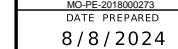


FOR INFORMATION ONLY	
ESTIMATED DISTURBED ACRES	0 ACRES

NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL
COLD MILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE
TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO
ENTIRE PROJECT LIMITS.

NO S.E. CORRECTION

ASPHALT FACTORS	
<u>COMBINED FACTOR</u>	
SP125B (76-22)	1.946 TONS/CY
BP-1 (64-22)	1.984 TONS/CY
TACK COAT	
MILLING	0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI	



COUNTY
CLAY

JOB NO.
JKU0059

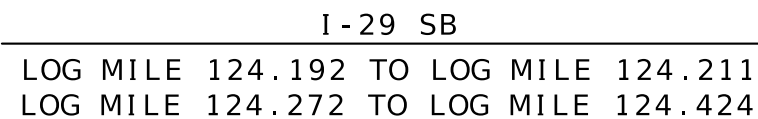
CONTRACT ID.

PROJECT NO.

BRIDGE NO.

[illegible]MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



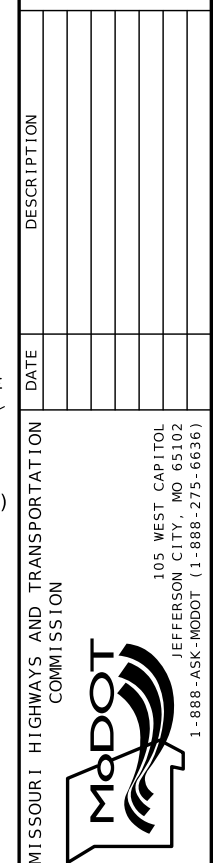
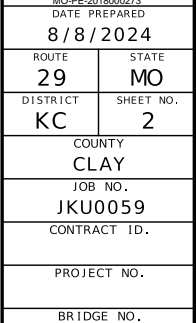
I - 29 NB					
LOG MILE	4.244	TO	LOG MILE	4.309	
LOG MILE	4.343	TO	LOG MILE	4.464	
LOG MILE	4.516	TO	LOG MILE	4.539	

NOT TO SCALE
TYPICAL SECTIONS
SHEET 1 OF 8

NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

NO S.E. CORRECTION

ASPHALT FACTORS	
<u>COMBINED FACTOR</u>	
SP125B (76-22)	1.946 TONS/CY
BP-1 (64-22)	1.984 TONS/CY
TACK COAT	
MILLING	0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI	



NOT TO SCALE

TYPICAL SECTIONS

SHEET 2 OF 8

NOTES: LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL
COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE
TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO
ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

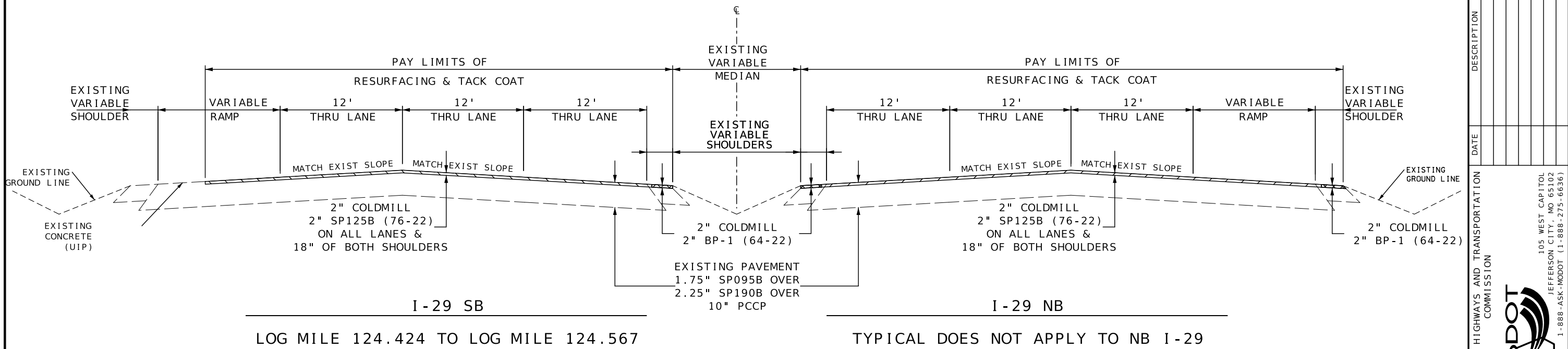
NO S.E. CORRECTION

MAINLINE TREATMENT TO BE USED ON REMAINING SHOULDERS
LESS THAN 3'.

ASPHALT FACTORS	
COMBINED FACTOR	
SP125B (76-22)	1.946 TONS/CY
BP-1 (64-22)	1.984 TONS/CY
TACK COAT	
MILLING	0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI	



DATE PREPARED 9/24/2024	
ROUTE 29	STATE MO
DISTRICT KC	SHEET NO. 2
COUNTY CLAY	
JOB NO. JKU0059	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	



NOT TO SCALE
TYPICAL SECTIONS
SHEET 3 OF 8

NOTES: LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

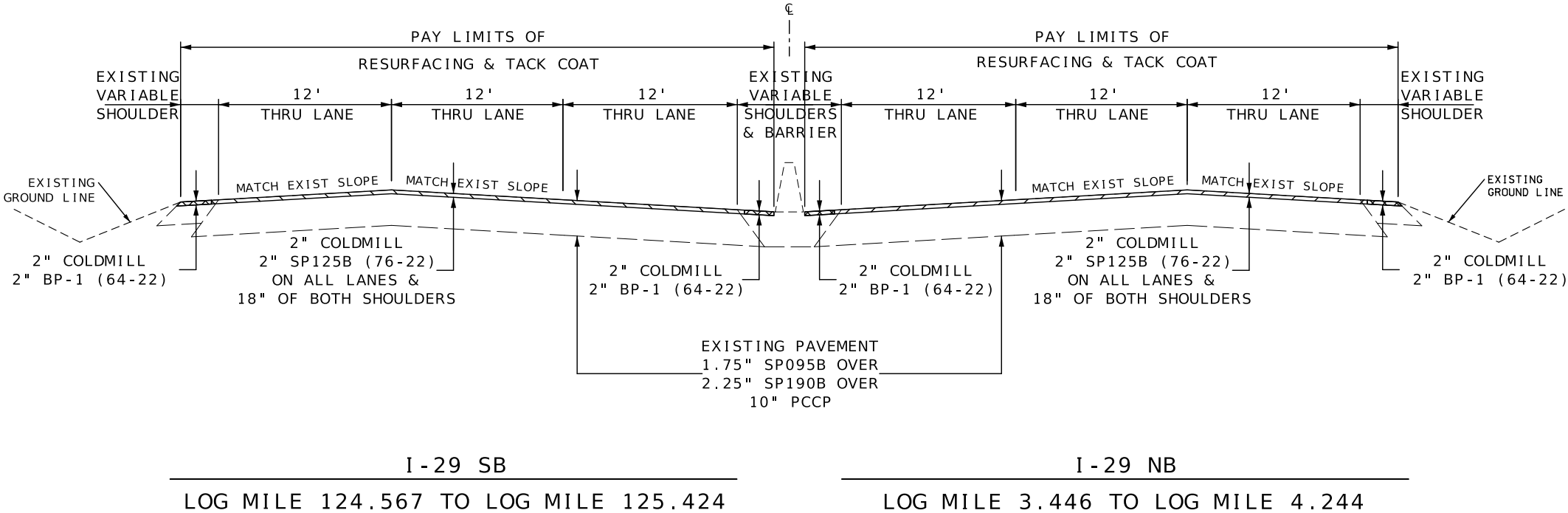
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SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

MAINLINE TREATMENT TO BE USED ON REMAINING SHOULDERS
LESS THAN 3'.

ASPHALT FACTORS	
COMBINED FACTOR	
SP125B (76-22)	1.946 TONS/CY
BP-1 (64-22)	1.984 TONS/CY
TACK COAT	
MILLING	0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI	



NOT TO SCALE
TYPICAL SECTIONS
SHEET 4 OF 8

STATE OF MISSOURI

CRAIG SKORSETH

NUMBER

PE-2018000273

PROFESSIONAL ENGINEER

Craig Skorseth

CRAIG SKORSETH - CIVIL

MO-PE-2018000273

DATE PREPARED

8/8/2024

ROUTE

29

STATE

MO

DISTRICT

KC

SHEET NO.

2

COUNTY

CLAY

JOB NO.

JKU0059

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

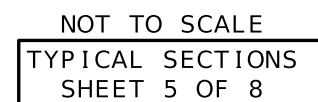
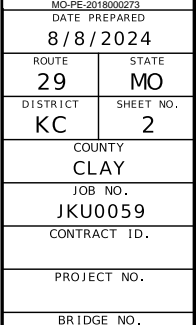
1-888-ASK-MODOT (1-888-275-6636)

MoDOT


NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL
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TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO
ENTIRE PROJECT LIMITS.

NO S.E. CORRECTION

ASPHALT FACTORS	
<u>COMBINED FACTOR</u>	
SP125B (76-22)	1.946 TONS/CY
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TACK COAT	
MILLING	0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI	

[illegible]

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

The logo for the Missouri Department of Transportation (MoDOT) features the word "MoDOT" in a stylized, bold, sans-serif font. The letters are white and set against a dark background. Below the text, there is a graphic element consisting of several curved, parallel lines that sweep upwards and to the right, resembling a stylized road or a bridge. The entire logo is enclosed within a white rectangular border.

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

NOTES: LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

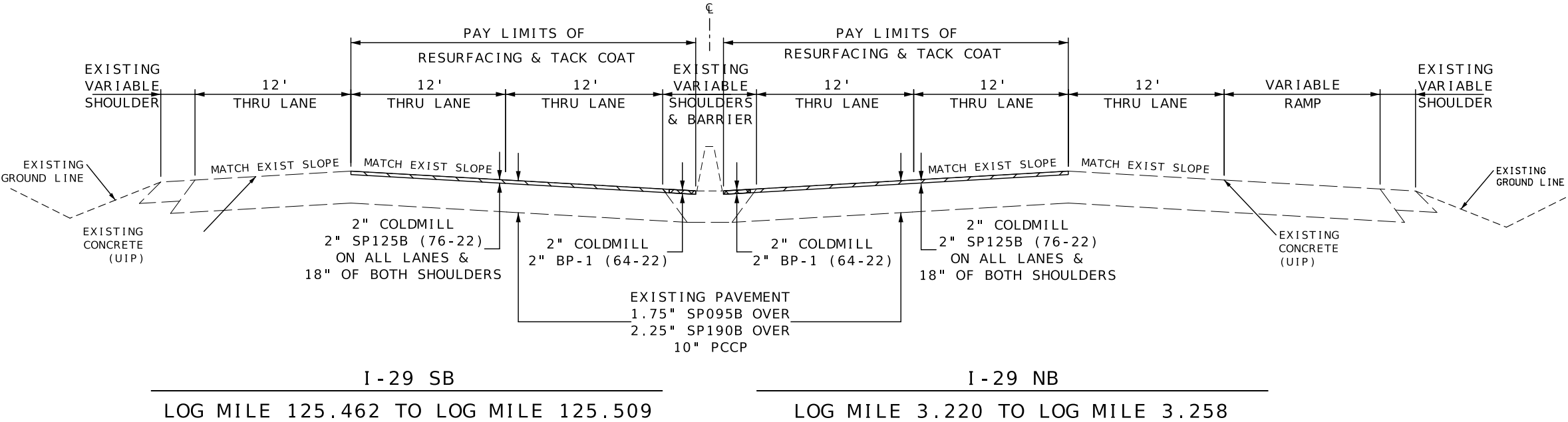
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SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

MAINLINE TREATMENT TO BE USED ON REMAINING SHOULDERS
LESS THAN 3'.

ASPHALT FACTORS	
COMBINED FACTOR	
SP125B (76-22)	1.946 TONS/CY
BP-1 (64-22)	1.984 TONS/CY
TACK COAT	
MILLING	0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI	



NOT TO SCALE
TYPICAL SECTIONS
SHEET 6 OF 8

STATE OF MISSOURI

CRAIG SKORSETH

NUMBER

PE-2018000273

PROFESSIONAL ENGINEER

Craig Skorseth

CRAIG SKORSETH - CIVIL

MO-PE-2018000273

DATE PREPARED

8/8/2024

ROUTE

29

DISTRICT

KC

STATE

MO

SHEET NO.

2

COUNTY

CLAY

JOB NO.

JKU0059

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

MoDOT

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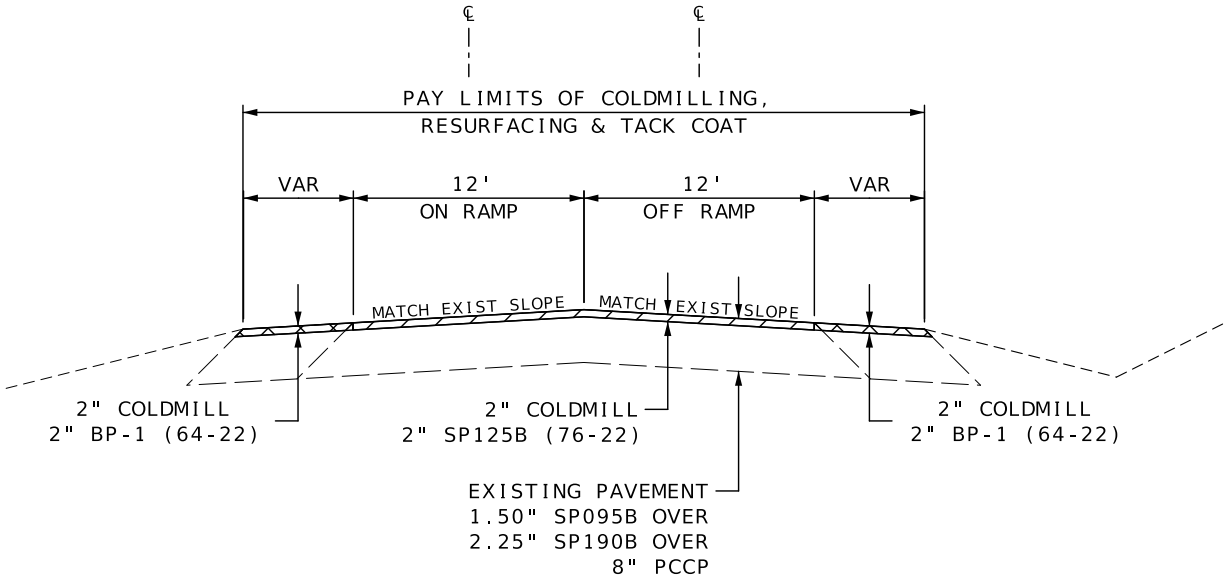
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NO S.E. CORRECTION

MAINLINE TREATMENT TO BE USED ON REMAINING SHOULDERS
LESS THAN 3'.

ASPHALT FACTORS	
COMBINED FACTOR	
SP125B (76-22)	1.946 TONS/CY
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TACK COAT	
MILLING	0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI	



PARVIN RD. TO I-29 S

LOG MILE 0.002 TO LOG MILE 0.108

PARVIN RD. TO I-29 N

LOG MILE 0.002 TO LOG MILE 0.108

I-29 S TO PARVIN RD.

LOG MILE 0.065 TO LOG MILE 0.165

I-29 N TO PARVIN RD.

LOG MILE 0.147 TO LOG MILE 0.256

NOT TO SCALE
TYPICAL SECTIONS
SHEET 7 OF 8



DATE PREPARED 8/8/2024	
ROUTE 29	STATE MO
DISTRICT KC	SHEET NO. 2
COUNTY CLAY	
JOB NO. JKU0059	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

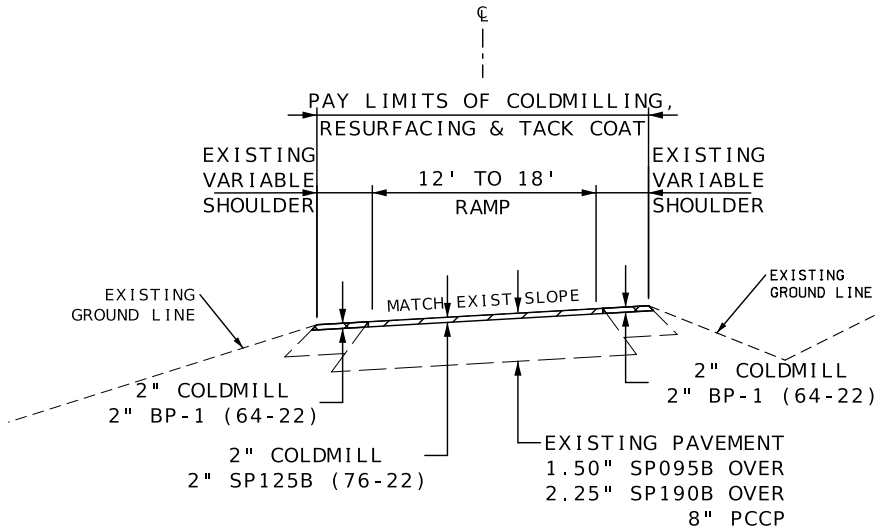
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NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

MAINLINE TREATMENT TO BE USED ON REMAINING SHOULDERS LESS THAN 3'.



I-29 S TO PARVIN RD.
LOG MILE 0.018 TO LOG MILE 0.065
PARVIN RD. TO I-29 S
LOG MILE 0.108 TO LOG MILE 0.247
I-29 N TO PARVIN RD.
LOG MILE 0.038 TO LOG MILE 0.147
PARVIN RD. TO I-29 N
LOG MILE 0.108 TO LOG MILE 0.143
I-29 S TO MO 210
LOG MILE 0.051 TO LOG MILE 0.095

ASPHALT FACTORS	
COMBINED FACTOR	
SP125B (76-22)	1.946 TONS/CY
BP-1 (64-22)	1.984 TONS/CY
TACK COAT	
MILLING	0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI	

STATE OF MISSOURI

CRAIG SKORSETH

NUMBER

PE-2018000273

PROFESSIONAL ENGINEER

MAINTENANCE ENGINEER
CRAIG SKORSETH - CIVIL
MO-PE-2018000273

DATE PREPARED

8/8/2024

ROUTE

29

STATE

MO

DISTRICT

KC

SHEET NO.

2

COUNTY

CLAY

JOB NO.

JKU0059

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

ASPHALT PAVEMENT QUANTITIES												
LOG MILE	LOG MILE	DIRECTION	LENGTH (FT)	LANE WIDTH (FT)	OUTSIDE SHOULDER WIDTH (FT)	INSIDE SHOULDER WIDTH (FT)	COLD MILLING (3" OR LESS) (SY)	SP125B PG 76-22 (TONS)	BP-1 PG 64-22 (TONS)	TACK COAT (GAL)	SHOULDER RUMBLE STRIP (STA.)	REMARKS
I 29 SB												
124.192	124.211	SB	100	39	6.5	4.5	555.6	46.66	13.42	55.6	2.00	
124.211	124.272	SB	322	51	6.5	4.5	2218.2	196.48	43.21	221.8	6.44	INCLUDES DECEL LANE
124.272	124.424	SB	803	39	10.5	4.5	4818.0	374.69	146.92	481.8	16.06	
124.424	124.486	SB	327	45.5	0.0	4.5	1816.7	178.01	17.95	181.7	6.54	OUTSIDE SHLDR CONCRETE
124.486	124.567	SB	428	37.5	0.0	12.5	2377.8	192.03	65.26	237.8	8.56	OUTSIDE SHLDR CONCRETE
124.567	125.424	SB	4525	39	8.5	12.5	30166.7	2111.40	1159.11	3016.7	90.50	
125.424	125.462	SB	201	45.5	6.5	12.5	1440.5	109.42	46.58	144.1	4.02	
125.462	125.509	SB	248	25.5	0.0	12.5	1047.1	75.66	37.81	104.7	4.96	OUTSIDE LANE CONCRETE
I 29 NB												
3.220	3.258	NB	201	25.5	0.0	12.5	848.7	61.32	30.65	84.9	4.02	OUTSIDE LANE CONCRETE
3.258	3.446	NB	993	43.5	0.0	8.5	5737.3	516.80	102.96	573.7	19.86	OUTSIDE SHLDR CONCRETE
3.446	4.244	NB	4213	37.5	8.5	10.5	26448.3	1890.21	976.41	2644.8	84.26	
4.244	4.309	NB	343	39	8.5	2.5	1905.6	170.31	35.56	190.6	6.86	INSIDE SHLDR GETS SP125B
4.309	4.343	NB	180	51	8.5	2.5	1240.0	115.22	18.66	124.0	3.60	INSIDE SHLDR GETS SP125B
4.343	4.464	NB	639	39	6.5	2.5	3408.0	317.28	50.66	340.8	12.78	INSIDE SHLDR GETS SP125B
4.464	4.516	NB	275	51	6.5	2.5	1833.3	176.02	21.80	183.3	5.50	INSIDE SHLDR GETS SP125B
4.516	4.539	NB	121	39	6.5	2.5	645.3	60.08	9.59	64.5	2.42	INSIDE SHLDR GETS SP125B
I 29 S TO PARVIN RD.												
0.018	0.065	RAMP	248	18	8.5	2.5	799.1	60.83	25.71	79.9	-	INSIDE SHLDR GETS SP125B
0.065	0.165	RAMP	528	13.5	8.5	0	1290.7	85.28	54.74	129.1	-	OFF RAMP SIDE
PARVIN RD. TO I 29 S												
0.002	0.108	RAMP	560	13.5	8.5	0	1368.9	90.45	58.06	136.9	-	ON RAMP SIDE
0.108	0.247	RAMP	734	18	10.5	8.5	3017.6	158.07	170.11	301.8	-	
I 29 S TO MO 210												
0.051	0.078	RAMP	143	17.5	4.5	0	349.6	29.94	7.85	35.0	-	INSIDE SHLDR CONCRETE
0.078	0.095	RAMP	90	21	4.5	2.5	280.0	25.30	4.94	28.0	-	INSIDE SHLDR GETS SP125B
I 29 N TO PARVIN RD.												
0.038	0.147	RAMP	576	18	10.5	6.5	2240.0	124.05	119.44	224.0	-	
0.147	0.256	RAMP	576	13.5	10.5	0	1536.0	93.03	73.77	153.6	-	OFF RAMP SIDE
PARVIN RD. TO I 29 N												
0.002	0.108	RAMP	560	13.5	10.5	0	1493.3	90.45	71.72	149.3	-	ON RAMP SIDE
0.108	0.143	RAMP	185	18	8.5	2.5	596.1	45.37	19.18	59.6	-	INSIDE SHLDR GETS SP125B
IRREGULARITIES												
124.192	125.509	SB	6954				-	98.78	-	-	-	IRREGULARITIES 75 TONS/MILE
3.220	4.539	NB	6964				-	98.92	-	-	-	IRREGULARITIES 75 TONS/MILE
0.018	0.165	RAMP	776				-	11.02	-	-	-	IRREGULARITIES 75 TONS/MILE
0.002	0.247	RAMP	1294				-	18.38	-	-	-	IRREGULARITIES 75 TONS/MILE
0.051	0.095	RAMP	232				-	3.30	-	-	-	IRREGULARITIES 75 TONS/MILE
0.038	0.256	RAMP	1151				-	16.35	-	-	-	IRREGULARITIES 75 TONS/MILE
0.002	0.143	RAMP	744				-	10.57	-	-	-	IRREGULARITIES 75 TONS/MILE
TOTAL							99478.4	7651.68	3382.07	9948.0	278.38	
USE							99479	7651.7	3382.1	9948	278.4	
NOTE: ANY AND ALL IRREGULARITIES MAY BE UNDERRUN AND NO ADJUSTMENT TO ANY UNIT BID COST WILL BE MADE												
SHOULDER AND LANE WIDTHS ACCOUNT FOR THE SP125B MAINLINE TREATMENT EXTENDING 18" ONTO SHOULDER												

MOBILIZATION

1 LUMP SUM

STATE OF MISSOURI

CRAIG SKORSETH

NUMBER

PE-2018000273

PROFESSIONAL ENGINEER

Craig Skorseth

CRAIG SKORSETH - CIVIL

MO-PE-2018000273

DATE PREPARED

8/8/2024

ROUTE

29

STATE

MO

DISTRICT

KC

SHEET NO.

3

COUNTY

CLAY

JOB NO.

JKU0059

CONTRACT ID.

PROJECT NO.

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DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

MoDOT

EFFECTIVE: 07-01-2024																						
SIGN	SIZE IN.	AREA SQ. FT.	QTY EACH	TOTAL AREA SQ. FT.	QTY RELOC EACH	TOTAL RELOC SQ. FT.	SIGN NUM.	DESCRIPTION	SIGN	SIZE IN.	AREA SQ. FT.	QTY EACH	TOTAL SQ. FT.	QTY RELOC EACH	TOTAL RELOC SQ. FT.	SIGN NUM.	DESCRIPTION	ITEM NUMBER	TOTAL QTY	DESCRIPTION		
WARNING SIGNS									GUIDE SIGNS									REGULATORY SIGNS				
WO1-1L	48X48	16.00						TURN (SYMBOL LEFT)	E05-1	36X48	12.00	2	24				GORE EXIT	6122008		IMPACT ATTENUATOR 40 MPH (SAND BARRELS)		
WO1-1R	48X48	16.00						TURN (SYMBOL RIGHT)	E05-2	48X36	12.00						EXIT OPEN	6122009		IMPACT ATTENUATOR 45 MPH (SAND BARRELS)		
WO1-2L	48X48	16.00						CURVE (SYMBOL LEFT)	E05-2a	48X36	12.00	2	24				EXIT CLOSED	6122010		IMPACT ATTENUATOR 50 MPH (SAND BARRELS)		
WO1-2R	48X48	16.00						CURVE (SYMBOL RIGHT)	GO20-1	60X24	10.00						ROAD WORK NEXT XX MILES	6122012		IMPACT ATTENUATOR 55 MPH (SAND BARRELS)		
WO1-3L	48X48	16.00						REVERSE TURN (SYMBOL LEFT)	GO20-2	48X24	8.00	6	48				END ROAD WORK	6122014		IMPACT ATTENUATOR 60 MPH (SAND BARRELS)		
WO1-3R	48X48	16.00						REVERSE TURN (SYMBOL RIGHT)	GO20-4	36X18	4.50						PILOT CAR FOLLOW ME	6122017		IMPACT ATTENUATOR 65 MPH (SAND BARRELS)		
WO1-4L	48X48	16.00	2	32				REVERSE CURVE (SYMBOL LEFT)	GO20-4a	42X30	8.75						PILOT CAR IN USE WAIT & FOLLOW	6122019		IMPACT ATTENUATOR 70 MPH (SAND BARRELS)		
WO1-4R	48X48	16.00						REVERSE CURVE (SYMBOL RIGHT)	GO20-4a	18X12	1.50						PILOT CAR IN USE WAIT & FOLLOW	6122020		REPLACEMENT SAND BARREL		
WO1-4bL	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL LEFT)	GO20-5aP	36X24	6.00	4	24				WORK ZONE (PLAQUE)	6122030		IMPACT ATTENUATOR (RELOCATION)		
WO1-4bR	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL RIGHT)	MO4-8a	24X18	3.00						END DETOUR	6123001		TRUCK MOUNTED ATTENUATOR (TMA)		
WO1-4cL	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL LEFT)	MO4-9L	48X36	12.00						DETOUR (LEFT)	6161008	6	ADVANCED WARNING RAIL SYSTEM		
WO1-4cR	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL RIGHT)	MO4-9R	48X36	12.00						DETOUR (RIGHT)	6161012		BUOYS (BOATS KEEP OUT)		
WO1-6	60X30	12.50						HORIZONTAL ARROW (SYMBOL)	MO4-9P	48X12	4.00						STREET NAME (PLAQUE)	6161013		BUOYS (NO WAKE)		
WO1-6a	72X36	18.00						HORIZ. ARROW (SYMBOL ON PERMANENT BARRICADE)	MO4-10L	48X18	6.00						DETOUR ARROW (LEFT)	6161014		SPECIAL SIGN ASSEMBLY (BOATS KEEP OUT)		
WO1-7	60X30	12.50						DOUBLE HEAD HORIZONTAL ARROW (SYMBOL)	MO4-10R	48X18	6.00						DETOUR ARROW (RIGHT)	6161025	300	CHANNELIZER (TRIM LINE)		
WO1-7a	72X36	18.00						DOUBLE HEAD HORIZ. ARROW (SYMBOL ON PERM. BARR.)	REGULATORY SIGNS								6161030	15	TYPE III MOVEABLE BARRICADE			
WO1-8	18X24	3.00						CHEVRON (SYMBOL)	R1-1	48X48	13.25						STOP	6161033	28	DIRECTION INDICATOR BARRICADE		
WO1-8a	30X36	7.50						CHEVRON (SYMBOL FOR DIVIDED HIGHWAYS)	R1-2	48TRI.	6.93	2	13.86				YIELD	6161040	2	FLASHING ARROW PANEL		
WO3-1	48X48	16.00						STOP AHEAD (SYMBOL)	R1-2a	36X36	9.00						TO ONCOMING TRAFFIC (PLAQUE)	6161047		TYPE III OBJECT MARKER		
WO3-2	48X48	16.00	2	32				YIELD AHEAD (SYMBOL)	R1-3P	30X12	2.50						ALL WAY (PLAQUE)	6161055	28	SEQUENTIAL FLASHING WARNING LIGHT		
WO3-3	48X48	16.00						SIGNAL AHEAD (SYMBOL)	R2-1	36X48	12.00	8	96				SPEED LIMIT 2@45, 2@55	6161070		TUBULAR MARKER		
WO3-4	48X48	16.00						BE PREPARED TO STOP	R3-1	48X48	16.00	1	16				NO RIGHT TURN (SYMBOL)	6161095		RADAR SPEED ADVISORY SYSTEM		
WO3-5	48X48	16.00						SPEED LIMIT AHEAD	R3-2	48X48	16.00	1	16				NO LEFT TURN (SYMBOL)			CHANGEABLE MESSAGE SIGN, COMMISSION FURNISHED/RETAINED		
WO4-1L	48X48	16.00						MERGE (SYMBOL FROM LEFT)	R3-3	36X36	9.00	2	18				NO TURNS	6161096				
WO4-1R	48X48	16.00						MERGE (SYMBOL FROM RIGHT)	R3-4	48X48	16.00						NO U-TURN (SYMBOL)			CHANGEABLE MESSAGE SIGN W/O COMM.		
WO4-1aL	48X48	16.00	2	32				MERGE (LEFT)	R3-7L	30X30	6.25						LEFT LANE MUST TURN LEFT	6161098A		INTERFACE - CONTRACTOR FURNISHED/RETAINED		
WO4-1aR	48X48	16.00	2	32				MERGE (RIGHT)	R3-7R	30X30	6.25						RIGHT LANE MUST TURN RIGHT			CHANGEABLE MESSAGE SIGN WITH COMM.		
WO5-1	48X48	16.00	4	64				ROAD/BRIDGE/RAMP NARROWS	R4-1	36X48	12.00						DO NOT PASS	6161099	4	INTERFACE - CONTRACTOR FURNISHED/RETAINED		
WO5-3	48X48	16.00						ONE LANE BRIDGE	R4-2	36X48	12.00						PASS WITH CARE	6162000A		WORK ZONE TRAFFIC SIGNAL SYSTEM		
WO5-5	48X48	16.00						NARROW LANES	R4-7a	36X48	12.00						KEEP RIGHT (HORIZONTAL ARROW)	6162002		TEMPORARY LONG-TERM RUMBLE STRIPS		
WO6-1	48X48	16.00						DIVIDED HIGHWAY (SYMBOL)	R4-8a	36X48	12.00						KEEP LEFT (HORIZONTAL ARROW)			TEMPORARY TRAFFIC BARRIER		
WO6-2	48X48	16.00						DIVIDED HIGHWAY END (SYMBOL)	R5-1	30X30	6.25						DO NOT ENTER	6173600D		CONTRACTOR FURNISHED/RETAINED		
WO6-3	48X48	16.00						TWO WAY TRAFFIC (SYMBOL)	R5-1a	36X24	6.00						WRONG WAY			TEMPORARY TRAFFIC BARRIER		
WO7-3a	30X24	5.00	2	10				NEXT XX MILES (PLAQUE)	R6-1L	54X18	6.75						ONE WAY ARROW (LEFT)	6173602B		CONTRACTOR FURNISHED/COMMISSION RETAINED		
WO8-1	48X48	16.00						BUMP	R6-1R	54X18	6.75						ONE WAY ARROW (RIGHT)	6174000A		TEMP. TRAFFIC BARRIER HEIGHT TRANSITION		
WO8-2	48X48	16.00						DIP	R6-2L	24X30	5.00						ONE WAY (LEFT)	6175010A		RELOCATING TEMPORARY TRAFFIC BARRIER		
WO8-3	48X48	16.00						PAVEMENT ENDS	R6-2R	24X30	5.00						ONE WAY (RIGHT)			TEMPORARY TRAFFIC BARRIER		
WO8-4	48X48	16.00						SOFT SHOULDER	R9-9	24X12	2.00						SIDEWALK CLOSED	6176000B		COMMISSION FURNISHED/RETAINED		
WO8-5	48X48	16.00						SLIPPERY WHEN WET (SYMBOL)	R9-11L	24X18	3.00						SIDEWALK CLOSED AHEAD, (ARROW LEFT) CROSS HERE			TEMP. TRAFFIC BARRIER HEIGHT TRANSITION		
WO8-6	48X48	16.00						TRUCK CROSSING	R9-11R	24X18	3.00						SIDEWALK CLOSED AHEAD, (ARROW RIGHT) CROSS HERE	6177000B		COMMISSION FURNISHED/RETAINED		
WO8-6c	48X48	16.00						TRUCK ENTRANCE	R10-6	24X36	6.00						STOP HERE ON RED (45° ARROW)	6208064A		TEMPORARY RAISED PAVEMENT MARKER		
WO8-7	36X36	9.00						LOOSE GRAVEL	R11-2	48X30	10.00	4	40				ROAD CLOSED	9029400		TEMPORARY TRAFFIC SIGNALS		
WO8-7a	36X36	9.00						FRESH OIL / LOOSE GRAVEL	R11-3a	60X30	12.50						ROAD CLOSED XX MILES AHEAD	9029401		TEMPORARY TRAFFIC SIGNALS AND LIGHTING		
WO8-9	48X48	16.00						LOW SHOULDER	R11-4	60X30	12.50						LOCAL TRAFFIC ONLY					
WO8-11	48X48	16.00						UNEVEN LANES	CONST-3A	60X48	20.00						ROAD CLOSED TO THRU TRAFFIC					
WO8-12	48X48	16.00						NO CENTER LINE	CONST-3X	56X12	4.67						FINE SIGN					
WO8-15	48X48	16.00						GROOVED PAVEMENT	MISCELLANEOUS SIGNS													
WO8-15P	30X24	5.00						MOTORCYCLE (PLAQUE)	CONST-5	48X36	12.00						POINT OF PRESENCE					
WO8-17L	48X48	16.00						SHOULDER DROP-OFF (SYMBOL LEFT)	CONST-5	96X48	32.00	2	64				POINT OF PRESENCE					
WO8-17R	48X48	16.00						SHOULDER DROP-OFF (SYMBOL RIGHT)	CONST-8	48X36	12.00	6	72				WORK ZONE NO PHONE ZONE					
WO8-17P	30X24	5.00						SHOULDER DROP-OFF (PLAQUE)														
W10-1	42RND.	9.62						RAILROAD CROSSING														
WO12-1	24X24	4.00						DOUBLE DOWN ARROW (SYMBOL)														
WO12-2	48X48	16.00						LOW CLEARANCE (SYMBOL)														
WO12-2x	24X18	3.00						LOW CLEARANCE (PLAQUE)														
WO12-2a	84X24	14.00						OVERHEAD LOW CLEARANCE (FEET AND INCHES)														
WO12-4	120X60	50.00						LOW CLEARANCE XX FT XX IN XX MILES AHEAD														
WO12-5	120X60	50.00						WIDTH RESTRICTION XX FT XX IN XX MILES AHEAD														
WO13-1	30X30	6.25						ADVISORY SPEED (PLAQUE)														
WO16-2	30X24	5.00						XXX FEET (PLAQUE)														
WO16-3	30X24	5.00						X MILE (PLAQUE)														
WO20-1	48X48	16.00	12	192				ROAD/BRIDGE/RAMP WORK AHEAD														
WO20-2	48X48	16.00						DETOUR AHEAD														
WO20-3	48X48	16.00	2	32				ROAD CLOSED AHEAD	616-10.05 TOTAL													
WO20-4	48X48	16.00						ONE LANE ROAD AHEAD	CONSTRUCTION SIGNS				1111									
WO20-5	48X48	16.00	4	64				RIGHT/CENTER/LEFT LANE CLOSED AHEAD	616-10.10						TOTAL							
WO20-5a	48X48	16.00	4	64				2 RIGHT/CENTER/LEFT LANES CLOSED AHEAD	RELOCATED SIGNS						0							
WO20-6a	48X48	16.00	2	32				RIGHT/CENTER/LEFT LANE CLOSED														
WO20-7a	48X48	16.00						FLAGGER (SYMBOL)														
WO21-2	36X36	9.00						FRESH OIL														
WO21-5	48X48	16.00	4	64				SHOULDER WORK / SHOULDER WORK AHEAD														
WO22-1	48X48	16.00						BLASTING ZONE AHEAD														
WO22-2	42X36	10.50						TURN OFF 2-WAY RADIO AND PHONE														
WO22-3	42X36	10.50						END BLASTING ZONE														
GO22-1	21X15	2.19	2	4.38				WET PAINT (ARROW PIVOTS)														

SUMMARY OF QUANTITIES
SHEET 3 OF 3

SPEED	SIGN SPACING (FT)	TAPER LENGTH (FT)		OPTIONAL BUFFER LENGTH (FT) (B)	CHANNELIZER SPACING (FT)	
		SHOULDER (1) (T1)	LANE (2) (T2)		TAPERS	BUFFER/ WORK AREAS
PERMANENT POSTED (MPH)	DIVIDED HIGHWAYS (S)					
0-35	200	70	245	280	35	40
40-45	500	150	540	400	40	80
50-55	1000	185	660	560	50	80
60-70	SA - 1000 SB - 1500 SC - 2640	235	840	840	60	120

NOTES:

(1) SHOULDER TAPER LENGTH BASED ON 10 FT. (STANDARD SHOULDER WIDTH) OFFSET.


(2) LANE TAPER LENGTH BASED ON 12 FT. (STANDARD LANE WIDTH) OFFSET.

(3) 5 CHANNELIZER DEVICE MINIMUM.

REMOVE AND/OR MODIFY EXISTING PAVEMENT MARKING AS NEEDED.


TEMPORARY PAVEMENT MARKING REQUIRED WITH LONG TERM LANE CLOSURES.

RAMP NARROWS SIGN IS REQUIRED WHEN RAMP WIDTH IS REDUCED.




WO20 - 1

2




WO20 - 5

5




WO20 - 6a

6




WO4 - 1 aL

6A




WO5 - 1

43



E05 - 1

49




GO20 - 5aP

R2 - 1

54


4



R2 - 1

25

(NORMAL SPEED)



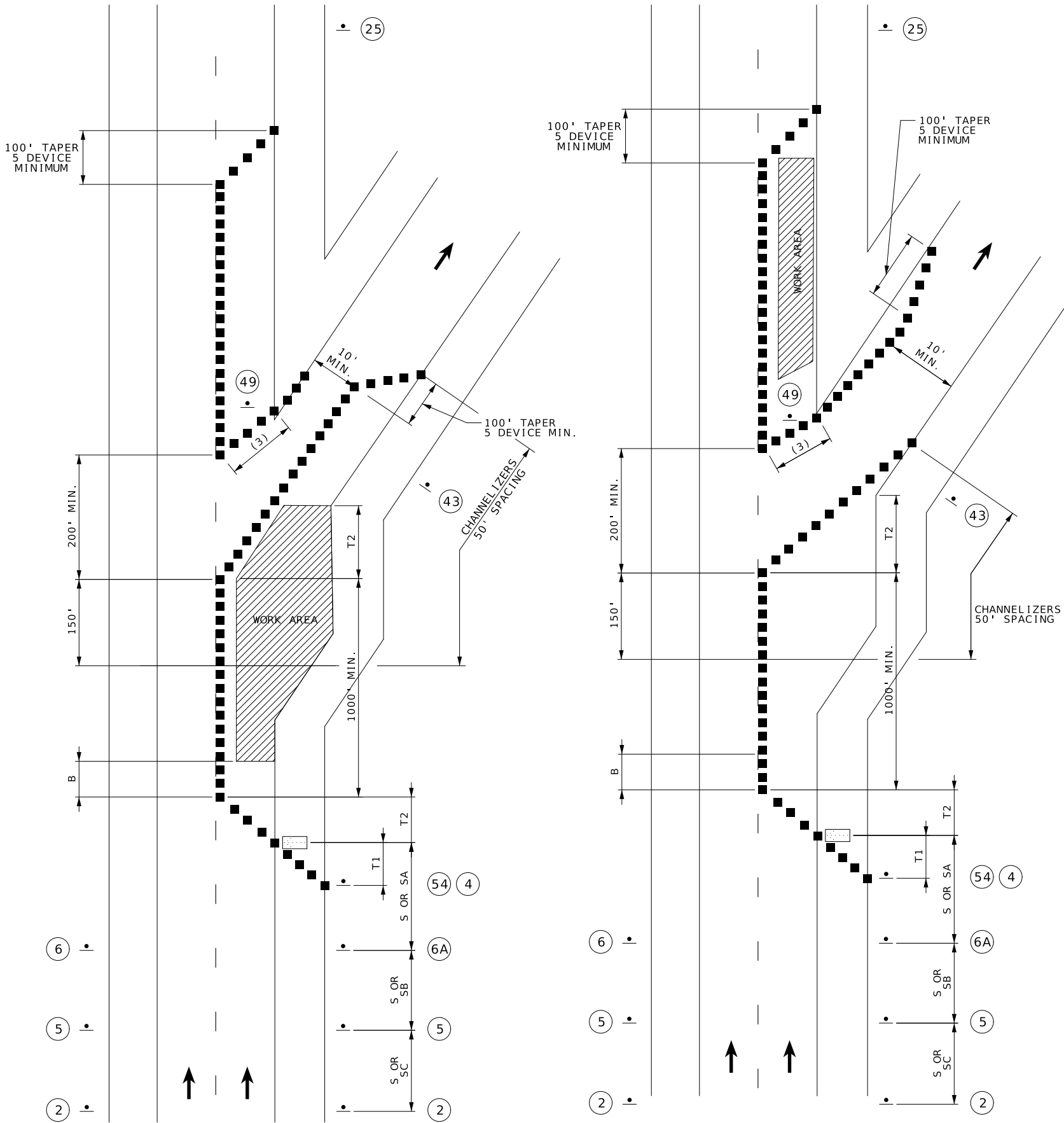
R2 - 1

25

(NORMAL SPEED)

NOT TO SCALE

TRAFFIC CONTROL
SHEET 3 OF 10



WORK IN THE VICINITY OF AN EXIT RAMP

STATE OF MISSOURI

CRAIG SKORSETH

NUMBER

PE-2018000273

PROFESSIONAL ENGINEER

Craig Skorseth

DATE PREPARED

8/8/2024

ROUTE

29

STATE

MO

DISTRICT

KC

SHEET NO.

6

COUNTY

CLAY

JOB NO.

JKU0059

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

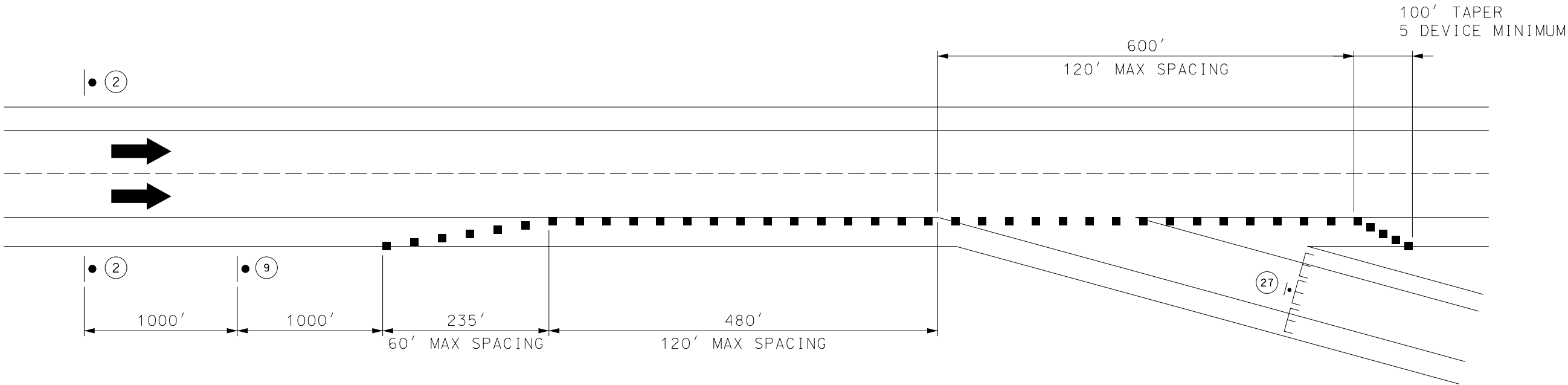
105 WEST CAPITOL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

MODOT

OFF RAMP CLOSURE



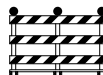
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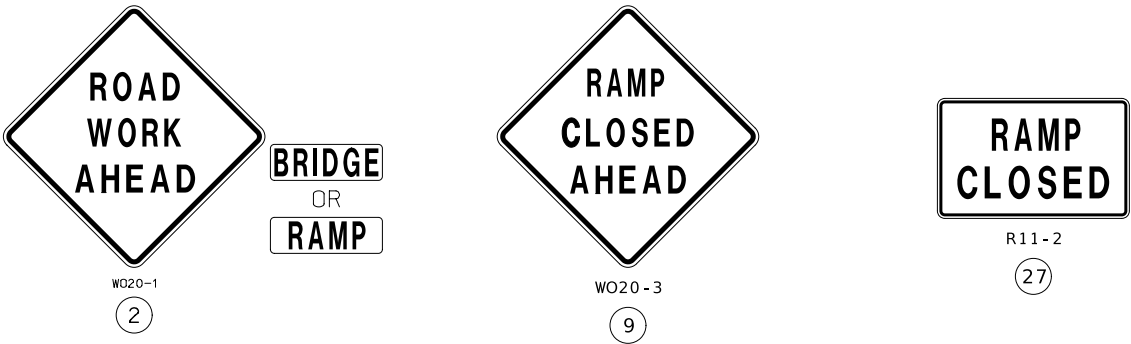
NO DIRECT PAY WILL BE MADE FOR RELOCATING, COVERING, UNCOVERING, OR REMOVING SIGNS.

ANY EXISTING SIGNING THAT CONFLICTS WITH THE TRAFFIC CONTROL SIGNING SHALL BE COMPLETELY COVERED OR REMOVED.

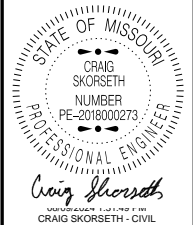
SPACING OF SIGNS SHOWN ON THE PLANS ARE MINIMUM AND MAY BE ADJUSTED BY THE ENGINEER TO MEET FIELD CONDITIONS.

TRAFFIC CONTROL LEGEND

- SIGN (SINGLE SIDED)
- CHANNELIZER
-  TYPE 3 MOVEABLE BARRICADES



NOT TO SCALE




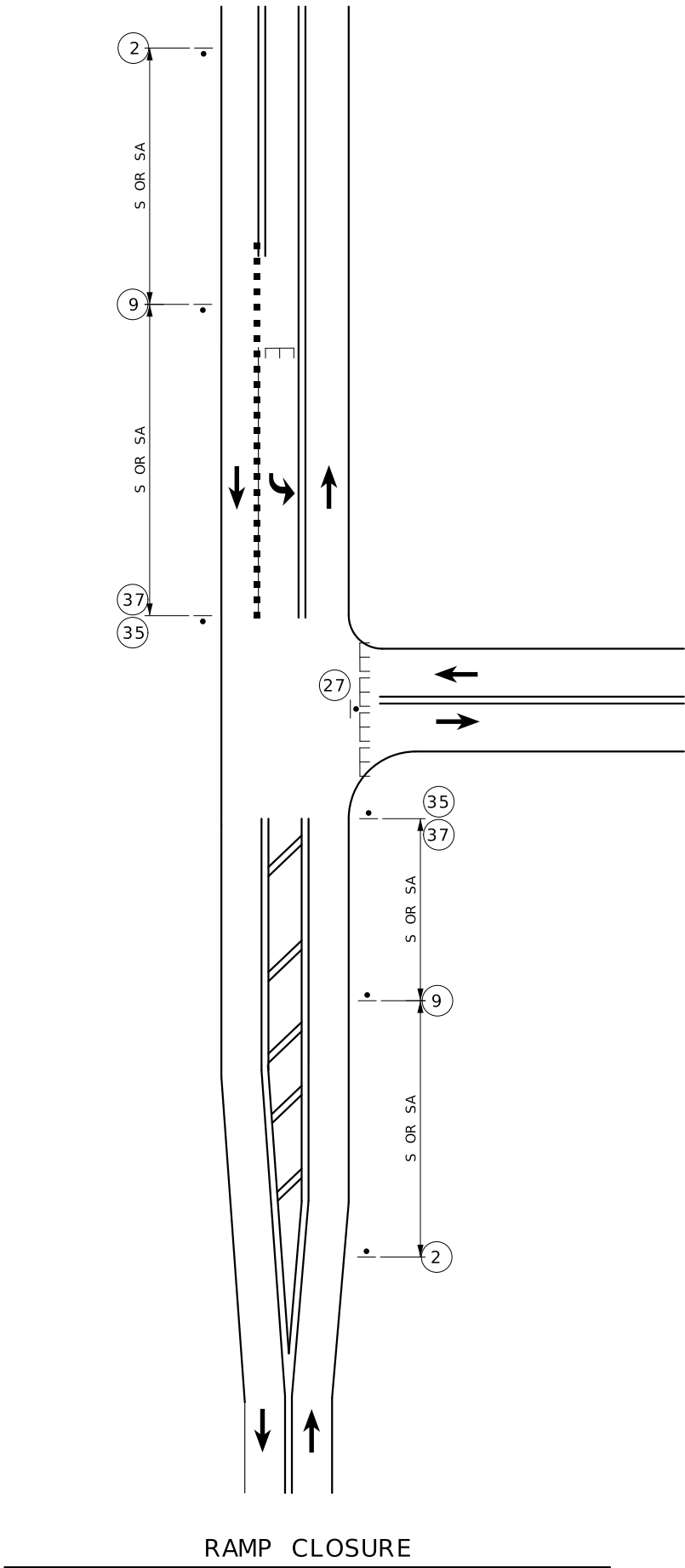
DATE PREPARED 8/8/2024	
ROUTE 29	STATE MO
DISTRICT KC	SHEET NO. 7
COUNTY CLAY	
JOB NO. JKU0059	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)





TRAFFIC CONTROL LEGEND

- SIGN (SINGLE SIDED)
- CHANNELIZER
- TYPE 3 MOVEABLE BARRICADES
- TYPICAL VIEW

SPEED	SIGN SPACING (FT)		TAPER LENGTH (FT)		OPTIONAL	CHANNELIZER SPACING (FT)	
PERMANENT POSTED (MPH)	UNDIVIDED HIGHWAYS (S)	DIVIDED HIGHWAYS (S)	SHOULDER (1) (T1)	LANE (2) (T2)	BUFFER LENGTH (FT) (B)	TAPERS	BUFFER/WORK AREAS
0-35	200	200	70	245	280	35	40
40-45	350	500	150	540	400	40	80
50-55	500	1000	185	660	560	50	80
60-70	1000	SA - 1000 SB - 1500 SC - 2640	235	840	840	60	120

RAMP
WORK
AHEAD

W020-1

2

RAMP
CLOSED
AHEAD

W020-3

9

RAMP
CLOSED

R11-2

27

NO
TURNS

R3-1

36

NO
TURNS

R3-2

37

NO
TURNS

R3-3

35

NO
TURNS

R3-3

35

NOTES:

NO DIRECT PAY WILL BE MADE FOR RELOCATING, COVERING, UNCOVERING, OR REMOVING SIGNS.

ANY EXISTING SIGNING THAT CONFLICTS WITH THE TRAFFIC CONTROL SIGNING SHALL BE COMPLETELY COVERED OR REMOVED.

SPACING OF SIGNS SHOWN ON THE PLANS ARE MINIMUM AND MAY BE ADJUSTED BY THE ENGINEER TO MEET FIELD CONDITIONS.

STATE OF MISSOURI
CRAIG SKORSETH
NUMBER
PE-2018000273
PROFESSIONAL ENGINEER

Craig Skorseth
CRAIG SKORSETH - CIVIL
MO-PE-2018000273

DATE PREPARED
8/8/2024

ROUTE
29

STATE
MO

DISTRICT
KC

SHEET NO.
8

COUNTY
CLAY

JOB NO.
JKU0059

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

NOTES:

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY PROVIDE ADDITIONAL PROTECTIVE TRUCKS EQUIPPED WITH PROPER WARNING DEVICES.

PROTECTIVE TRUCK AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE HAZARD WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

FLASHING ARROW PANELS AND SIGNS SHALL BE INCIDENTAL TO TRUCK MOUNTED ATTENUATORS, WHEREVER USED. NO ADDITIONAL PAYMENT WILL BE MADE.

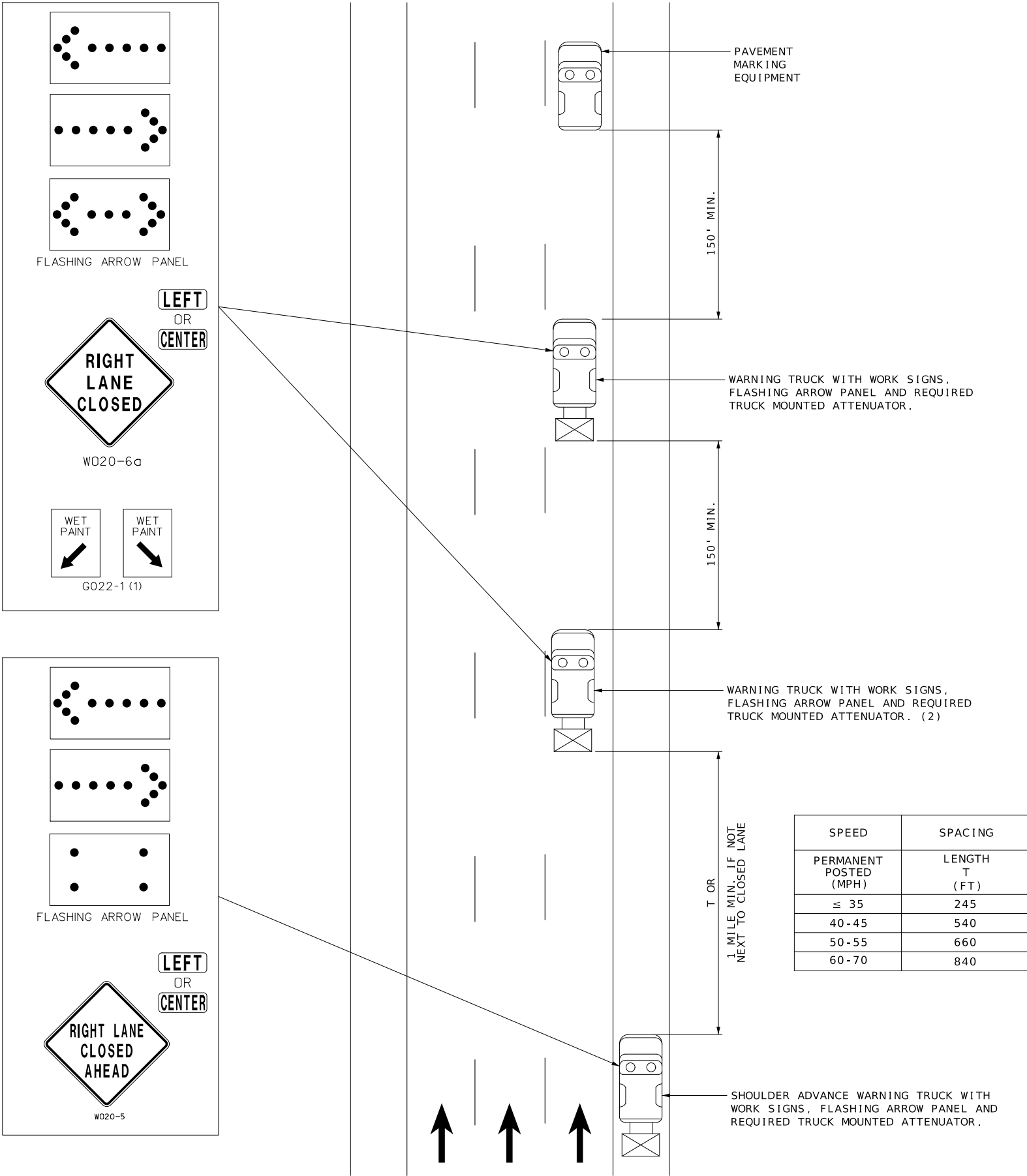
(1) WET PAINT SIGNS ARE INSTALLED TO INDICATE THE SIDE IN WHICH THE PAVEMENT MARKING MATERIAL IS BEING APPLIED. AT THE CONTRACTOR'S OPTION, A FRONT FACING WET PAINT SIGN MAY BE INSTALLED ON THE LEFT SIDE OF THE PAVEMENT MARKING EQUIPMENT.

(2) WARNING TRUCK IS POSITIONED AT THE NO TRACK POINT OF THE PAVEMENT MARKING MATERIAL, OR SPACING SHOWN, WHICH EVER IS GREATER.

FLASHING ARROW PANELS SHALL, AS A MINIMUM, BE TYPE B, WITH A SIZE OF 60 X 30 INCHES.

VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED IN A MANNER SUCH THAT THEY ARE NOT OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS ON VEHICLE-MOUNTED SIGNS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.

A FLASHING ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED. WHEN MORE THAN ONE LANE CLOSED, A SEPARATE FLASHING ARROW PANEL BOARD SHALL BE USED FOR EACH CLOSED LANE.



STRIPING ON MULTI-LANE HIGHWAY

STATE OF MISSOURI

CRAIG SKORSETH

NUMBER

PE-2018000273

PROFESSIONAL ENGINEER

Craig Skorseth

CRAIG SKORSETH - CIVIL

MO-PE-2018000273

DATE PREPARED

8/8/2024

ROUTE

29

STATE

MO

DISTRICT

KC

SHEET NO.

12

COUNTY

CLAY

JOB NO.

JKU0059

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

8/8/2024

DISTRICT	SHEET NO.
KC	13

CLAY

JKU0059

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(3) REAR WARNING TRUCK IS POSITIONED AT THE NO TRACK POINT OF THE PAVEMENT MARKING MATERIAL, OR VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE, OR SPACING SHOWN, WHICHEVER IS GREATER.



TRAFFIC CONTROL
SHEET 10 OF 10