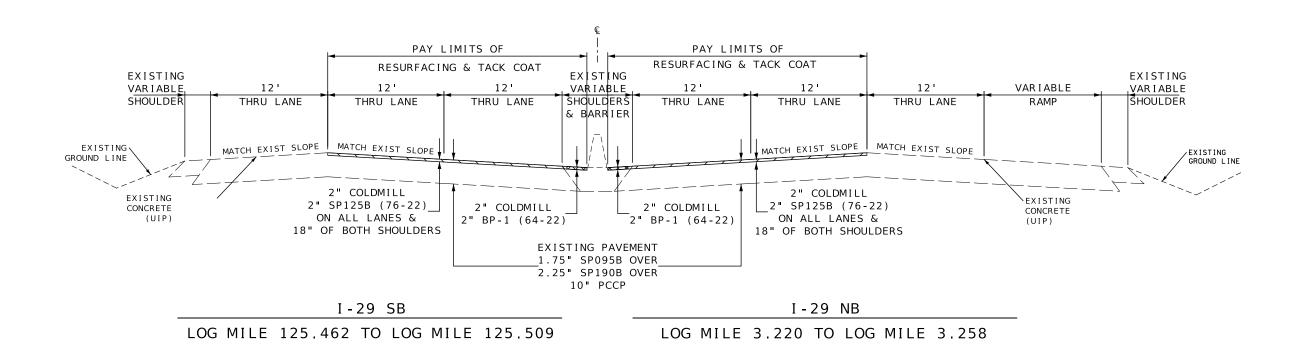
COMBINED FACTOR

SP125B (76-22) 1.946 TONS/CY BP-1 (64-22) 1.984 TONS/CY

TACK COAT

MILLING 0.10 GAL/SY

IRREGULARITIES: 75 TONS/MI



CRAIG SKORSETH NUMBER SOUND STATE CONTRACT ID.

CRAIG SKORSETH NUMBER SOUND SO

BRIDGE NO.

DATE DESCRIPTION

ISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

MADOT

105 WEST CAPITO
JEFFERSON CITY, MO 6510

NOT TO SCALE
TYPICAL SECTIONS
SHEET 6 OF 8

NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

MAINLINE TREATMENT TO BE USED ON REMAINING SHOULDERS LESS THAN 3'.

ASPHALT FACTORS

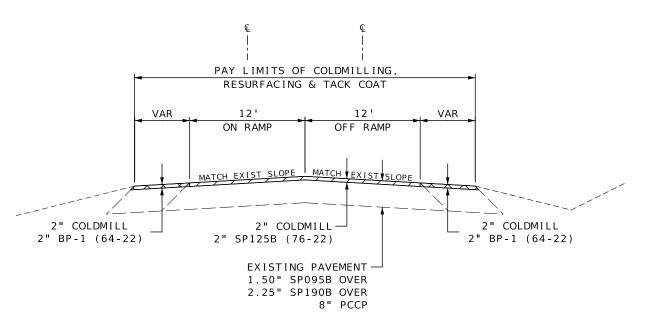
COMBINED FACTOR

SP125B (76-22) 1.946 TONS/CY BP-1 (64-22) 1.984 TONS/CY

TACK COAT

MILLING 0.10 GAL/SY

IRREGULARITIES: 75 TONS/MI



PARVIN RD. TO I-29 S

LOG MILE 0.002 TO LOG MILE 0.108

PARVIN RD. TO I-29 N

LOG MILE 0.002 TO LOG MILE 0.108

I-29 S TO PARVIN RD.

LOG MILE 0.065 TO LOG MILE 0.165

I-29 N TO PARVIN RD.

LOG MILE 0.147 TO LOG MILE 0.256

CRAIG SKORSETH CIVIL MODE 2018000273

CRAIG SKORSETH CIVIL MODE 2018000273

CRAIG SKORSETH CIVIL MODE 201800073

DATE PREPARED

8 / 8 / 2024

ROUTE

2.9 MO

DISTRICT SHEET NO.

K.C. 2

CONTRACT ID.

CLAY
JOB NO.
JKU0059

BRIDGE NO.

RTATION DATE DESCRIPTION

CAPITOL

MO 65102

MISSOURI HIGHWAYS AND TRANSPORTATI
COMMISSION

MODOT

1.888.ASK MODOT

1.8

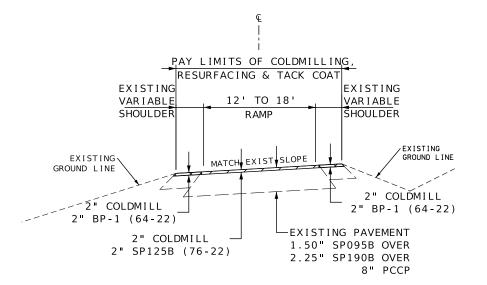
NOT TO SCALE
TYPICAL SECTIONS
SHEET 7 OF 8

NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

MAINLINE TREATMENT TO BE USED ON REMAINING SHOULDERS LESS THAN 3° .



I-29 S TO PARVIN RD.

LOG MILE 0.018 TO LOG MILE 0.065

PARVIN RD. TO I-29 S

LOG MILE 0.108 TO LOG MILE 0.247

I-29 N TO PARVIN RD.

LOG MILE 0.038 TO LOG MILE 0.147

PARVIN RD. TO I-29 N

LOG MILE 0.108 TO LOG MILE 0.143

I-29 S TO MO 210

LOG MILE 0.051 TO LOG MILE 0.095

ASPHALT FACTORS

COMBINED FACTOR

0.10 GAL/SY

SP125B (76-22) 1.946 TONS/CY BP-1 (64-22) 1.984 TONS/CY

TACK COAT

MILLING

IRREGULARITIES: 75 TONS/MI

OF M/SS CRAIG SKORSETH NUMBER PE-2018000273

CRAIG SKORSETH - CIVIL

8/8/2024

ROUTE STATE

29 MO

DISTRICT SHEET NO

COUNTY CLAY

JOB NO.
JKU0059
CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DATE DESCRIPTION

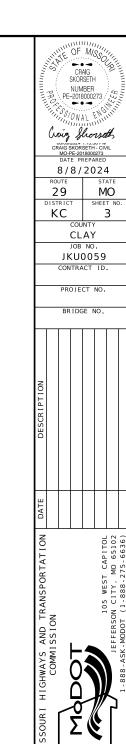
SSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION
MODOT

105 WEST CAPITOL
JEFFERSON CITY, MO 65102

NOT TO SCALE
TYPICAL SECTIONS
SHEET 8 OF 8

						ASPHALT PAVEME						
OG MILE	LOG MILE	DIRECTION	LENGTH	LANE WIDTH	OUTSIDE SHOULDER	INSIDE SHOULDER	COLD MILLING	SP125B	BP - 1	TACK COAT	SHOULDER RUMBLE	REMARKS
					WIDTH	WIDTH	(3" OR LESS)	PG 76-22	PG 64-22		STRIP	
			(FT)	(FT)	(FT)	(FT)	(SY)	(TONS)	(TONS)	(GAL)	(STA.)	
29 SB				•								
124.192	124.211	SB	100	39	6.5	4.5	555.6	46.66	13.42	55.6	2.00	
124.211	124.272	SB	322	51	6.5	4.5	2218.2	196.48	43.21	221.8	6.44	INCLUDES DECEL LANE
124.272	124.424	SB	803	39	10.5	4.5	4818.0	374.69	146.92	481.8	16.06	
124.424	124.486	SB	327	45.5	0.0	4.5	1816.7	178.01	17.95	181.7	6.54	OUTSIDE SHLDR CONCRETE
124.486	124.567	SB	428	37.5	0.0	12.5	2377.8	192.03	65.26	237.8	8.56	OUTSIDE SHLDR CONCRETE
124.567	125.424	SB	4525	39	8.5	12.5	30166.7	2111.40	1159 11	3016.7	90.50	
125.424	125.462	SB	201	45.5	6.5	12.5	1440.5	109.42	46.58	144.1	4.02	
125.462	125.509	SB	248	25.5	0.0	12.5	1047.1	75.66	37.81	104.7	4.96	OUTSIDE LANE CONCRETE
29 NB												
3.220	3.258	NB	201	25.5	0.0	12.5	848.7	61.32	30.65	84.9	4.02	OUTSIDE LANE CONCRETE
3.258	3.446	NB	993	43.5	0.0	8.5	5737.3	516.80	102.96	573.7	19.86	OUTSIDE SHLDR CONCRETE
3.446	4.244	NB	4213	37.5	8.5	10.5	26448.3	1890.21	976.41	2644.8	84.26	
4.244	4.309	NB	343	39	8.5	2.5	1905.6	170.31	35.56	190.6	6.86	INSIDE SHLDR GETS SP125B
4.309	4.343	NB	180	51	8.5	2.5	1240.0	115.22	18.66	124.0	3.60	INSIDE SHLDR GETS SP125B
4.343	4.464	NB	639	39	6.5	2.5	3408.0	317.28	50.66	340.8	12.78	INSIDE SHLDR GETS SP125B
4.464	4.516	NB	275	51	6.5	2.5	1833.3	176.02	21.80	183.3	5.50	INSIDE SHLDR GETS SP125B
4.516	4.539	NB	121	39	6.5	2.5	645.3	60.08	9.59	64.5	2.42	INSIDE SHLDR GETS SP125B
29 S T	O PARVIN	RD.										
0.018	0.065	RAMP	248	18	8.5	2.5	799.1	60.83	25.71	79.9	-	INSIDE SHLDR GETS SP125B
0.065	0.165	RAMP	528	13.5	8.5	0	1290.7	85.28	54.74	129.1	-	OFF RAMP SIDE
ARVIN RI	D. TO I 2	9 S		•								
0.002	0.108	RAMP	560	13.5	8.5	0	1368.9	90.45	58.06	136.9	-	ON RAMP SIDE
0.108	0.247	RAMP	734	18	10.5	8.5	3017.6	158.07	170.11	301.8	-	
29 S T	O MO 210	I.	-								l l	
0.051	0.078	RAMP	143	17.5	4.5	0	349.6	29.94	7.85	35.0	_	INSIDE SHLDR CONCRETE
0.078	0.095	RAMP	90	21	4.5	2.5	280.0	25.30	4.94	28.0	_	INSIDE SHLDR GETS SP125B
	O PARVIN					2.0	200.0	23.30			I I	
0.038	0.147	RAMP	576	18	10.5	6.5	2240.0	124.05	119.44	224.0	-	
0.038	0.256	RAMP	576	13.5	10.5	0.5	1536.0	93.03	73.77	153.6	<u>-</u>	OFF RAMP SIDE
ARVIN RI			370	15.5	10.5	U	1550.0	93.03	/3.//	155.0	-	OTT RAIMF STDL
0.002	0.108	RAMP	560	13.5	10.5	0	1493.3	90.45	71.72	149.3	-	ON RAMP SIDE
0.108	0.108	RAMP	185	18	8.5	2.5	596.1	45.37	19.18	59.6	<u> </u>	INSIDE SHLDR GETS SP125B
RREGULA		KAME	103	10	0.5	2.3	390.1	45.57	19.10	39.0	-	INSIDE SHEDN GETS SF123B
124.192	125.509	SB	6954				-	98.78	_	_	_	IRREGULARITIES 75 TONS/MILE
3.220	4.539	NB	6954				<u>-</u>	98.78	-	-	-	IRREGULARITIES 75 TONS/MILE
0.018	0.165	RAMP	776	+			<u>-</u>	11.02	_	-	-	IRREGULARITIES 75 TONS/MILE
0.018	0.165	RAMP	1294				<u>-</u>	18.38	-	-	-	IRREGULARITIES 75 TONS/MILE
0.002	0.247	RAMP	232	+			<u>-</u>	3.30	-	-	-	IRREGULARITIES 75 TONS/MILE
0.031	0.093	RAMP	1151				-	16.35	-	-	<u>-</u>	IRREGULARITIES 75 TONS/MILE
0.038	0.236	RAMP	744	+			-	10.57	-	-	-	IRREGULARITIES 75 TONS/MILE
0.002	0.143	IVAME	744	1		TOTAL	99478 4	7651.68	3382 07	9948 0	278.38	IMALGOLANTITES /3 TONS/MILE
						USE	99479	7651.7	3382.1	9948	278.4	

MOBILIZATION
1 LUMP SUM



	FULL DEPTH PAVEMENT REPAIR													
LOG MILE	LOG MILE	DIRECTION	LENGTH	LANE WIDTH	NO. OF	CONCRETE	SUBGRADE	TYPE 1 OR 5	FULL DEPTH	DOWEL BARS	REMARKS			
					REPAIRS		COMPACTION	AGG BASE	SAW CUT					
			(FT)	(FT)		(SY)	(SY)	(SY)	(L.F.)	(EACH)				
1 29 SB														
124.192	125.509	SB	6954	12	30	240.00	24.0	24.0	1080.0	600	PLACE AS NEEDED			
I 29 NB														
3.220	4.539	NB	6964	12	30	240.00	24.0	24.0	1080.0	600	OUTSIDE LANE CONCRETE			
					TOTAL	480.00	48.0	48.0	2160.0	1200				
	USE 480.0 48 48 2160 1200													
NOTE : PAVEME	ENT REPAIR	LOCATIONS WILL	. BR VARIFIED E	Y THE ENGINEER										
PAVEMENT RE	EPAIR SHALL	BE COMPLETED	BEFORE TOP LIF	T OF ASPHALT I	S PLACED.						· ·			

					PAVEMENT MA	ARK I NG		
HIGH BUI	LD WATERE	ORNE PAVE	MENT MARKING F	PAINT, TYPE L	BEADS		PREFORMED THERMOPLASTIC	
							PAVEMENT MARKING TAPE	
LOG MILE	LOG MILE	LOCATION	4" YELLOW	6" WHITE	6" YELLOW	12" WHITE	24" WHITE	REMARKS
			(L.F.)	(L.F.)	(L.F.)	(L.F.)	(L.F.)	
I 29 SB	l		(=1.1)	(=1.1)	(=1117	(=1117)	(2111)	
124.192	125.509	I RT I		6953.8				RIGHT EDGELINE
124.192	125 509	LT		0933.0	6953.8			LEFT EDGELINE
124.192	125 509	CL LT & RT		3476.9	0955.0			INTERMITTENT BETWEEN LANES
124.132	124 267	RT RT		50.2				DOTTED
124 267	124 289	RT		30.2		116.2		GORE AREA 12"
124.380	124 446	RT				348.5		GORE AREA 12"
124.446	124 575	RT		170.3		340.3		DOTTED
125.425	125 462	RT		48.8			+	DOTTED
125.423	125 493	RT		70.0		348.5		GORE AREA 12"
125.462	125 493	RT				340.3	94.0	GORE AREA 24" HASH
I 29 NB	123.433	101					34.0	SOME AREA 24 HASH
3.220	1 520	RT I	1	6964.3			1	RIGHT EDGELINE
3.220	4.539	LT		0904.3	6964.3			LEFT EDGELINE
3.220	3.897	CL LT & RT		1787.3	0904.3			INTERMITTENT BETWEEN LANES
3.220	3.245	RT RT		1/0/.3		132.0		GORE AREA 12"
3.245	3.400	RT		204.6		132.0		DOTTED
3.897	4.539	CL RT		847.4				INTERMITTENT BETWEEN LANES
3.897	4.539	CL KI		047.4		847.4		DOTTED
4.337	4.364	RT				142.6		GORE AREA 12"
4.447	4.465	RT				95.0		GORE AREA 12"
	O PARVIN					93.0		GORE AREA 12
			1	776 2	1		Т	DICHT FROM INF
0.018	0.165	RT		776.2	240.2			RIGHT EDGELINE
0.018	0.065	LT CL	1056.0		248.2			DOUBLE SOLID CENTERLINE
0.065			1056.0					DOUBLE SOLID CENTERLINE
	D. TO I 2				1			
0.002	0.247	RT		1293.6				RIGHT EDGELINE
0.108	0.247	LT			733.9			LEFT EDGELINE
	O MO 210						<u>, </u>	
0.051	0.095	RT		232.3				RIGHT EDGELINE
0.051	0.086	LT		184.8				LEFT EDGELINE
0.086	0.095	LT			47.5			LEFT EDGELINE
	O PARVIN						,	
0.038	0.256	RT		1151.0				RIGHT EDGELINE
0.038	0.065	LT		142.6				LEFT EDGELINE
0.065	0.256	LT			1008.5			LEFT EDGELINE
	D. TO I 2	9 N						
0.002	0.143	RT		744.5				RIGHT EDGELINE
0.002	0.108	CL	1119.4					DOUBLE SOLID CENTERLINE
0.108	0.127	LT			100.3			
0.127	0.143	LT		84.5				
		TOTAL	2175.4	25113.1	16056.5	2030.2	94.0	
		USE	2176	25114	16057	2031	94	

													EFFECTIVE: 07-01-2024	Т	шии	Ши
	TOTAL	QTY TOTAL	SIGN						QTY	TOTALSIGN	I I		1 1 201.12	1	1, 1, 1,	MISSO
SIZE AREA	QTY AREA	RELOCRELOC	NUM.			SIZE	AREA	оту Т	TOTAL RELOC	RELOC NUM.				1111	CRA	NG ∴ E
SIGN IN. SQ.FT	EACH SQ.FT.	EACH SQ.FT.			SIGN	IN.	SQ.FT.	EACH S	Q.FT. EACH	SQ.FT.		ITEM TOTAI	_	1	SKORS NUME	
	WARNING S	IGNS		DESCRIPTION				GU I DE	SIGNS		DESCRIPTION	NUMBER QTY	DESCRIPTION		PE-20180	000273 🛴
WO1-1L 48X48 16.00				TURN (SYMBOL LEFT)	E05-1	36X48	12.00	2	24		GORE EXIT	6122008	IMPACT ATTENUATOR 40 MPH (SAND BARRELS)	11	11,58,0NA	
WO1-1R 48X48 16.00				TURN (SYMBOL RIGHT)	E05-2	48X36					EXIT OPEN	6122009	IMPACT ATTENUATOR 45 MPH (SAND BARRELS)	11	Croin St	
WO1-2L 48X48 16.00				CURVE (SYMBOL LEFT)	E05-2a		12.00	2	24		EXIT CLOSED	6122010	IMPACT ATTENUATOR 50 MPH (SAND BARRELS)	ł I	CRAIG SKORS	
WO1-2R 48X48 16.00 WO1-3L 48X48 16.00				CURVE (SYMBOL RIGHT) REVERSE TURN (SYMBOL LEFT)	GO20-1 GO20-2	60X24 48X24		6	48		ROAD WORK NEXT XX MILES END ROAD WORK	6122012	IMPACT ATTENUATOR 55 MPH (SAND BARRELS) IMPACT ATTENUATOR 60 MPH (SAND BARRELS)	ł⊢	MO-PE-201	18000273
WO1-3R 48X48 16.00				REVERSE TURN (SYMBOL RIGHT)	GO20-2 GO20-4			0	40		PILOT CAR FOLLOW ME	6122017	IMPACT ATTENUATOR 65 MPH (SAND BARRELS)	1 I	8/8/2	
WO1-4L 48X48 16.00				REVERSE CURVE (SYMBOL LEFT)	GO20-4a						PILOT CAR IN USE WAIT & FOLLOW	6122019	IMPACT ATTENUATOR 70 MPH (SAND BARRELS)	1 🗀	ROUTE	STATE
WO1-4R 48X48 16.00)			REVERSE CURVE (SYMBOL RIGHT)	GO20-4a	18X12	1.50				PILOT CAR IN USE WAIT & FOLLOW	6122020	REPLACEMENT SAND BARREL	1 ⊢,	29 DISTRICT	MO SHEET NO.
WO1-4bL 48X48 16.00				DOUBLE ARROW REVERSE CURVE (SYMBOL LEFT)	GO20 - 5 al			4	24		WORK ZONE (PLAQUE)	6122030	IMPACT ATTENUATOR (RELOCATION)] [`	KC	3
WO1-4bR 48X48 16.00				DOUBLE ARROW REVERSE CURVE (SYMBOL RIGHT)		24X18					END DETOUR	6123001	TRUCK MOUNTED ATTENUATOR (TMA)	⇃尴	COUN	
WO1-4cL 48X48 16.00 WO1-4cR 48X48 16.00				TRIPLE ARROW REVERSE CURVE (SYMBOL LEFT) TRIPLE ARROW REVERSE CURVE (SYMBOL RIGHT)	MO4 - 9L MO4 - 9R	48X36 48X36					DETOUR (LEFT) DETOUR (RIGHT)	6161008 6 6161012	ADVANCED WARNING RAIL SYSTEM BUOYS (BOATS KEEP OUT)	ł⊩	CL A	
WO1-6 60X30 12.50				HORIZONTAL ARROW (SYMBOL)		48X12					STREET NAME (PLAQUE)	6161013	BUOYS (NO WAKE)	1 I	JKU0	
WO1-6a 72X36 18.00				HORIZ. ARROW (SYMBOL ON PERMANENT BARRICADE)	MO4 - 10L						DETOUR ARROW (LEFT)	6161014	SPECIAL SIGN ASSEMBLY (BOATS KEEP OUT)	1 🗀	CONTRAC	
WO1-7 60X30 12.50)			DOUBLE HEAD HORIZONTAL ARROW (SYMBOL)	MO4 - 10R	48X18	6.00				DETOUR ARROW (RIGHT)	6161025 300	CHANNELIZER (TRIM LINE)	1 📙		
WO1-7a 72X36 18.00				DOUBLE HEAD HORIZ. ARROW (SYMBOL ON PERM. BARR.)				REGŲL	ATORY SIG	NS		6161030 15	TYPE III MOVEABLE BARRICADE		PROJEC	I NO.
WO1-8 18X24 3.00				CHEVRON (SYMBOL)	R1 - 1		13.25		13.06		STOP	6161033 28	DIRECTION INDICATOR BARRICADE	┧┝╴	BRIDGE	ē NO.
WO1-8a 30X36 7.50 WO3-1 48X48 16.00				CHEVRON (SYMBOL FOR DIVIDED HIGHWAYS) STOP AHEAD (SYMBOL)	R1-2 R1-2a	36X36	6.93	2	13.86		TO ONCOMING TRAFFIC (PLAQUE)	6161040 2 6161047	FLASHING ARROW PANEL TYPE III OBJECT MARKER	$+$ \perp		
WO3-2 48X48 16.00				YIELD AHEAD (SYMBOL)	R1-2a R1-3P	30X12		+			ALL WAY (PLAQUE)	6161047	SEQUENTIAL FLASHING WARNING LIGHT	$\{1]$	$ \top$	
WO3-3 48X48 16.00				SIGNAL AHEAD (SYMBOL)	R2 - 1		12.00	8	96		SPEED LIMIT 2@45, 2@55	6161070	TUBULAR MARKER	11		
WO3-4 48X48 16.00				BE PREPARED TO STOP	R3-1		16.00	1	16		NO RIGHT TURN (SYMBOL)	6161095	RADAR SPEED ADVISORY SYSTEM] [
WO3-5 48X48 16.00				SPEED LIMIT AHEAD	R3-2		16.00	1	16		NO LEFT TURN (SYMBOL)		CHANGEABLE MESSAGE SIGN,	$\ \ _{\mathbf{z}}$		
WO4-1L 48X48 16.00				MERGE (SYMBOL FROM LEFT)	R3-3		9.00	2	18		NO TURNS	6161096	COMMISSION FURNISHED/RETAINED	1		
WO4-1R 48X48 16.00 WO4-1aL 48X48 16.00				MERGE (SYMBOL FROM RIGHT)	R3-4	48X48					NO U-TURN (SYMBOL)	161610000	CHANGEABLE MESSAGE SIGN W/O COMM.			
WO4-1aL 48X48 16.00 WO4-1aR 48X48 16.00				MERGE (LEFT) MERGE (RIGHT)	R3 - 7L R3 - 7R	30X30					RIGHT LANE MUST TURN LEFT	6161098A	INTERFACE - CONTRACTOR FURNISHED/RETAINED CHANGEABLE MESSAGE SIGN WITH COMM.			
WO5-1 48X48 16.00				ROAD/BRIDGE/RAMP NARROWS	R4 - 1	36X48					DO NOT PASS	6161099 4	INTERFACE - CONTRACTOR FURNISHED/RETAINED			
WO5-3 48X48 16.00				ONE LANE BRIDGE	R4-2	36X48					PASS WITH CARE	6162000A	WORK ZONE TRAFFIC SIGNAL SYSTEM	11		
WO5-5 48X48 16.00				NARROW LANES	R4-7a	36X48	12.00				KEEP RIGHT (HORIZONTAL ARROW)	6162002	TEMPORARY LONG-TERM RUMBLE STRIPS] [
WO6-1 48X48 16.00				DIVIDED HIGHWAY (SYMBOL)	R4-8a	36X48					KEEP LEFT (HORIZONTAL ARROW)		TEMPORARY TRAFFIC BARRIER			
WO6-2 48X48 16.00 WO6-3 48X48 16.00				DIVIDED HIGHWAY END (SYMBOL)	R5-1 R5-1a	30X30					DO NOT ENTER WRONG WAY	6173600D	CONTRACTOR FURNISHED/RETAINED TEMPORARY TRAFFIC BARRIER	ł⊢		+++
WO7-3a 30X24 5.00				TWO WAY TRAFFIC (SYMBOL) NEXT XX MILES (PLAQUE)	R6-1L	36X24 54X18					ONE WAY ARROW (LEFT)	6173602B	CONTRACTOR FURNISHED/COMMISSION RETAINED			
WO8 -1 48X48 16.00				BUMP	R6 - 1R	54X18					ONE WAY ARROW (RIGHT)	6174000A	TEMP. TRAFFIC BARRIER HEIGHT TRANSITION	à		
WO8-2 48X48 16.00				DIP	R6-2L	24X30					ONE WAY (LEFT)	6175010A	RELOCATING TEMPORARY TRAFFIC BARRIER	_z		
WO8-3 48X48 16.00				PAVEMENT ENDS	R6-2R	24X30	5.00				ONE WAY (RIGHT)		TEMPORARY TRAFFIC BARRIER	1 ⊡	2	1TOL 5102 636)
WO8-4 48X48 16.00				SOFT SHOULDER	R9-9	24X12	2.00				SIDEWALK CLOSED	6176000B	COMMISSION FURNISHED/RETAINED	<u>-</u>	CAPI MO 65 75-66
WO8-5 48X48 16.00 WO8-6 48X48 16.00				SLIPPERY WHEN WET (SYMBOL) TRUCK CROSSING	R9-11L	24X18	3 00				SIDEWALK CLOSED AHEAD, (ARROW LEFT) CROSS HERE		TEMP. TRAFFIC BARRIER HEIGHT TRANSITION COMMISSION FURNISHED/RETAINED	8		T 0
WO8-6 48X48 16.00 WO8-6c 48X48 16.00				TRUCK ENTRANCE	K9-IIL	24/10	3.00				SIDEWALK CLOSED AHEAD,	6177000B 6208064A	TEMPORARY RAISED PAVEMENT MARKER	- I S	<u></u>	WES T≺,
WO8-7 36X36 9.00				LOOSE GRAVEL	R9-11R	24X18	3.00				(ARROW RIGHT) CROSS HERE	9029400	TEMPORARY TRAFFIC SIGNALS		Ž.	105 SON CI
WO8-7a 36X36 9.00				FRESH OIL / LOOSE GRAVEL	R10-6	24X36	6.00				STOP HERE ON RED (45^ ARROW)	9029401	TEMPORARY TRAFFIC SIGNALS AND LIGHTING	1 ≝	ž Z	1 SON
WO8-9 48X48 16.00				LOW SHOULDER	R11-2	48X30	10.00	4	40		ROAD CLOSED] ⊵	SIC	DDO
WO8-11 48X48 16.00				UNEVEN LANES							ROAD CLOSED XX MILES AHEAD			∐₹	L S	■ H
WO8-12 48X48 16.00				NO CENTER LINE	R11-3a						LOCAL TRAFFIC ONLY			∤ 		ASH
WO8-15 48X48 16.00 WO8-15P 30X24 5.00				GROOVED PAVEMENT MOTORCYCLE (PLAQUE)	R11-4 CONST-3						ROAD CLOSED TO THRU TRAFFIC FINE SIGN			┨⋛	§႘ () ‹	88
WO8-17L 48X48 16 00				SHOULDER DROP-OFF (SYMBOL LEFT)	CONST - 3			_			SPEEDING/PASSING (PLATE)			1 ₫	Ž]
WO8-17R 48X48 16.00				SHOULDER DROP-OFF (SYMBOL RIGHT)				<u>и і s</u> ċе	LLANEOUS	SIGNS]	•	, =	┊╶┆	
WO8-17P 30X24 5.00				SHOULDER DROP-OFF (PLAQUE)	CONST - 5						POINT OF PRESENCE]		ا ظ	7	<i>[</i> ([
W10-1 42RND 9.62				RAILROAD CROSSING	CONST - 5			_	64		POINT OF PRESENCE			 JÖ	ξ Δ	E
WO12-1 24X24 4.00				DOUBLE DOWN ARROW (SYMBOL)	CONST-8	48X36	12.00	6	72		WORK ZONE NO PHONE ZONE	-		155		_
W012-2 48X48 16.00 W012-2x 24X18 3.00				LOW CLEARANCE (SYMBOL) LOW CLEARANCE (PLAQUE)								1		Σ		
WO12-2A 84X24 14.00				OVERHEAD LOW CLEARANCE (FEET AND INCHES)				_				1				
WO12-4 120X60 50.00				LOW CLEARANCE XX FT XX IN XX MILES AHEAD]				
WO12-5 120X60 50.00				WIDTH RESTRICTION XX FT XX IN XX MILES AHEAD]				
WO13-1 30X30 6.25				ADVISORY SPEED (PLAQUE)												
WO16-2 30X24 5.00				XXX FEET (PLAQUE)								-				
WO16-3 30X24 5.00 WO20-1 48X48 16.00				X MILE (PLAQUE) ROAD/BRIDGE/RAMP WORK AHEAD	 			+				1				
WO20-1 48X48 16.00				DETOUR AHEAD				-+				†				
WO20-3 48X48 16.00				ROAD CLOSED AHEAD	616-10	0.05		 T	OTAL	' 		1				
WO20-4 48X48 16.00				ONE LANE ROAD AHEAD	CONSTR		N SIGN									
WO20-5 48X48 16.00				RIGHT/CENTER/LEFT LANE CLOSED AHEAD	616-10					TOTAL						
WO20-5a 48X48 16.00				2 RIGHT/CENTER/LEFT LANES CLOSED AHEAD	RELOCA	TED S	IGNS			0						
WO20-6a 48X48 16.00				RIGHT/CENTER/LEFT LANE CLOSED												
WO20-7a 48X48 16.00 WO21-2 36X36 9.00				FLAGGER (SYMBOL) FRESH OIL												
WO21-2 30X30 3.00 WO21-5 48X48 16.00				SHOULDER WORK / SHOULDER WORK AHEAD								C	UMMARY OF QUANTITIES			
WO22-1 48X48 16.00				BLASTING ZONE AHEAD								3				
WO22-2 42X36 10.50				TURN OFF 2-WAY RADIO AND PHONE									SHEET 3 OF 3	- 1		

END BLASTING ZONE WET PAINT (ARROW PIVOTS)

WO22-2 42X36 10.50

WO22-3 42X36 10.50 GO22-1 21X15 2.19 2 4.38

REVISED: 4/10/2024

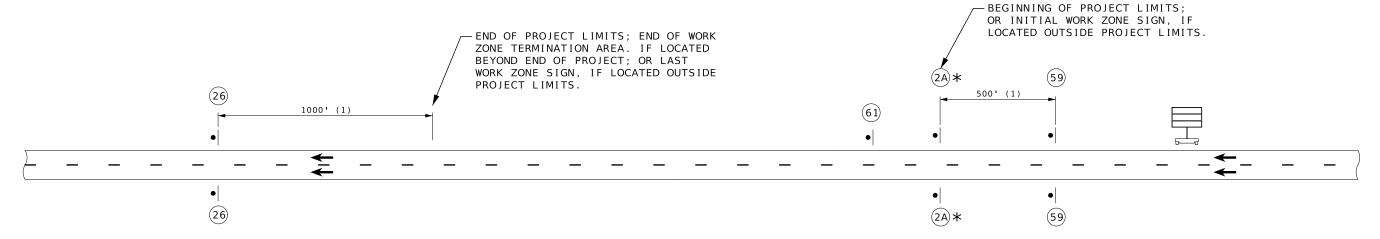
- 1. ANY EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL SIGNING SHALL BE COMPLETELY COVERED OR REMOVED.
- 2. TEMPORARY SIGNING SHOWN IS FOR WORK ON ONE SIDE OF THE ROAD. FOR WORK ON THE OTHER SIDE, REVERSE ORDER OF THE SIGNS AND CHANNELIZERS.
- SIGNING SHOWN SHALL REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE.
- 4. ALTERNATE TRAFFIC CONTROL MAY BE USED AS NEEDED AT THE APPROVAL OF THE ENGINEER.
- REFER TO STANDARD DRAWING 616.10, 619.10, AND 620.10 FOR ADDITIONAL DETAILS AND 903.03 FOR SIGN AND SIGN MOUNTING REQUIREMENTS.
- SPEED LIMIT SIGNS INDICATING THE NORMAL SPEED LIMIT SHALL BE INSTALLED AT THE END OF THE WORK ZONE, PROVIDED NO FURTHER WORK ZONES WILL BE ENCOUNTERED WITHIN THE NEXT 1/2 MILE.
- TEMPORARY SPEED LIMIT SIGNS SHALL BE COVERED OR REMOVED WHEN THE CONDITIONS REQUIRING REDUCED SPEEDS DO NOT EXIST. 8. NO DIRECT PAYMENT WILL BE MADE FOR THE RELOCATION OF CHANNELIZERS, CONSTRUCTION SIGNS, OR FLASHING ARROW PANEL.
- ALL SIGNS SHALL BE PORTABLE MOUNT AND ARE TO BE MOVED AS WORK PROGRESSES, UNLESS OTHERWISE NOTED. ALL TRAFFIC CONTROL ITEMS SHALL BE REMOVED FROM THE ROADWAY DURING NON-WORKING HOURS.
- 10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING PROPER TRAFFIC CONTROL SETUPS THROUGHOUT CONSTRUCTION AS DESCRIBED IN THESE PLANS OR AS APPROVED BY THE ENGINEER.
- 11. WHERE MINIMUM LANE WIDTHS CANNOT BE ACHIEVED ON THE SIDE STREETS, THE CONTRACTOR SHALL CLOSE HALF OF THE APPROACH AND POSITION A FLAGGER AT EACH END OF THE WORK AREA. LOCATION TO BE DETERMINED BY THE ENGINEER. ADVANCE FLAGGER SIGNING SHALL BE INSTALLED AS OUTLINED IN THE MUTCD.
- 12. THE CONTRACTOR SHALL MAINTAIN PUBLIC ACCESS TO ALL BUSINESSES AND STREETS. NO STREET OR ENTRANCE SHALL BE COMPLETELY BLOCKED.
- 13. SPACING & DISTANCES OF TRAFFIC CONTROL DEVICES ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD & APPROVED BY THE ENGINEER.
- 14. TRAFFIC CONTROL SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- 15. ADJUST ADVANCE WARNING SIGN SPACING TO AVOID SIGN PLACEMENT WITHIN THE LIMITS OF THE RAILROAD RIGHT OF WAY.

WORK ZONE

NO PHONE

ZONE

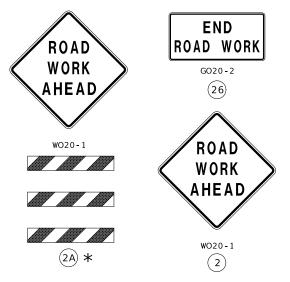
CONST-8 (59) (2)



- (1) DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS. WHERE TRAFFIC BACKUPS ARE EXPECTED BEYOND THE ADVANCE WARNING AREA, ADDITIONAL SIGNING MAY BE NEEDED.
- (2) THE "WORK ZONE NO PHONE ZONE" SIGN IS PLACED A MINIMUM OF 500 FEET BEFORE THE ROAD WORK AHEAD SIGN.
- (3) CMS PLACEMENT SHALL BE APPROVED BY THE ENGINEER.

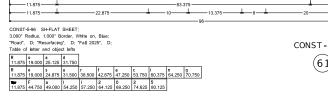
(3)

TYPICAL BEGINNING AND END OF PROJECT (DIVIDED HIGHWAY)



★SIGN 2A WITH AWRS MUST BE GROUND OR SKID MOUNTED.

Road Resurfacing



Completed as Promised -5P-60 SHF-FLAT SHEET FLUORESCENT

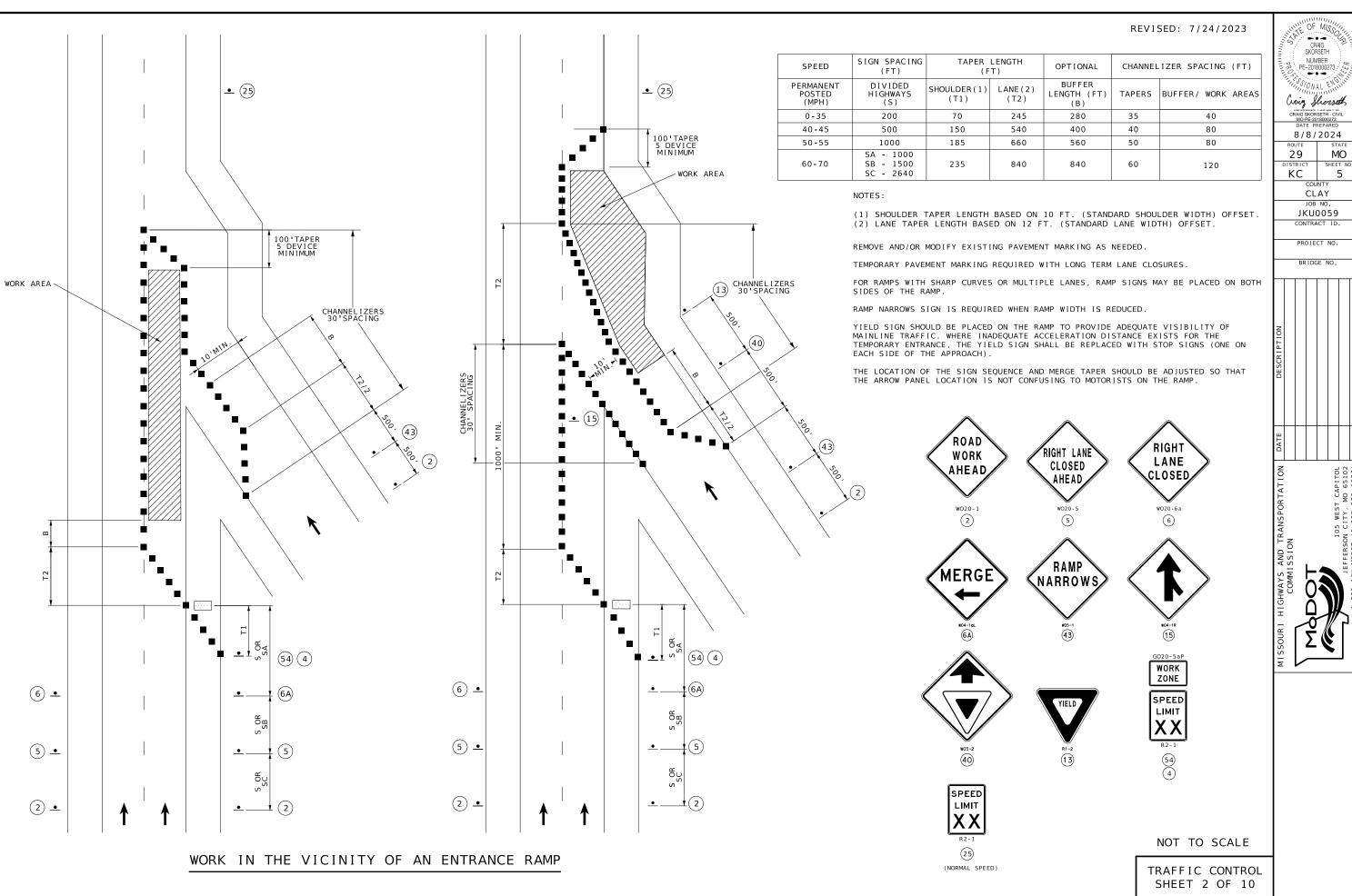
NOT TO SCALE

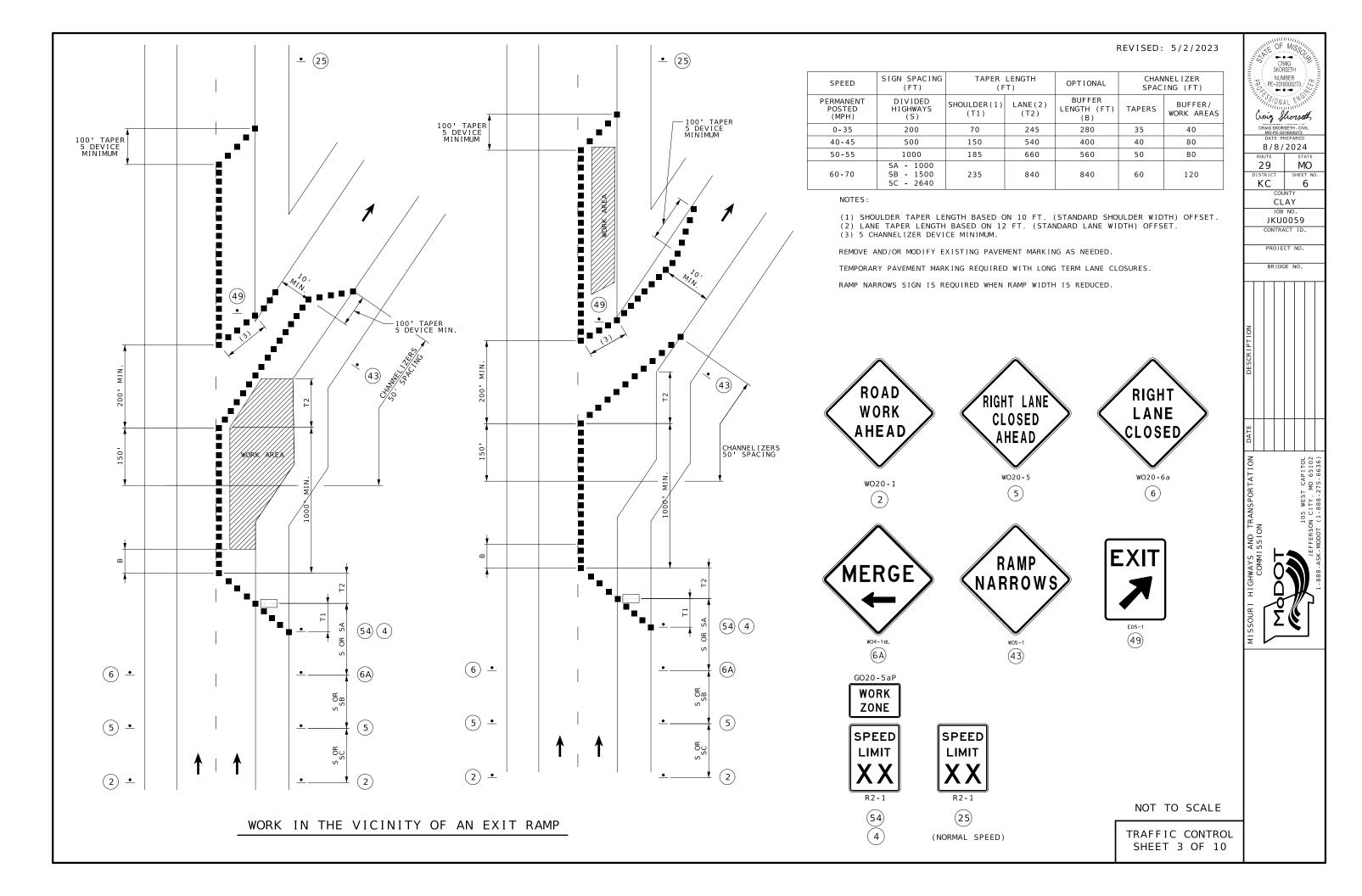
CONST - 5 - 96

(61)(2)

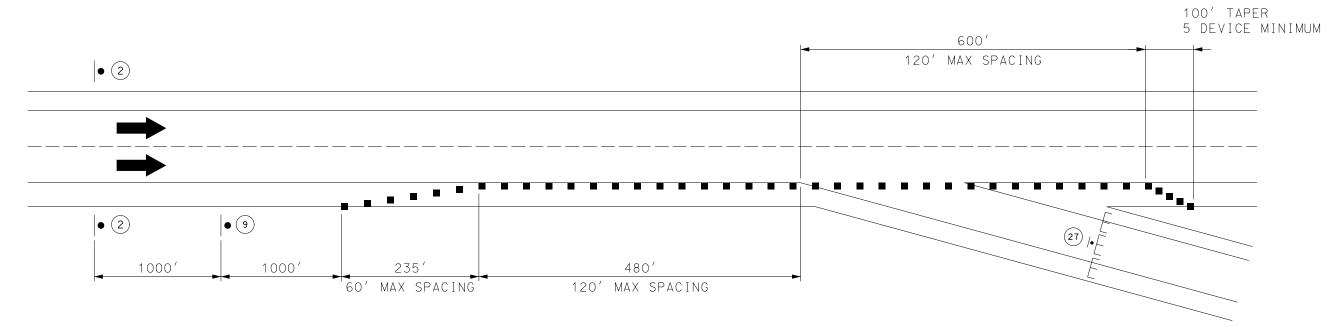
TRAFFIC CONTROL SHEET 1 OF 10







OFF RAMP CLOSURE



NOTES:

NO DIRECT PAY WILL BE MADE FOR RELOCATING, COVERING, UNCOVERING, OR REMOVING SIGNS.

ANY EXISTING SIGNING THAT CONFLICTS WITH THE TRAFFIC CONTROL SIGNING SHALL BE COMPLETELY COVERED OR REMOVED.

SPACING OF SIGNS SHOWN ON THE PLANS ARE MINIMUM AND MAY BE ADJUSTED BY THE ENGINEER TO MEET FIELD CONDITIONS.

ROAD WORK AHEAD BRIDGE OR RAMP





TRAFFIC CONTROL LEGEND

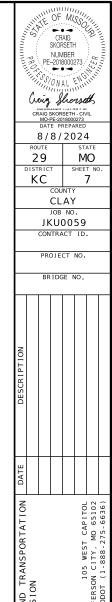
- SIGN (SINGLE SIDED)
- CHANNELIZER

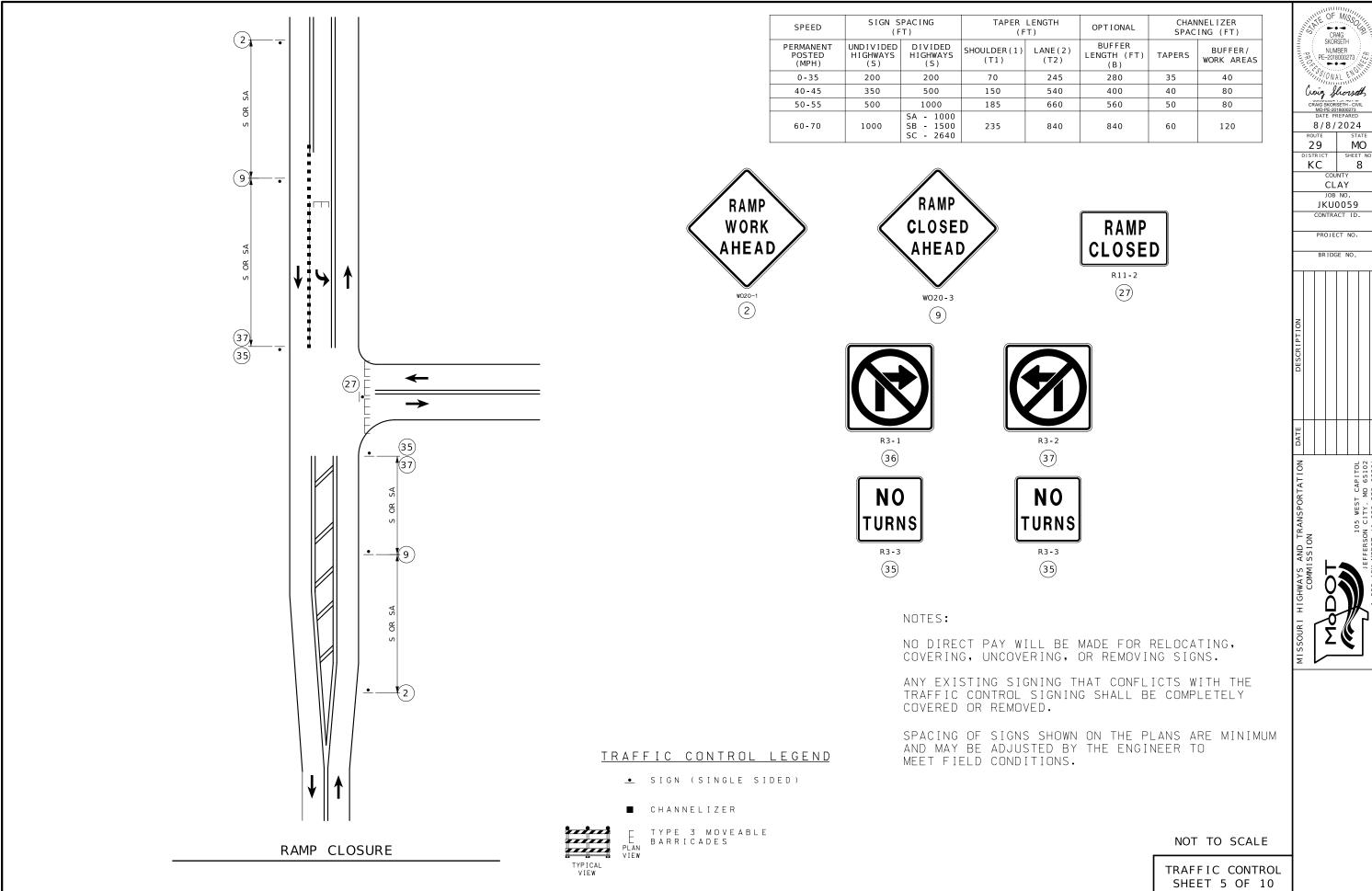


TYPE 3 MOVEABLE
BARRICADES
PLAN
VIEW

NOT TO SCALE

TRAFFIC CONTROL SHEET 4 OF 10





SPEED	SIGN SPACING (FT)		LENGTH T)	OPTIONAL		INELIZER ING (FT)
PERMANENT POSTED (MPH)	DIVIDED HIGHWAYS (S)	SHOULDER(1) (T1)	LANE(2) (T2)	BUFFER LENGTH (FT) (B)	TAPERS	BUFFER/ WORK AREAS
0-35	200	70	245	280	35	40
40 - 45	500	150	540	400	40	80
50-55	1000	185	660	560	50	80
60-70	SA - 1000 SB - 1500 SC - 2640	235	840	840	60	120

- (1) SHOULDER TAPER LENGTH BASED ON 10 FT. (STANDARD SHOULDER WIDTH) OFFSET.
- (2) LANE TAPER LENGTH BASED ON 12 FT. (STANDARD LANE WIDTH) OFFSET.

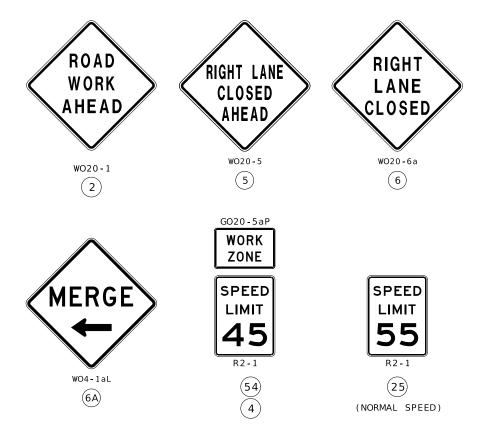
TEMPORARY PAVEMENT MARKING REQUIRED WITH LONG TERM CLOSURES.

REMOVE AND/OR MODIFY ANY EXISTING PAVEMENT MARKING AS NEEDED.

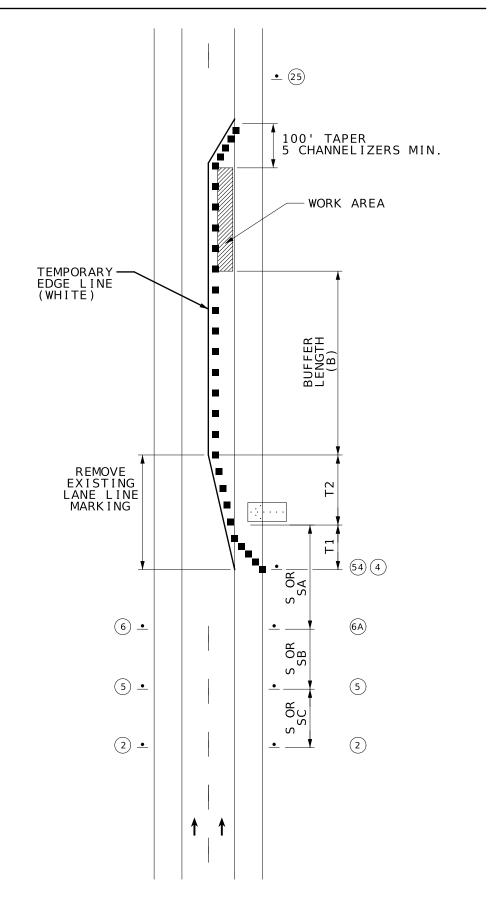
THIS INFORMATION ALSO SHALL BE USED WHEN WORK IS BEING PERFORMED IN THE LANE ADJACENT TO THE MEDIAN ON A DIVIDED HIGHWAY. IN THIS CASE, THE LEFT LANE CLOSED SIGNS AND THE CORRESPONDING MERGE OR LANE ENDS SIGNS SHALL BE SUBSTITUTED.

WHEN A SIDE ROAD INTERSECTS THE HIGHWAY WITHIN THE TTC ZONE, ADDITIONAL TTC DEVICES SHALL BE PLACED AS NEEDED.

AN ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED.WHEN MORE THAN ONE FREEWAY LANE IS CLOSED, A SEPARATE ARROW BOARD SHALL BE USED FOR EACH CLOSED LANE.



STATIONARY LANE CLOSURE ON A DIVIDED HIGHWAY



CRAIG SKORSETH NUMBER PE-2018000273

REVISED: 8/3/2023

CRAIG SKORSETH - CIVIL

DATE PREPARED

8/8/2024

ROUTE STATE

ROUTE STATE
29 MO
DISTRICT SHEET N
KC 9

COUNTY
CLAY

JKU0059 CONTRACT ID.

PROJECT NO.

DESCRIPTION DESCRIPTION

MISSOURI HIGHW

NOT TO SCALE

TRAFFIC CONTROL SHEET 6 OF 10

SPEED	SIGN SPACING (FT)		LENGTH T)	OPTIONAL	LONG I - TUD I NAL	CHANNELIZER SPACING (FT)			
NORMAL POSTED (MPH)	DIVIDED (S)	SHOULDER(1)	LANE(2) (T2)	BUFFER LENGTH (FT) (B)	TRANSITION (X)	TAPERS	BUFFER/ WORK AREAS		
0-35	200	70	245	280	490	35	40		
40-45	500	150	540	400	1080	40	80		
50-55	1000	185	660	560	1320	50	80		
60-70	SA - 1000 SB - 1500 SC - 2640	235	840	840	1680	60	120		

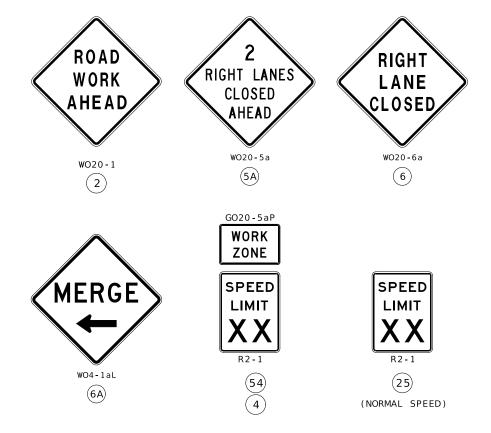
- (1) SHOULDER TAPER LENGTH BASED ON 10 FT. (STANDARD SHOULDER WIDTH) OFFSET.
- (2) LANE TAPER LENGTH BASED ON 12 FT. (STANDARD LANE WIDTH) OFFSET.

REMOVE AND/OR MODIFY ANY EXISTING PAVEMENT MARKING AS NEEDED.

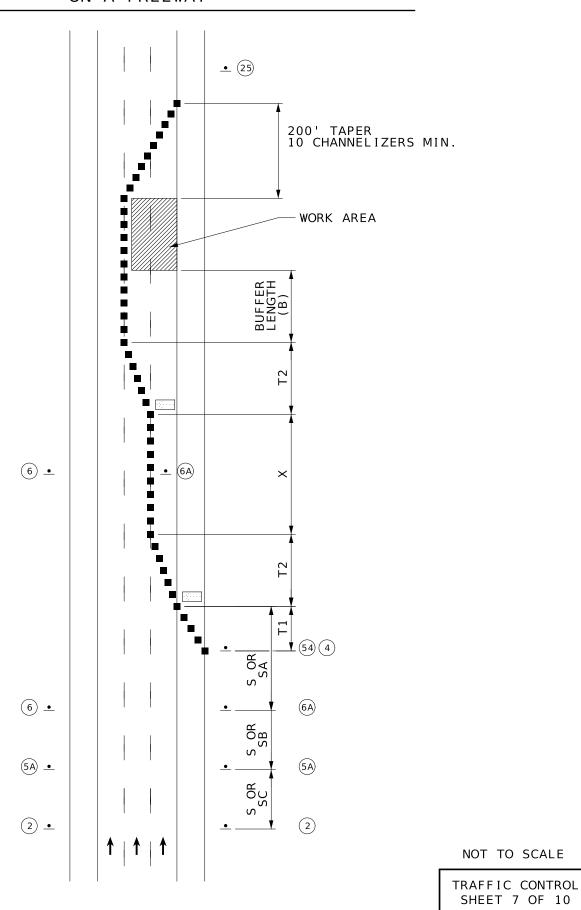
TEMPORARY PAVEMENT MARKING REQUIRED WITH LONG TERM LANE CLOSURES.

ALTERNATIVE TEMPORARY TRAFFIC CONTROL SET-UPS SHOULD BE CONSIDERED WHEN REDUCTION IN CAPACITY CANNOT BE TOLERATED.

A FLASHING ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED. WHEN MORE THAN ONE FREEWAY LANE IS CLOSED, A SEPARATE FLASHING ARROW BOARD SHALL BE USED FOR EACH CLOSED LANE.



DOUBLE LANE CLOSURE ON A FREEWAY



REVISED: 8/3/2023

CRAIG SKORSETH

NUMBER PE-2018000273

Croin Shorseth

8/8/2024

CLAY

JKU0059

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

MO

10

29

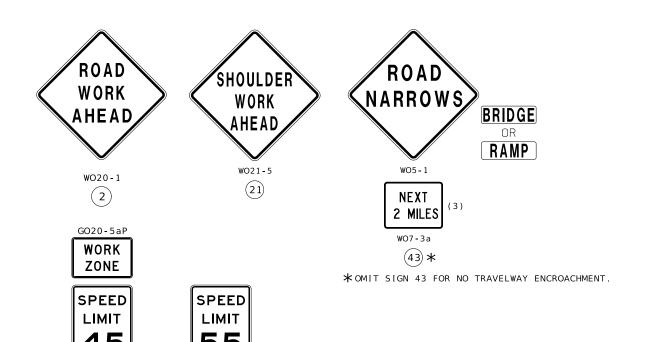
KC

SPEED	SIGN SPACING (FT)		LENGTH T)	OPTIONAL		INELIZER ING (FT)
PERMANENT POSTED (MPH)	DIVIDED HIGHWAYS (S)	SHOULDER(1)	LANE(2) (T2)	BUFFER LENGTH (FT) (B)	TAPERS	BUFFER/ WORK AREAS
0-35	200	70	245	280	35	40
40-45	500	150	540	400	40	80
50-55	1000	185	660	560	50	80
60-70	SA - 1000 SB - 1500 SC - 2640	235	840	840	60	120

- (1) SHOULDER TAPER LENGTH BASED ON 10 FT. (STANDARD SHOULDER WIDTH) OFFSET.
- (2) LANE TAPER LENGTH BASED ON 12 FT. (STANDARD LANE WIDTH) OFFSET.

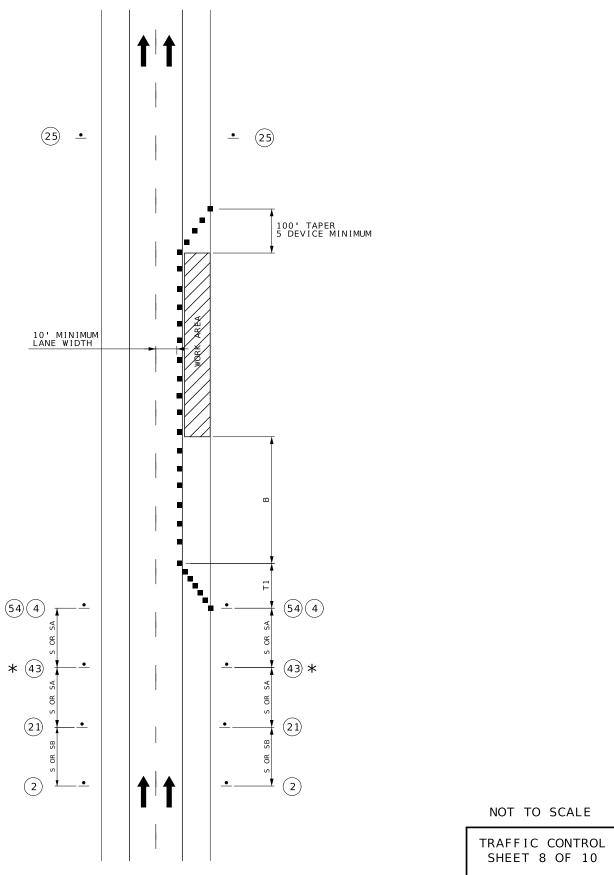
R2-1
(25)
(NORMAL SPEED)

(3) NEXT XX MILES SIGN NOT REQUIRED FOR NARROW LANE SECTIONS LESS THAN ONE MILE.



SHOULDER WORK WITH MINOR TRAVELWAY ENCROACHMENT OR NO ENCROACHMENT

(DIVIDED HIGHWAY)



OF MISSON

REVISED: 1/9/2024

Craig Shorseth

B / 8 / 2024

29 MO
DISTRICT SHEET NO
KC 11

COUNTY
CLAY

JKU0059

PROJECT NO.

DESCRIPTION

BRIDGE NO.

MISSOURI HIGHWAYS AND TRANSPORTAT COMMISSION

MADOT

105 WEST CAPI

JEFFERSON CITY, MO 65



VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR

(1) WET PAINT SIGNS ARE INSTALLED TO INDICATE THE SIDE IN WHICH THE PAVEMENT MARKING MATERIAL IS BEING APPLIED. AT THE CONTRACTOR'S OPTION, A FRONT FACING WET PAINT SIGN MAY BE INSTALLED ON THE LEFT

FLASHING ARROW PANELS SHALL, AS A MINIMUM, BE TYPE B, WITH A SIZE OF 60 X 30 INCHES.

ARE NOT OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS ON VEHICLE-MOUNTED SIGNS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK

A FLASHING ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED. WHEN MORE THAN ONE LANE CLOSED, A SEPARATE FLASHING ARROW PANEL BOARD SHALL BE USED FOR EACH CLOSED LANE.

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY PROVIDE ADDITIONAL PROTECTIVE TRUCKS EQUIPPED WITH PROPER WARNING DEVICES.

PROTECTIVE TRUCK AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE HAZARD WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE

FLASHING ARROW PANELS AND SIGNS SHALL BE INCIDENTAL TO TRUCK MOUNTED ATTENUATORS, WHEREVER USED. NO ADDITIONAL PAYMENT WILL BE MADE.

- SIDE OF THE PAVEMENT MARKING EQUIPMENT.
- (2) WARNING TRUCK IS POSITIONED AT THE NO TRACK POINT OF THE PAVEMENT MARKING MATERIAL, OR SPACING SHOWN, WHICH EVER IS GREATER.

VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED IN A MANNER SUCH THAT THEY IS NOT IN PROGRESS.

CRAIG SKORSETH NUMBER ISSIONAL ENG Croig Shorseth

8/8/2024

29 MO KC 12

CLAY

JKU0059 CONTRACT ID.

PROJECT NO.

BRIDGE NO.

SPACING PERMANENT LENGTH POSTED (MPH) (FT) 245 ≤ 35 540 40-45 50-55 660 60-70 840 SHOULDER ADVANCE WARNING TRUCK WITH WORK SIGNS, FLASHING ARROW PANEL AND REQUIRED TRUCK MOUNTED ATTENUATOR.

PAVEMENT

MARKING

EQUIPMENT

WARNING TRUCK WITH WORK SIGNS,

WARNING TRUCK WITH WORK SIGNS, FLASHING ARROW PANEL AND REQUIRED TRUCK MOUNTED ATTENUATOR. (2)

TRUCK MOUNTED ATTENUATOR.

Σ

MIN. IF CLOSED

MILE I

FLASHING ARROW PANEL AND REQUIRED

STRIPING ON MULTI-LANE HIGHWAY

FLASHING ARROW PANEL

LANE

CLOSED

W020-6a

G022-1(1)

FLASHING ARROW PANEL

RIGHT LANE` CLOSED AHEAD

W020-5

LEFT

WET PAINT

WET PAINT

LEFT

NOT TO SCALE

TRAFFIC CONTROL SHEET 9 OF 10



UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY PROVIDE ADDITIONAL PROTECTIVE TRUCKS EQUIPPED WITH PROPER WARNING DEVICES.

PROTECTIVE TRUCKS AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE HAZARD WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED IN A MANNER SUCH THAT THEY ARE NOT OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS ON VEHICLE-MOUNTED SIGNS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.

FLASHING ARROW PANELS AND SIGNS SHALL BE INCIDENTAL TO TRUCK MOUNTED ATTENUATORS, WHEREVER USED. NO ADDITIONAL PAYMENT WILL BE MADE.

- (1) TRUCK IS OPTIONAL ON TWO-LANE UNDIVIDED HIGHWAYS IF SIGNING AND ARROW BOARD ARE MOUNTED ON THE PAVEMENT MARKING EQUIPMENT.
- (2) WET PAINT SIGNS ARE INSTALLED TO INDICATE THE SIDE IN WHICH THE PAVEMENT MARKING MATERIAL IS BEING APPLIED. AT THE CONTRACTOR'S OPTION, A FRONT FACING WET PAINT SIGN MAY BE INSTALLED ON THE LEFT SIDE OF THE PAVEMENT MARKING EQUIPMENT.
- (3) REAR WARNING TRUCK IS POSITIONED AT THE NO TRACK POINT OF THE PAVEMENT MARKING MATERIAL, OR VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE, OR SPACING SHOWN, WHICHEVER IS GREATER.

CAAG SKORSETH NUMBER PE-2016000273

CRAIG SKORSETH - CIVIL MO-PE-2018000273

DATE PREPARED

8 / 8 / 2024

ROUTE STATE

29 MO

COUNTY CLAY

JOB NO.

JKU0059

CONTRACT ID.

PROJECT NO.

DESCRIPTION

COMMISSION
COMMISSION
COMMISSION
TO DOT
TO SEE CAPITO
THEFERSON CITY MO 6110

NOT TO SCALE

TRAFFIC CONTROL SHEET 10 OF 10

