DESIGN DESIGNATION

A.A.D.T. - 2024 = 2990 A.A.D.T. - 2044 = 3648 D.H.V. = 7% T = 4% V = 55 M.P.H.

D = 52% W, 48% EFUNCTIONAL CLASSIFICATION- MINOR ARTERIAL

> NO NEW RIGHT OF WAY WILL BE ACQUIRED FOR THIS PROJECT.

CONVENTIONAL SYMBOLS (USED IN PLANS)

(0020 111 1211110	.,	
	EXISTING	NEW
BUILDINGS AND STRUCTURES GUARD RAIL GUARD CABLE CONCRETE RIGHT-OF-WAY MARKER STEEL RIGHT-OF-WAY MARKER LOCATION SURVEY MARKER UTILITIES	0000	····
FIBER OPTICS OVERHEAD CABLE TV UNDERGROUND CABLE TV OVERHEAD TELEPHONE UNDERGROUND TELEPHONE OVERHEAD POWER UNDERGROUND POWER SANITARY SEWER STORM SEWER GAS WATER	- FOOTVUTV OT UT OE UE S S S G W SAN	- OTV - UTV - OT - UT - OE - UE - S
MANHOLE	TAN E	€
FIRE HYDRANT	wv	ĵ
WATER VALVE	"" ***	€
WATER METER	#""⊕)
DROP INLET	, E	
DITCH BLOCK	=	⊨
GROUND MOUNTED SIGN	SIGN	_
LIGHT POLE		
H-FRAME POWER POLE		
TELEPHONE PEDESTAL FENCE CHAIN LINK	PED	=
WOVEN WIRE GATE POST	—— x	(—— (
BENCHMARK	Вм)

NOTE: DASHED OR OPEN SYMBOLS INDICATE EXISTING FEATURES

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

PLANS FOR PROPOSED STATE HIGHWAY

FRANLKIN COUNTY

KEY MAP LOCATION OF FRANKLIN COUNTY DISCLAIMER
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PAVEMENT OVERLAY, EROSION REPAIR AT | BOX CULVERT, SIGNING REPLACEMENT MISSOURI RIVER NEW HAVEN WASHINGTON

NOT TO SCALE

THE EXISTENCE AND APPROXIMATE LOCATION OF UTILITY FACILITIES KNOWN TO EXIST, AS SHOWN ON THE PLANS, ARE BASED ON THE BEST INFORMATION AVAILABLE TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE. RELIANCE UPON THIS INFORMATION IS DONE AT THE RISK AND PERIL OF THE USER, AND THE COMMISSION SHALL NOT BE LIABLE FOR ANY DAMAGES THAT MAY ARISE FROM ANY ERROR IN THE INFORMATION. IT IS, THEREFORE, THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE, LOCATION AND STATUS OF ANY FACILITY. SUCH VERIFICATION INCLUDES DIRECT CONTACT WITH THE LISTED UTILITIES.

INDEX OF SHEETS

1112 271 01 0112 11	_
DESCRIPTION	SHEET NUMBER
TITLE SHEET	1
TYPICAL SECTIONS (TS) (2 SHEETS)	2
QUANTITIES (QU) (7 SHEETS)	3
PLAN SHEETS (PP)	4-23
REFERENCE POINTS (RP)	24
COORDINATE POINTS (CP)	25
DRAINAGE SHEETS (DR)	26
TRAFFIC CONTROL SHEETS (TC)	27 - 33
SIGNING (SN)	34-41
i e e e e e e e e e e e e e e e e e e e	

PB PE-2	BER 8779 LENGILLI LILLILLI J. HOTEL O.33:18 AM DRBIS - CIVIL 28779
11/8,	/2024
ROUTE	STATE
KK	MO
DISTRICT	SHEET NO.
DISTRICT SL COU	SHEET NO.
DISTRICT SL COU FRAN JOB	SHEET NO. 1
DISTRICT SL COU FRAN JOB J 653	SHEET NO. 1 NTY KLIN NO.
DISTRICT SL COU FRAN JOB J 653 CONTRA	SHEET NO. 1 NTY KLIN NO. 619B

LENGTH OF PROJECT

BEGINNING OF PROJECT STA. 0 + 58.37 END OF PROJECT STA. 265 + 65.13

26,506.76 FEET APPARENT LENGTH

FOUATIONS FOR CONSTRUCTION C.

TOTAL CORRECTIONS -234.95 FEET NET LENGTH OF PROJECT 26,271.81 FEET STATE LENGTH 4.98 MILES FOR INFORMATION ONLY ESTIMATED DISTURBED ACRES

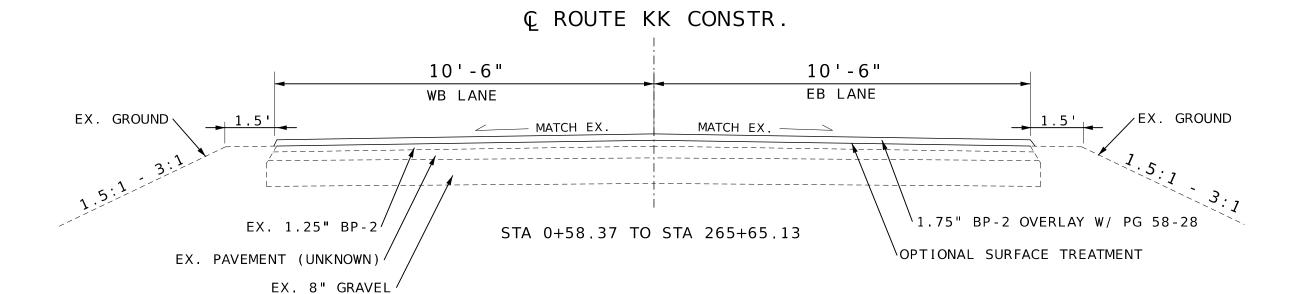
0.00 ACRES

APPLICATION RATES:

BP-2: 1.96 TONS/CY TACK: 0.10 GAL/SY

OPTIONAL SURFACE TREATMENT: 0.25 GAL/SY

TYPICAL SECTION- ROUTE KK



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NOT TO SCALE



10/17/2024 9:58:21 AM SHIRLEY J. NORRIS - CIVI MO-PE-28779

| DATE | PREPARED | 10 / 15 / 2024 | ROUTE | STATE | KK | MO | DISTRICT | SHEET NO. | SL | 2

FRANKLIN
JOB NO.
J6S3619B
CONTRACT ID.

PROJECT NO.

DESCRIPTION

BRIDGE NO.

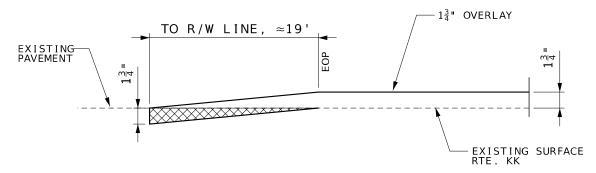
ISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

MADOT

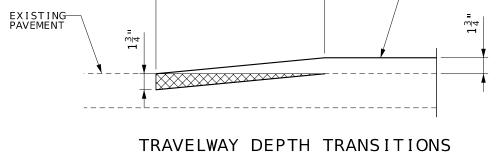
105 WEST CAPITO

JEFFERSON CITY, MO 6510

FYPICAL SECTION SHEFT 1 OF 2

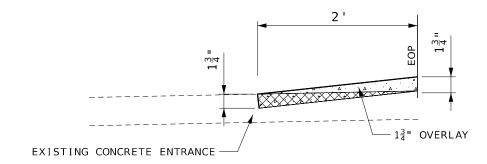


TYPICAL SIDE ROAD ENTRANCE (COLDMILLED TRANSITION)

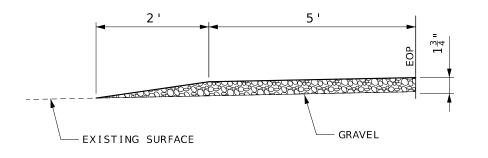


175'

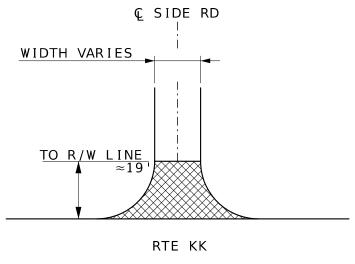
BEGINNING AND END OF PROJECT AND EXCEPTIONS



TYPICAL CONCRETE ENTRANCE 2' COLDMILLED TRANSITION



TYPICAL UNPAVED ENTRANCE (FIELD, PRIVATE OR COUNTY ROAD)



= MODIFIED COLDMILLING DEPTH TRANSITION

-1³" OVERLAY

NOT TO SCALE

10/15/2024 KK MO SL 2 FRANKLIN J6S3619B CONTRACT ID. PROJECT NO BRIDGE NO.

TYPICAL SECTION SHEET 2 OF 2

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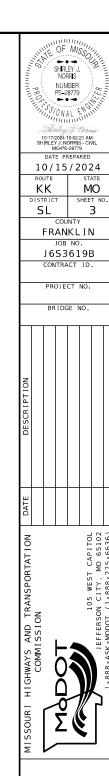
DISCLAIMER

	REMOVAL OF IMPROVEMENTS - ROADWAY								
PLAN SHEET NO.	ROADWAY	STATION	STATION	LOCATION /OFFSET	DESCRIPTION OF REMOVAL				
26	ROUTE KK	234+70.00	235+05	30' RT	APPROX. 35' BARBED WIRE LIVESTOCK FENCE				
26	ROUTE KK	234+71.27	235+01.24	47'- 65' LT	DOWNED TREES/DRIFT AT BOX 4 INLET (APPROX. 38 CY)				
23	ROUTE KK	260+26.82	260+51.82	1' - 14' RT	75.6' SAW CUT AROUND EX. TRANSVERSE RUMBLE STRIP				
23	ROUTE KK	260+26.82	260+51.82	1' - 14' RT	CONCRETE TRANSVERSE RUMBLE REMOVAL (35.6 SY)				
23	ROUTE KK	263+06.54	263+31.54	0.5' - 12.5'	74' SAW CUT AROUND EX. TRANSVERSE RUMBLE STRIP				
23	ROUTE KK	263+06.54	263+31.54	0.5' - 12.5'	CONCRETE TRANSVERSE RUMBLE REMOVAL (33.4 SY)				
PAY TOTAL 1 LUMP SUM									

NOTE: INCLUDES 'REMOVAL OF IMPROVEMENTS- SIGN REMOVAL' IN LUMP SUM PAY ITEM

PLAN SHEET NO.	ROADWAY	STATION	STATION	LOCATION /OFFSET	DESCRIPTION OF REMOVAL
4	ROUTE KK	2+25	-	RT	SIGN ONLY
4	ROUTE KK	4+73	-	RT	SIGN ONLY
4	ROUTE KK	4+81	-	LT	SIGN, POST, AND STUB
4	ROUTE KK	7+61	-	LT	SIGN ASSEMBLY, POST, AND STUB
6	ROUTE KK	28+36	-	RT	SIGN, POST, AND STUB
7	ROUTE KK	44+18	-	RT	POST AND STUB
7	ROUTE KK	48+32	-	LT	SIGN, POST, AND STUB
7	ROUTE KK	48+90	-	LT	SIGN, POST, AND STUB
8	ROUTE KK	60+60	-	RT	SIGN ASSEMBLY ONLY
8	ROUTE KK	67+62	-	LT	SIGN ASSEMBLY, POST, AND STUB
8	ROUTE KK	67+94	-	RT	SIGN ASSEMBLY, POST, AND STUB
9	ROUTE KK	71+42	-	RT	SIGN, POST, AND STUB
9	ROUTE KK	72+00	-	RT	SIGN, POST, AND STUB
9	ROUTE KK	72+58	-	RT	SIGN, POST, AND STUB
9	ROUTE KK	72+99	-	LT	SIGN ONLY
9	ROUTE KK	73+22	-	RT	SIGN, POST, AND STUB
9	ROUTE KK	73+86	-	RT	SIGN, POST, AND STUB
9	ROUTE KK	74+98	-	LT	SIGN ASSEMBLY, POST, AND STUB
9	ROUTE KK	77+87	-	RT	SIGN ASSEMBLY ONLY
10	ROUTE KK	85+63	-	LT	SIGN ASSEMBLY ONLY
10	ROUTE KK	88+22	-	LT	SIGN ONLY
10	ROUTE KK	93+80	-	RT	SIGN ASSEMBLY, POST, AND STUB
10	ROUTE KK	96+23	-	RT	SIGN, POST, AND STUB
11	ROUTE KK	97+18	-	RT	SIGN, POST, AND STUB

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QUANTITY SHEE SHEET 1 OF 7

REMOVAL OF IMPROVEMENTS - SIGN REMOVAL								
PLAN SHEET NO.	ROADWAY	STATION	STATION	LOCATION /OFFSET	DESCRIPTION OF REMOVAL			
11	ROUTE KK	98+18	-	RT	SIGN, POST, AND STUB			
11	ROUTE KK	99+13	-	RT	SIGN ONLY			
11	ROUTE KK	99+78	-	RT	SIGN, POST, AND STUB			
11	ROUTE KK	100+13	-	RT	SIGN ONLY			
11	ROUTE KK	105+77	-	LT	SIGN ASSEMBLY, POST, AND STUB			
12	ROUTE KK	115+08	-	RT	SIGN, POST, AND STUB			
12	ROUTE KK	117+08	-	RT	SIGN ONLY			
12	ROUTE KK	117+87	-	RT	SIGN ONLY			
12	ROUTE KK	118+72	-	RT	SIGN ONLY			
12	ROUTE KK	119+56	-	RT	SIGN ONLY			
12	ROUTE KK	120+30	_	RT	SIGN ONLY			
12	ROUTE KK	121+15	_	RT	SIGN ONLY			
12	ROUTE KK	121+94	-	RT	SIGN ONLY			
12	ROUTE KK	122+73	_	RT	SIGN ONLY			
13	ROUTE KK	124+63	-	RT	SIGN ASSEMBLY, POST, AND STUB			
13	ROUTE KK	133+98	_	LT	SIGN, POST, AND STUB			
13	ROUTE KK	134+67	-	LT	SIGN, POST, AND STUB			
14		138+12	-	LT	SIGN, POST, AND STUB			
14	ROUTE KK			LT	SIGN ASSEMBLY, POST, AND STUB			
	ROUTE KK	141+76	-					
14	ROUTE KK	143+40	-	RT	SIGN ONLY			
15	ROUTE KK	151+21	-	LT	SIGN ASSEMBLY, POST, AND STUB			
15	ROUTE KK	156+56	-	RT	SIGN ASSEMBLY, POST, AND STUB			
15	ROUTE KK	158+18	-	LT	SIGN ONLY			
15	ROUTE KK	158+97	-	LT	SIGN ONLY			
15	ROUTE KK	159+77	-	LT	SIGN ONLY			
15	ROUTE KK	160+56	-	LT	SIGN ONLY			
15	ROUTE KK	160+88	-	LT	SIGN ONLY			
15	ROUTE KK	161+67	-	LT	SIGN ONLY			
15	ROUTE KK	161+98	=	LT	SIGN ONLY			
16	ROUTE KK	165+01	-	RT	SIGN, POST, AND STUB			
16	ROUTE KK	165+80	-	RT	SIGN, POST, AND STUB			
16	ROUTE KK	166+64	-	RT	SIGN, POST, AND STUB			
16	ROUTE KK	168+54	-	RT	SIGN, POST, AND STUB			
16	ROUTE KK	169+34	-	RT	SIGN, POST, AND STUB			
16	ROUTE KK	172+60	-	LT	SIGN ONLY			
16	ROUTE KK	173+44	-	LT	SIGN ONLY			
16	ROUTE KK	174+18	-	LT	SIGN ONLY			
16	ROUTE KK	175+03	_	LT	SIGN ONLY			
16	ROUTE KK	175+82	-	LT	SIGN ONLY			
16	ROUTE KK	176+61	_	LT	SIGN ONLY			
16	ROUTE KK	177+40	-	LT	SIGN ONLY			
17	ROUTE KK	180+68	_	LT	SIGN ASSEMBLY, POST, AND STUB			
17	ROUTE KK	184+30	-	RT	SIGN ONLY			
18	ROUTE KK	196+23	-	LT	SIGN ASSEMBLY, POST, AND STUB			
18	ROUTE KK	198+34	-	RT	SIGN ASSEMBLY, POST, AND STOR			
20	ROUTE KK	227+24		RT				
			-		SIGN, POST, AND STUB SIGN ASSEMBLY, POST, AND STUB			
21	ROUTE KK	240+23	-	RT				
21	ROUTE KK	241+71	=	RT	SIGN, POST, AND STUB			
21	ROUTE KK	243+08	-	RT	SIGN, POST, AND STUB			
21	ROUTE KK	244+24	-	RT	SIGN, POST, AND STUB			
21	ROUTE KK	245+46	-	RT	SIGN, POST, AND STUB			
22	ROUTE KK	246+72	-	RT	SIGN, POST, AND STUB			
22	ROUTE KK	249+30	-	LT	SIGN ASSEMBLY, POST, AND STUB			
23	ROUTE KK	260+75	-	LT	SIGN ONLY			
23	ROUTE KK	262+80	-	RT	SIGN, POST, AND STUB			
23	ROUTE KK	263+31	-	RT	SIGN, POST, AND STUB			
23	ROUTE KK	264+19	-	LT	SIGN ONLY			
23	ROUTE KK	265+00	-	LT	SIGN ONLY			
23	ROUTE KK	265+86	-	LT	SIGN, POST, AND STUB			
2.2	ROUTE KK	266+08	-	RT	SIGN, POST, AND STUB			
23								

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OF MISSON
SHREY J. MORRES
NUMBER
PE-28779
JOYAL ENION
SONAL ENION
SONAL ENION
SONAL ENION
MORRES OF LONG
MORRES

MO-PE-28779

DATE PREPARED

10/15/2024

ROUTE STATE
KK MO
DISTRICT SHEET NO.
SL 3

FRANKLIN

JOB NO.
J6S3619B
CONTRACT ID.

PROJECT NO.

BRIDGE NO.

TION

N DATE

ISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION
105 WEST CAPITO
165 WEST CAPITO
167 WEST CAPITO

QUANTITY SHEET SHEET 2 OF 7

	ASPHALT OVERLAY								
SHEET	STATION	STATION	ROADWAY	1.75" BP-2 w/ PG 58-28	TACK COAT	MODIFIED COLDMILLING DEPTH TRANSITIONS	MISC. OPTIONAL SURFACE TREATMENT		
				TONS	GAL	SY	GAL		
4	0+58.37	2+33.36	ROUTE KK	51.75	54.33	543.3	135.8		
4 - 23	2+33.36	263+90.1	ROUTE KK	6155.42	6461.98		16155.0		
23	263+90.1	265+65.1	ROUTE KK	46.49	48.81	488.1	122.0		
SUBT	SUBTOTALS FROM 'SIDE ROADS AND ENTRANCES' QUANTITY BOX				109.70	1096.7			
FOR 'SAFTEY EDGE' QUANTITY			70.18						
FOR SURFACE/ WIDTH IRREGULARITIES (10%)				642.83					
			SUBTOTAL	7071.2	6674.8	2128.1	16412.8		
			PAY TOTAL	7071	6675	2128	16413		

NOTE: TACK COAT APPLICATION RATE AT 0.1 GAL/SY, OPTIONAL SURFACE TREATMENT AT 0.25 GAL/SY

	SIDE ROADS AND ENTRANCES								
SHEET NUMBER	ROADWAY	NUMBER OF ENTRANCES		1.75" BP-2 w/ PG 58-28	TACK COAT	MODIFIED COLDMILLING DEPTH TRANSITIONS	GRAVEL A OR CRUSHED STONE B	REMARKS	
			SF	TONS	GAL	SY	TONS		
4 - 22	ROUTE KK, RT	29	10826.4	72.9	76.5	764.8	39.4		
4-23	ROUTE KK, LT	43	10505.0	31.6	33.2	331.9	75.2	CONTAINS 2 HYBRID ASPHALT/GRAVEL DRIVEWAYS	
			SUBTOTAL	104.5	109.7	1096.7	114.6		
			PAY TOTAL	*	*	*	115		

* SEE 'ASPHALT OVERLAY' QUANTITY BOX FOR PAY TOTALS

NOTE: GRAVEL APPLICATION RATE AT 0.09 TONS/SY, TACK COAT APPLICATION RATE AT 0.1 GAL/SY

MOBILIZATION - 1 LUMP SUM

CLEARING AND GRUBBING
NO DIRECT PAY

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DISCLAIMER

CONTRACTOR FURNISHED SURVEYING AND STAKING - 1 LUMP SUM

SHRLEY J.
NORPIS
NUMBER
PE-28779
PE-28779
Solvat E.
11082224 10:2851 AM
SHIRLEY J. MORRIS - CML

DATE PREPARED

11/8/2024

ROUTE STATE
KK MO

DISTRICT SHEET NO.

SL 3

FRANKLIN

JOB NO.

J6S3619B

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

E DESCRIPTION

COMMISSION
COMMISSION
TODOT

105 WEST CAPITOL
JEFFERSON CITY, MO 65102

DUANTITY SHEET SHEET 3 OF 7

				PERMANENT PA	AVEMENT MARK	INGS			
) WATERBORNE PA PAINT (TYPE P		PREFORMED THERMOPLASTIC PAVEMENT MARKINGS		
PLAN SHEET NO.	STATION	STATION	ROADWAY	4" SOLID WHITE (L.F.)	4" SOLID YELLOW (L.F.)	4" INTERM. YELLOW (L.F.)	24" SOLID WHITE STOP BAR (L.F.)	12" WHITE YIELD LINE TRIANGLES (EACH)	
4-23	0+58.37	265+65.13	ROUTE KK	51900.0	45109.0	1844.8	48	26	
			SUBTOTALS	51900	45109	1845	48	26	
PAY TOTALS				51900	469	954	48	26	

FENCE REPLACEMENT								
SHEET NUMBER	STATION	STATION	OFFSET	ROADWAY / LOCAT I ON	WOVEN WIRE FENCE			
					(LF)			
26	234+86	235+42	27' - 32' RT	ROUTE KK	40			
				SUBTOTAL	40			
				PAY TOTAL	40			

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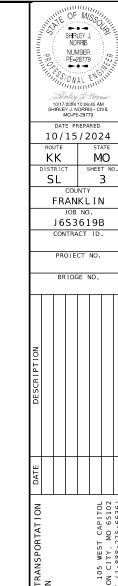


QUANTITY SHEET SHEET 4 OF 7

	FULL-DEPTH PAVEMENT POST RUMBLE STRIP REMOVAL											
SHEET	STATION	STATION	ROADWAY	8" BIT. BASE PAVEMENT MIXTURE w/ PG 64-22	TYPE 1 AGGREGATE FOR BASE (4" THICK)	REMARKS						
				TONS	SY							
23	260+26.82	260+51.82	ROUTE KK EB	15.37	35.60	FULL-DEPTH REPLACEMENT PAVEMENT AFTER						
23	263+06.54	263+31.54	ROUTE KK EB	14.42	33.40	CONCRETE TRANSVERSE RUMBLE STRIP REMOVAL						
			SUBTOTAL	29.79	69.00							
			PAY TOTAL	29.8	69							

	DRAINAGE - PERMANENT EROSION CONTROL													
PLAN SHEET NO.	ROADWAY	LOCATION/ OFFSET	STATION	STATION	ROCK LINING (CULVERT OUTLET)				PLACING TYPE 4 ROCK DITCH LINER	BEDDING MATERIAL FOR ROCK DITCH LINER C.Y.	PERMANENT EROSION CONTROL GEOTEXTILE S.Y.			
26	ROUTE KK	35' - 87' LT	234+68	235+18		65.4	65.4	50.9	50.9	20.4	235.6			
26	ROUTE KK	32' - 68' RT	234+96	235+47	142									
				SUBTOTAL	142	65.4	65.4	50.9	50.9	20.4	235.6			
				PAY TOTAL	142	65	65	51	51	20	236			

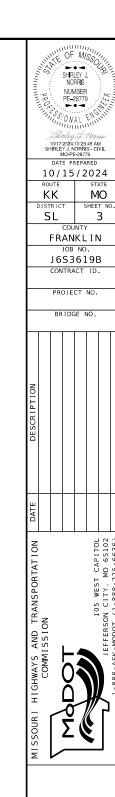
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				PART I AL	DEPTH P	AVEMENT REPAIRS		
LANE	WIDTH (FT)	LENGTH (FT)	REPAIR THICKNESS (IN)	AREA (SF)	VOLUME (CY)	FURNISHING AND PLACING BITUMINOUS MATERIAL FOR CLASS C PARTIAL DEPTH PAVEMENT REPAIR	REMOVALS FOR CLASS C PARTIAL DEPTH PAVEMENT REPAIR	REMARKS
DTE VV ED	7 5	30	4	225	2 70	TONS 5.39	SY	
RTE. KK EB	7.5	1036	4	11401	2.78	273.07	25.00 1266.81	
RTE. KK EB	7.5	289	4	2165	26.73	51.85	240.53	
RTE. KK EB	11	574	4	6314	77.95	151.22	701.56	
RTE. KK EB	11	656	4	7216	89.09	172.83	801.78	
RTE. KK EB	11	1148	4	12628	155.90	302.45	1403.11	
RTE. KK EB	11	328	4	3608	44.54	86.41	400.89	
RTE. KK EB	7.5	656	4	4920	60.74	117.84	546.67	
RTE. KK EB	7.5	118	4	886	10.93	21.21	98.40	ALL 7.5' WIDTH REPAIRS ARE LOCATED IN THE
RTE. KK EB	7.5	98	4	738	9.11	17.68	82.00	RIGHT WHEEL-PATH AREA.
RTE. KK EB	7.5	426	4	3198	39.48	76.59	355.33	
RTE. KK EB	7.5	492	4	3690	45.56	88.38	410.00	
RTE. KK EB	7.5	82	4	615	7.59	14.73	68.33	
RTE. KK EB	7.5	98	4	738	9.11	17.68	82.00	
RTE. KK EB	7.5	98	4	738	9.11	17.68	82.00	
RTE. KK EB	7.5	623	4	4674	57.70	111.95	519.33	
RTE. KK EB	7.5	115	4	861	10.63	20.62	95.67	
RTE. KK EB	7.5	279	4	2091	25.81	50.08	232.33	
RTE. KK EB	7.5	246	4	1845	22.78	44.19	205.00	
RTE. KK EB	7.5	492	4	3690	45.56	88.38	410.00	
		•						
RTE. KK WB	11	107	4	1177	14.53	28.19	130.78	
RTE. KK WB	7.5	330	4	2475	30.56	59.28	275.00	
RTE. KK WB	11	207	4	2277	28.11	54.54	253.00	
RTE. KK WB	7.5	1071	4	8033	99.17	192.38	892.50	
RTE. KK WB	7.5	480	4	3600	44.44	86.22	400.00	
RTE. KK WB	7.5	346	4	2595	32.04	62.15	288.33	
RTE. KK WB	7.5	225	4	1688	20.83	40.42	187.50	
RTE. KK WB	7.5	700	4	5250	64.81	125.74	583.33	
RTE. KK WB	7.5 7.5	350 217	4	2625 1628	32.41	62.87 38.98	291.67 180.83	
RTE. KK WB	11	913	4	10043	123.99	240.54	1115.89	ALL 7.5' WIDTH REPAIRS ARE LOCATED IN THE
RTE KK WB	7.5	250	4	1875	23.15	44.91	208.33	RIGHT WHEEL-PATH AREA.
RTE KK WB	11	650	4	7150	88.27	171.25	794.44	
RTE KK WB	7.5	1050	4	7875	97.22	188.61	875.00	
RTE. KK WB	7.5	101	4	758	9.35	18.14	84.17	
RTE. KK WB	7.5	200	4	1500	18.52	35.93	166.67	
RTE KK WB	7.5	90	4	675	8.33	16.17	75.00	
RTE. KK WB	11	422	4	4642	57.31	111.18	515.78	
RTE. KK WB	11	1055	4	11605	143.27	277.95	1289.44	
RTE. KK WB	7.5	870	4	6525	80.56	156.28	725.00	
RTE. KK WB	7.5	167	4	1253	15.46	30.00	139.17	
RTE. KK WB	7.5	130	4	975	12.04	23.35	108.33	
RTE. KK WB	7.5	50	4	375	4.63	8.98	41.67	
			· · · · · · · · · · · · · · · · · · ·		TOTAL	3804.29	17648.57	
					PAY TOTAL	3804.3	17648.6	

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UANTITY SHEET SHEET 6 OF 7

													EFFECTIVE: 07-01-2024		
		TOTAL OTY	TOTALSIGN	I				ОТҮ	TOTAL S I GI	NI			LITECTIVE: 07-01-2024	JIIII.	E OF MISSOUR
	SIZE AREA OTY	1 1 '	RELOC NUM.			S 1 7 E A	REALOTY	1 '	RELOC NUM					1.74	SHRLEY J
l I c i c N					CICN			1 1		1	,	TOTAL		HIII	NORRIS
SIGN	IN SQ FT EACH		SQ FI	DESCRIPTION	SIGN	IN. SO		SQ FT EACH	SQ FT	- DECCRIPTION		TOTAL	DECCRIPTION.	=======================================	NUMBER PE-28779
W01 11		NING SIGNS		DESCRIPTION TURN (COMPONENTS)	F0F 1	26740 1		DE SIGNS		DESCRIPTION	NUMBER		DESCRIPTION	11/6	
	48X48 16.00 48X48 16.00			TURN (SYMBOL LEFT) TURN (SYMBOL RIGHT)	E05-1 E05-2	36X48 1				GORE EXIT EXIT OPEN	6122008		IMPACT ATTENUATOR 45 MPH (SAND BARRELS)	11/1	NAME FRANCE
WO1-1R WO1-2L	48X48 16.00			CURVE (SYMBOL RIGHT)	E05-2a	48X36 1 48X36 1				EXIT CLOSED	6122009		IMPACT ATTENUATOR 45 MPH (SAND BARRELS) IMPACT ATTENUATOR 50 MPH (SAND BARRELS)		Lirley J. Norres
	48X48 16.00			CURVE (SYMBOL RIGHT)	GO20 - 1		0.00 2	20		ROAD WORK NEXT 2 MILES	6122012		IMPACT ATTENUATOR 55 MPH (SAND BARRELS)	10/ SHIRI	17/2024 10:25:06 AM LEY J. NORRIS - CIVIL MO-PE-28779
WO1 - 3L	48X48 16.00			REVERSE TURN (SYMBOL LEFT)	GO20 - 2	48X24 8		16		END ROAD WORK	6122014		IMPACT ATTENUATOR 60 MPH (SAND BARRELS)		ATE PREPARED
WO1-3R	48X48 16.00			REVERSE TURN (SYMBOL RIGHT)		36X18 4				PILOT CAR FOLLOW ME	612201		IMPACT ATTENUATOR 65 MPH (SAND BARRELS)		/15/2024
WO1-4L	48X48 16.00			REVERSE CURVE (SYMBOL LEFT)	GO20-4a	42X30 8	3.75			PILOT CAR IN USE WAIT & FOLLOW	6122019)	IMPACT ATTENUATOR 70 MPH (SAND BARRELS)	ROUT KK	
WO1 - 4R	48X48 16.00			REVERSE CURVE (SYMBOL RIGHT)	GO20-4a	18X12 1	1.50 15	22.5		PILOT CAR IN USE WAIT & FOLLOW	6122020)	REPLACEMENT SAND BARREL	DISTRI	
WO1-4bL	48X48 16.00			DOUBLE ARROW REVERSE CURVE (SYMBOL LEFT)	GO20 - 5 al	P 36X24 6	5.00			WORK ZONE (PLAQUE)	6122030)	IMPACT ATTENUATOR (RELOCATION)	SL	
	48X48 16.00			DOUBLE ARROW REVERSE CURVE (SYMBOL RIGHT)		24X18 3				END DETOUR	612300		TRUCK MOUNTED ATTENUATOR (TMA)		COUNTY
	48X48 16.00			TRIPLE ARROW REVERSE CURVE (SYMBOL LEFT)	MO4 - 9L	48X36 1				DETOUR (LEFT)	6161008		ADVANCED WARNING RAIL SYSTEM	FI	RANKLIN
	48X48 16.00			TRIPLE ARROW REVERSE CURVE (SYMBOL RIGHT)		48X36 1				DETOUR (RIGHT)	6161012		BUOYS (BOATS KEEP OUT)	. I	ЈОВ NO. 653619B
WO1 63	60X30 12.50 72X36 18.00			HORIZONTAL ARROW (SYMBOL) HORIZ. ARROW (SYMBOL ON PERMANENT BARRICADE)	MO4 - 9P	48X12 4				STREET NAME (PLAQUE) DETOUR ARROW (LEFT)	6161013		BUOYS (NO WAKE) SPECIAL SIGN ASSEMBLY (BOATS KEEP OUT)		ONTRACT ID.
WO1 - 0a	60X30 12.50			DOUBLE HEAD HORIZONTAL ARROW (SYMBOL)		48X18 6				DETOUR ARROW (RIGHT)	6161025		CHANNELIZER (TRIM LINE)		
	72X36 18.00			DOUBLE HEAD HORIZ. ARROW (SYMBOL ON PERM. BARR.)	1104 1010	10/(10)		ULATORY SIG	NS	DETOCK FRANCE (KTGITT)	6161030		TYPE III MOVEABLE BARRICADE	Р	ROJECT NO.
WO1-8	18X24 3.00			CHEVRON (SYMBOL)	R1-1	48X48 1		1 1 1	· · · -	STOP	6161033		DIRECTION INDICATOR BARRICADE		DIDGE NG
WO1-8a	30X36 7.50			CHEVRON (SYMBOL FOR DIVIDED HIGHWAYS)	R1-2	48TRI. 6				YIELD	6161040		FLASHING ARROW PANEL	"	BRIDGE NO.
WO3 - 1	48X48 16.00			STOP AHEAD (SYMBOL)	R1-2a	36X36	9.00			TO ONCOMING TRAFFIC (PLAQUE)	6161047	7	TYPE III OBJECT MARKER		\top
WO3 - 2	48X48 16.00			YIELD AHEAD (SYMBOL)	R1-3P	30X12 2				ALL WAY (PLAQUE)	6161055		SEQUENTIAL FLASHING WARNING LIGHT		
WO3 - 3	48X48 16.00			SIGNAL AHEAD (SYMBOL)	R2-1	36X48 1				SPEED LIMIT XX	6161070		TUBULAR MARKER		
WO3 - 4	48X48 16.00 4	64		BE PREPARED TO STOP	R3-1	48X48 1				NO RIGHT TURN (SYMBOL)	6161095	5	RADAR SPEED ADVISORY SYSTEM		
WO3 - 5	48X48 16.00			SPEED LIMIT AHEAD	R3 - 2	48X48 1				NO LEFT TURN (SYMBOL)		.	CHANGEABLE MESSAGE SIGN,		
WO4-1L WO4-1R	48X48 16.00 48X48 16.00			MERGE (SYMBOL FROM RIGHT)	R3-3 R3-4	36X36 9				NO TURNS	6161096)	COMMISSION FURNISHED/RETAINED		
	48X48 16.00			MERGE (SYMBOL FROM RIGHT) MERGE (LEFT)	R3 - 7L	30X30 6				NO U-TURN (SYMBOL) LEFT LANE MUST TURN LEFT	6161098	A 3 **	CHANGEABLE MESSAGE SIGN W/O COMM. INTERFACE - CONTRACTOR FURNISHED/RETAINED	<u>-</u>	
	48X48 16.00			MERGE (RIGHT)	R3 - 7R	30X30 6				RIGHT LANE MUST TURN RIGHT	-		CHANGEABLE MESSAGE SIGN WITH COMM.	SCF	
WO5 - 1	48X48 16.00			ROAD/BRIDGE/RAMP NARROWS	R4-1	36X48 1				DO NOT PASS	6161099	9	INTERFACE - CONTRACTOR FURNISHED/RETAINED	E E	
WO5 - 3	48X48 16.00			ONE LANE BRIDGE	R4-2	36X48 1				PASS WITH CARE	6162000		WORK ZONE TRAFFIC SIGNAL SYSTEM		
WO5 - 5	48X48 16.00			NARROW LANES	R4-7a	36X48 1	2.00			KEEP RIGHT (HORIZONTAL ARROW)	6162002	2	TEMPORARY LONG-TERM RUMBLE STRIPS		
WO6 - 1	48X48 16.00			DIVIDED HIGHWAY (SYMBOL)	R4-8a	36X48 1	2.00			KEEP LEFT (HORIZONTAL ARROW)			TEMPORARY TRAFFIC BARRIER		
WO6 - 2	48X48 16.00			DIVIDED HIGHWAY END (SYMBOL)	R5-1	30X30 6				DO NOT ENTER	6173600	D	CONTRACTOR FURNISHED/RETAINED		++++
WO6 - 3	48X48 16.00			TWO WAY TRAFFIC (SYMBOL)	R5-1a	36X24 6				WRONG WAY			TEMPORARY TRAFFIC BARRIER	삗	
WO7 - 3a	30X24 5.00			NEXT XX MILES (PLAQUE)	R6-1L	54X18 6				ONE WAY ARROW (LEFT)	6173602		CONTRACTOR FURNISHED/COMMISSION RETAINED	DA	
WO8 - 1	48X48 16.00			BUMP DIP	R6-1R	54X18 6				ONE WAY (LEET)	6174000		TEMP. TRAFFIC BARRIER HEIGHT TRANSITION		\bot
WO8 - 2 WO8 - 3	48X48 16.00 48X48 16.00			PAVEMENT ENDS	R6-2L R6-2R	24X30 5				ONE WAY (LEFT) ONE WAY (RIGHT)	6175010	A	RELOCATING TEMPORARY TRAFFIC BARRIER TEMPORARY TRAFFIC BARRIER	Ιz	TOL 102
WO8 - 4	48X48 16.00			SOFT SHOULDER	R9 - 9	24X12 2				SIDEWALK CLOSED	6176000	В	COMMISSION FURNISHED/RETAINED	ΙĔ	P11
WO8 - 5	48X48 16.00			SLIPPERY WHEN WET (SYMBOL)		1				SIDEWALK CLOSED AHEAD,	1	_	TEMP. TRAFFIC BARRIER HEIGHT TRANSITION	_∀_	CAPIT MO 651
WO8 - 6	48X48 16.00			TRUCK CROSSING	R9-11L	24X18 3	3.00			(ARROW LEFT) CROSS HERE	6177000	в	COMMISSION FURNISHED/RETAINED	٦ ق	TSI.
WO8-6c	48X48 16.00			TRUCK ENTRANCE						SIDEWALK CLOSED AHEAD,	6208064	А	TEMPORARY RAISED PAVEMENT MARKER	ISP	M E
WO8 - 7	36X36 9.00			LOOSE GRAVEL	R9-11R	24X18 3	3.00			(ARROW RIGHT) CROSS HERE	9029400)	TEMPORARY TRAFFIC SIGNALS	[₹	105 ON CI
	36X36 9.00			FRESH OIL / LOOSE GRAVEL	R10-6	24X36 6				STOP HERE ON RED (45^ ARROW)	902940	ı L	TEMPORARY TRAFFIC SIGNALS AND LIGHTING	F S	RSOL
	48X48 16.00			LOW SHOULDER	R11-2	48X30 1	0.00			ROAD CLOSED				SI	E /
	48X48 16.00 15			UNEVEN LANES	D11 3	60430	2 50			ROAD CLOSED XX MILES AHEAD	 			ANE 11 SS	
l	48X48 16.00 15 48X48 16.00	240		NO CENTER LINE	—	60X30 1				ROAD CLOSED TO THRU TRAFFIC				Y S	
l	30X24 5.00			GROOVED PAVEMENT MOTORCYCLE (PLAQUE)		60X30 1 60X48 2			+	FINE SIGN				≱ ⊃	
	48X48 16.00			SHOULDER DROP-OFF (SYMBOL LEFT)		X 56X12 4				SPEEDING/PASSING (PLATE)				Ϊ́Θ	Á 11 1 °
l	48X48 16.00			SHOULDER DROP-OFF (SYMBOL RIGHT)	33,13,	100/12/		CELLANEOUS	SIGNS		l'			ΙΞ	1 ////
l	30X24 5.00			SHOULDER DROP-OFF (PLAQUE)	CONST - 5	48X36 1			1 - 1	POINT OF PRESENCE	1			 	2
l	42RND. 9.62			RAILROAD CROSSING	—	96X48 3				POINT OF PRESENCE	1	TEM	PORARY TRAFFIC CONTROL	l lo	Z (6
WO12-1	24X24 4.00			DOUBLE DOWN ARROW (SYMBOL)	CONST - 8	48X36 1	2.00 2	24		WORK ZONE NO PHONE ZONE				55	-
l	48X48 16.00			LOW CLEARANCE (SYMBOL)									QUANTITY	IΞL	
	24X18 3.00			LOW CLEARANCE (PLAQUE)									LUMP SUM 1		
	84X24 14.00			OVERHEAD LOW CLEARANCE (FEET AND INCHES)											
	120X60 50.00			LOW CLEARANCE XX FT XX IN XX MILES AHEAD	<u> </u>						*	NO DI	RECT PAYMENT WILL BE MADE		
	120X60 50.00 30X30 6.25			WIDTH RESTRICTION XX FT XX IN XX MILES AHEAD		+		 	 		F	OR REL	OCATING SIGNS OR DEVICES		
				ADVISORY SPEED (PLAQUE)							*	* NOT	INCLUDED IN TEMORARY TRAFFIC		
	30X24 5.00 30X24 5.00			XXX FEET (PLAQUE) X MILE (PLAQUE)		+ +			 		1		LUMP SUM BID		H
	48X48 16.00 10	160		ROAD/BRIDGE/RAMP WORK AHEAD		+ +			 			CIVINOL	LONII JOIN DID		7 1 1
l	48X48 16.00			DETOUR AHEAD		 					1				SHEE: 7
l -	48X48 16.00			ROAD CLOSED AHEAD	616-10	0.05		TOTAL	' 	1	1				
l –	48X48 16.00 4	64		ONE LANE ROAD AHEAD			SIGNS						DISCLAIMER		. QUANTITY SHEET 7 OF
l	48X48 16.00			RIGHT/CENTER/LEFT LANE CLOSED AHEAD	616-10				TOTAL		THE PE	neess 1	ONAL WHOSE SIGNATURE AND PERSONAL		⊢ Z⊢
l	48X48 16.00			2 RIGHT/CENTER/LEFT LANES CLOSED AHEAD	RELOCA	TED SIG	GNS		*				HEREON ASSUMES RESPONSIBILITY		LE.
	48X48 16.00			RIGHT/CENTER/LEFT LANE CLOSED									AT APPEARS ON THIS PAGE, AND		δ <u>Ξ</u>
	48X48 16.00 4	64		FLAGGER (SYMBOL)									PURSUANT TO SECTION 327.411 RSMO)		BS
WO21-2	36X36 9.00			FRESH OIL									ON. ESTIMATES, REPORTS, OR OTHER	1	2B

CMS LOCATIONS TO BE DETERMINED BY THE ENGINEER

SIGN AND DEVICE LOCATION WILL BE FIELD VERIFIED AND

APPROVED BY THE ENGINEER

SPECIFICATION, ESTIMATES, REPORTS, OR OTHER

INTENDED TO BE USED FOR ANY PART OR PARTS OF

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THE PROJECT TO WHICH THIS PAGE REFERS.

WO22-1 48X48 16.00

WO22-2 42X36 10.50

WO22-3 42X36 10.50

WO21-5 48X48 16.00 2 32

GO22-1 21X15 2.19 2 4.38

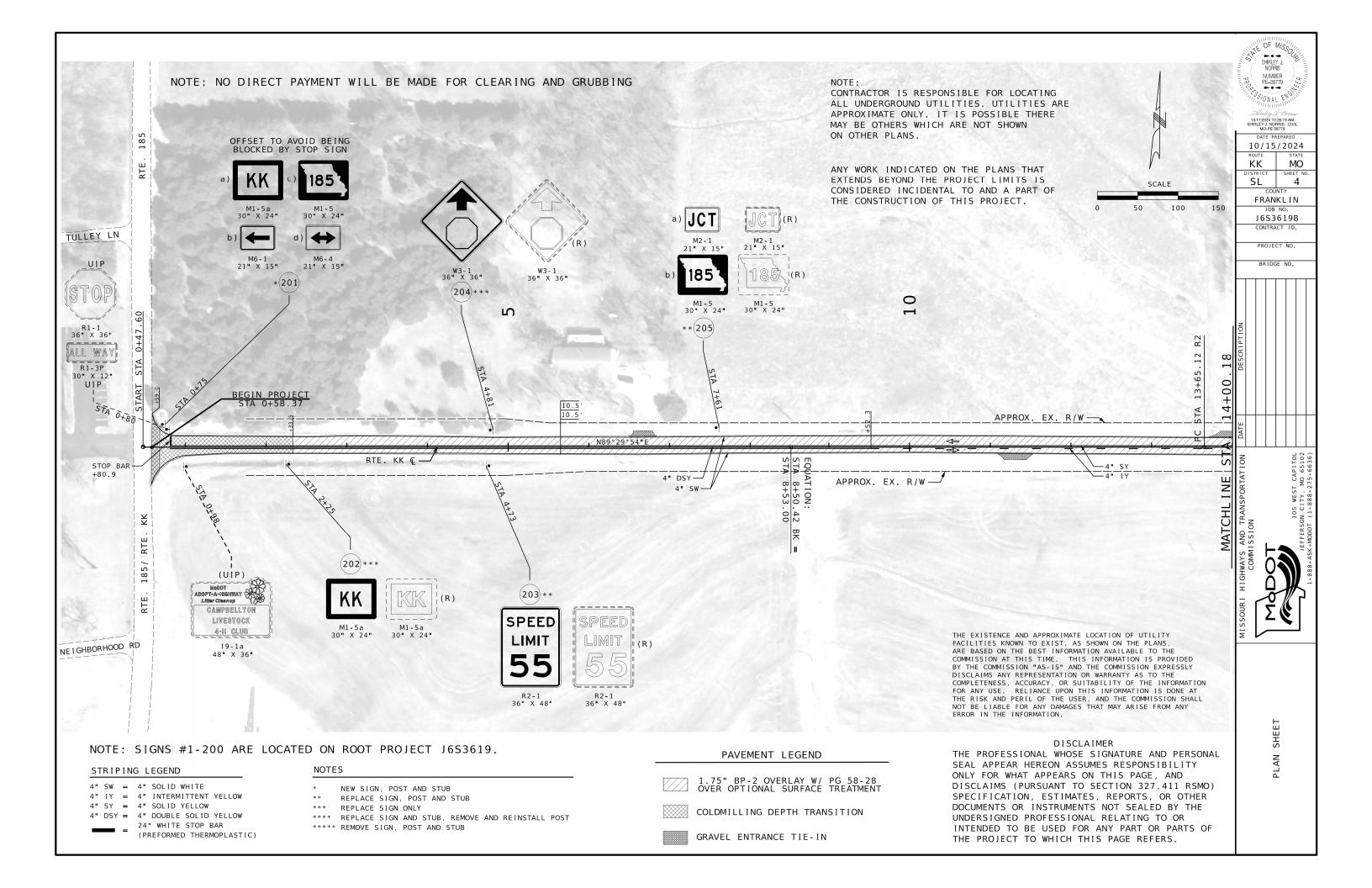
BLASTING ZONE AHEAD

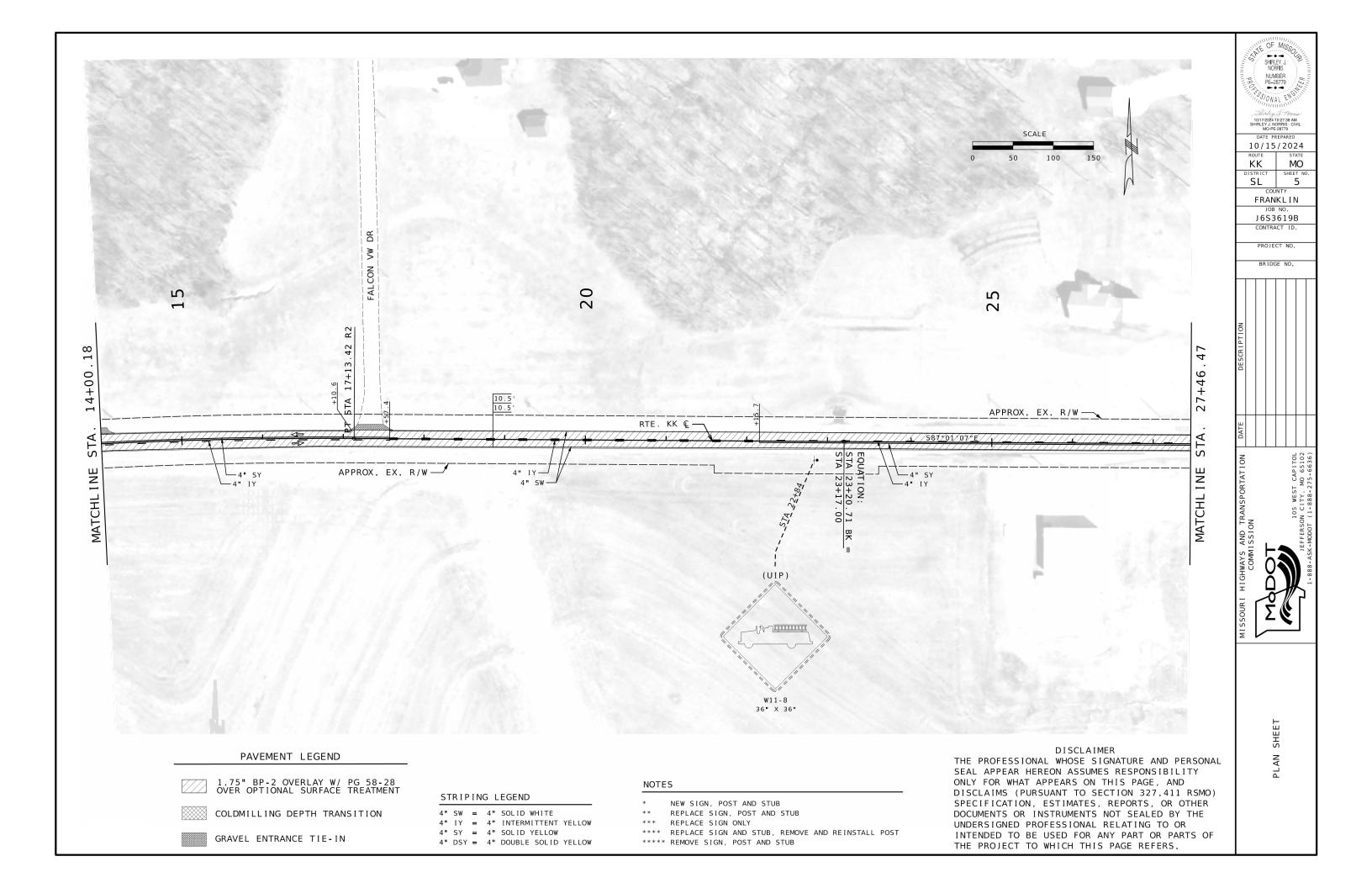
WET PAINT (ARROW PIVOTS)

END BLASTING ZONE

SHOULDER WORK / SHOULDER WORK AHEAD

TURN OFF 2-WAY RADIO AND PHONE





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***** REMOVE SIGN, POST AND STUB

STRIPING LEGEND

- 4" SW = 4" SOLID WHITE
- 4" IY = 4" INTERMITTENT YELLOW
- 4" SY = 4" SOLID YELLOW
- 4" DSY = 4" DOUBLE SOLID YELLOW

PAVEMENT LEGEND



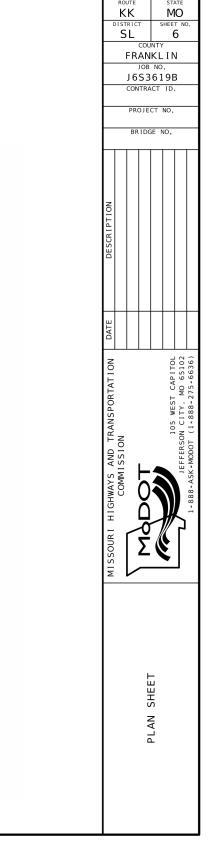
1.75" BP-2 OVERLAY W/ PG 58-28 OVER OPTIONAL SURFACE TREATMENT



COLDMILLING DEPTH TRANSITION

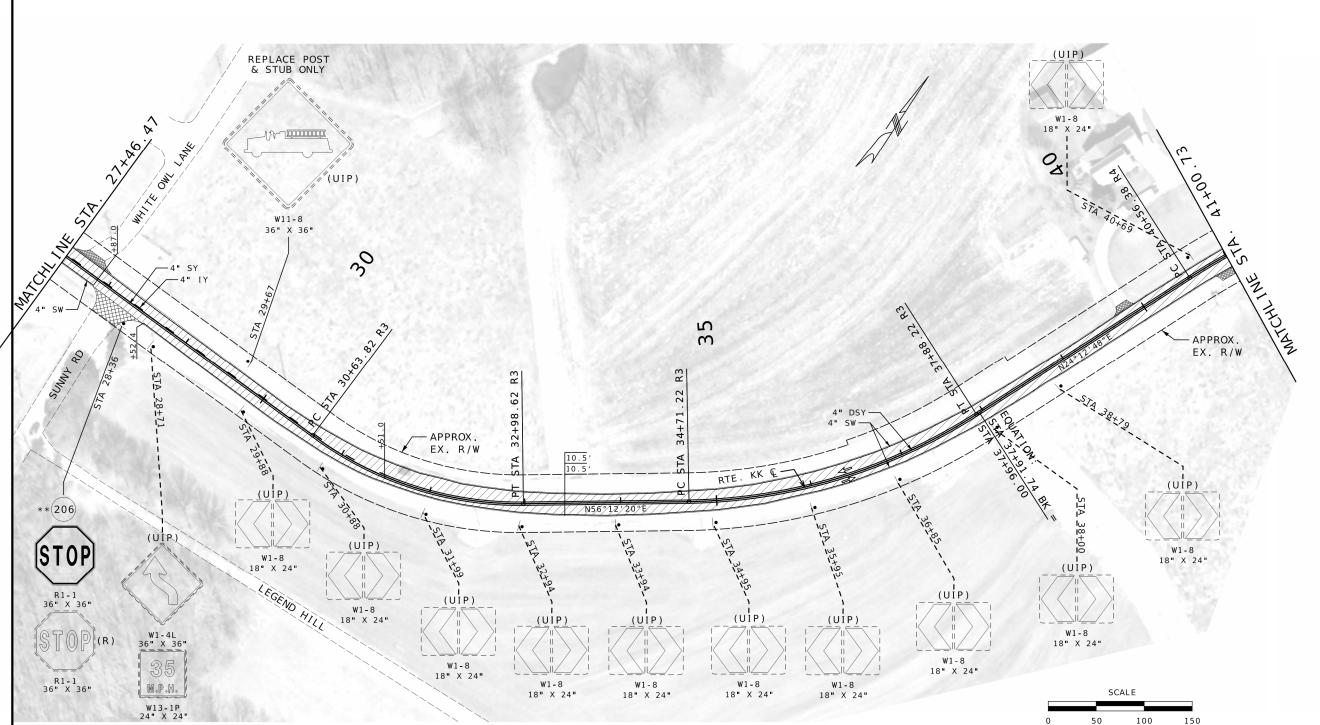


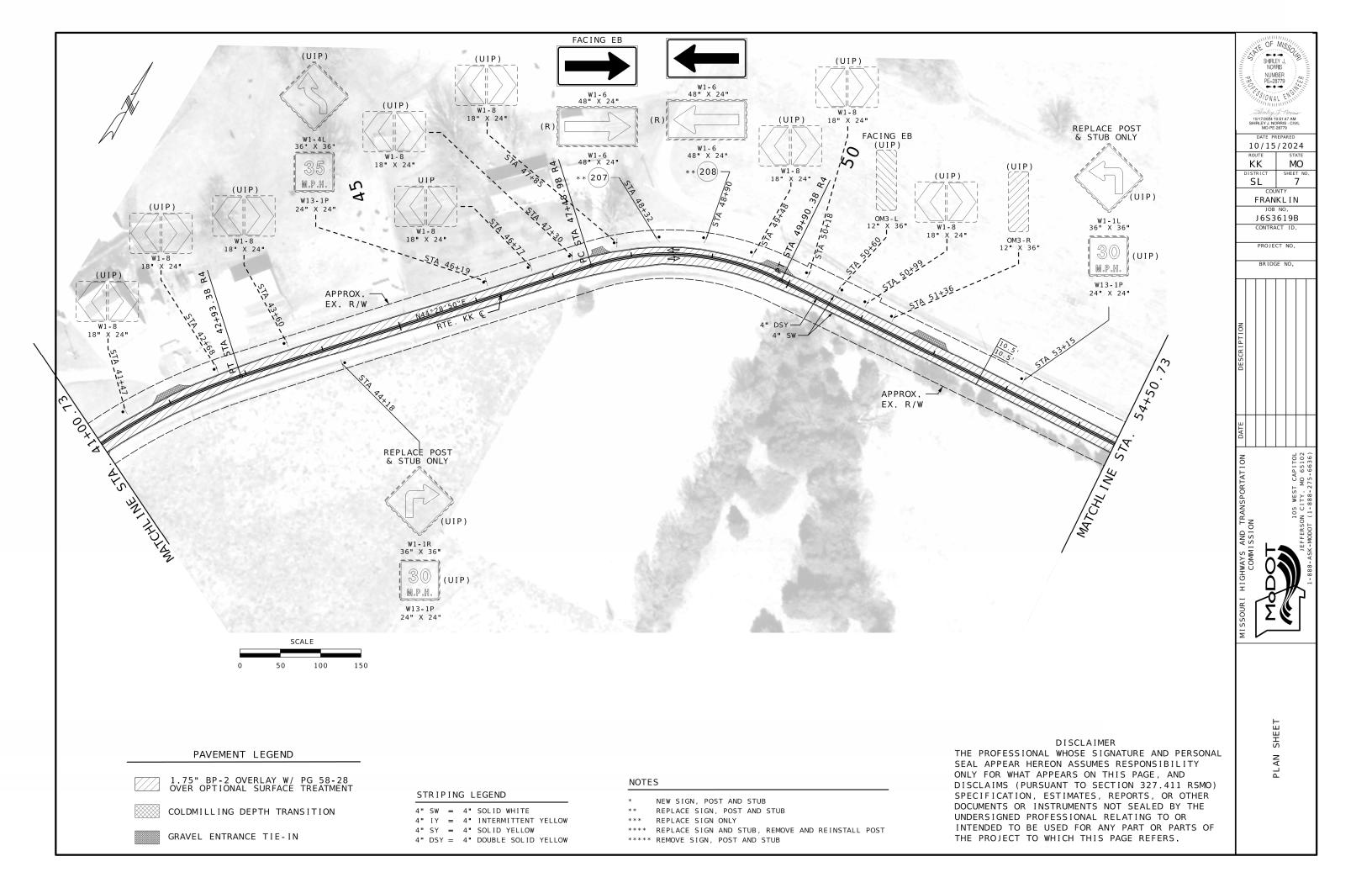
GRAVEL ENTRANCE TIE-IN

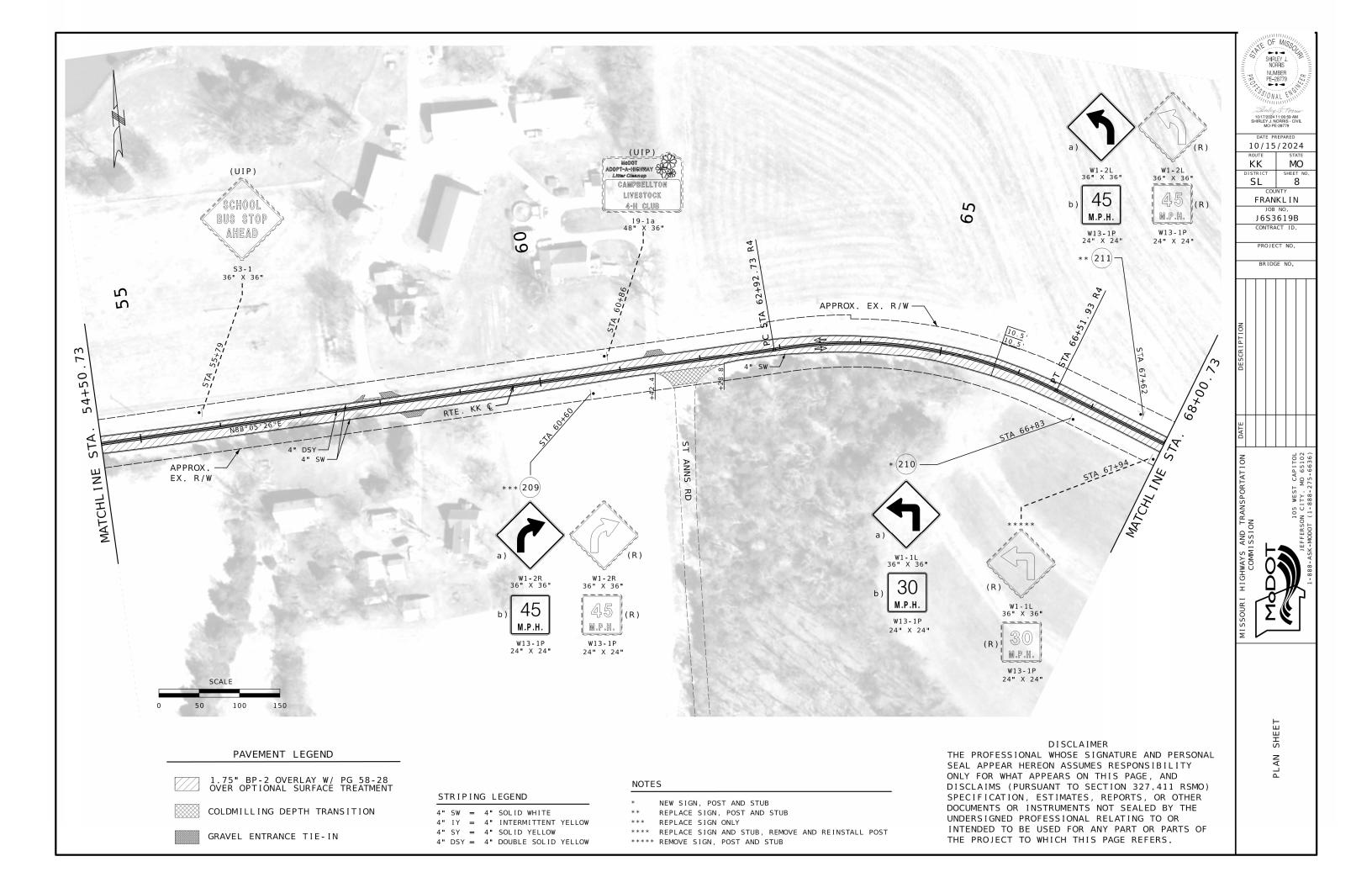


SHIRLEY J. NORRIS

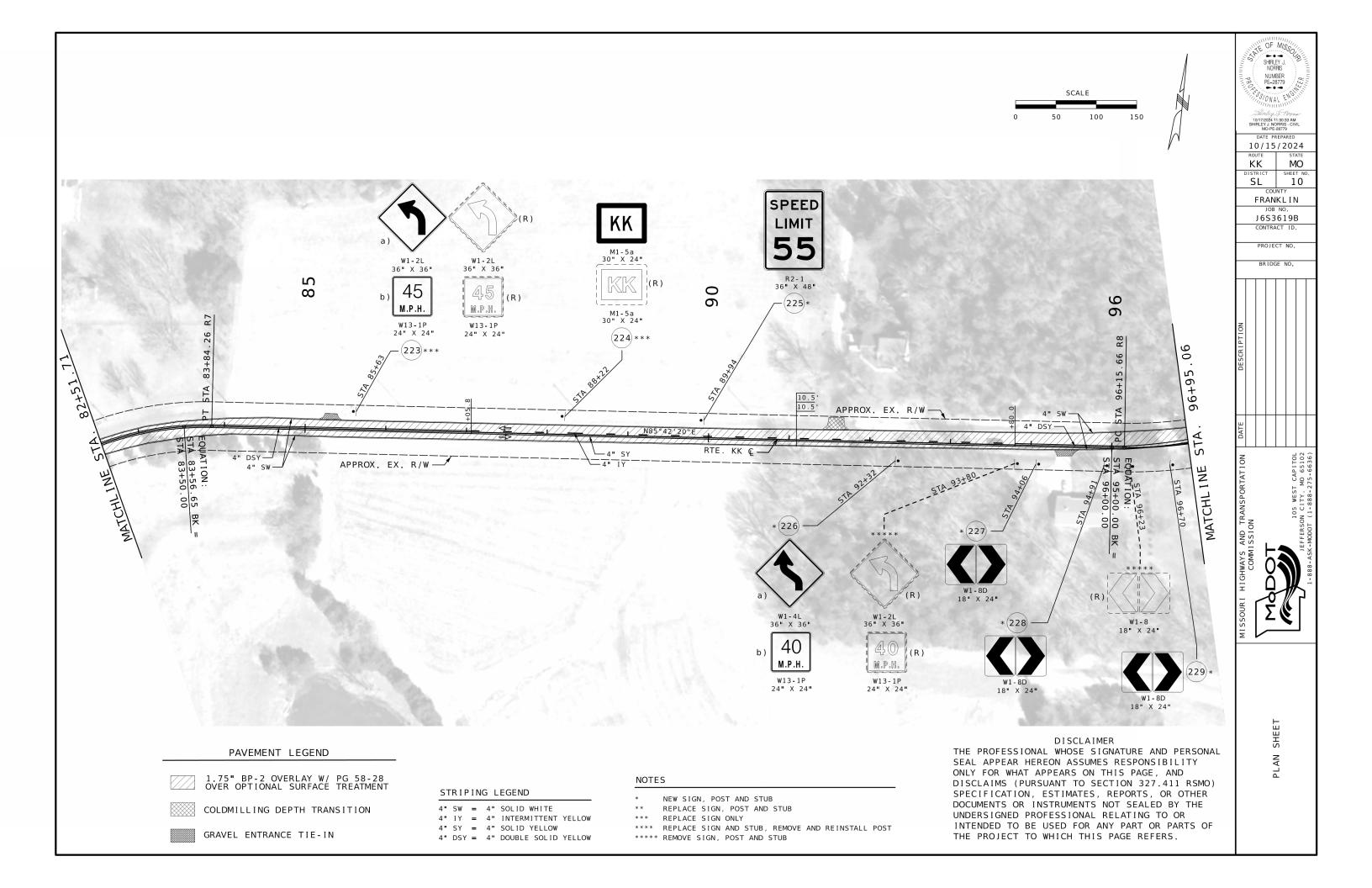
10/15/2024

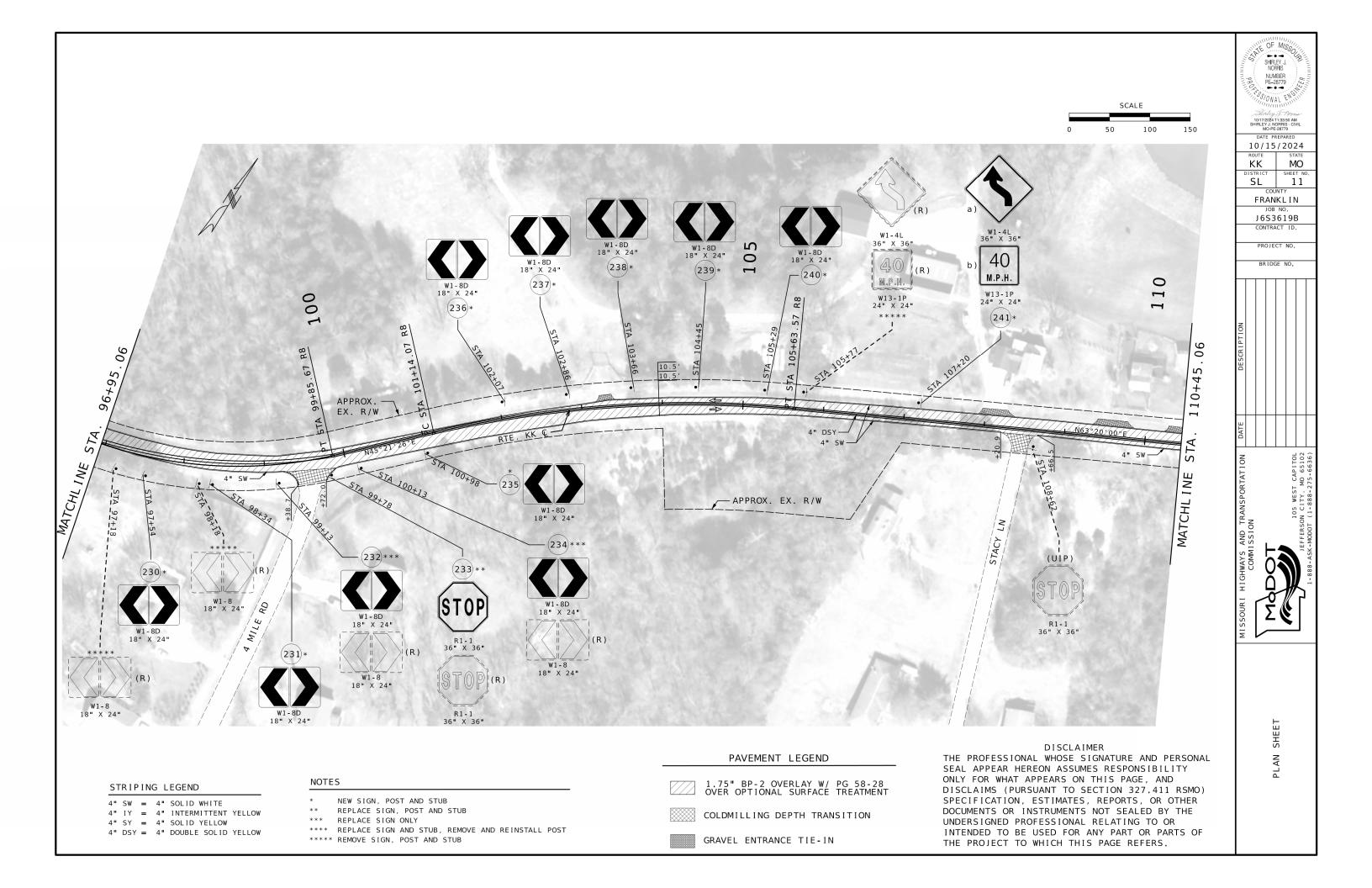






DISCLAIMER PAVEMENT LEGEND NOTES THE PROFESSIONAL WHOSE SIGNATURE AND PERSONAL SEAL APPEAR HEREON ASSUMES RESPONSIBILITY NEW SIGN, POST AND STUB 1.75" BP-2 OVERLAY W/ PG 58-28 OVER OPTIONAL SURFACE TREATMENT REPLACE SIGN, POST AND STUB ONLY FOR WHAT APPEARS ON THIS PAGE, AND *** REPLACE SIGN ONLY DISCLAIMS (PURSUANT TO SECTION 327.411 RSMO) **** REPLACE SIGN AND STUB, REMOVE AND REINSTALL POST SPECIFICATION, ESTIMATES, REPORTS, OR OTHER COLDMILLING DEPTH TRANSITION ***** REMOVE SIGN, POST AND STUB DOCUMENTS OR INSTRUMENTS NOT SEALED BY THE UNDERSIGNED PROFESSIONAL RELATING TO OR GRAVEL ENTRANCE TIE-IN INTENDED TO BE USED FOR ANY PART OR PARTS OF THE PROJECT TO WHICH THIS PAGE REFERS. 10/15/2024 ΚK MO SL 9 STRIPING LEGEND 4" SW = 4" SOLID WHITE FRANKLIN 4" IY = 4" INTERMITTENT YELLOW 4" SY = 4" SOLID YELLOW J6S3619B (UIP) 4" DSY = 4" DOUBLE SOLID YELLOW CONTRACT ID, PROJECT NO. 30 W1-1R 36" X 36" b) M.P.H. 30 W13-1P 24" X 24" M.P.H. ALL IS CHALMERS INSTALL 4" X 72" SIGN POST DELINEATOR W13-1P (DL-3 YELLOW) 24" X 24" **** OM3-R OM3-R 12" X 36" 12" X 36" (221)* APPROX. (217) * * * 5 4" DSY-APPROX. EX. R/W (222)*** \$74 73+22 (218)** FACING WB SHEET W1-2R 36" X 36" W1-2R W1-6 48" X 24" W1-6 48" X 24" M.P.H. W13-1P W13-1P 24" X 24" 18" X 24" W1-8 18" X 24"





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STRIPING LEGEND

4" SW = 4" SOLID WHITE

4" IY = 4" INTERMITTENT YELLOW 4" SY = 4" SOLID YELLOW

4" DSY = 4" DOUBLE SOLID YELLOW

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- REPLACE SIGN, POST AND STUB
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***** REMOVE SIGN, POST AND STUB

PAVEMENT LEGEND

1.75" BP-2 OVERLAY W/ PG 58-28 OVER OPTIONAL SURFACE TREATMENT



COLDMILLING DEPTH TRANSITION

GRAVEL ENTRANCE TIE-IN



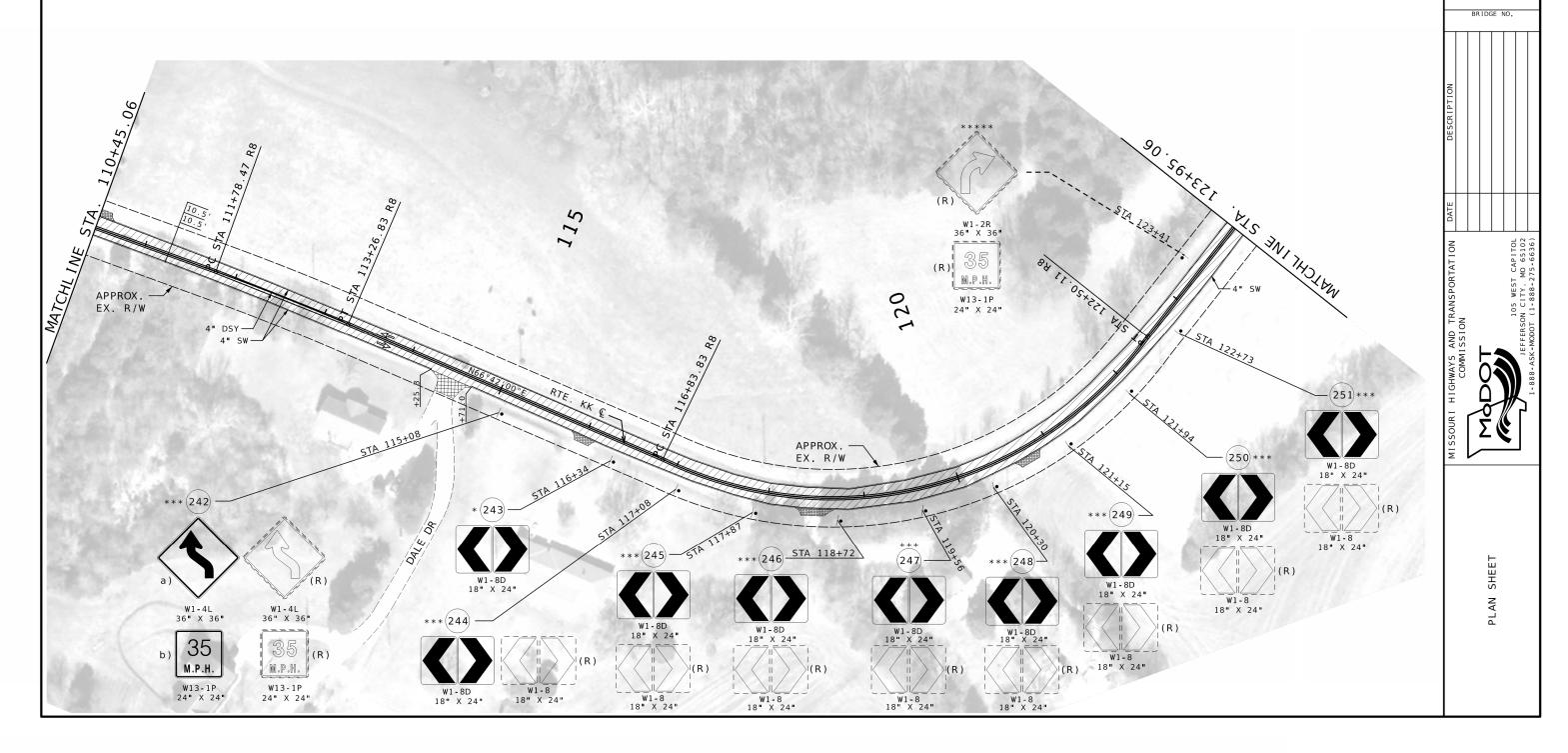


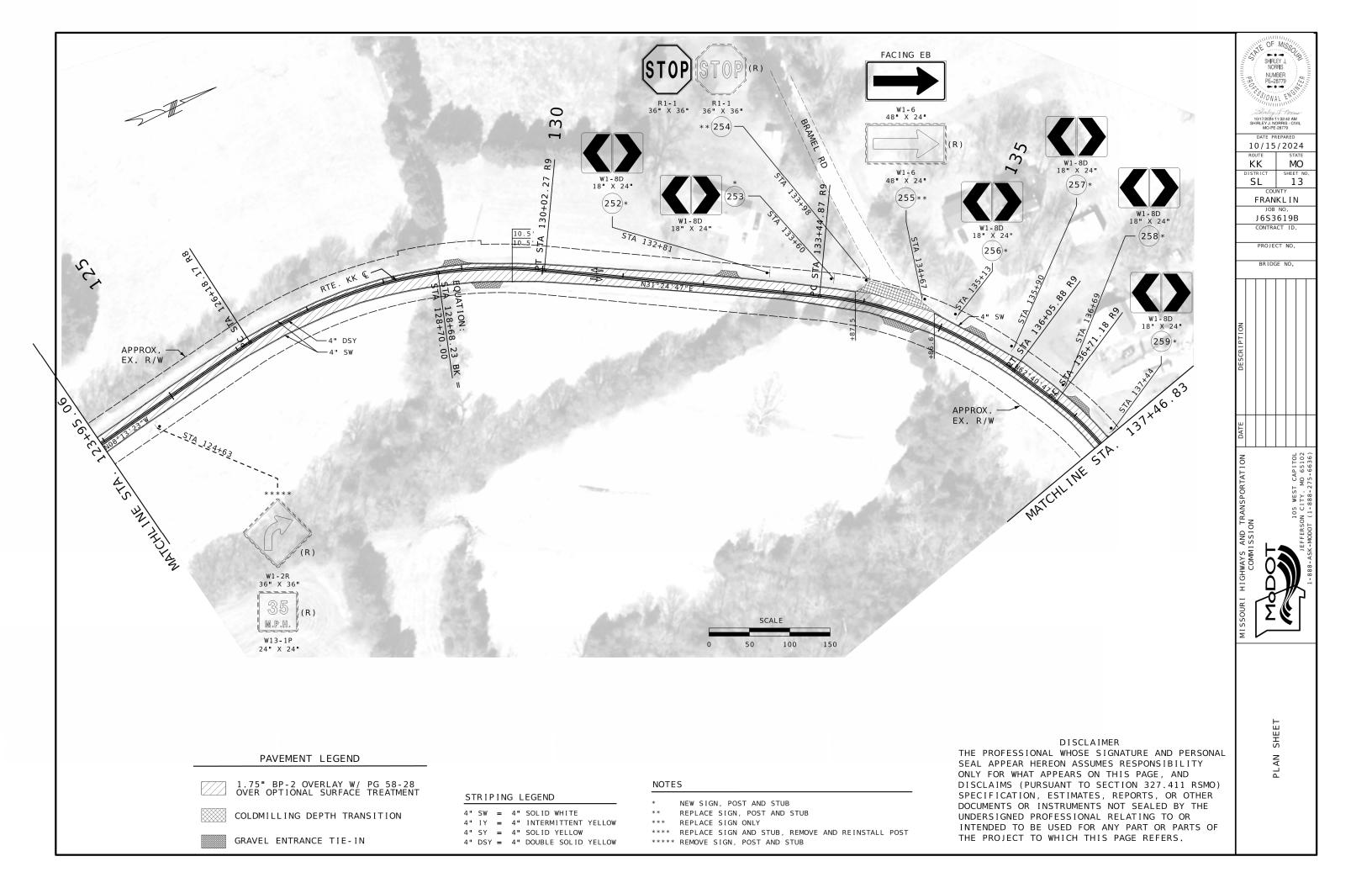
PE-co.

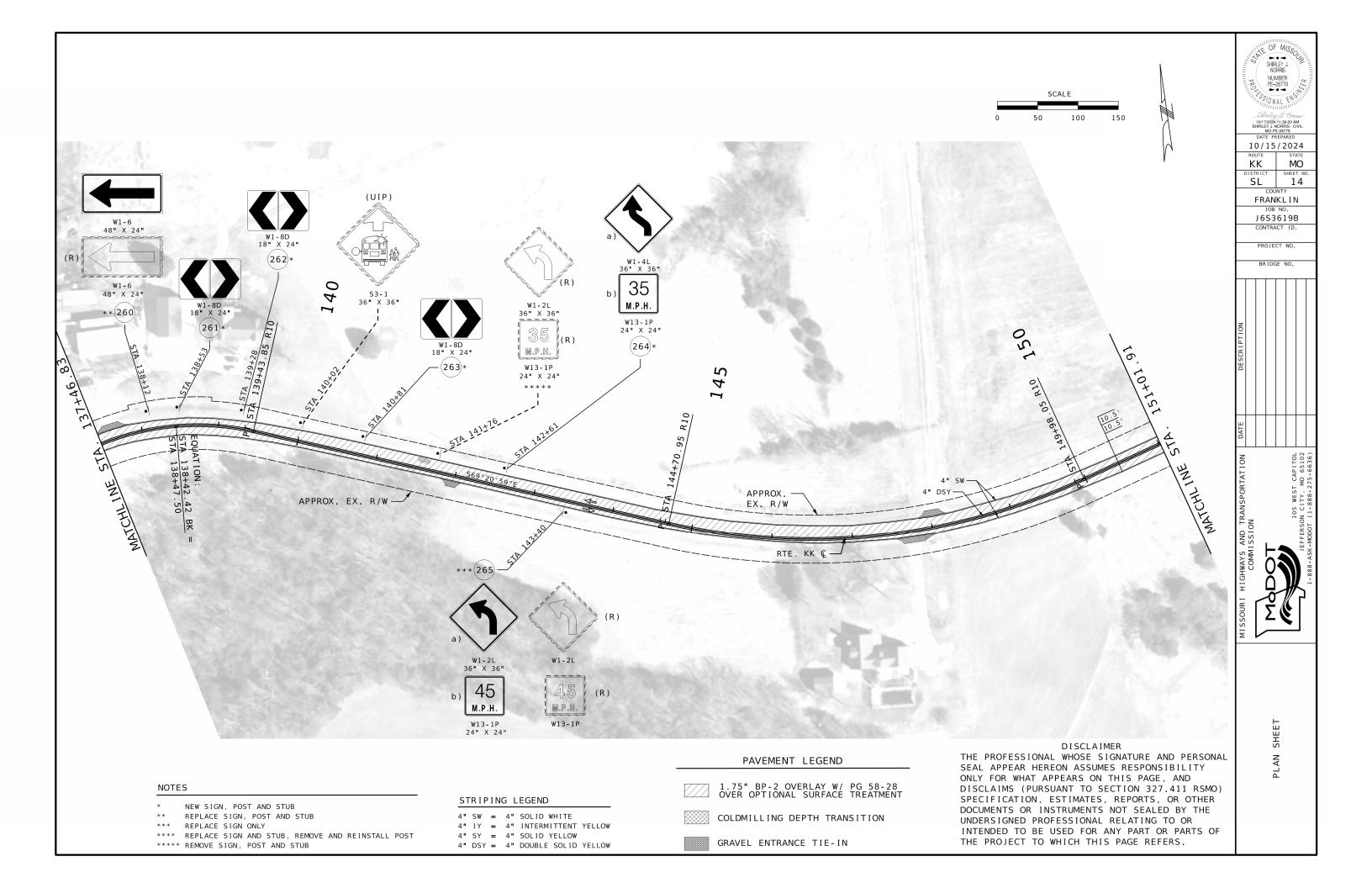
12 FRANKLIN

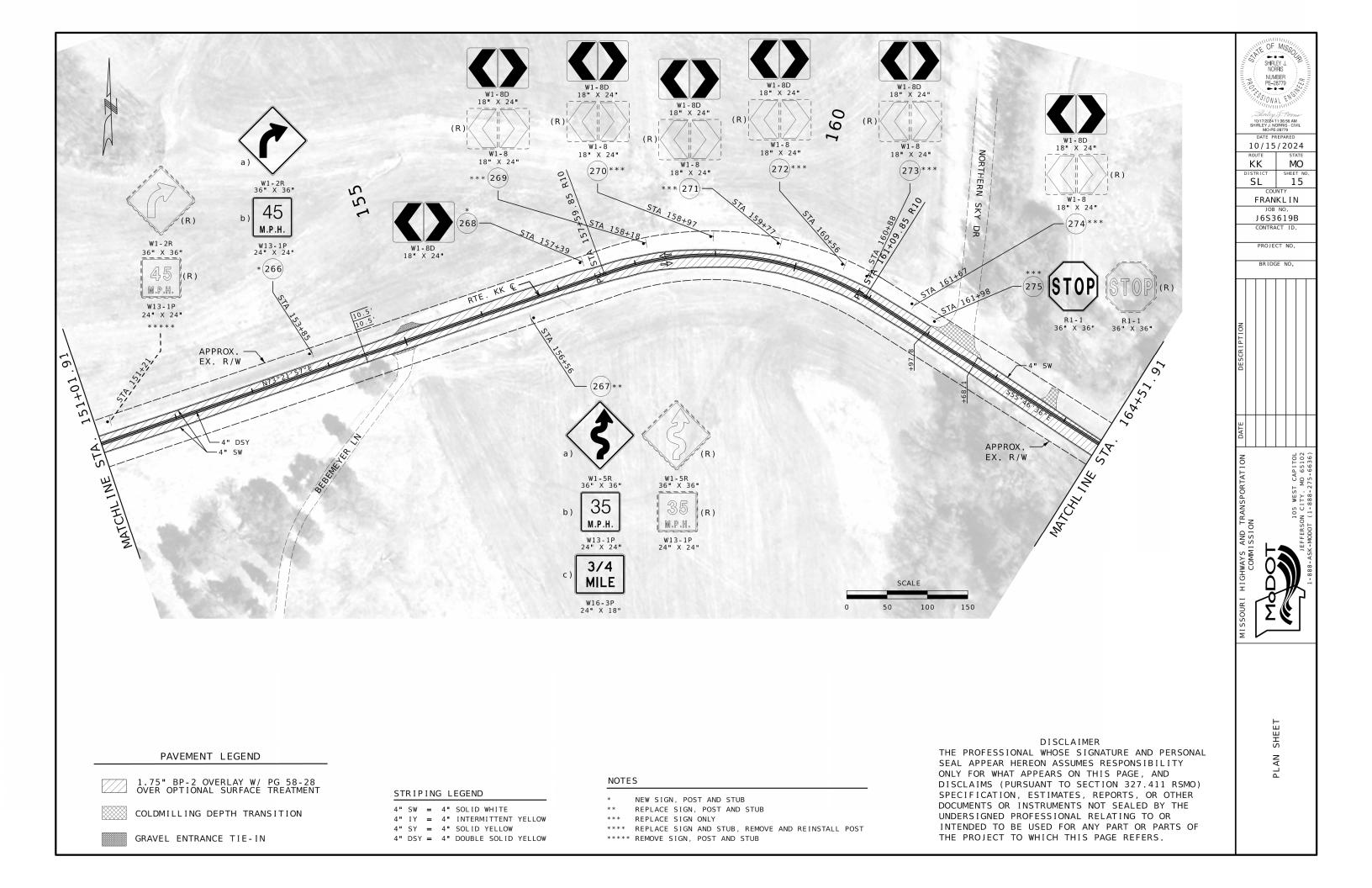
J6S3619B CONTRACT ID,

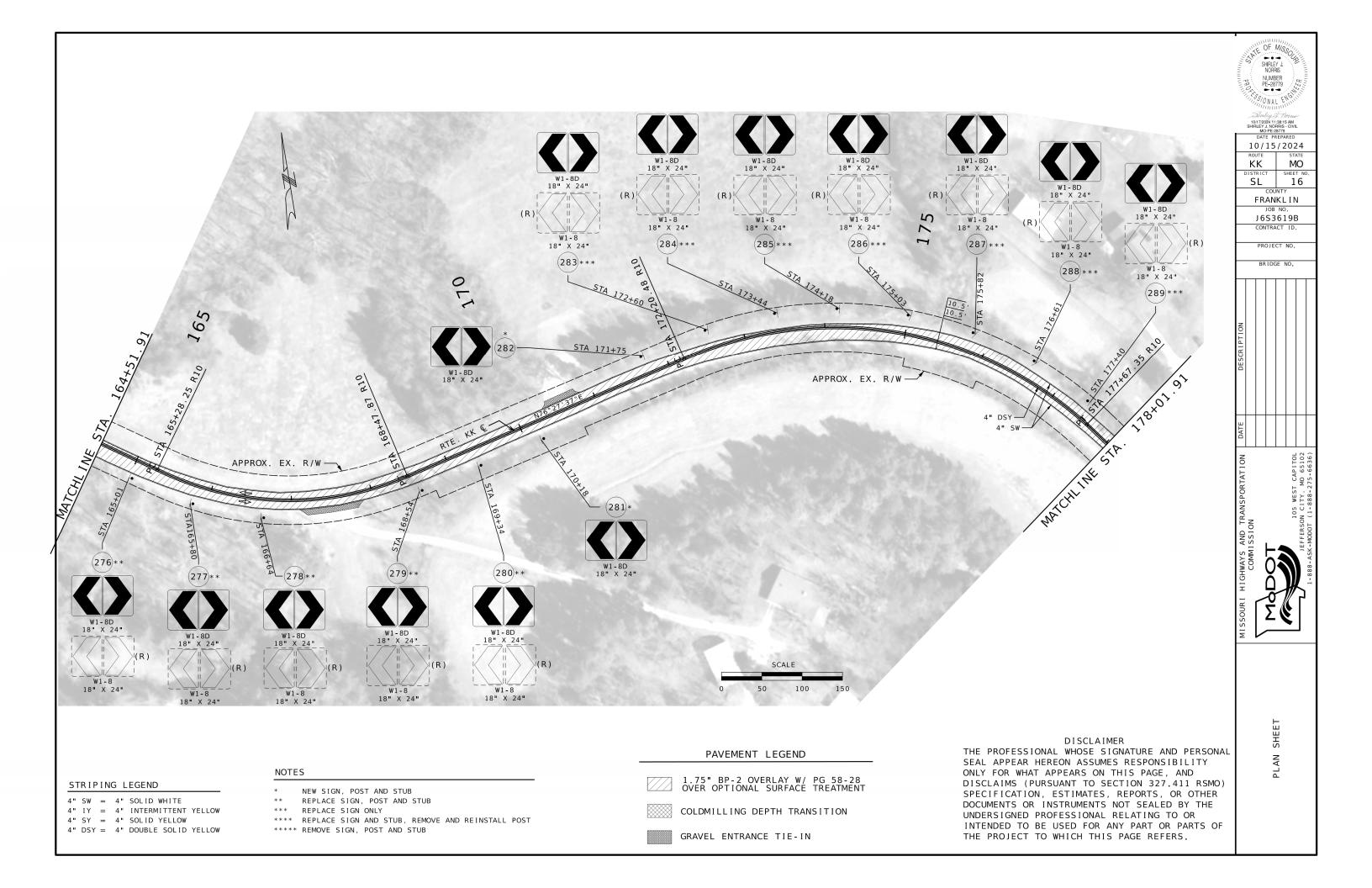
PROJECT NO.











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- ***** REMOVE SIGN, POST AND STUB

STRIPING LEGEND

- 4" SW = 4" SOLID WHITE
- 4" IY = 4" INTERMITTENT YELLOW
- 4" SY = 4" SOLID YELLOW
- 4" DSY = 4" DOUBLE SOLID YELLOW
- 24" WHITE STOP BAR
 - (PREFORMED THERMOPLASTIC)

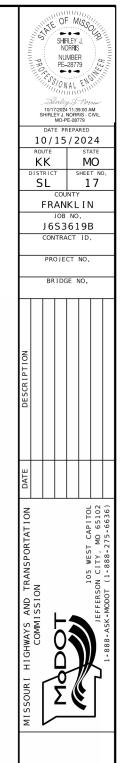
PAVEMENT LEGEND

1.75" BP-2 OVERLAY W/ PG 58-28 OVER OPTIONAL SURFACE TREATMENT



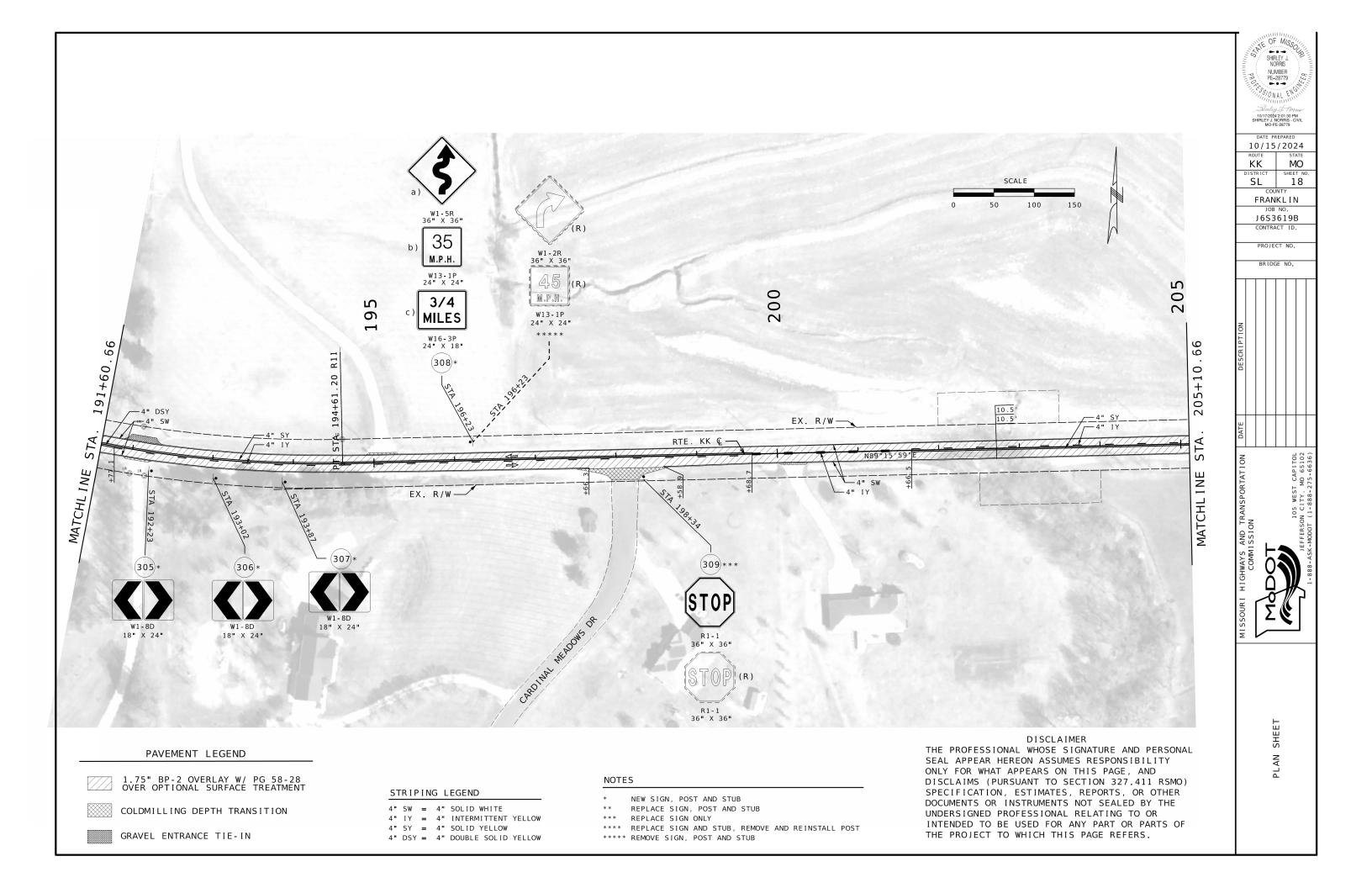
COLDMILLING DEPTH TRANSITION

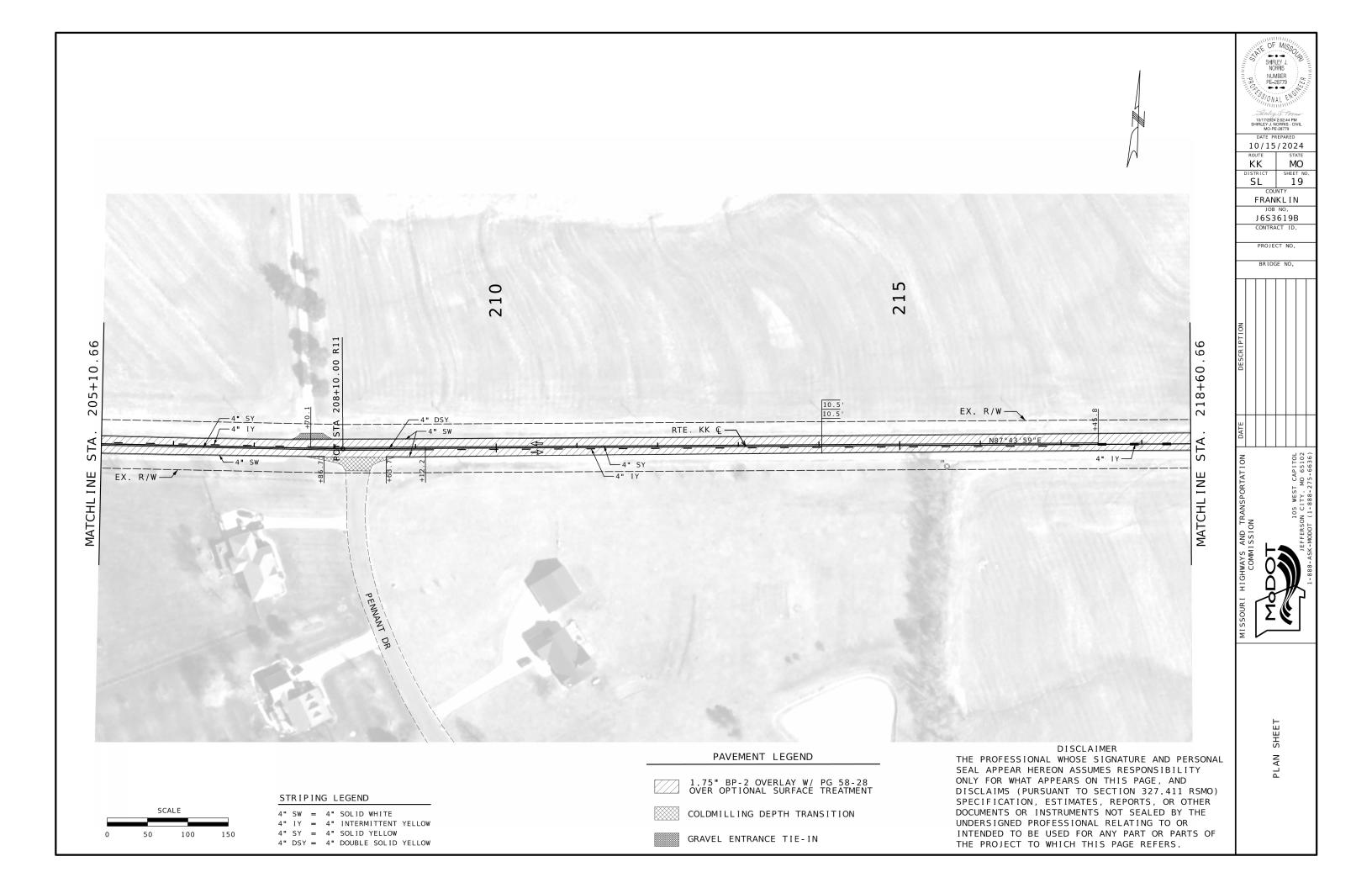
GRAVEL ENTRANCE TIE-IN

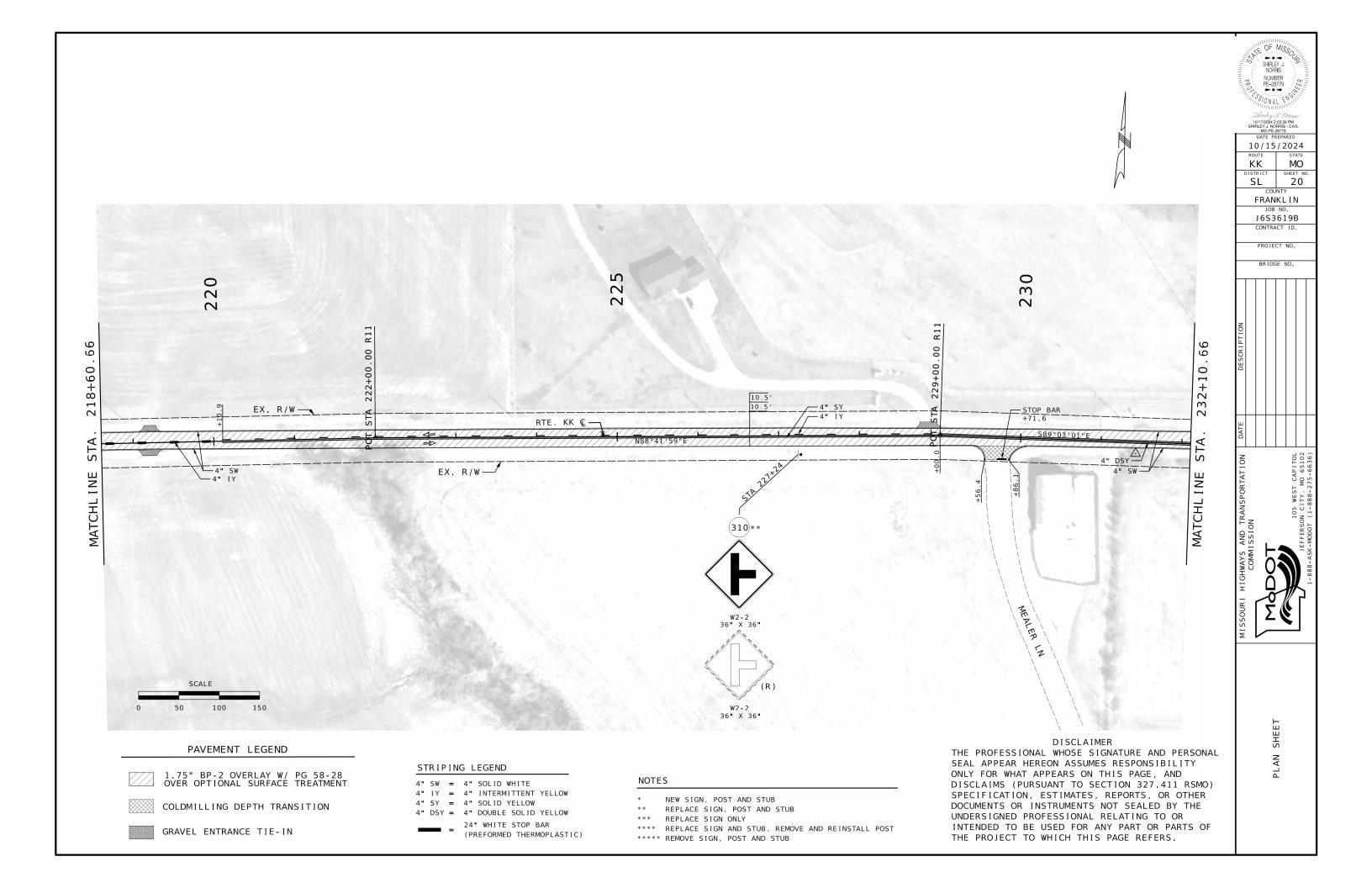


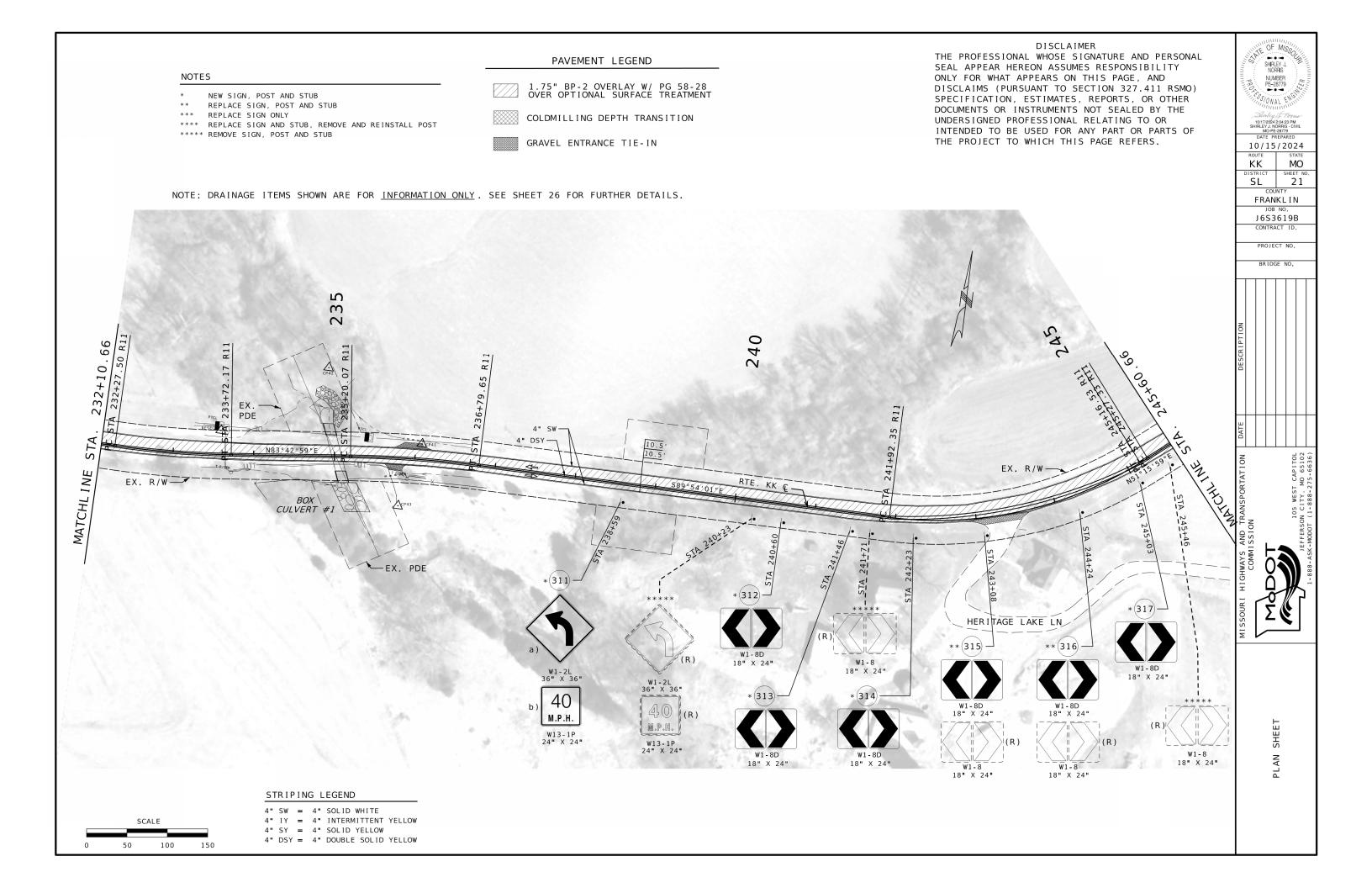
			J 6 CON	COL	KL NO 61	INO.	7 N 3	o
	DESCRIPTION		bK.					
	DATE							
MATCHLINES	MISSOURI HIGHWAYS AND TRANSPORTATION	COMMISSION				105 WEST CAPITOL	JEFFERSON CITY, MO 65102	1-888-ASK-MODOT (1-888-275-6636)
				H L L L L L L L L L L L L L L L L L L L	PLAN SHEEL			

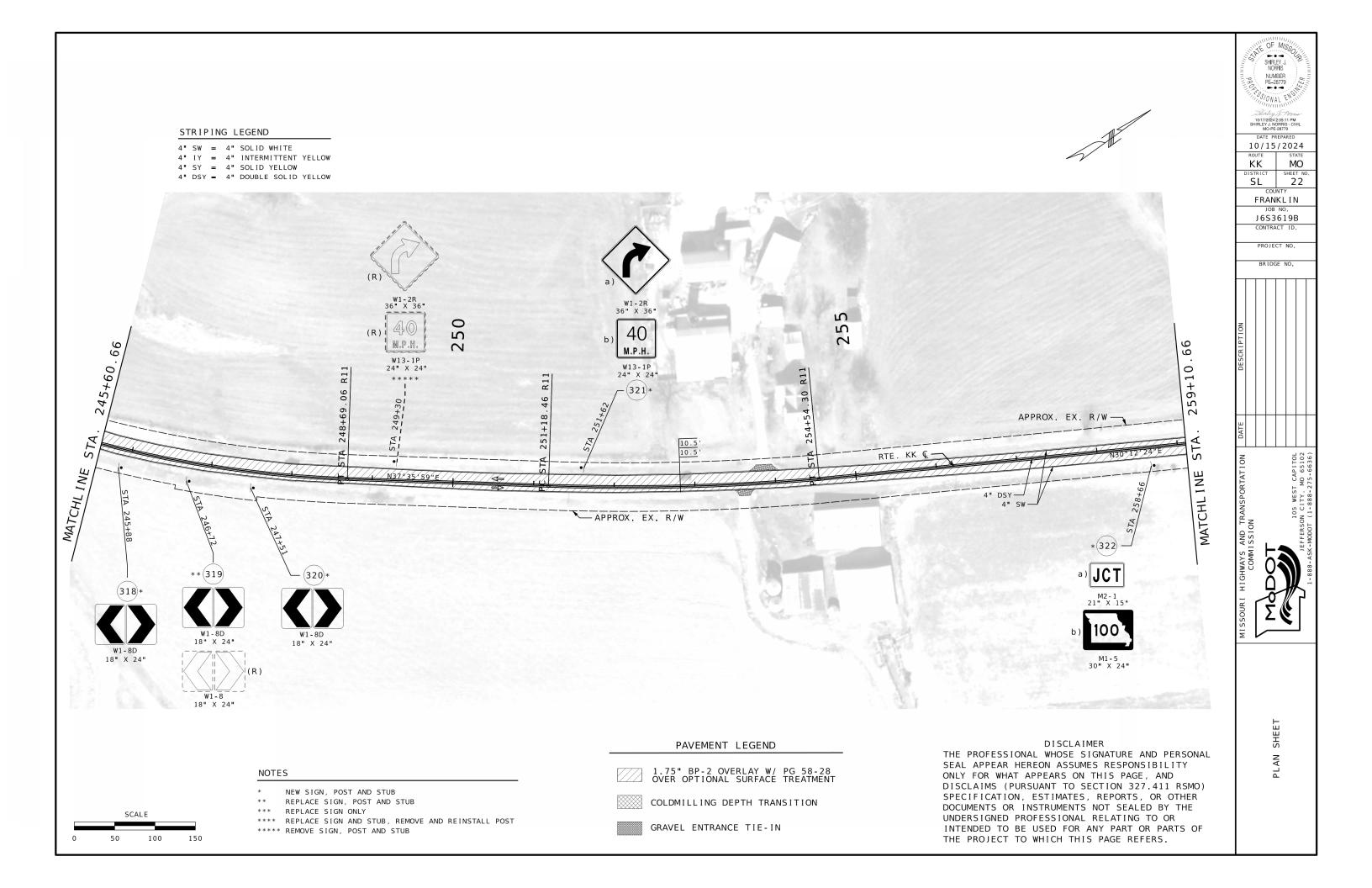
W1-5R 36" X 36" 35 (R) 190 M.P.H. (290)* W13-1P 24" X 24" 2 APPROX. - ∞ EX. R/W APPROX. EX. R/W 4" DSY STOP BAR-*(302) *(292) (295)*** W1-8D 18" X 24" KOPP W1-8D 18" X 24" *(298) R1-1 36" X 36" W1-8D 18" X 24" W1-8D 18" X 24" (R) R1-1 36" X 36"

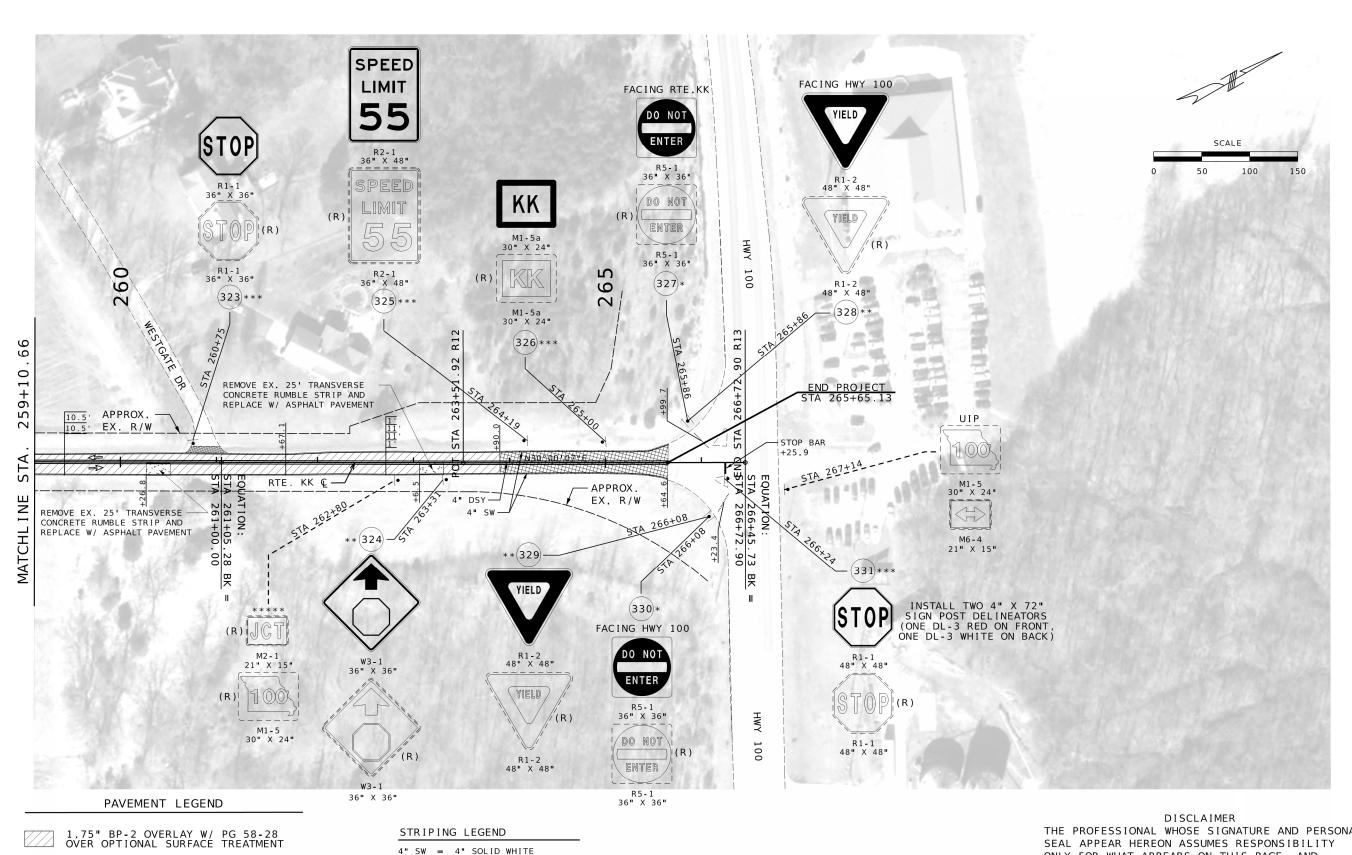












NOTES

NEW SIGN, POST AND STUB

***** REMOVE SIGN, POST AND STUB

*** REPLACE SIGN ONLY

REPLACE SIGN, POST AND STUB

**** REPLACE SIGN AND STUB, REMOVE AND REINSTALL POST

4" IY = 4" INTERMITTENT YELLOW

4" DSY = 4" DOUBLE SOLID YELLOW

24" WHITE STOP BAR

(PREFORMED THERMOPLASTIC)

12" WHITE YEILD TRIANGLES (PREFORMED THERMOPLASTIC)

4" SY = 4" SOLID YELLOW

COLDMILLING DEPTH TRANSITION

GRAVEL ENTRANCE TIE-IN

TRANSVERSE CONCRETE RUMBLE STRIP

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PLAN SHEET

SHRLEY J. NORTHS NUMBER PE-2879

10/15/2024

FRANKLIN
JOB NO.
J6S3619B

CONTRACT ID,

PROJECT NO.

BRIDGE NO.

MO SHEET NO 23

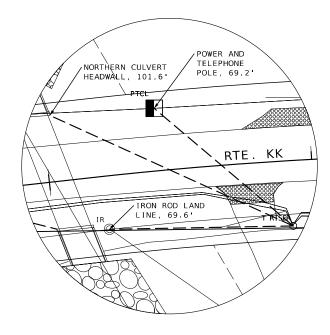
ΚK

SL

REFERENCE POINTS



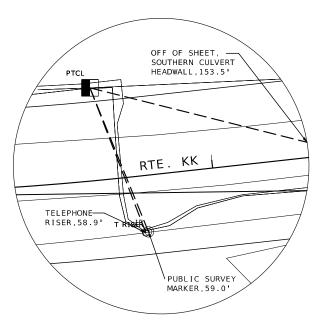
REFERENCE POINTS NOT TO SCALE



WP 1

TELEPHONE RISER, EAST OF EX. BOX CULVERT 4 RT OF RTE. KK ©

N: 656776.1910 E: 993479.9730 ELEV = 505.4580'



WP 2

POWER & TELEPHONE POLE, WEST OF EX. BOX CULVERT 4, LT OF RTE. KK ©

> N: 993516.0400' E: 656537.5120' ELEV = 523.6200'

DOURT HIGHWAYS AND TRANSPORTATION
COMMISSION
BEIDGE NO.

BEIDGE NO.

BEIDGE NO.

CASE TO A PROPER TO A PROPER TO A PROPER TO A PROPERTY OF A P

POINTS

SHIRLEY J. NORRIS NUMBER PE-28779

10/15/2024

FRANKLIN

MO

24

KK

SL

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NOTE: COORDINATES LISTED ARE NOTE MODIFIED STATE PLANE MODIFIED STATE PLANE ALL PROJECT COORDINATES HAVE BEEN PROJECTED FROM THE MISSOURI STATE PLANE COORDINATE (SPC) SYSTEM OF 1983 USING AN AVERAGE PROJECT PROJECTION (GRID TO GROUND) FACTOR. TO GET BACK TO STATE PLANE COORDINATES MULTIPY THE PROJECT COORDINATES BY THE AVERAGE GRID FACTOR AS SHOWN IN THE "REFERENCE CONTROL INFORMATION" PORTION OF THIS TABLE.

PROJECT COORDIN	ATE INFOR	MATION			
COORDINATE SYSTEM	MODIFIED S	TATE PLANE	(GROUN	ND)	
HORIZONTAL DATUM	NAD 83 (20	11) EPOCH	2010.0		
VERTICAL DATUM	NAVD 1988;	GNSS DER	I VED		
GEOID MODEL	GEOID 18 (CONUS)			
ELEVATIONS	GNSS				
DETERMINED BY	GNSS				
PROJECT PROJECTION	FACTOR	1.0000649			
REFERENCE CONTR	OL INFORM	1AT I ON			
COORDINATE SYSTEM	MISSOURI C	OORDINATE	SYSTEM	OF	1983
CONTROL STATION	MISSOURI C	ORS			

REFERENCE CON	TRC	L INF	ORI	MATION							
COORDINATE SYSTE	M I	41SSOUR	I	COORDINATE	SYSTEM	OF	1983				
CONTROL STATION	1	MISSOURI CORS									
DESIGNATION	MOE	MODOT DRAKE CORS ARP									
CORS_ID	MOE	MODR									
PID	DL6	DL6300									
LATITUDE	38€	38�27'45.47416" N									
LONGITUDE	091	091�27'54.94360" W									
NORTHING (M)	292	222.72	40								
EASTING (M)	165	758.73	60								
ZONE	EAS	T									
PROJECT AVERAGE	GR I	D FACTO)R	0.9999351	0						

EXAMPLE OF PROJECT COORDINATE TO S.P.C.

PROJECT NORTHING X AVERAGE GRID FACTOR = STATE PLANE NORTHING PROJECT EASTING X AVERAGE GRID FACTOR = STATE PLANE EASTING

EXAMPLE: CONTROL POINT # 1

 $N 999835.528 \times 0.9999351 = N 999770.6387$ $E 617321.295 \times 0.9999351 = E 617281.2308$

LINEAR UNIT CONVERSION

1 METER = 3.280833333 US SURVEY FEET (USFT)

				COOR	DINATE POINT	LISTING		
CUEET				MOD I F I E	ED STATE PLANE	(GROUND)		GPK
SHEET NO.	STATION	LOCATION	OFFSET (US FT)	NORTHING (US SURVEY FT)	EASTING (US SURVEY FT)	ELEVATION (US SURVEY FT)	DESCRIPTION	POINT
PRO JEC.	T CONTROL PO	INTS						
21	233+58.68	CL ROUTE KK	29.76 LT	993510.7610'	656540.1560'	522.6200'	"TEMP"	RESEC1
21	236+07.26	CL ROUTE KK	19.27 LT	993524.8800'	656789.4530'	508.2240'	COORDINATE POINT 41	CP41
21	234+91.72	CL ROUTE KK	109.20 LT	993604 1690'	656663.3570	504.6410	COORDINATE POINT 42	CP42
21	235+82.11	CL ROUTE KK	58.78' RT	993445.5310'	656769.1800'	503.1640'	COORDINATE POINT 43	CP43
AL I GNMI	ENTS							
4	0+47.60	CL ROUTE KK	-	990446.6565'	636365.3885'	-	BEGIN RTE. KK CENTERLINE	-
4	0+58.37	CL ROUTE KK	-	990446.7509'	636376.1619'	-	BEGIN PROJECT LIMITS	-
21	235+02.98	CL ROUTE KK	-	993496.8573'	656686.4991'	-	RTE. KK CL AT EX. BOX CULVERT #1	-
23	266+36.32	CL ROUTE KK	-	995363.4931	658860.1602	-	END PROJECT LIMITS	-
23	266+72.90	CL ROUTE KK	-	995371.6468'	658864.8681	-	END RTE. KK CENTERLINE (STA EQ)	-

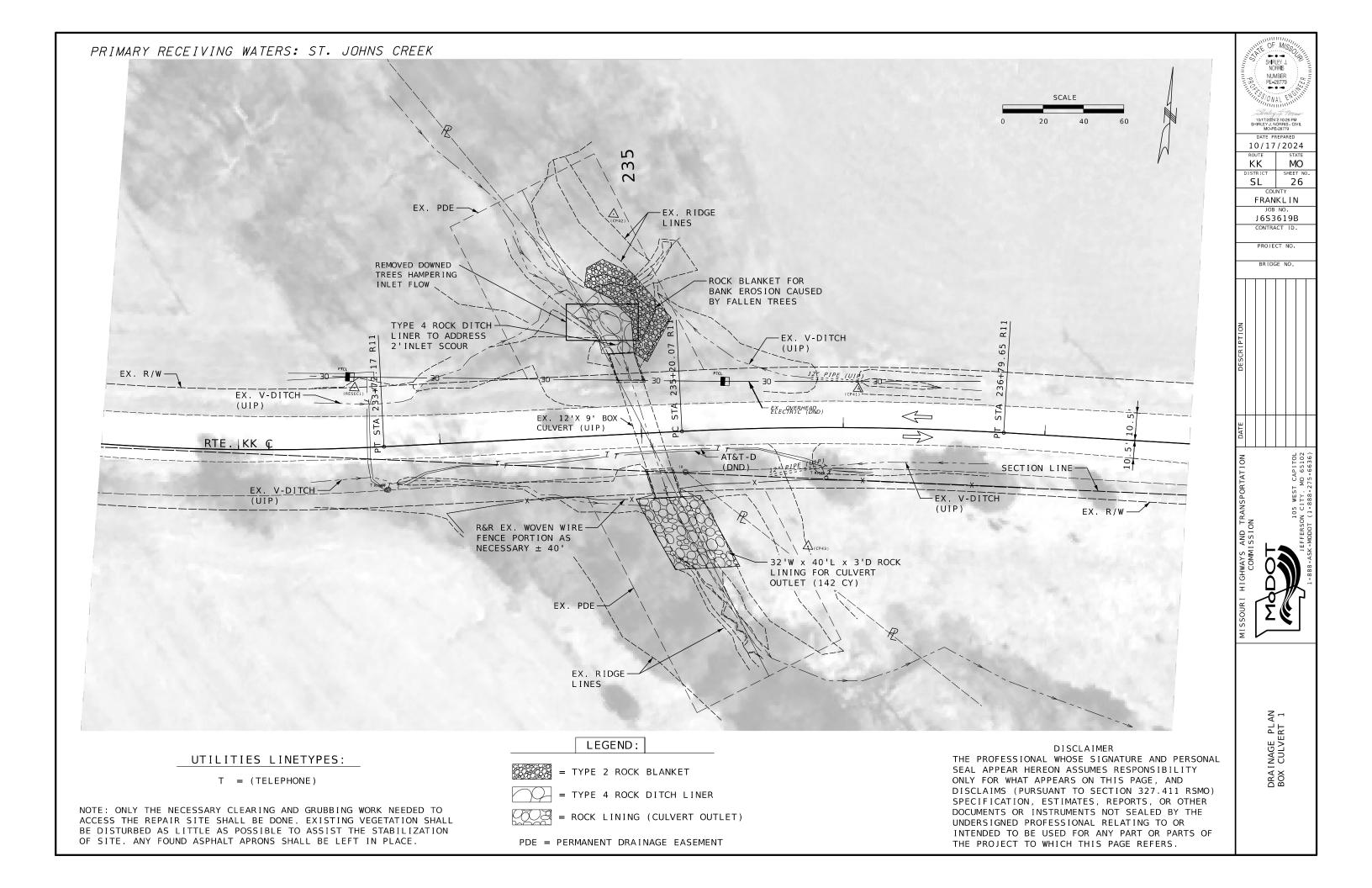
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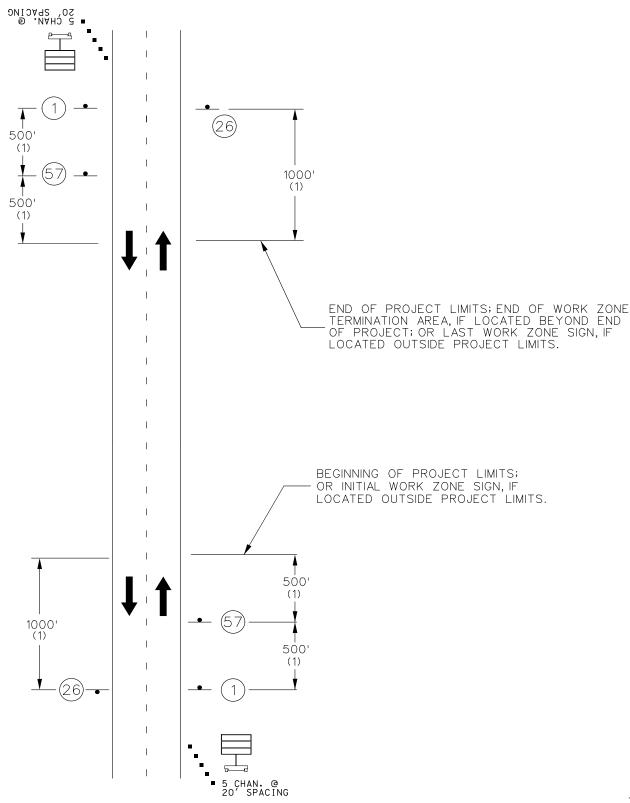
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SHIRLEY J. 11/20/2024 KK MO SL 25 FRANKLIN J6S3619B CONTRACT ID. PROJECT NO. BRIDGE NO.

> SHEET POINTS



BEGIN/END OF PROJECT SIGNING



SIGN GO20-1 IS REQUIRED PER EPG 616.6.56.

SIGN GO20-2 IS USED ON ALL PROJECTS WHERE SIGN GO20-1 IS USED.

OTHER SIGNS SUCH AS DETOUR OR ALTERNATE ROUTE SIGNING MAY BE USED OUTSIDE THE PROJECT LIMITS.

ANY EXISTING SIGNING THAT CONFLICTS WITH THE TRAFFIC CONTROL SIGNING SHALL BE COMPLETELY COVERED OR REMOVED.

(1) DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS. WHERE TRAFFIC BACKUPS ARE EXPECTED BEYOND THE ADVANCE WARNING AREA, ADDITIONAL SIGNING MAY BE NEEDED.

(2) THE "WORK ZONE NO PHONE ZONE" SIGN IS PLACED A MINIMUM OF 500 FEET BEFORE THE ROAD WORK AHEAD SIGN.

NO DIRECT PAY WILL BE MADE FOR THE RELOCATION OF CONSTRUCTION SIGNS OR DEVICES.

SIGN AND DEVICE LOCATION WILL BE FIELD VERIFIED AND APPROVED BY THE ENGINEER.

CMS LOCATIONS TO BE DETERMINED BY THE ENGINEER

SHIRLEY J. NORRIS NUMBER PE-28779 SSONAL ENG 10/15/2024 KK MO SHEET NO 27 SL FRANKLIN LOB NO J6S3619B

CONTRACT ID.

PROJECT NO. BRIDGE NO.

TRAFFIC CONTROL SHEET BEGIN/END OF PROJECT SIGNING

ROAD WORK NEXT 2 MILES GO20-1

WORK ZONE NO PHONE ZONE

CONST-8 (2) (57)

G020-2 (26)

END

ROAD WORK

TRAFFIC CONTROL LEGEND

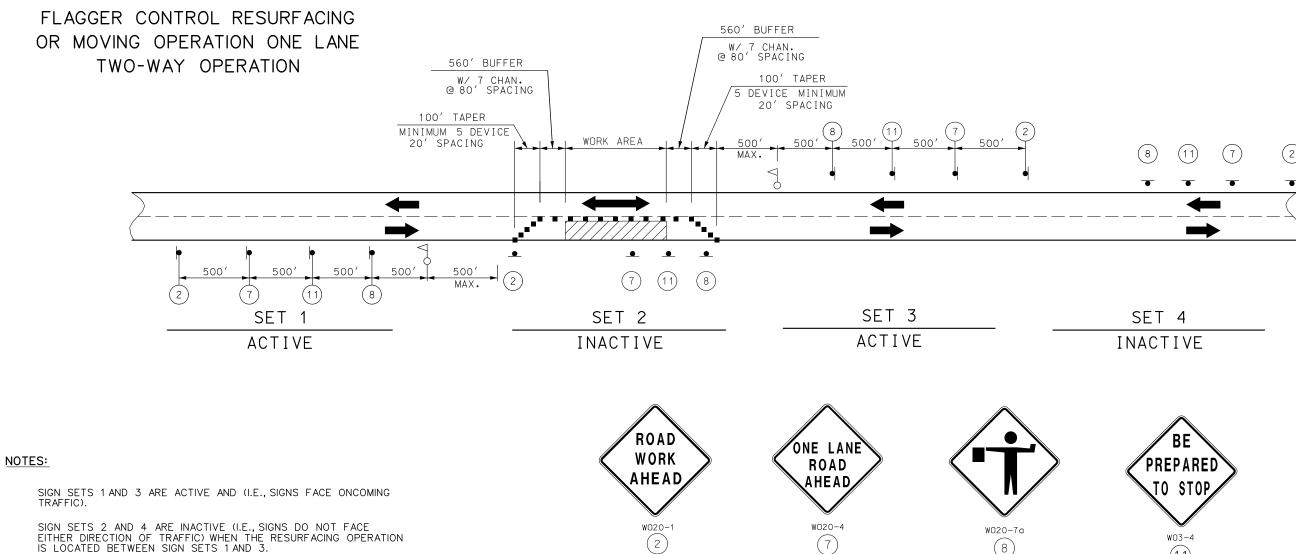
CHANNELIZER (TRIM-LINE)

CMS (CHANGEABLE MESSAGE BOARD)

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SIGN (SINGLE SIDED)

NOT TO SCALE



WHEN SIGN SETS 2 AND 4 ARE ACTIVE, SIGN SETS 1 AND 3 BECOME INACTIVE AND ARE ADVANCED TO BECOME SETS 2 AND 4 WITH SIGN LEGENDS TURNED AWAY FROM BOTH DIRECTIONS OF TRAFFIC. WHEN THE RESURFACING OPERATION ADVANCES TO BETWEEN SIGN SET 2 AND 4. SIGN SETS 2 AND 4 BECOME ACTIVE (IE, NEW SIGN SETS 1 AND 3) AND SIGN SETS 1 AND 3 ADVANCED IN THE DIRECTION OF THE OPERATION (IE, NEW SIGN SETS 2 AND 4)

DAYLIGHT FLAGGING OPERATIONS ONLY.

CHANNELIZING DEVICES LOCATED DOWNSTREAM OF THE ONE-LANE, TWO-WAY TAPER ARE OPTIONAL. THESE DEVICES SHOULD BE ELIMINATED WHEN THEIR USE WILL REDUCE THE USABLE LANE WIDTH, INCLUDING ANY ACCEPTABLE SHOULDERS, TO LESS THAN 10' OR WILL SIGNIFICANTLY AFFECT THE RESURFACING OPERATION.

SPACINGS MAY BE ADJUSTED AS NECESSARY TO MEET FIELD CONDITIONS.

NO DIRECT PAY WILL BE MADE FOR THE RELOCATION OF CONSTRUCTION SIGNS OR DEVICES.

SIGN AND DEVICE LOCATION WILL BE FIELD VERIFIED AND APPROVED BY THE ENGINEER.

CMS LOCATIONS TO BE DETERMINED BY THE ENGINEER





TRAFFIC CONTROL LEGEND

SIGN (SINGLE SIDED)

CHANNELIZER (TRIM-LINE) WORK AREA

FLAGGER

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TRAFFIC CONTROL SHEET FLAGGER CONTROL MOVING OPERATION

SHIRLEY J. NORRIS

NUMBER PE-28779

10/17/2024 2:12:36 PM SHIRLEY J. NORRIS - CIV MO-PE-28779

10/15/2024

FRANKLIN J6S3619B CONTRACT ID. PROJECT NO BRIDGE NO

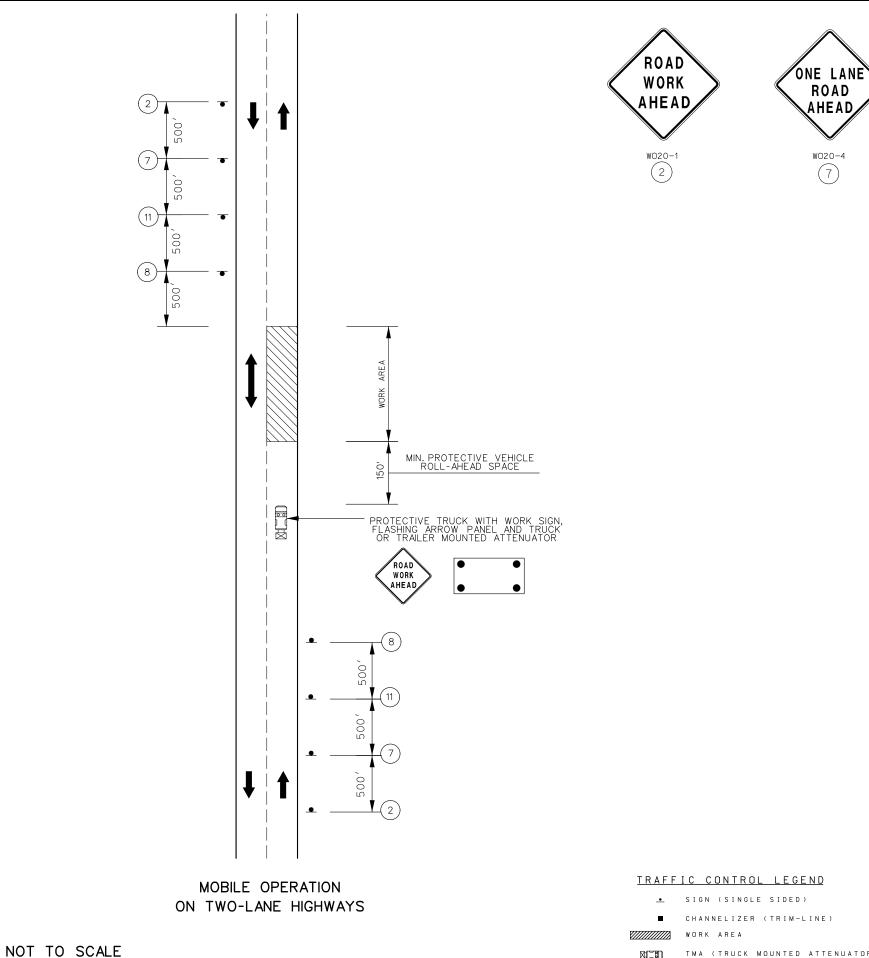
MO

28

ΚK

 SL

NOT TO SCALE





TMA (TRUCK MOUNTED ATTENUATOR)

MIN





(11)

NOTES:

FLASHING ARROW PANELS SHALL BE OPERATED IN CAUTION MODE.

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY PROVIDE ADDITIONAL PROTECTIVE TRUCKS EQUIPPED WITH PROPER WARNING DEVICES.

THE FLAGGER POSITION SHOULD BE LOCATED WHERE THEY WILL PROVIDE THE MOST VALUE TO THE MOTORIST AND THE WORK CREW.

PROTECTIVE TRUCK AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE HAZARD WARNING LIGHTS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED IN A MANNER SUCH THAT THEY ARE NOT OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGEND ON VEHICLE-MOUNTED SIGNS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.

NO DIRECT PAY WILL BE MADE FOR THE RELOCATION OF CONSTRUCTION SIGNS OR DEVICES.

SIGN AND DEVICE LOCATION WILL BE FIELD VERIFIED AND APPROVED BY THE ENGINEER.

CMS LOCATIONS TO BE DETERMINED BY THE ENGINEER

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SHIRLEY J. NUMBER PE-28779

10/15/2024 KK MO

SHEET NO

SL 29 FRANKLIN

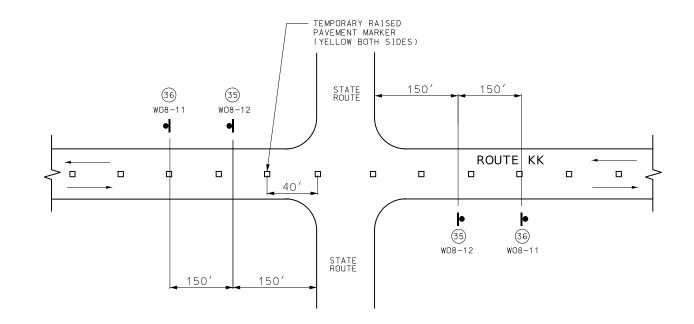
LOB NO J6S3619B CONTRACT ID.

PROJECT NO.

BRIDGE NO.

TRAFFIC CONTROL SHEET MOBILE OPERATION ON TWO-LANE HIGHWAYS

SIGN SPACING FOR MAINLINE



SIGN SPACING AT STATE ROUTE INTERSECTIONS

(1) ADDITIONAL SIDEROADS MAY BE SIGNED AS DIRECTED BY THE ENGINEER.

TRAFFIC CONTROL LEGEND

- SIGN (SINGLE SIDED)
- TEMPORARY RAISED PAVEMENT MARKER





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NOTES: SIGN 35 AND TEMPORARY PAVEMENT MARKING INSTALLED WHERE CENTERLINE STRIPING HAS BEEN COVERED OR REMOVED. SIGNS ARE TO REMAIN IN PLACE UNTIL THE PERMANENT CENTERLINE PAVEMENT MARKINGS ARE IN PLACE. SIGNS SHALL BE COVERED OR REMOVED WHEN PAVEMENT CENTERLINE MARKING HAS BEEN INSTALLED.

SIGN (35) IS PLACED AT APPROXIMATELY TWO-MILE INTERVALS AND AT STATE ROUTE JUNCTIONS. WHEN THE INSTALLATION AT A JUNCTION IS WITHIN ONE-EIGHTH MILE OF THE NORMAL MAINLINE SIGN (35), THE LATTER MAY BE ELIMINATED.

ALL SIGNS SHALL BE POST MOUNTED AND IN ACCORDANCE WITH STANDARD PLAN 616.10 AND 903.03.

TEMPORARY RAISED PAVEMENT MARKERS SHALL BE INSTALLED BY THE END OF EACH DAY.

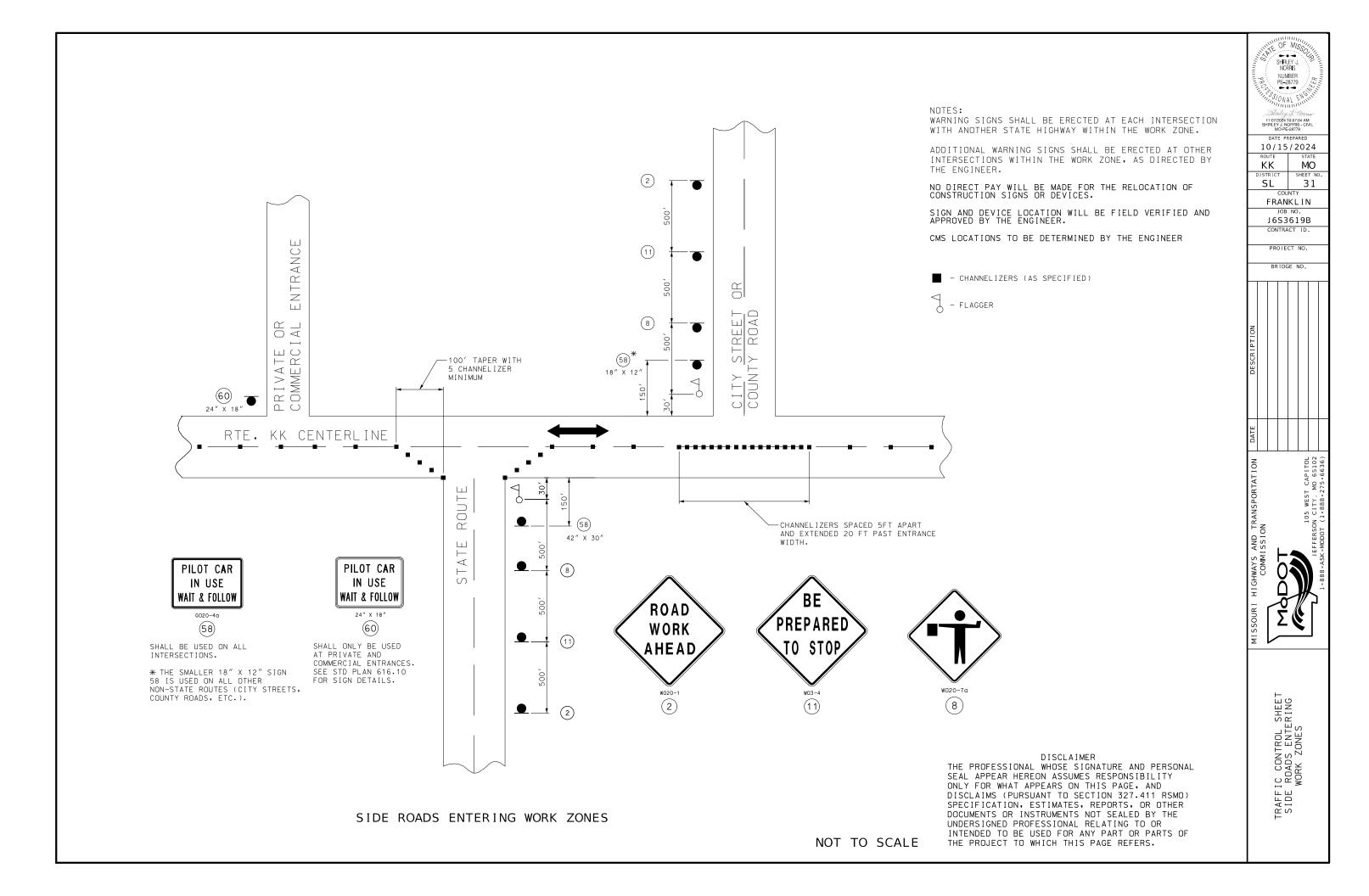
NO DIRECT PAY WILL BE MADE FOR RELOCATIONOF CONSTRUCTION SIGNS OR DEVICES.

SIGN AND DEVICE LOCATION WILL BE FIELD VERIFIED AND APPROVED BY THE ENGINEER.

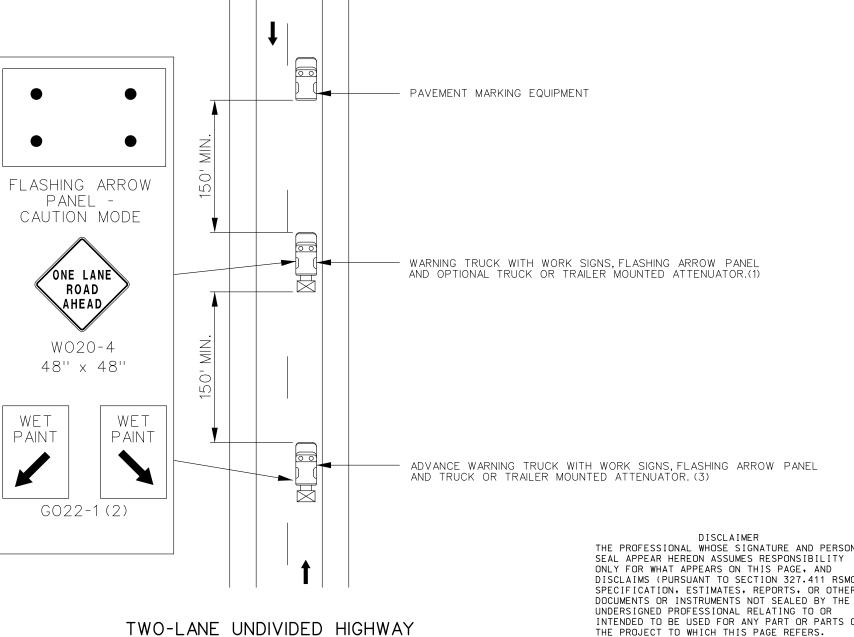
CMS LOCATIONS TO BE DETERMINED BY ENGINEER.

SHIRLEY J. NORRIS NUMBER PE-28779 10/15/2024 KK MO SL 30 FRANKLIN J6S3619B CONTRACT ID. PROJECT NO. BRIDGE NO

TRAFFIC CONTROL SHEET NO CENTER STRIPE/UNEVEN LANES



CENTER LINE / EDGE LINE STRIPING ON TWO-LANE HIGHWAYS



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NOTES:

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY PROVIDE ADDITIONAL PROTECTIVE TRUCKS EQUIPPED WITH PROPER WARNING DEVICES.

PROTECTIVE TRUCK AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE HAZARD WARNING LIGHTS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

- (1) TRUCK IS OPTIONAL ON TWO-LANE UNDIVIDED HIGHWAYS IF SIGNING AND ARROW BOARD IS MOUNTED ON THE PAVEMENT MARKING EQUIPMENT.
- (2) WET PAINT SIGNS ARE INSTALLED TO INDICATE THE SIDE IN WHICH THE PAVEMENT MARKING MATERIAL IS BEING APPLIED. AT THE CONTRACTOR'S OPTION. A FRONT FACING WET PAINT SIGN MAY BE INSTALLED ON THE LEFT SIDE OF THE PAVEMENT MARKING EQUIPMENT.
- (3) ADVANCE WARNING TRUCK IS POSITIONED AT THE NO TRACK POINT OF THE PAVEMENT MARKING MATERIAL OR SPACING SHOWN, WHICHEVER IS GREATER.

NUMBER PE-28779 ISSIONAL EN

10/15/2024

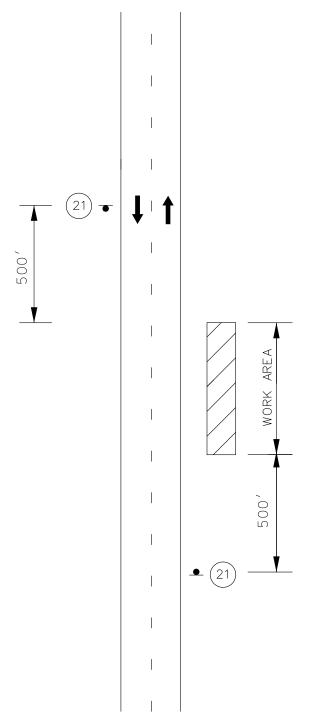
KK MO SL 32

FRANKLIN J6S3619B CONTRACT ID.

PROJECT NO.

BRIDGE NO.

RAFFIC CONTROL SHEET LINE / EDGE LINE STRIPING ON TWO-LANE HIGHWAYS





TRAFFIC CONTROL LEGEND

• SIGN (SINGLE SIDED)

WORK AREA

WORK BEYOND THE SHOULDER

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NOTES: ONLY APPLICABLE WHEN WORK IS WITHIN THE CLEAR ZONE.

ON MULTI-LANE, DIVIDED HIGHWAYS, SIGN ADVISING OF THE SHOULDER WORK OR THE CONDITION OF THE SHOULDER SHOULD BE PLACED ONLY ON THE SIDE OF THE AFFECTED SHOULDER.

VEHICLE HAZAED WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

NO DIRECT PAY WILL BE MADE FOR RELOCATION OF CONSTRUCTION SIGNS OR DEVICES.

SIGN AND DEVICE LOCATION WILL BE FIELD VERIFIED AND APPROVED BY THE ENGINEER.

CMS LOCATIONS TO BE DETERMINED BY ENGINEER.

SHIRLEY J NORRIS NUMBER PE-28779 10/15/2024 KK MO SL 33 FRANKLIN JOB NO J6S3619B CONTRACT ID. PROJECT NO. BRIDGE NO.

TRAFFIC CONTROL SHEET WORK BEYOND THE SHOULDER

		SIC	GNS			CONCRETE FOOTINGS EMBEDED	:I			TURA POST			PIPE P	OSTS	5 *		CK I I		U- CHANNE POST	L		2	PEI		TED S	QUARE S	STEEL TUBE	POST		BREAK -	EFFECTIVE: 07-01-2024 REMARKS	SHRLEY J. NORRIS NUMBER
9 S I GI NO .	SIGN	CL	DRZ EAR LOCATION NOT (ROUTE)	LT OR RT	SIGN DTL. SHT.	ITEM NO. 9031010	POST DES NO	POSTP NO.1N	POST F	Ю. 3 РЕ	SS TO	M NO SIZ	PE POST POST	PER	TOTAL ITEM NO 903122	2 X AT 2	3/8" 55 LB	BARS S/FT	ITEM NO.	POST NO.1	POST NO.2	TOTAL		ANCHORS DRIVEN 7-GA ITEM NO	CONCRETE 7-GA ITEM NO 9031274	POST POST NO.1 NO.2	2.25" TOTAL INSERT	ANCHOR DRIVEN 7 - GA ITEM NO	CONCRETE 7-GA ITEM NO	AWAY ASSEMBLY ITEM NO. 9031241	AND OTHER REQUIRED ITEMS	PE-28779 PE-28779 PE-28779 POPULATION OF PM
201	ASSEM	S1	TD. ROUTE KK	****	NO.	CY	—	LF	LF				. LF LF	₩	LBS	I	N. L	67 43	5 LF	LF	LF	LF	EA	EA	EA	LF LF 12.75	LF EA 12.75	EA 1	EA	EA		SHIRLEY J. NORRIS - CIVIL MO-PE-28779 DATE PREPARED
202			ROUTE KK	_												7 50	0010.	0/ 43	<u>'</u>							12.73	12.73	1			NO NEW POST	10/15/2024
203			ROUTE KK	_																						13.00	13.00	1	1		NO NEW POST	ROUTE STATE KK MO
204		4+81	ROUTE KK	_																						10.00	10.00	1			NO NEW POST	DISTRICT SHEET NO.
206	36X36		ROUTE KK	_																12.00		12.00	1									SL 34
207 208			ROUTE KK	_																	_	23.50	2						1			FRANKLIN
	48X24 ASSEM	48+90 . 60+60	ROUTE KK	_																10.00	113.30	23.30	2								NO NEW POST	јов NO. Ј6S3619B
210	ASSEM	. 66+83	ROUTE KK	_																						14.25	14.25	1				CONTRACT ID.
211	ASSEM 18X24	70+21	ROUTE KK	_																0 25		0 75	1			14.25	14.25	1				PROJECT NO.
212			ROUTE KK	_																8.25		8.25	1									- Thoseer No.
214	48X24	71+42	ROUTE KK	RT																10.75	15.50	26.25	2									BRIDGE NO.
215			ROUTE KK	_			-	\vdash	-+	-+	+			1						8.25	1	8.25	1					1	1			+
217			ROUTE KK	_			1	\vdash		-+			+ + -							3.23		0.23	1						1		NO NEW POST	1
218			ROUTE KK	_																	15.50	26.25	2									1
219			ROUTE KK				1	\vdash					+ + -							8.25		8.25	1						+			12
	ASSEM	76+49	ROUTE KK	_																0.25		0.23				15.00	15.00	1				12
	ASSEM	77+87	ROUTE KK	_																											NO NEW POST	4 <u>~</u>
223	ASSEM 30X24	. 85+63 88+22	ROUTE KK	_																											NO NEW POST NO NEW POST	- SESC
225			ROUTE KK																							13.00	13.00	1			NO NEW 1031	<u>-</u> -
	ASSEM	. 92+32	ROUTE KK	_																						15.25	15.25	1]
227 228			ROUTE KK																	8.00		8.00	1									4
229			ROUTE KK	_																8.00		8.00	1									1
230			ROUTE KK																	8.00		8.00	1									
231			ROUTE KK	_																8.00		8.00	1								NO NEW POST	-
233			ROUTE KK																	11.00)	11.00	1								NO NEW 1031	105 WEST CAPITOL 105 WEST CAPITOL 108 WEST CAPITOL 108 WEST CAPITOL 1088-275-6636)
234			ROUTE KK																												NO NEW POST	AP I - 665.
235 236			ROUTE KK																	8.25		8.25	1									T C MO MO 275
237			ROUTE KK																	8.00		8.00	1									SPC WES ITY,
238	-		ROUTE KK																	8.00		8.00	1						1			RAN 105 N C
239 240	18X24		ROUTE KK																	8.00		8.00	1 1									TON TOO
241	ASSEM	. 107+20	ROUTE KK																							13.50	13.50	1				AND SS:
242	ASSEM 18X2		ROUTE KK												-					8.25		8.25	1						1		NO NEW POST	ASK ASK
	18X24		ROUTE KK																	0.23		0.23	1								NO NEW POST	# S O
245			ROUTE KK																												NO NEW POST]º ∧ W :
246 247	18X24		ROUTE KK						-+	_	-			1						1									1		NO NEW POST NO NEW POST	
248	18X24		ROUTE KK	RT																											NO NEW POST	Sour Z Sour
249			ROUTE KK	_						$-\top$				1						1								1			NO NEW POST	
250	18X24		ROUTE KK				1	\vdash	_																				+		NO NEW POST NO NEW POST	-[Σ
252	18X24	132+81	ROUTE KK	LT																8.00		8.00	1									<u> </u>
253			ROUTE KK		\sqcup				\perp		\perp									8.00	1	8.00	1					1	<u> </u>			4
254	36X36		ROUTE KK						+	-+	-		+ + -	1						11.00	_	11.00	2					1	+			-
256	18X24	135+13	ROUTE KK	LT																8.00		8.00	1									1
	18X24		ROUTE KK	_			1							1						8.00		8.00	1					1				4
258 259			ROUTE KK	-			1	\vdash	-+					+						8.00	1	8.00	1					1	+			m
	48X24		ROUTE KK	LT			1							Į,,		1,,,,				10.00	.,	23.50	2									_ P
			SUBTO	TAL	1_	0.00						0			43				0.0			375	39	<u> </u>			121 -	9	0			1 .
			* BREAK	AWAY A	SSEME	BLY IS INCIDEN E TOTALED WITH	NTAL F	OR STE	RUCTU	IRAL ST	EEL AI	ND PIPE																DISCLA	IMER			E D
			DACKI	.10 DAP	.5 ANI	F IOIVEED MILL	יאו כיי	JUNA	اد ع،	LL UN		. 0313.															PROFESSIONAL	WHOSE	SIGNATU			SHEE
											-			ST	RUCT	JRAL S	TEEL	L POS	ST AND F	<u> </u>	IG DA		BLE			ONLY	APPEAR HEREO FOR WHAT APP	EARS O	N THIS I	PAGE, AN	ND	S
											Ī	POST DE	S. NOM. SIZE LB	WEI		STUB LENGTH	DIA		VEL GROUND	6:1 G	RADE	4:1	GRADE		2:1 GRAD						RSMO) OT LAGCATED	1
	R(OUND PIPE	POST AND	<u> FO</u>	<u>1 TO</u>	NG DATA T	<u>ABL</u> E				F	1	W6	9.0	0.75	3'-0"	15	3 '	-0 0.14	DEPTH 3 2	0.15	3'-3"	0.16	3 6		الالاسمارا الاسمارا	IFECALIONIUS MEDOTOS MORTINS F	DMA#E	121.1401 ISA 121.1401 ISA	BUIDANIONE BUCKEDOCRIM	COTHE	1
	NOM.S			TUB NGTH		FOOTING		ICRETE				3	W8 1	.5.0	1.25	4'-0"	28	4	0 0.47	4 2	0.73	4'-9"	0.74	5 0	0.78		ME RTOOF IN PROFE					1
	(IN.	5.79	0.48 4	3½°	1.7	A DEPTH 2 4~6"	(C.Y.				5	W10 2	26.0	1.83	5'-0"	36	5	0 1.31	5 2		5'-5"	1.43	5 6 5 9	1.52	THE (NDED TO BE US PROJECT TO WH					1
	4	10.79	0.90 5	31/2	18	8" 5~6"	(36				6	W12 3	5.0	2.92	5'-6"	36	5	6 1.44	5 9	1.52	5 11	1.56	6 3	1.65	1	5525. 10 111				D 23	

			S I GNS		CONCRETE FOOT I NGS			RUCTI				PIP	E PO	OSTS	*		BACK I NG	U- CHANNE	L			PEF	RFORA	TED SQU	ARE S	STEEL	TUBE				EFFECTIVE: 07-01-2024	SHRLEY J. NORRIS
902	SIGNA		TABULATED ON D-37A	SHEET	EMBEDED		STE	EL PO	OSTS	; *							BARS**	POST		1	2 I	IN. POS	ST ANCHORS			2.	5 IN. P	OST ANCHOR	S	BREAK - AWAY	REMARKS AND OTHER	NUMBER PE-28779
S I GN	SIGN SIZE	STATION	HORZ CLEAR LOCATION OF IF NOT (ROUTE) RT	F SIGN R DTL. F SHT.	9031010	POST DES NO.	NO . 1 N	OST POS O. 2 NO.	3 PER FT	903121	O SIZE	E NO.1	NO.2	LBS PER FT	ITEM N 90312	L IO NC 20 EAG	CH LGTH TOTAL NO		NO.1	POST NO.2	ITEM NO. 9031270A	DRIVEN 12-GA ITEM NO 9031271A	DRIVEN 7-GA. ITEM NO. 9031273A	CONCRETE PO 7-GA. NO ITEM NO. 9031274	. 1 NO . 2	ITEM NO 9031280	INSERT (6 FT) ITEM NO 9031272	DR I VEI 7 - GA . I TEM NO A9031281	CONCRETI 7-GA. D. ITEM NO. A 9031285	ITEM NO. 9031241	REQUIRED ITEMS	10/17/2042:19:53 PM SHIRLEY J. NORRIS - CIVIL MO-PE-2877
	18X24	138+53	ROUTE KK LT		CY		LF	LF LF		LBS	IN.	LF	LF		LBS		IN. LF LB:	LF	8.00	LF	8.00	EA 1	EA	EA L	F LF	LF	EA	EA	EA	EA		DATE PREPARED
	18X24 18X24	139+28 140+81	ROUTE KK LT																8.00		8.00	1 1										10/15/2024 ROUTE STATE
264 A		142+61 143+40	ROUTE KK LT				+		-															14.	25	14.25	-	1			NO NEW POST	KK MO DISTRICT SHEET NO.
266 A	ASSEM.	153+85	ROUTE KK LT	Г																				13.		13.50		1				SL 35
	ASSEM. 18X24	156+56 157+39	ROUTE KK RT																8.00		8.00	1		15.	75	15.75	1	1				FRANKLIN JOB NO.
269 270	18X24 18X24	158+18 158+97	ROUTE KK LT ROUTE KK LT																												NO NEW POST NO NEW POST	J6S3619B
271	18X24	159+77	ROUTE KK LT	Г																											NO NEW POST	
	18X24 18X24	160+56 160+88	ROUTE KK LT																												NO NEW POST NO NEW POST	PROJECT NO.
274	18X24 36X36	161+67	ROUTE KK LT ROUTE KK LT																												NO NEW POST	BRIDGE NO.
276	18X24	161+98 165+01	ROUTE KK RT	г															8.00		8.00	1									NO NEW POST	
	18X24 18X24	165+80 166+64	ROUTE KK RT				+	+	-		+		-			+			8.00	-	8.00	1 1			+		1	+				1
279	18X24 18X24	168+54 169+34	ROUTE KK RT	г			_												8.00		8.00	1										
281	18X24	170+18	ROUTE KK RT	г															8.00		8.00	1										
	18X24 18X24	171+75 172+60	ROUTE KK LT																8.00		8.00	1									NO NEW POST	- SCR
284	18X24	173+44	ROUTE KK LT																												NO NEW POST	
	18X24 18X24	174+18 175+03	ROUTE KK LT ROUTE KK LT																												NO NEW POST NO NEW POST	<u> </u>
	18X24 18X24	175+82 176+61	ROUTE KK LT																												NO NEW POST NO NEW POST]
289	18X24	177+40	ROUTE KK LT	г																											NO NEW POST	1
290 291	18X24 18X24	178+19 181+69	ROUTE KK LT																8.75		8.75	1 1										- DAT
292 293	18X24 18X24	182+48 183+26	ROUTE KK RT ROUTE KK RT																8.00		8.00 8.00	1 1										2 20°0
294	18X24	183+87	ROUTE KK RT	г															8.00		8.00	1										T10 T10 4P1T 653
	36X36 18X24	184+30 184+64	ROUTE KK RT				_												8.25		8.25	1									NO NEW POST	DRTA ST C/ . MO
297	18X24	185+42	ROUTE KK RT	г															8.25		8.25	1										VSPC
	18X24 18X24	186+22 187+02	ROUTE KK RT	r															8.25		8.25	1 1										TRANSPORTATION ION 105 WEST CAPITOL RSON CITY, MO 65102 OT (1-888-275-6636)
300 301	18X24 18X24	187+81 188+61	ROUTE KK RT ROUTE KK RT																8.25		8.25 8.25	1 1										T N E
302	18X24	189+49	ROUTE KK RT	г															8.25		8.25	1										IS A MMIS LEF LEF LEF LEF
303 304	18X24	190+28 191+07	ROUTE KK RT																8.25		8.25 8.25	1 1										CO C
305 306		192+23 193+02	ROUTE KK RT			H	-+									+			8.25		8.25 8.25	1 1			+							
307	18X24	193+87	ROUTE KK RT	г			\perp												8.25		8.25	1		-	7.	*	-	<u> </u>				1 \ \Q \ \ \ \
308 A	36X36	196+23 198+34	ROUTE KK LT ROUTE KK RT	г																				15.	/5	15.75	1	1			NO NEW POST	Jy 2 &
310 311		227+24 238+59	ROUTE KK RT				\dashv												12.25		12.25	1		13.	50	13.50		1				ĮĘ V
312	18X24	240+60	ROUTE KK RT	г															8.00		8.00	1		13		13.30		<u> </u>				
313 314		241+46 242+23	ROUTE KK RT													+			8.00		8.00	1 1			+							
315		243+08 244+24	ROUTE KK RT																8.00		8.00	1 1			1							
317	18X24	245+03	ROUTE KK RT	г			\perp												8.00		8.00	1			\perp							
318 319	18X24 18X24	245+88 246+72	ROUTE KK RT				+	+	+			-				+			8.00		8.00	1 1			+			+				m
	18X24		ROUTE KK RT	г						-				<u> </u>	_				8.00	<u> </u>	8.00	1					<u> </u>					90 94
			* BREAKAWAY ** BACKING BA	ASSEMB	0.00 LY IS INCIDENT TOTALED WITH	TAL FO	OR STRU CTURAL	UCTURAL STEEL	STEEL OR PI	O AND I PE POS	PIPE P	OSTS.			0	_	0	0.0			312	38	-	- 🔀	XXXX THF	73 PROFESS		DISCL WHOSE		- JRE AND	PERSONAL	D-29
										POS	T DES	. NOM		POST WEIG	GHT			VEL GROUND	6:1 G	FOOT I	NG 4:1	GRADE		2:1 GRADE	SEAL ONL`	APPEAG FOR W	R HEREO HAT APF	ON ASS PEARS	UMES RES	SPONSIBI PAGE, A N 327.41	LITY ND	ΣS
	RO	UND PI	PE POST AND F	11 T <u>00</u>	NG DATA TA	<u>ABLE</u>	<u> </u>				NO.	SIZ W6	E LB	S/FT 9.0	0.75	/ L	ENGTH DE 3'-0" 15" 3'	PTH C.Y. 0 0.14	DEPTH 3 2	C.Y. 0.15	3 - 3"	0.16	3 - 6		SPE	CIFICAT	ION, ES	STIMAT	ES, REPO	ORTS, OR SEALED B	OTHER	
F	NOM.SIZ (IN.)	ZE WE	EIGHT STUB		FOOTING	CON	CRETE Y.				3	W6 W8	1	5.0	1.25		4 - 6 28 4	0 0 47 6 0 71	4 - 2	0.73	4'-9"	0.74	5 0	0.78	UNDE	ERSIGNE) PROFE	ESSION	AL RELA	TING TO	OR	
	2½ 4	5.79		1	2" 4~6" 8" 5~6"	0	.13				5	W10 W10 W12	2	6.0	1.83 2.17 2.92		5 0 36 5	-0 1.31 -0 1.31 -6 1.44	5 - 2	1.37	5'-5"	1.43	5 6 5 9	1.52						ART OR P E REFERS		†

						_														_													EFFECTIVE: 07-01-2024	SHRLI	EY J. 💝 🖹
			SIGNS	5		CONCRETE FOOT INGS			RUCT				PIP	E P	OSTS	*		BACK I		U- CHANNE	L					ATED S	SQUARE S							NOR NUM PE-28	
	02 SIGNA	u SIGNS	TABUI AT	FD ON D-	37A SHEE	EMBEDED		SIE	EL P	0513	>↑							BARS*	k	POST		1	1	2 IN. P	OST ANCHOR	5		2.	5 IN. P	OST ANCHOR	5	BREAK -	REMARKS AND OTHER	11/1/38/0NA	FENGILLI
	N SIGN	STATION	HORZ CLEAR	LOCATION	LT SIG	GN ITEM NO.	POST DES NO.	POST P	OST POS	ST LBS	TOTAL	L PIPE OSIZE	POST NO.1	POST NO. 2		TOTAL TEM NO.	NO.	X 3/8" E 2.55 LBS	FT	ITEM NO.	POST NO.1	NO.2	ITEM NO	12-GA	N DRIVEN A 7-GA O ITEM NO	ONCRET 7-GA ITEM NO	POST POST NO 1 NO 2	ITEM NO	INSERT (6 FT)	DRIVEN 7-GA ITEM NO	CONCRET 7-GA. ITEM NO.	EASSEMBLY	REQUIRED ITEMS		2 norris
			IF NOT STD.	<u> </u>	**** NO		NO.	LF	LF LF		903121 LBS	IN.	LF	LF	F1 9	LBS	EACH	LGTH TOTA		9031250A LF	LF	LF	9031270 LF	EA	EA	A 9031274 EA	LF LF	9031280 LF	9031272 <i>A</i> EA	EA	9031285 EA	9031241 EA		10/15	5/2024
	1 ASSEM.	251+62		ROUTE KK																							13.50	13.50		1				KK	MO
	ASSEM.	258+66		ROUTE KK						_																	11.25	11.25		1				DISTRICT	SHEET NO
	36X36 36X36	260+75 263+31		ROUTE KK						_	-	-								-	12.2	-	12.25	1						-			NO NEW POST	SL	36
324	_	264+19		ROUTE KK																	12.2	2	12.25	1									NO NEW POST		UNTY NKLIN
326	30X24	265+00		ROUTE KK	(LT																												NO NEW POST		NO.
32	7 36X36	265+86		ROUTE KK	LT																11.0	0	11.00	1											3619B
328	3 48X48	265+86		ROUTE KK		0.36						4	15.75	5	10.79	170																		CONTR.	ACT ID.
329		266+08		ROUTE KK		0.36						4	15.75	5	10.79	170																			
330		266+08		ROUTE KK																	10.2	5	10.25	1										PROJE	CT NO.
33	1 48X48	266+24		ROUTE KK	(LT									1																			NO NEW POST	BRID	GE NO.
			+												+ +															1	1			-	
		1	-	SUBTO	TAL 3	0.72					0					340			0	0.0			34	3	-	-		25	0	2	0	-			

^{*} BREAKAWAY ASSEMBLY IS INCIDENTAL FOR STRUCTURAL STEEL AND PIPE POSTS. ** BACKING BARS ARE TOTALED WITH STRUCTURAL STEEL OR PIPE POSTS.

		CONCRETE FOOTINGS			TRUC					PII	PE PC	STS	*		BACK I	NG		U- CHANNEL				PEI	RFORAT	ED S	QUAF	RE S	STEEL	TUBE			
		EMBEDED		S٦	EEL	POST:	S*								BARS*	*		POST				2 IN. POS					2.	5 IN. PO			BREAK -
										1	1	1	1										ANCHORS						ANCHORS		AWAY
			POST	POST	POST	POST	LBS	TOTAL	PIPE	POST	POST	LBS	TOTAL		:" X 3/8 T 2.55				POST NO.1	POST NO.2		DRIVEN 12-GA						(6 FT)	DR I VEN	7-GA	ASSEM.
		ITEM NO.	DES	NO.1		NO.3	PER	ITEM NO	SIZE				ITEM NO	NO.		,		ITEM NO.		11012	ITEM NO.	ITEM NO	ITEM NO.	ITEM NO.			ITEM NO.	ITEM NO.	ITEM NO	ITEM NO.	ITEM NO.
		9031010 CY	NO.	I F	1	1	FT	9031210	1 1	I.F.	I.F.	FT	9031220 LBS	EACH	LGTH IN.	TOTAL	NO. LBS	9031250A L F	LE	1.5	90312704	9031271A	9031273A EA	9031274	LF	LF	9031280 LF	9031272A	9031281A	9031285 EA	9031241 EA
SUB-	TOTAL 1	0.00						0	1111				4.3		IN.		43	0.0			375	39	-	-		-	121	-	9	0	
-	TOTAL 2	0.00						1					0					0.0			312	38			 		73	2	-		
		0.00						1 0					1 0				0	0.0			312	30	-						<u> </u>	\longrightarrow	
SUB	TOTAL 3	0.72						0					340				0	0.0			34	3	-	-			25	0	2	0	_
PAY	TOTAL	0.7						-					380			·	-	0			721	80	-	-		·	219	2	16	0	_

		POST							FOOTIN	IG			
POST DES.	NOM.	WE I	GHT	STUB	DIA.	LEVEL (GROUND	6:1 G	RADE	4:1 GR	ADE	3:1 OR 2	:1 GRADE
NO.	SIZE	LBS/FT	LBS/IN	LENGTH		DEPTH	C.Y.	DEPTH	C.Y.	DEPTH	C.Y.	DEPTH	C.Y.
1	W6	9.0	0.75	3'-0"	15 •	3 - 0	0.14	3 - 2	0.15	3'-3"	0.16	3 - 6	0.17
2	W6	15.0	1.25	4'-0"	24 •	4 - 0	0.47	4 2	0.50	4'-3"	0.51	4 6	0.54
3	W8	18.0	1.50	4'-6"	28 •	4 6	0.71	4 8	0.73	4'-9"	0.74	5 0	0.78

STRUCTURAL STEEL POST AND FOOTING DATA TABLE

DISCLAIMER THE PROFESSIONAL WHOSE SIGNATURE AND PERSONAL SEAL APPEAR HEREON ASSUMES RESPONSIBILITY ONLY FOR WHAT APPEARS ON THIS PAGE, AND DISCLAIMS (PURSUANT TO SECTION 327.411 RSMO) SPECIFICATION, ESTIMATES, REPORTS, OR OTHER DOCUMENTS OR INSTRUMENTS NOT SEALED BY THE UNDERSIGNED PROFESSIONAL RELATING TO OR INTENDED TO BE USED FOR ANY PART OR PARTS OF THE PROJECT TO WHICH THIS PAGE REFERS.

D-29 SHEET 3 OF

ROUN	ID PIP	E POST	AND	FOO	TING	DATA	TABLE
NOM.SIZE	WE I	GHT	STU	В	FO	OTING	CONCRETE
(IN.)	LBS/FT	LBS/IN	LENG	TH	DIA.	DEPTH	C.Y.
21/2	5.79	0.48	4 - 3	1/2	12"	4~6"	0.13
4	10 70	000		1/=	10	F C !!	0 20

EFFECTIVE: 10-01-2016

												5	TANE	DARD	SIG	SN AS	SSEM	BL I E	S (1 OF	3)											
																Т	YPE															
NUMBER			SH	SH	SH	SH	SH	SH	SH	SH	SH	SH	SH	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SH	SH
MB			M1 - 5	M1 - 5	M1-5A	M2 - 1	M6 - 1	M6 - 4	R1-1	R1-1	R1-2	R2-1	R5-1	W1-1L	W1-1R	W1 - 2L	W1-2R	W1-4L	W1 - 5R	W1-6	W1 - 8D	W2-2	W3 - 1	W13-1P	W13-1P	W13-1P	W13-1P	W16-1P	OM3 - R	DL - 3_Y	DL - 3_R	DL -3_W
\exists	LOG	LOCATION					1		1			S	IGN D	ESCR	IPTIC	ON, S	IZES	& NU	JMBER	OF	EACH								-			
	MILE				_		←																<u> </u>						2			
SIGN			185	100	KK	JCT	M6-1	+	(STOP)	(STOP)	V	S S	ENTER	(1)	⟨ ₽⟩	(3)		(\$)	 <\$>	—		()		30 M.P.H.	35 M.P.H.	40 m.p.h.	45 n.p.s.	3/4 MILES		4"x72"		ı
S			M1-5	M1-5	H1-5a	121 Hv1 E	M6-1	211415	R1.1	R1-1	1.00 LV 4.01	135	R5-1	26 1 1 26 1	. <u></u>	W-11	 	 		40	100.24	X.	× × × × × × × × × × × × × × × × × × ×	W13-1P	V13-1P	V13-1P	N13-1P	W16-3P	000-R	40.220	41,472	48.4731
201	0+75	ROUTE KK - LT	1	50 X24	1	21 X13	1	1	30 X30	40 X40	40 X40	30 846	30 X30	30 X30	30 X30	36 X36	30 X30	36 X36	36 X36	40 324	10 XZ4	36 X36	36 X36	24 X24	24 X24	24 824	24 X24	24 X10	12 X30	4 X/2	4 X/2	4 X/Z
202	2+25	ROUTE KK - RT			1																											
203	4+73	ROUTE KK - RT										1																				-
204	7+61	ROUTE KK - LT	1			1																	1							-		
206	28+36	ROUTE KK - RT	1			1			1																							$\overline{}$
207	48+32	ROUTE KK - LT							1											1										$\overline{}$		
208	48+90	ROUTE KK - LT																		1												
209	60+60	ROUTE KK - RT															1										1					
210	66+83	ROUTE KK - RT												1										1						\longrightarrow		-
211	67+62 70+21	ROUTE KK - LT	-													1					1						1			\longrightarrow	\longrightarrow	
212	70+21	ROUTE KK - RT																			1									\vdash	\rightarrow	
214	71+42	ROUTE KK - RT																		1	<u> </u>									$\overline{}$		
215	72+00	ROUTE KK - RT																			1											
216	72+58	ROUTE KK - RT																			1									\Box	\Box	
217	72+99	ROUTE KK - LT	ļ																										1			
218	73+22 73+86	ROUTE KK - RT	-																	1	1										$\overline{}$	
220	74+44	ROUTE KK - RT																			1										-	
221	76+49	ROUTE KK - LT													1									1						1		
222	77+87	ROUTE KK - RT															1										1					
223	85+63	ROUTE KK - LT														1											1					
224	88+22	ROUTE KK - LT	1		1																									-		
225	89+94 92+32	ROUTE KK - LT ROUTE KK - RT	<u> </u>									1						1								1						
226	94+06	ROUTE KK - RT																1			1					1						
228	94+91	ROUTE KK - RT																			1											
229	96+70	ROUTE KK - RT																			1											
230	97+54	ROUTE KK - RT																			1											
231	98+34	ROUTE KK - RT	<u> </u>																		1											
232	99+13 99+78	ROUTE KK - RT							1												1											
234	100+13	ROUTE KK - RT							1												1										-	
235	100+98	ROUTE KK - RT																			1											
236	102+07	ROUTE KK - LT																			1											
237	102+86	ROUTE KK - LT																			1											
238	103+66	ROUTE KK - LT	-				1														1											
239	104+45 105+29	ROUTE KK - LT	-																		1											
	103+29	ROUTE KK - LT					1											1			1					1				$\overline{}$		
	115+08	ROUTE KK - RT																1							1	_					$\overline{}$	
	116+34	ROUTE KK - RT																			1											
	117+08	ROUTE KK - RT																			1									\Box	\Box	
245		ROUTE KK - RT	-																		1									\longrightarrow		
246	118+72 119+56	ROUTE KK - RT																			1									$\overline{}$	\rightarrow	
247	120+30	ROUTE KK - RT																			1									$\overline{}$	\rightarrow	
249	121+15	ROUTE KK - RT																			1									$\overline{}$		
250	121+94	ROUTE KK - RT																			1											
251	122+73	ROUTE KK - RT																			1									\Box	\Box	
252	132+81	ROUTE KK - LT	-																		1									\longrightarrow		
253 254	133+60 133+98	ROUTE KK - LT							1												1									$\overline{}$	\rightarrow	
255	133+98	ROUTE KK - LT							1											1										\longrightarrow	\rightarrow	
256	135+13	ROUTE KK - LT																		-	1									-		
257	135+90	ROUTE KK - LT																			1											
		SUBTOTAL 1	2	0	3	1	1	1	3	0	0	2	0	1	1	2	2	3	0	5	32	0	1	2	1	2	4	0	1	1	0	0

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NOTE: SIGNS #1-200 ARE LOCATED ON ROOT PROJECT J6S3619.

SHIRLEY J NORRIS NUMBER PE-28779 10/15/2024 ROUTE KK MO SHEET NO. SL FRANKLIN JOB NO.
J6S3619B
CONTRACT ID. PROJECT NO. BRIDGE NO.

> D-30 STANDARD SIGN ASSEMBLIES SHEET 1 OF 4

EFFECTIVE: 10-01-2016

												S	TANE	DARD	SIG	SN AS		BLIE	S (2	2 OF	3)											
																	YPE									1						
NUMBER			SH	SH	SH	SH	SH	SH	SH	SH	SH	SH	SH	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF
l ₩			M1 - 5	M1-5	M1 - 5A	M2 - 1	M6 - 1	M6 - 4	R1-1	R1-1	R1-2	R2-1	R5-1	W1-1L	W1 - 1R	W1 - 2L	W1-2R	W1-4L	W1-5R	W1-6	W1-8D	W2 - 2	W3 - 1	W13-1P	W13-1P	W13-1P	W13-1P	W16-3P	OM3 - R	DL - 3_Y	DL - 3_R	DL - 3_W
≥	LOG	LOCATION				•						S	I GN [DESCR	IPTI	ON, S	IZES	& NU	JMBER	OF	EACH											\neg
ᇋᆝ	MILE						←			$\overline{\wedge}$	700	SPEED		$\overline{\Delta}$	\sim	Δ	Δ		<u> </u>				<u> </u>						0			\Box
SIGN			185	100 M1-5	KK M3-5a	JCT	 	←→ M6-4	STOP	STOP)	TEST .	SPEED LIMIT 55	DO NOT ENTER			3			\$	—		(F)		30 M.P.H. W13-1P	35 M.P.H. V13-1P	40 M.P.H. V13-1P	45 M.P.H.	3/4 MILES		ı I I		
S			^{M1-5} 30 • x 2 4 ¹	м1-5 "30" x 2 4	130 ×24		ئے 21 * 15 *	21 × 15	81-1 36 × 36 °	48 ×48	48"×48"	36 ×48	36"×36"	" 36 × 36	** 36"x36"	₩ 36"x36"	36 × 36	" 36" x 36"	36 * x 36 "	48 ×24	18"x24			W13-1P	v13-19 24 × 24	vi3-1₽ 124"x24"	N13-1P 24 ×24	^{¥16-39} 24 [®] x 18 [®]	12"x36"	4"x72"	4 ×72	4 × 72
258	136+69	ROUTE KK - LT																			1											
259	137+44	ROUTE KK - LT																			1									\vdash		
260 261	138+12 138+53	ROUTE KK - LT ROUTE KK - LT																		1	1											
262	139+28	ROUTE KK - LT																			1											-
263	140+81	ROUTE KK - LT																			1											
264	142+61	ROUTE KK - LT																1							1							
265	143+40	ROUTE KK - RT														1											1			\longrightarrow		
266 267	153+85 156+56	ROUTE KK - LT ROUTE KK - RT															1		1						1		1	1		-		-
268	157+39	ROUTE KK - LT																	-		1							-		-		-
269	158+18	ROUTE KK - LT																			1											
270	158+97	ROUTE KK - LT																			1											
271	159+77 160+56	ROUTE KK - LT ROUTE KK - LT	-																		1											——
273	160+36	ROUTE KK - LT																			1											
274	161+67	ROUTE KK - LT																			1											
275	161+98	ROUTE KK - LT							1																							
276	165+01	ROUTE KK - RT																			1											
277	165+80	ROUTE KK - RT																			1									\vdash		-
278 279	166+64 168+54	ROUTE KK - RT ROUTE KK - RT																			1											
280	169+34	ROUTE KK - RT																			1											
281	170+18	ROUTE KK - RT																			1											
282	171+75	ROUTE KK - LT																			1											
283	172+60 173+44	ROUTE KK - LT																			1									-		
284	173+44	ROUTE KK - LT ROUTE KK - LT																			1									$\overline{}$		
286	175+03	ROUTE KK - LT																			1											
287	175+82	ROUTE KK - LT																			1											
288	176+61	ROUTE KK - LT																			1											
289	177+40 178+19	ROUTE KK - LT ROUTE KK - LT																			1											
291	181+69	ROUTE KK - RT																			1											$\overline{}$
292	182+48	ROUTE KK - RT																			1									1		
293	183+26	ROUTE KK - RT																			1											
294	183+87	ROUTE KK - RT							1												1									\vdash		
295 296	184+30 184+64	ROUTE KK - RT							1												1											-
	185+42	ROUTE KK - RT																			1											
298	186+22	ROUTE KK - RT																			1											
299	187+02	ROUTE KK - RT																			1									\longrightarrow		
300	187+81 188+61	ROUTE KK - RT																			1											
302	189+49	ROUTE KK - RT																			1											
303	190+28	ROUTE KK - RT																			1											\neg
304	191+07	ROUTE KK - RT																			1											
305	192+23	ROUTE KK - RT																			1											
306	193+02 193+87	ROUTE KK - RT																			1											-
308	196+23	ROUTE KK - LT																	1		1				1			1		$\overline{}$	\rightarrow	-
309	198+34	ROUTE KK - RT							1																							
-	227+24	ROUTE KK - RT																				1								\Box	\Box	
311	238+59	ROUTE KK - RT														1					1					1				\vdash		-
312	240+60 241+46	ROUTE KK - RT																			1									-		
314	242+23	ROUTE KK - RT																			1									-		-
315	243+08	ROUTE KK - RT																			1											
		SUBTOTAL 2	0	0	0	0	0	0	3	0	0	0	0	0	0	2	1	1	2	1	47	1	0	0	3	1	2	2	0	0	0	0

10/15/2024 KK MO SHEET NO SL 38 FRANKLIN JOB NO.
J6S3619B
CONTRACT ID. PROJECT NO. BRIDGE NO. (2)

SHIRLEY J. NORRIS NUMBER PE-28779

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												5	TANI	DARD	SIG	in As	SEM	BLIE	S (:	1 OF	3)											
																Т	YPE															
NUMBER			SH	SH	SH	SH	SH	SH	SH	SH	SH	SH	SH	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF
BE			M1 - 5	M1-5	M1 - 5A	M2 - 1	M6 - 1	M6-4	R1-1	R1-2	R1-2	R2-1	R5-1	W1-11	W1 - 1R	W1 - 21	W1 - 2R	W1-41	W1-5R	W1-6	W1 - 8D	W2-2	W3 - 1	W13-1P	W13-1P	W13-1P	W13-1P	W16-3P	OM3 - R	DI - 3 Y	DI -3 R	DL - 3_W
🗐	LOG	LOCATION													IPTIO																	
1 1		LOCATION						ı					I GIV L	JESCK	17110	אוע, א	1263	Q NC		UF	LACH					ı						
N N N	MILE		185	100	кк	LICT	HS-1	4	STOP	STOP	V	SPEED LIMIT 55	DO NOT ENTER	4				4	(\$)	—	()	(F)	♦	30	35	40	45	3/4			(
SI			M1 -5	100 M1-5	KK M1-Sa	JCT ×2-1	\rightarrow	↔			Y.	55	ENTER				(🛂	41-9	W1-80			30 M.P.H.	35 M.P.H.	40 M.P.H.	45 M.P.H.	3/4 MILES			(
			30 x24	30"x24"	30 ×24	21"x15"	M6-1	21 " ×15"	36 ×36	48 ×48	48"×48	0.7.1	36 ×36	1					1	48 ×24	18"x24	36 ×36	36"x36	24 x24	24 ×24	24"x24	1	24 ×18	OM3 = R	4"x72"	4 ×72	4 ×72
316	244+24	ROUTE KK - RT																			1											
317	245+03	ROUTE KK - RT																			1											
318	245+88	ROUTE KK - RT																			1										'	
319	246+72	ROUTE KK - RT																			1											
320	247+51	ROUTE KK - RT																			1											
321	251+62	ROUTE KK - LT															1									1					<u> </u>	
322	258+66	ROUTE KK - LT		1		1																									'	
323	260+75	ROUTE KK - LT							1																						<u> </u>	
324	263+31	ROUTE KK - RT																					1								<u> </u>	
325	264+19	ROUTE KK - LT										1																			'	
326	265+00	ROUTE KK - LT			1																										<u></u> '	
327	265+86	ROUTE KK - LT											1																		'	
328	265+86	ROUTE KK - LT									1																				<u> </u>	
329	266+08	ROUTE KK - RT									1																				<u></u> '	
330	266+08	ROUTE KK - RT											1																		<u></u> '	
331	266+24	ROUTE KK - RT								1																					1	1
L		SUBTOTAL 3	0	1	1	1	0	0	1	1	2	1	2	0	0	0	1	0	0	0	5	0	1	0	0	1	0	0	0	0	1	1

											STAN	IDARE	SIC	GN A	SSEN	1BL I I	ES (TOTA	AL)											
														Т	YPE															
	SH	SH	SH	SH	SH	SH	SH	SH	SH	SH	SH	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SHF	SH
	M1 - 5	M1 - 5	M1 - 5A	M2 - 1	M6 - 1	M6 - 4	R1-1	R1-1	R1-2	R2-1	R5-1	W1-1L	W1 - 1R	W1 - 2L	W1 - 2R	W1-4L	W1-5R	W1-6	W1-8D	W2 - 2	W3 - 1	W13-1P	W13-1P	W13-1P	W13-1P	W16-3P	OM3 - R	DL - 3_Y	DL - 3_R	DL-3
										S	I GN [DESCR	IPTIO	ON, S	IZES	& NU	JMBER	OF	EACH		Į.									
	185°	100 M1-5	KK H1-59	JCT M2-1	M6 - 1	↔	STOP RI-1	STOP	MELD H1-3	SPEED LIMIT 55	DO NOT ENTER	(1)	(P)	3	(*)	\$	\$	—	()	()	\$	30 M.P.H. W13-1P	35 M.P.H. V13-19	40 M.P.H.	45 M.P.H. W13-1P	3/4 MILES	Sign 2 - 1		ļ	
			30 x24	21 ×15	21"x15	21 ×15	36"x36"	48 ×48	48 ×48	36 ×48	36 ×36	36"x36	36 ×36	36"×36"	36"x36	36"x36"		48 ×24		36"x36"	36 x36	24"x24"	24 x24	24 ×24	24"x24	24 ×18	12 ×36	4 x72	4"x72"	4 " x
SUBTOTAL 1	2	0	3	1	1	1	3	0	0	2	0	1	1	2	2	3	0	5	32	0	1	2	1	2	4	0	1	1	0	(
SUBTOTAL 2	0	0	0	0	0	0	3	0	0	0	0	0	0	2	1	1	2	1	47	1	0	0	3	1	2	2	0	0	0	(
SUBTOTAL 3	0	1	1	1	0	0	1	1	2	1	2	0	0	0	1	0	0	0	5	0	1	0	0	1	0	0	0	0	1	
TOTAL	2	1	4	2	1	1	7	1	2	3	2	1	1	4	4	4	2	6	84	1	2	2	4	4	6	2	1	1	1	·

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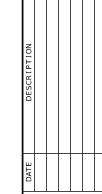
route **KK** MO SHEET NO. SL 39

FRANKLIN

JOB NO.
J6S3619B
CONTRACT ID.

PROJECT NO.

BRIDGE NO.



D-30 STANDARD SIGN ASSEMBLIES SHEET 3 OF 4

			SIG	N SUMMARY			
					SIZE, TYPE & SQ	UARE FEET	
STANDARD SIGN OR	SIGN DETAIL	NO.		FLAT SHEET	FLAT SHEET	STRUCTURAL	STRUCTURAL
SPECIAL SIGN NUMBER	SHEET NO.	EACH	SIZE	SH	FLUORESCENT SHF *	ST	FLUORESCENT STF *
				ITEM NO. 9035004A	ITEM NO. 9035069A	ITEM NO. 9035011A	ITEM NO. 9035071A
M1 - 5	STANDARD	2	30"x24"	10			
M1 - 5	STANDARD	1	30"x24"	5			
M1-5A (KK)	STANDARD	4	30"x24"	20			
M2 - 1	STANDARD	2	21"x15"	4.38			
M6 - 1	STANDARD	1	21"x15"	2.19			
M6 - 4	STANDARD	1	21"x15"	2.19			
R1-1	STANDARD	7	36"x36"	63			
R1-1	STANDARD	1	48"x48"	16			
R1-2	STANDARD	2	48"x48"	32			
R2-1 (55)	STANDARD	3	36"x48"	36			
R5-1	STANDARD	2	36"x36"	18			
W1 - 1 L	STANDARD	1	36"x36"		9		
W1 - 1R	STANDARD	1	36"x36"		9		
W1-2L	STANDARD	4	36"x36"		36		
W1 - 2R	STANDARD	4	36"x36"		36		
W1 - 4 L	STANDARD	4	36"x36"		36		
W1 - 5R	STANDARD	2	36"x36"		18		
W1 - 6	STANDARD	6	48"x24"		48		
W1 - 8D	STANDARD	84	18"x24"		252		
W2 - 2	STANDARD	1	36"x36"		9		
W3 - 1	STANDARD	2	36"x36"		18		
W13-1P (30)	STANDARD	2	24"x24"		8		
W13-1P (35)	STANDARD	4	24"x24"		16		
W13-1P (40)	STANDARD	4	24"x24"		16		
W13-1P (45)	STANDARD	6	24"x24"		24		
W16-3P	STANDARD	2	24"x18"		6		
OM3 - R	STANDARD	1	12"x36"		3		
DL - 3_Y	STANDARD	1	4"×72"		2		
DL - 3_R	STANDARD	1	4"x72"	2			
DL - 3_W	STANDARD	1	4"×72"	2			
	SUE	ЗТОТА	ALS	212.8	546.0	0.0	0.0
	PAY	TOT	ALS	213	546	0	0

* ORANGE, YELLOW & YELLOW/GREEN

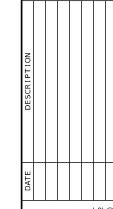
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IN TO THE PREPARED TO THE PREP

PROJECT NO.

BRIDGE NO.



COMMISSION

COMMISSION

MODOT

105 WEST CAPITO

JEFFERSON CITY, MO 6510

D-30 SIGN SUMMARY SHEET 4 OF 4

SIGN NO.	221
STATION	76+49, LT
ROADWAY	ROUTE KK

4"

NOTES:

DELINEATORS SHALL BE FABRICATED FROM 0.080" ALUMINUM.

DELINEATORS SHALL BE COVERED WITH ASTM TYPE IX OF XI SHEETING ON ONE SIDE AS SPECIFICED.

BOLT HOLES IN DELINEATORS SHALL BE 3/8" DIAMETER AND THE SPACING DIMENSIONS ARE BASED ON CENTER OF HOLES.

DL-3 Y SHF-FLAT SHEET FLUORESCENT; NO BORDER, YELLOW

SIGN NO. 331 (2) STATION 266+24, RT ROADWAY ROUTE KK

4"

NOTES:

DELINEATORS SHALL BE FABRICATED FROM 0.080 ALUMINUM.

DELINEATORS SHALL BE COVERED WITH ASTM TYPE IX OF XI SHEETING ON ONE SIDE AS SPECIFICED.

BOLT HOLES IN DELINEATORS SHALL BE 3/8" DIAMETER AND THE SPACING DIMENSIONS ARE BASED ON CENTER OF HOLES.

ONE DL-3 R SH-FLAT SHEET NO BORDER, RED (FRONT SIDE) ONE DL-3_W SH-FLAT SHEET NO BORDER, WHITE (BACK SIDE)

SHIRLEY J NORRIS 10/15/2024 KK MO SL 41 FRANKLIN J6S3619B CONTRACT ID. PROJECT NO. BRIDGE NO.

D-31 SIGN DETAIL SHEET SHEET 1 OF 1

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