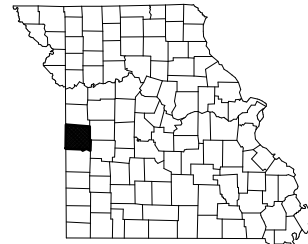


A.A.D.T. CONST. - 2025 = 7255
A.A.D.T. DESIGN - 2045 = 8016
DVH (E/W) = 48.1%/51.9%
T = 17.91%
V = 35/45/55/60 M.P.H.

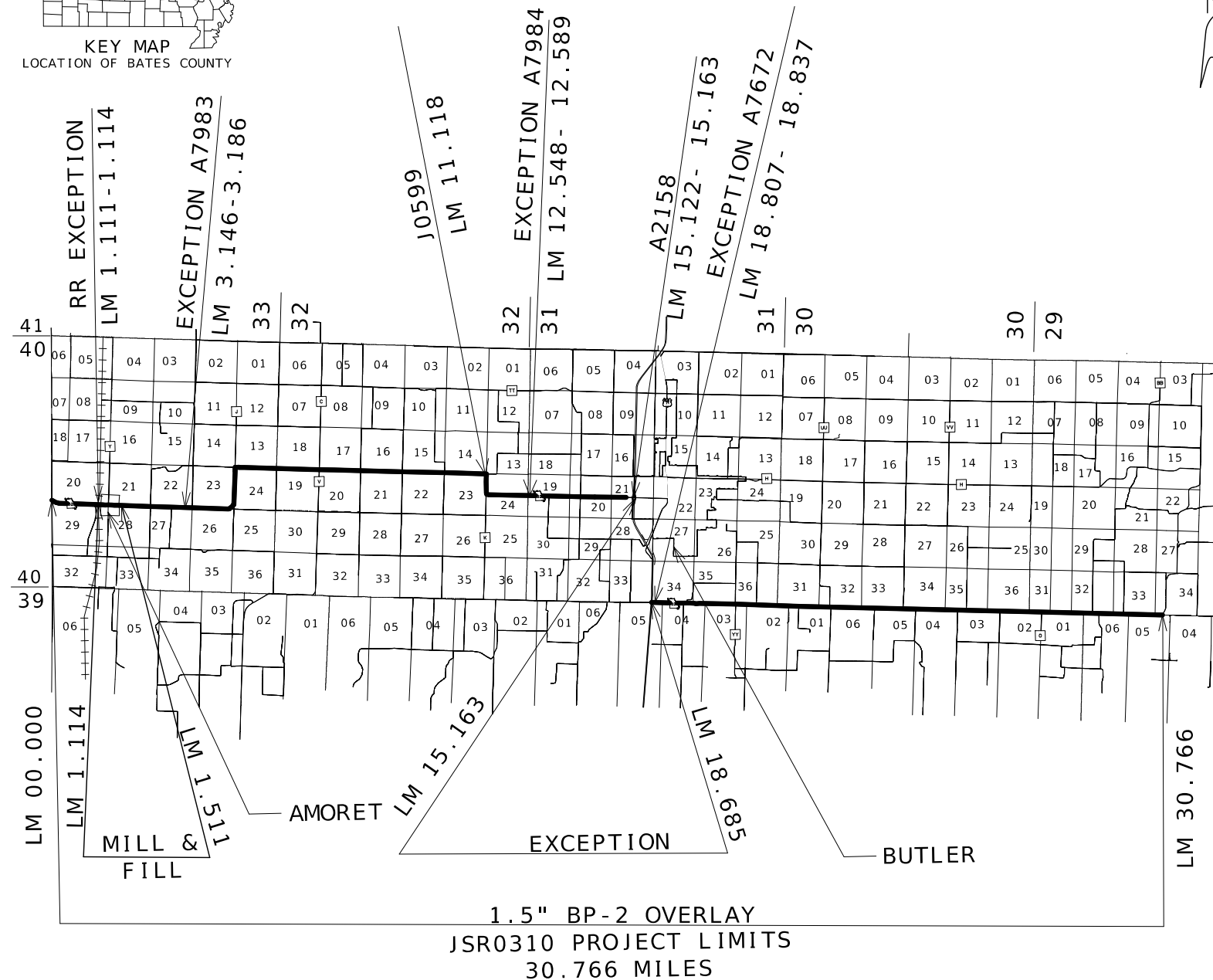
NO NEW R/W REQUIRED

	EXISTING	NEW
BUILDINGS AND STRUCTURES		
GUARD RAIL		
GUARD CABLE		
CONCRETE RIGHT-OF-WAY MARKER		
STEEL RIGHT-OF-WAY MARKER		
LOCATION SURVEY MARKER		
FIBER OPTICS	— FO —	— FO —
OVERHEAD CABLE TV	— OTV —	— OTV —
UNDERGROUND CABLE TV	— UTV —	— UTV —
OVERHEAD TELEPHONE	— OT —	— OT —
UNDERGROUND TELEPHONE	— UT —	— UT —
OVERHEAD POWER	— OE —	— OE —
UNDERGROUND POWER	— UE —	— UE —
SANITARY SEWER	— S —	— S —
STORM SEWER	— SS —	— SS —
GAS	— G —	— G —
WATER	— W —	— W —
MANHOLE		
FIRE HYDRANT		
WATER VALVE		
WATER METER		
DROP INLET		
DITCH BLOCK		
GROUND MOUNTED SIGN		
LIGHT POLE		
H-FRAME POWER POLE		
TELEPHONE PEDESTAL		
FENCE		
CHAIN LINK		
WOVEN WIRE		
GATE POST		
BENCHMARK		

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
PLANS FOR PROPOSED
STATE HIGHWAY
BATES COUNTY



KEY MAP 
LOCATION OF BATES COUNTY

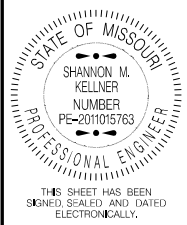


THE EXISTENCE AND APPROXIMATE LOCATION OF UTILITY FACILITIES KNOWN TO EXIST, AS SHOWN ON THE PLANS, ARE BASED ON THE BEST INFORMATION AVAILABLE TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE. RELIANCE UPON THIS INFORMATION IS DONE AT THE RISK AND PERIL OF THE USER, AND THE COMMISSION SHALL NOT BE LIABLE FOR ANY DAMAGES THAT MAY ARISE FROM ANY ERROR IN THE INFORMATION. IT IS, THEREFORE, THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE, LOCATION AND STATUS OF ANY FACILITY. SUCH VERIFICATION INCLUDES DIRECT CONTACT WITH THE LISTED UTILITIES.

DESCRIPTION

SHEET
NUMBER

TITLE SHEET -----	1
TYPICAL SECTIONS (TS) (7 SHEETS)---	2
QUANTITIES (QU) (5 SHEETS)-----	3
SPECIAL SHEETS (SS)-----	4
TRAFFIC CONTROL SHEETS (TC)-----	5-13
BRIDGE DRAWINGS (B)	
A2151-----	1-3



DATE PREPARED	
8/19/2025	
ROUTE	STATE
52	MO
DISTRICT	SHEET NO.
SW	1
COUNTY	
BATES	
JOB NO.	
JSR0310	
CONTRACT ID.	
f	
PROJECT NO.	
BRIDGE NO.	

[illegible]MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

LOG MILES FROM 2023 ARAN

BEGINNING OF PROJECT	LM. 00.000
END OF PROJECT	LM. 30.766
APPARENT LENGTH	156,578.48 FEET

EQUATIONS AND EXCEPTIONS:

RR CROSSING	LM 1.111 - 1.114
A7983	LM 3.146 - 3.186
A7984	LM 12.548 - 12.589
I-49	LM 15.163 - 18.685
A7672	LM 18.807 - 18.837

TOTAL CORRECTIONS	19303.68	FEET
NET LENGTH OF PROJECT	143140.8	FEET
STATE LENGTH	27.13	MILES

FOR INFORMATION ONLY	
ESTIMATED DISTURBED ACRES	00 ACRES

NOTES: LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

EXCEPTIONS:

LOG MILE 1.111 TO LOG MILE 1.114 - RR CROSSING

LOG MILE 3.146 TO LOG MILE 3.186 - BRIDGE A7983

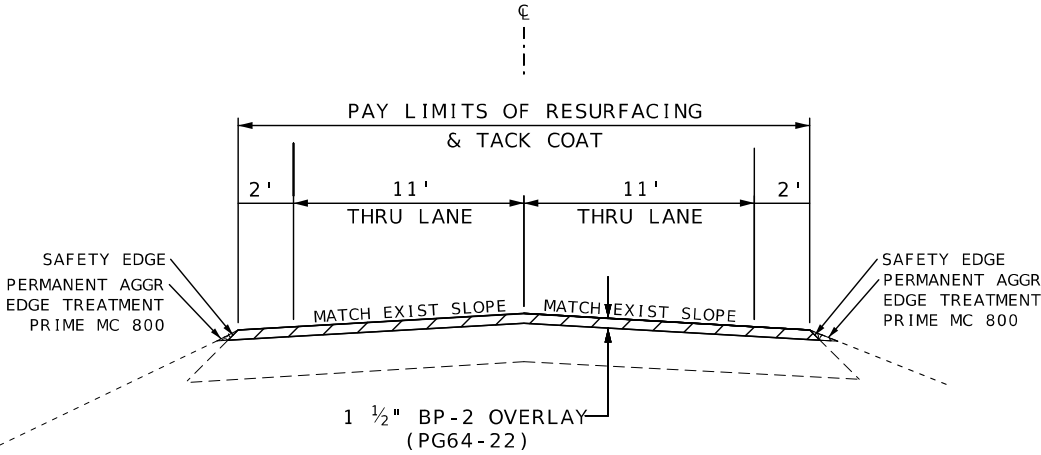
LOG MILE 12.548 TO LOG MILE 12.589 - BRIDGE A7984

LOG MILE 15.163 TO LOG MILE 18.685 - 52

LOG MILE 18.807 TO LOG MILE 18.837 - BRIDGE A7672

REVISED: 8/26/2024

ASPHALT FACTORS	
COMBINED FACTOR	
BP-2 (PG64-22)	2.000 TONS/CY
TACK COAT	
OVERLAY	0.08 GAL/SY
MILLING	0.10 GAL/SY
PRIME LIQUID ASPHALT MC 800	
PERMANENT AGGREGATE EDGE TREATMENT	0.25 GAL/SY
IRREGULARITIES: 75 TONS/MI	



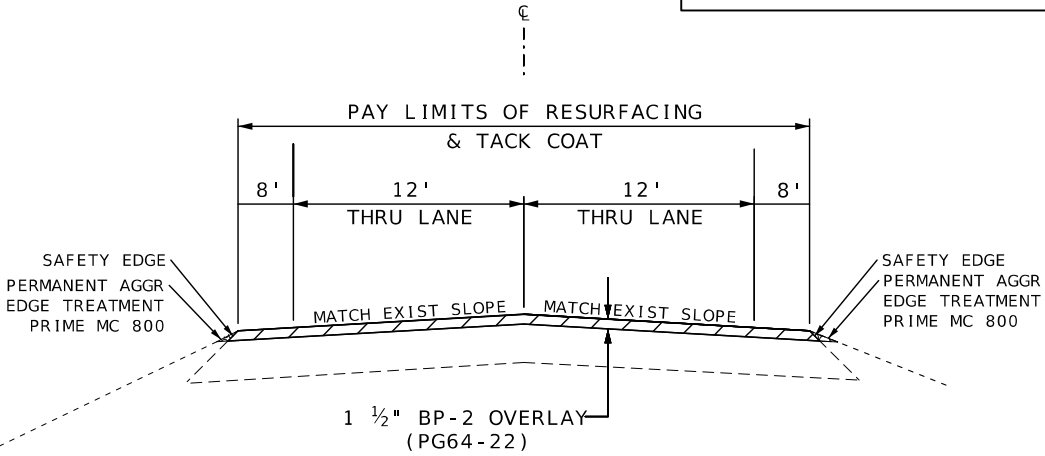
ROUTE 52

LOG MILE 0.000 TO LOG MILE 1.111

LOG MILE 1.511 TO LOG MILE 3.146

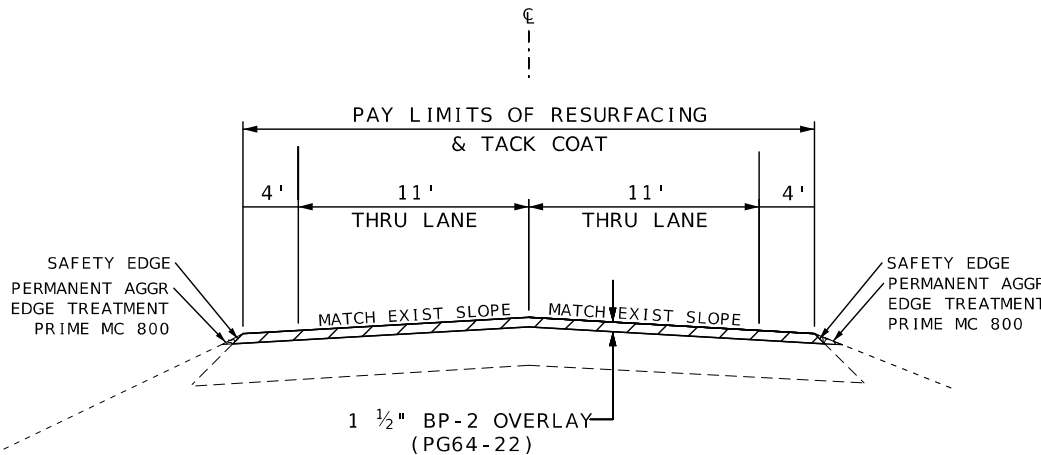
LOG MILE 3.186 TO LOG MILE 12.548

LOG MILE 12.589 TO LOG MILE 14.888



ROUTE 52

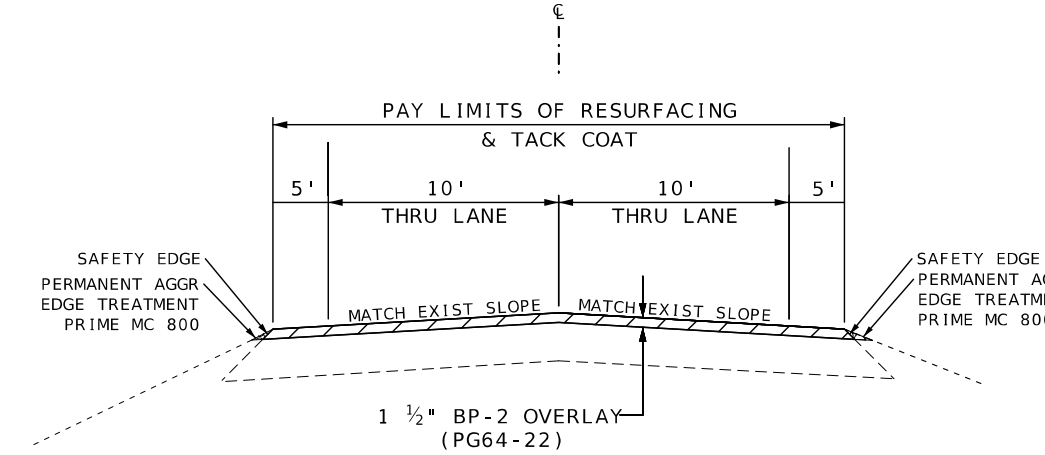
LOG MILE 14.888 TO LOG MILE 15.163



ROUTE 52

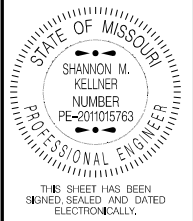
LOG MILE 18.685 TO LOG MILE 18.807

LOG MILE 18.837 TO LOG MILE 18.943



ROUTE 52

LOG MILE 18.943 TO LOG MILE 30.766



DATE PREPARED 7/23/2025	
ROUTE 52	STATE MO
DISTRICT SW	SHEET NO. 2
COUNTY BATES	
JOB NO. JSR0310	
CONTRACT ID.	

PROJECT NO.

BRIDGE NO.

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

NOTES: LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

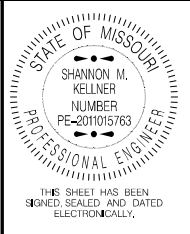
NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL
COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE
TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO
ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

REVISED: 8/26/2024

ASPHALT FACTORS	
COMBINED FACTOR	
BP-2 (PG64-22)	2.000 TONS/CY
TACK COAT	
OVERLAY	0.08 GAL/SY
MILLING	0.10 GAL/SY
PRIME LIQUID ASPHALT MC 800	
PERMANENT AGGREGATE	0.25 GAL/SY
EDGE TREATMENT	
IRREGULARITIES: 75 TONS/MI	



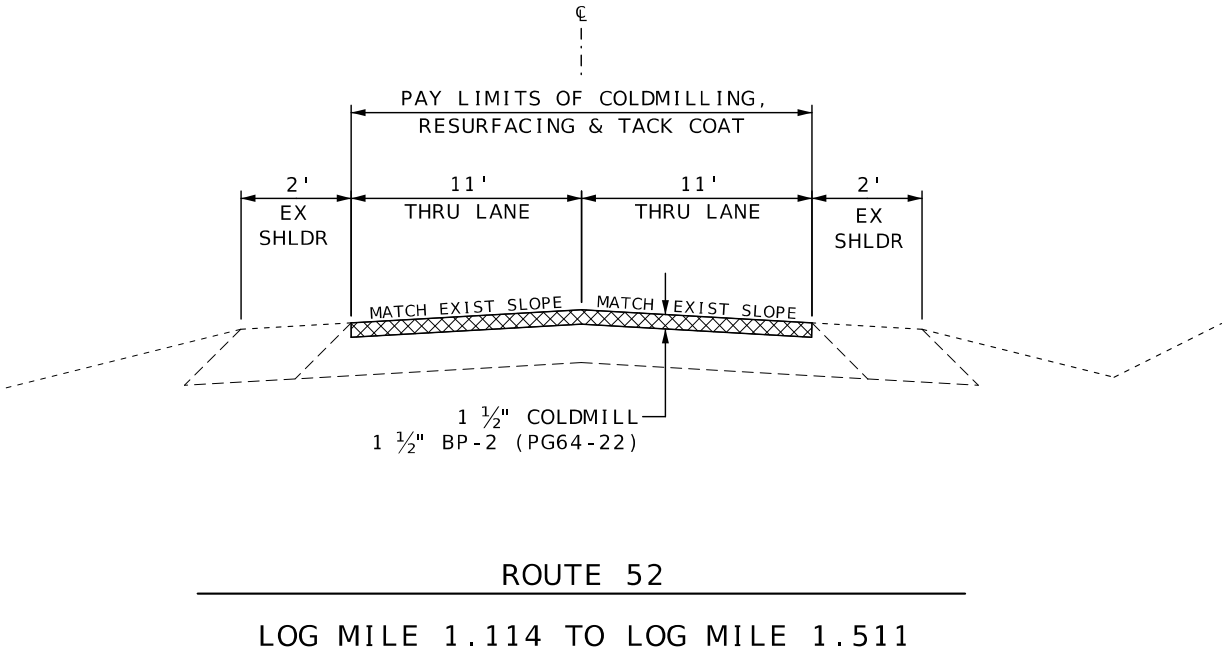
DATE PREPARED 7/23/2025	
ROUTE 52	STATE MO
DISTRICT SW	SHEET NO. 2
COUNTY BATES	
JOB NO. JSR0310	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION



105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)






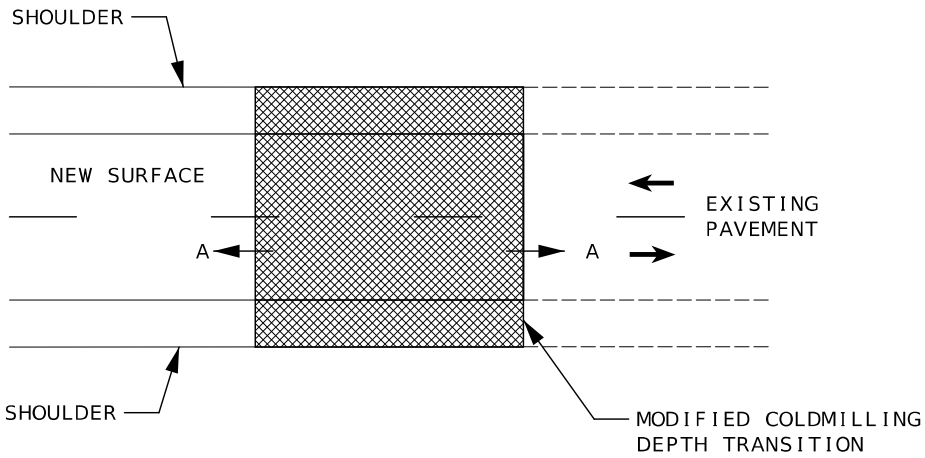
DATE PREPARED 7/23/2025	
ROUTE 52	STATE MO
DISTRICT SW	SHEET NO. 2
COUNTY BATES	
JOB NO. JSR0310	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

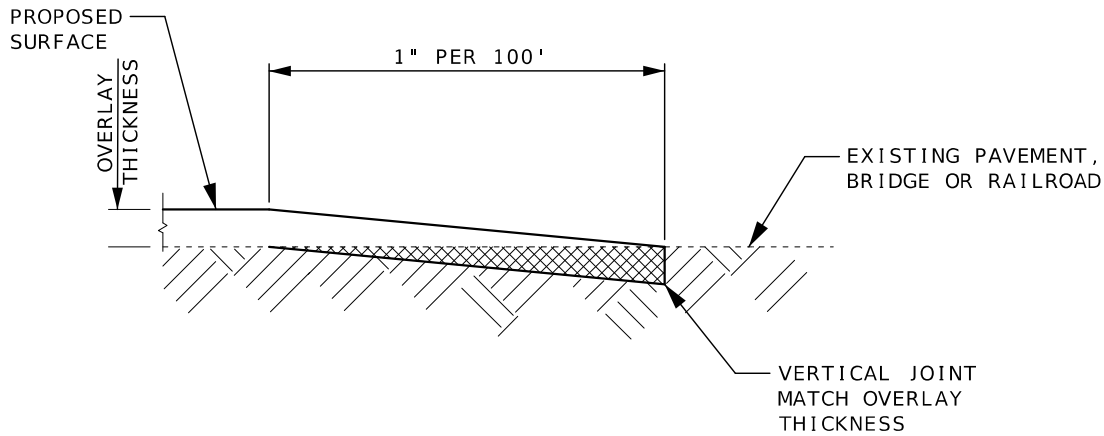
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION



105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



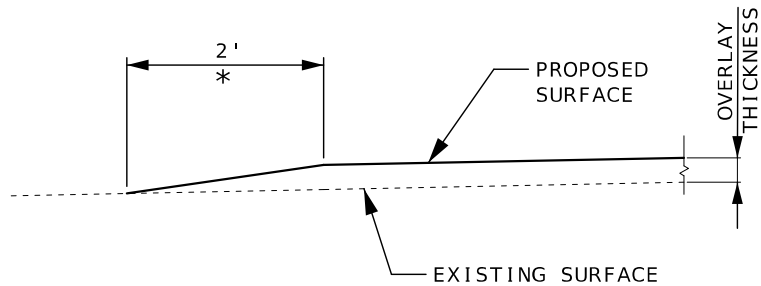
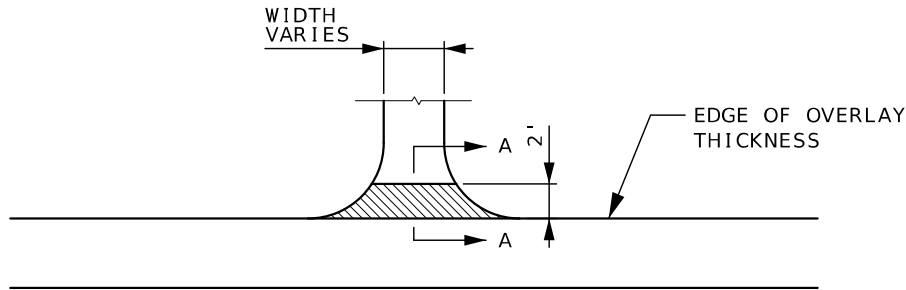
MAINLINE, BRIDGE, AND RAILROAD PLAN



SECTION A-A

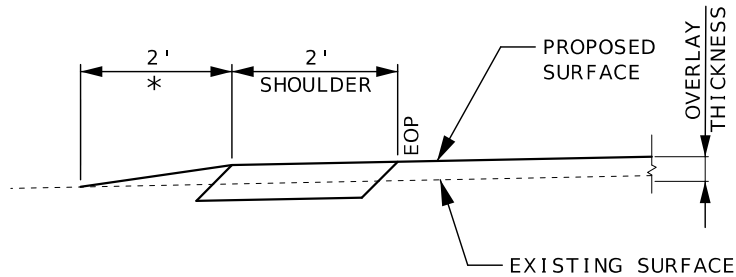
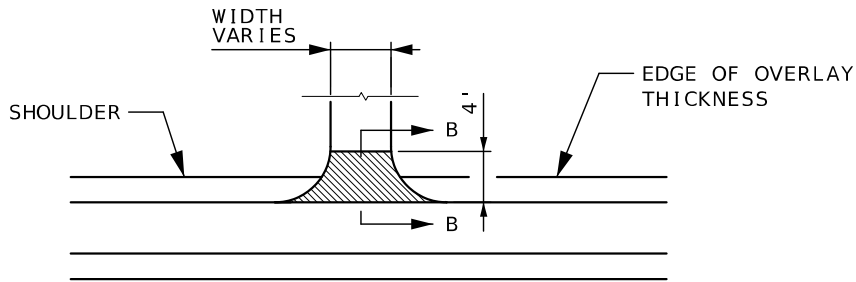
TRANSVERSE JOINT TRANSITION FOR SINGLE LIFT
PROJECT BEGINNING, END, BRIDGE, OR RAILROAD

NOTE: MILL EXISTING CONCRETE 1" MAXIMUM.

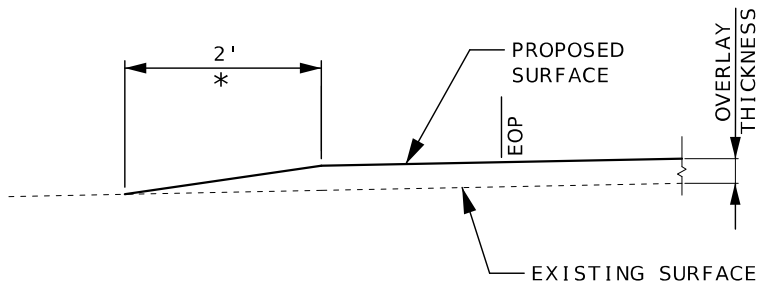


SECTION A-A
TYPICAL ENTRANCE - NO SHOULDER
(FIELD, PRIVATE OR COUNTY ROAD)
*TAPER AT 1:1 FOR FIELD ENTRANCE

NOTE: 4 TONS GRAVEL A OR CRUSHED STONE
CALCULATED PER ENTRANCE.
6 TONS CALCULATED PER COUNTY ROAD.



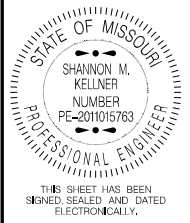
AGGREGATE & EARTH ENTRANCES



ASPHALT & CONCRETE ENTRANCES

SECTION B-B
TYPICAL ENTRANCE - EXIST/NEW SHOULDER
(FIELD, PRIVATE OR COUNTY ROAD)
*TAPER AT 1:1 FOR FIELD ENTRANCE

NOTES: DO NOT BUILD 2' MODIFIED A3 SHOULDERS
ACROSS CONCRETE OR ASPHALT ENTRANCES,
STREETS OR ROADS.
4 TONS GRAVEL A OR CRUSHED STONE B
CALCULATED PER ENTRANCE.
6 TONS CALCULATED PER COUNTY ROAD.



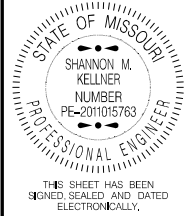
DATE PREPARED 7/23/2025	
ROUTE 52	STATE MO
DISTRICT SW	SHEET NO. 2
COUNTY BATES	
JOB NO. JSR0310	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

MoDOT



DATE PREPARED

7/23/2025	
-----------	--

ROUTE	STATE
52	MO

52	MO
DISTRICT	SHEET NO.

SW	2
----	---

COUNTY RATES	
Admission	100.00
Room	100.00
Board	100.00
Laundry	100.00
Telephone	100.00
Light	100.00
Heat	100.00
Insurance	100.00
Medical	100.00
Nursing	100.00
Pharmacy	100.00
Transportation	100.00
Other	100.00
Total	1000.00

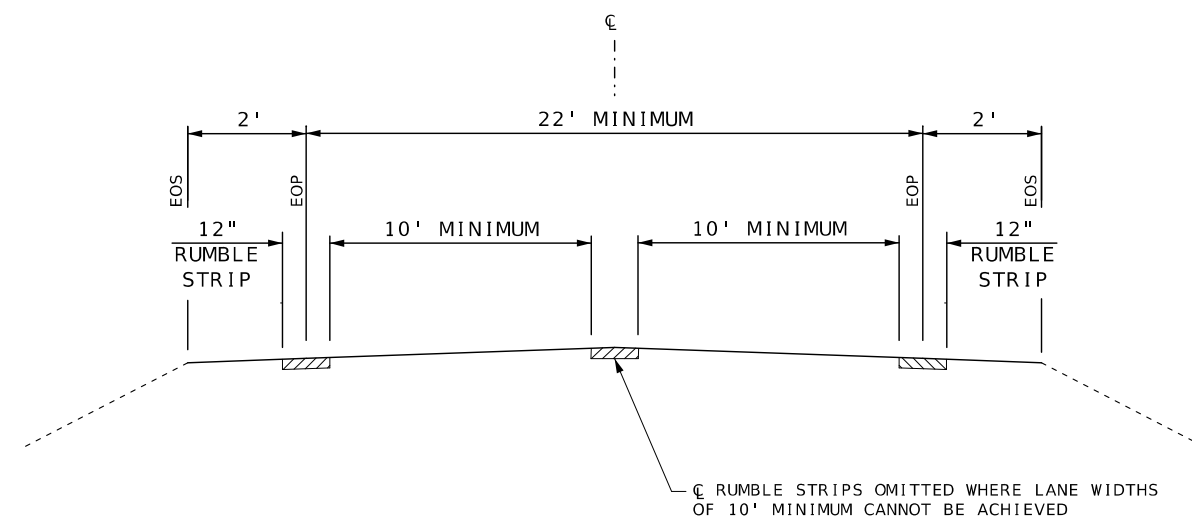
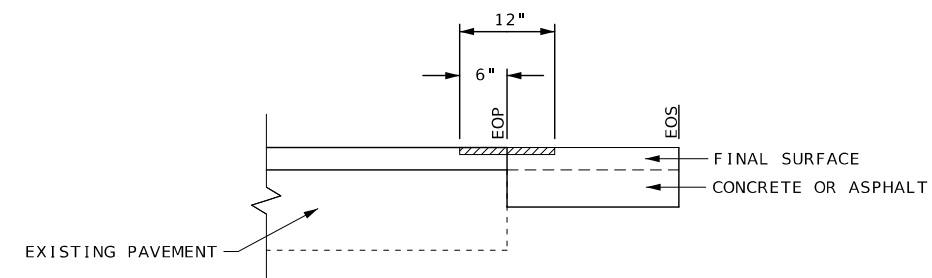
DATE
JOB NO.

JSR0310

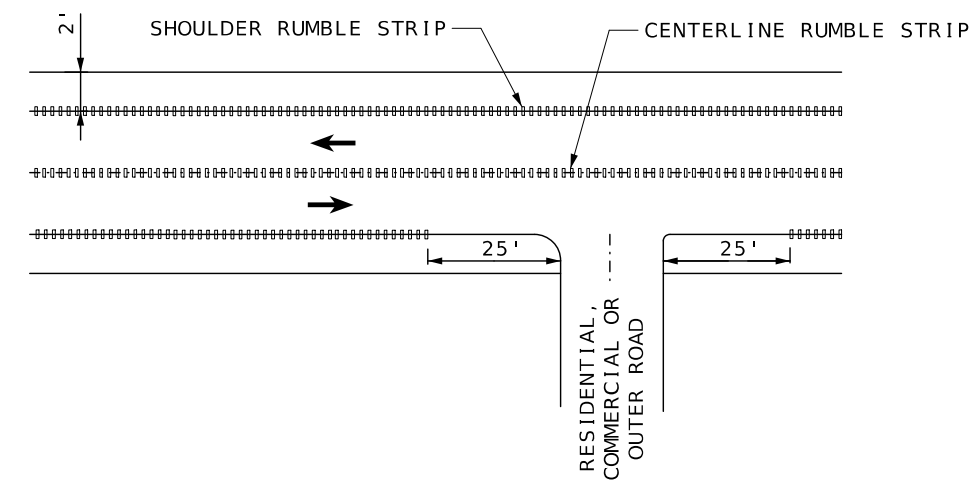
RUMBLE STRIPS FOR 2' MODIFIED A3 SHOULDERS

AS APPROVED BY THE ENGINEER, A 6" MINI RUMBLE STRIP
MAY BE USED WHERE A 12" RUMBLE STRIP DOES NOT FIT.

RUMBLE STRIP DETAILS



INTERSECTION RUMBLE STRIP LAYOUT

[illegible]MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

THIS SHEET HAS BEEN
SIGNED, SEALED AND DATED
ELECTRONICALLY.

DATE PREPARED
7/23/2025

ROUTE	STATE
52	MO

DISTRICT	SHEET NO
SW	3

COUNTY
BATES

JOB NO.
JSR0310

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

[illegible]

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102

MGS GUARDRAIL										
LOG MILE TO LOG MILE			LOCATION	MGS GUARDRAIL	MASH CRASHWORTHY END TERMINAL	MGS HEIGHT AND BLOCK TRANSITION	MGS BRIDGE APPROACH TRANSITION "REGULAR/ NO CURB"	MGS BRIDGE APPROACH TRANSITION "THRIE-BEAM"	SHAPING SLOPES CL III	REMARKS
				FT	EA	EA	EA	EA	100FT	
14.967	-	15.067	RT	575.0	1				0.8	
14.967	-	15.067	LT	575.0	1				0.8	
15.017	-	15.127	RT	25.0				1		
15.017	-	15.127	LT	25.0				1		
15.157	-	15.177	RT	25.0				1		
15.157	-	15.177	LT	25.0				1		
	-									
	-									
			TOTAL	1250.0	2	0	0	4	1.6	ALL GUARDRAIL LENGTH IS ROUNDED UP TO THE NEAREST 12.5'
			USE	1250	2	0	0	4	2	

REMOVAL OF IMPROVEMENTS								
ROUTE	LOG MILE	-	LOG MILE	LT / RT	UNIT	TOTAL	DESCRIPTION	REMARKS
52	14.967	-	15.067	LT	f t	625	GUARDRAIL	
52	14.967	-	15067	RT	f t	625	GUARDRAIL	
52	15.017	-	15.217	LT	f t	62.5	GUARDRAIL	
52	15.017	-	15.217	RT	f t	62.5	GUARDRAIL	
52	15.157	-	15.177	LT	f t	62.5	GUARDRAIL	
52	15.157	-	15.177	RT	f t	62.5	GUARDRAIL	
1 LUMP SUM								

PERMANENT AGGREGATE EDGE TREATMENT							
ROUTE	LOG MILE	TO	LOG MILE	LENGTH	AGGREGATE 200 TON/MILE*	PRIME MC 800 590 GAL/MILE*	REMARKS
				MILES	TON	GAL	
52	0.000	-	1.106	1.106	221.2	653	
52	1.501		3.146	1.645	329.0	971	
52	3.186		12.548	9.362	1872.4	5524	
52	12.589		15.087	2.498	499.6	1474	
52	18.843	-	30.776	11.933	2386.6	7040	
TOTALS					5308.8	15662	*INCLUDES 2' WIDTH BOTH SIDES OF ROADWAY

BITUMINOUS SHOULDER RUMBLE STRIP						
LOG MILE		LOG TO MILE	LENGTH FT	LOCATION	RUMBLE STRIP STA	REMARKS
0.000	-	1.058	5586.2	LT EOP	55.9	
0.000	-	1.058	5586.2	RT EOP	55.9	RT EOP
1.816		3.146	7022.4	LT EOP	70.2	LT EOP
1.816		3.146	7022.4	RT EOP	70.2	RT EOP
3.186		12.548	49431.4	LT EOP	494.3	LT EOP
3.186		12.548	49431.4	RT EOP	494.3	RT EOP
12.589		14.886	12128.2	LT EOP	121.3	LT EOP
12.589		14.886	12128.2	RT EOP	121.3	RT EOP
18.924		30.766	62525.8	LT EOP	625.3	LT EOP
18.924		30.766	62525.8	RT EOP	625.3	RT EOP
	-		-	LT/RT	-255.3	SUBTRACT ENT'S/SIDEROADS & BEFORE/AFTER THEM
TOTAL					2478.7	TOTAL
USE					2478.7	USE

SUMMARY OF QUANTITIES
SHEET 2 OF 5

PAVEMENT MARKING										
ROUTE	LOG MILE	TO	LOG MILE	LOCATION	WATERBORNE PAINT, TYPE P BEADS			THERMOPLASTIC		REMARKS
					4" SOLID WHITE	4" INT YELLOW	4" SOLID YELLOW	24" MIDBLOCK	RR CROSSING	
					LF	LF	LF	L.F.	EACH	
52.+	0.000		0.178	LT/RT	1880.0		1880.0			
52.+	0.178		0.339	LT/RT	1700.0	210.0	850.0			
52.+	0.339		0.478	LT/RT	1470.0	180.0	730.0			
52.+	0.478		0.819	LT/RT	3600.0	450.0	1800.0			
52.+	0.819		0.857	LT/RT	400.0		200.0			
52.+	0.857		1.018	LT/RT	1700.0	210.0	850.0			
52.+	0.998			LT/RT					1	RR CROSSING
52.+	1.018		1.111	LT/RT	980.0		490.0			
52.+	1.119		1.496	LT/RT	3980.0		1990.0			
52.+	1.218			LT/RT				10		MIDBLOCK CROSSING
52.+	1.237			LT/RT					1	RR CROSSING
52.+	1.298			LT/RT				10		MIDBLOCK CROSSING
52.+	1.358			LT/RT				10		MIDBLOCK CROSSING
52.+	1.438			LT/RT				10		MIDBLOCK CROSSING
52.+	1.496		1.797	LT/RT	3180.0	400.0	1590.0			
52.+	1.498			LT/RT				10		MIDBLOCK CROSSING
52.+	1.797		1.877	LT/RT	840.0		420.0			
52.+	1.877		2.096	LT/RT	2310.0	290.0	1160.0			
52.+	2.096		2.177	LT/RT	860.0	110.0				
52.+	2.177		2.316	LT/RT	1470.0	180.0	730.0			
52.+	2.316		2.378	LT/RT	650.0	80.0				
52.+	2.378		2.478	LT/RT	1060.0	130.0	530.0			
52.+	2.478		2.578	LT/RT	1060.0	130.0				
52.+	2.578		2.776	LT/RT	2090.0	260.0	1050.0			
52.+	2.776		2.816	LT/RT	420.0		210.0			
52.+	2.816		2.976	LT/RT	1690.0	210.0	840.0			
52.+	2.976		3.476	LT/RT	5280.0	660.0				
52.+	3.476		3.676	LT/RT	2110.0	260.0	1060.0			
52.+	3.676		3.716	LT/RT	420.0		210.0			
52.+	3.716		4.056	LT/RT	3590.0	450.0	1800.0			
52.+	4.056		4.480	LT/RT	4480.0	560.0	2240.0			
52.+	4.480		4.660	LT/RT	1900.0	240.0	950.0			
52.+	4.660		4.700	LT/RT	420.0	50.0				
52.+	4.700		4.879	LT/RT	1890.0	240.0	950.0			
52.+	4.879		5.458	LT/RT	6110.0		3060.0			
52.+	5.458		5.618	LT/RT	1690.0	210.0	840.0			
52.+	5.618		5.678	LT/RT	630.0	80.0				
52.+	5.678		5.797	LT/RT	1260.0	160.0	630.0			
52.+	5.797		5.818	LT/RT	220.0	30.0				
52.+	5.818		6.576	LT/RT	8000.0	1000.0	4000.0			
52.+	6.576		6.676	LT/RT	1060.0	130.0				
52.+	6.676		6.815	LT/RT	1470.0	180.0	730.0			
52.+	6.815		6.856	LT/RT	430.0	50.0				
52.+	6.856		6.976	LT/RT	1270.0	160.0	630.0			
52.+	6.976		7.056	LT/RT	840.0	110.0				
52.+	7.056		7.415	LT/RT	3790.0	470.0	1900.0			
52.+	7.415		7.515	LT/RT	1060.0	130.0				
52.+	7.515		7.854	LT/RT	3580.0	450.0	1790.0			
52.+	7.854		8.234	LT/RT	4010.0	500.0				
52.+	8.234		8.414	LT/RT	1900.0	240.0	950.0			
52.+	8.414		8.434	LT/RT	210.0	30.0				
52.+	8.434		8.594	LT/RT	1690.0	210.0	840.0			
52.+	8.594		8.973	LT/RT	4000.0	500.0				
52.+	8.973		9.113	LT/RT	1480.0	180.0	740.0			
52.+	9.113		9.172	LT/RT	620.0		310.0			
52.+	9.172		9.372	LT/RT	2110.0	260.0	1060.0			
52.+	9.372		9.473	LT/RT	1070.0	130.0				
52.+	9.473		9.792	LT/RT	3370.0	420.0	1680.0			
52.+	9.792		10.011	LT/RT	2310.0	290.0				
52.+	10.011		10.391	LT/RT	4010.0	500.0	2010.0			
52.+	10.391		10.471	LT/RT	840.0		420.0			
52.+	10.471		10.871	LT/RT	4220.0	530.0	2110.0			
52.+	10.871		10.950	LT/RT	830.0	100.0				
52.+	10.950		11.091	LT/RT	1490.0	190.0	740.0			
52.+	11.091		11.712	LT/RT	6560.0		3280.0			
52.+	11.712		11.892	LT/RT	1900.0	240.0	950.0			
52.+	11.892		12.531	LT/RT	6750.0	840.0				
52.+	12.531		12.950	LT/RT	4420.0	550.0	2210.0			
52.+	12.950		12.990	LT/RT	420.0	50.0				
52.+	12.990		13.587	LT/RT	6300.0	790.0	3150.0			
52.+	13.587		13.808	LT/RT	2330.0	290.0	1170.0			
52.+	13.808		14.007	LT/RT	2100.0	260.0	1050.0			
		-								
SUBTOTAL 1					147780	15560	58780	50	2	

STATE OF MISSOURI

SHANNON M. KELLNER

NUMBER PE-2011016783

PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY.

DATE PREPARED
7/23/2025

ROUTE
52

DISTRICT
SW

STATE
MO

SHEET NO.
3

COUNTY
BATES

JOB NO.
JSR0310

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

																EFFECTIVE: 04-01-2024				
SIGN	SIZE IN.	AREA SQ.FT.	QTY EACH	TOTAL AREA SQ.FT.	QTY RELOC EACH	TOTAL RELOC SQ.FT.	SIGN NUM.	DESCRIPTION	SIGN	SIZE IN.	AREA SQ.FT.	QTY EACH	TOTAL SQ.FT.	QTY RELOC EACH	TOTAL RELOC SQ.FT.	SIGN NUM.	DESCRIPTION	ITEM NUMBER	TOTAL QTY	DESCRIPTION
WARNING SIGNS									GUIDE SIGNS											
W01-1L	48X48	16.00						TURN (SYMBOL LEFT ARROW)	E05-1	36X48	12.00						GORE EXIT	6122008		IMPACT ATTENUATOR 40 MPH (SAND BARRELS)
W01-1R	48X48	16.00						TURN (SYMBOL RIGHT ARROW)	E05-2	48X36	12.00						EXIT OPEN	6122009		IMPACT ATTENUATOR 45 MPH (SAND BARRELS)
W01-2L	48X48	16.00						CURVE (SYMBOL LEFT ARROW)	E05-2a	48X36	12.00						EXIT CLOSED	6122010		IMPACT ATTENUATOR 50 MPH (SAND BARRELS)
W01-2R	48X48	16.00						CURVE (SYMBOL RIGHT ARROW)	GO20-1	60X24	10.00	2	20				ROAD WORK NEXT XX MILES	6122012		IMPACT ATTENUATOR 55 MPH (SAND BARRELS)
W01-3L	48X48	16.00						REVERSE TURN (SYMBOL LEFT ARROW)	GO20-2	48X24	8.00	2	16				END ROAD WORK	6122014		IMPACT ATTENUATOR 60 MPH (SAND BARRELS)
W01-3R	48X48	16.00						REVERSE TURN (SYMBOL RIGHT ARROW)	GO20-4	36X18	4.50	2	9				PILOT CAR FOLLOW ME	6122017		IMPACT ATTENUATOR 65 MPH (SAND BARRELS)
W01-4L	48X48	16.00						REVERSE CURVE (SYMBOL LEFT ARROW)	GO20-4a	42X30	8.75	0					PILOT CAR IN USE WAIT & FOLLOW	6122019		IMPACT ATTENUATOR 70 MPH (SAND BARRELS)
W01-4R	48X48	16.00						REVERSE CURVE (SYMBOL RIGHT ARROW)	GO20-4a	18X12	1.50	2	3				PILOT CAR IN USE WAIT & FOLLOW	6122020		REPLACEMENT SAND BARREL
W01-4bL	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL LT ARROWS)	GO20-5aP	36X24	6.00						WORK ZONE (PLAQUE)	6122030		IMPACT ATTENUATOR (RELOCATION)
W01-4bR	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL RT ARROWS)	MO4-8a	24X18	3.00						END DETOUR	6123001		TRUCK MOUNTED ATTENUATOR (TMA)
W01-4cL	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL LT ARROWS)	MO4-9L	48X36	12.00						DETOUR (LEFT ARROW)	6161008	2	ADVANCED WARNING RAIL SYSTEM
W01-4cR	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL RT ARROWS)	MO4-9R	48X36	12.00						DETOUR (RIGHT ARROW)	6161012		BUOYS (BOATS KEEP OUT)
W01-6	60X30	12.50						HORIZONTAL ARROW (SYMBOL)	MO4-9P	48X12	4.00						STREET NAME (PLAQUE)	6161013		BUOYS (NO WAKE)
W01-6a	72X36	18.00						HORIZ. ARROW (SYMBOL ON PERMANENT BARRICADE)	MO4-10L	48X18	6.00						DETOUR (ARROW LEFT)	6161014		SPECIAL SIGN ASSEMBLY (BOATS KEEP OUT)
W01-7	60X30	12.50						DOUBLE HEAD HORIZONTAL ARROW (SYMBOL)	MO4-10R	48X18	6.00						DETOUR (ARROW RIGHT)	6161025	100	CHANNELIZER (TRIM LINE)
W01-7a	72X36	18.00						DOUBLE HEAD HORIZ. ARROW (SYMBOL ON PERM. BARR.)	REGULATORY SIGNS								6161030		TYPE III MOVEABLE BARRICADE	
W01-8	18X24	3.00						CHEVRON (SYMBOL)	R1-1	48X48	13.25						STOP	6161033		DIRECTION INDICATOR BARRICADE
W01-8a	30X36	7.50						CHEVRON (SYMBOL FOR DIVIDED HIGHWAYS)	R1-2	48TRI.	6.93						YIELD	6161040		FLASHING ARROW PANEL
W03-1	48X48	16.00						STOP AHEAD (SYMBOL)	R1-2a	36X36	9.00						TO ONCOMING TRAFFIC (PLAQUE)	6161047		TYPE III OBJECT MARKER
W03-2	48X48	16.00						YIELD AHEAD (SYMBOL)	R1-3P	30X12	2.50						ALL WAY (PLAQUE)	6161055		SEQUENTIAL FLASHING WARNING LIGHT
W03-3	48X48	16.00						SIGNAL AHEAD (SYMBOL)	R2-1	36X48	12.00						SPEED LIMIT XX	6161070		TUBULAR MARKER
W03-4	48X48	16.00	2	32				BE PREPARED TO STOP	R3-1	48X48	16.00						NO RIGHT TURN (SYMBOL)	6161095		RADAR SPEED ADVISORY SYSTEM
W03-5	48X48	16.00						SPEED LIMIT AHEAD	R3-2	48X48	16.00						NO LEFT TURN (SYMBOL)	6161096		CHANGEABLE MESSAGE SIGN, COMMISSION FURNISHED/RETAINED
W04-1L	48X48	16.00						MERGE (SYMBOL FROM LEFT)	R3-3	36X36	9.00						NO TURNS			CHANGEABLE MESSAGE SIGN W/O COMM.
W04-1R	48X48	16.00						MERGE (SYMBOL FROM RIGHT)	R3-4	48X48	16.00						NO U-TURN (SYMBOL)	6161098A		CHANGEABLE MESSAGE SIGN W/O COMM. INTERFACE - CONTRACTOR FURNISHED/RETAINED
W04-1aL	48X48	16.00						MERGE (ARROW SYMBOL)	R3-7L	30X30	6.25						LEFT LANE MUST TURN LEFT			CHANGEABLE MESSAGE SIGN WITH COMM. INTERFACE - CONTRACTOR FURNISHED/RETAINED
W04-1aR	48X48	16.00						MERGE (ARROW SYMBOL)	R3-7R	30X30	6.25						RIGHT LANE MUST TURN RIGHT	6161099		WORK ZONE TRAFFIC SIGNAL SYSTEM
W05-1	48X48	16.00						ROAD/BRIDGE/RAMP NARROWS	R4-1	36X48	12.00						DO NOT PASS		4	TEMPORARY LONG-TERM RUMBLE STRIPS
W05-3	48X48	16.00						ONE LANE BRIDGE	R4-2	36X48	12.00						PASS WITH CARE	6162000A		TEMPORARY TRAFFIC BARRIER
W05-5	48X48	16.00						NARROW LANES	R4-8a	36X48	12.00						KEEP LEFT (HORIZONTAL ARROW)	6162002		CONTRACTOR FURNISHED/RETAINED
W06-1	48X48	16.00						DIVIDED HIGHWAY (SYMBOL)	R4-7a	36X48	12.00						KEEP RIGHT (HORIZONTAL ARROW)	6173600D		TEMPORARY TRAFFIC BARRIER
W06-2	48X48	16.00						DIVIDED HIGHWAY END (SYMBOL)	R5-1	30X30	6.25						DO NOT ENTER			CONTRACTOR FURNISHED/RETAINED
W06-3	48X48	16.00						TWO WAY TRAFFIC (SYMBOL)	R5-1a	36X24	6.00						WRONG WAY		TEMPORARY TRAFFIC BARRIER	
W07-3a	30X24	5.00						NEXT XX MILES (PLAQUE)	R6-1L	54X18	6.75						ONE WAY ARROW (LEFT)	6173602B		CONTRACTOR FURNISHED/COMMISSION RETAINED
W08-1	48X48	16.00						BUMP	R6-1R	54X18	6.75						ONE WAY ARROW (RIGHT)	6174000A		TEMP. TRAFFIC BARRIER HEIGHT TRANSITION
W08-2	48X48	16.00						DIP	R6-2L	24X30	5.00						ONE WAY (LEFT)	6175010A		RELOCATING TEMPORARY TRAFFIC BARRIER
W08-3	48X48	16.00						PAVEMENT ENDS	R6-2R	24X30	5.00						ONE WAY (RIGHT)		TEMPORARY TRAFFIC BARRIER	
W08-4	48X48	16.00						SOFT SHOULDER	R9-9	24X12	2.00						SIDEWALK CLOSED	6176000B		COMMISSION FURNISHED/RETAINED
W08-5	48X48	16.00						SLIPPERY WHEN WET (SYMBOL)	R9-11L	24X18	3.00						SIDEWALK CLOSED AHEAD, (ARROW LEFT) CROSS HERE	6177000B		TEMP. TRAFFIC BARRIER HEIGHT TRANSITION
W08-6	48X48	16.00						TRUCK CROSSING	R9-11R	24X18	3.00						SIDEWALK CLOSED AHEAD, (ARROW RIGHT) CROSS HERE			COMMISSION FURNISHED/RETAINED
W08-6c	48X48	16.00						TRUCK ENTRANCE	R10-6	24X36	6.00						STOP HERE ON RED (45° ARROW)	6208064A		TEMPORARY RAISED PAVEMENT MARKER
W08-7	36X36	9.00						LOOSE GRAVEL	R11-2	48X30	10.00						ROAD CLOSED	9029400		TEMPORARY TRAFFIC SIGNALS
W08-7a	36X36	9.00						FRESH OIL/LOOSE GRAVEL									ROAD CLOSED	9029401		TEMPORARY TRAFFIC SIGNALS AND LIGHTING
W08-9	48X48	16.00						LOW SHOULDER	R11-3a	60X30	12.50						ROAD CLOSED XX MILES AHEAD			
W08-11	48X48	16.00	62	992				UNEVEN LANES	R11-4	60X30	12.50						LOCAL TRAFFIC ONLY			
W08-12	48X48	16.00	32	512				NO CENTER LINE	R11-4	60X30	12.50						ROAD CLOSED TO THRU TRAFFIC			
W08-15	48X48	16.00						GROOVED PAVEMENT	CONST-3A	60X48	20.00						FINE SIGN			
W08-15P	30X24	5.00						MOTORCYCLE (PLAQUE)	CONST-3X	56X12	4.67						SPEEDING/PASSING (PLATE)			
W08-17L	48X48	16.00						SHOULDER DROP-OFF (SYMBOL LEFT)	MISCELLANEOUS SIGNS											
W08-17R	30X24	5.00						SHOULDER DROP-OFF (SYMBOL RIGHT)	CONST-5	48X36	12.00						POINT OF PRESENCE			
W08-17P	30X24	5.00						SHOULDER DROP-OFF (PLAQUE)	CONST-5	96X48	32.00	2	64				POINT OF PRESENCE			
W10-1	42RND.	9.62						RAILROAD CROSSING	CONST-7	48X24	8.00						RATE OUR WORK ZONE			
W012-1	24X24	4.00						DOUBLE DOWN ARROW (SYMBOL)	CONST-7	72X36	18.00	2	36				RATE OUR WORK ZONE			
W012-2	48X48	16.00						LOW CLEARANCE (SYMBOL)	CONST-8	48X36	12.00	2	24				WORK ZONE NO PHONE ZONE			
W012-2X	24X18	3.00						LOW CLEARANCE (PLAQUE)	SPECIAL	36X78	19.50	14	273				DETOUR ROUTE ASSEMBLY			
W012-2a	84X24	14.00						OVERHEAD LOW CLEARANCE (FEET AND INCHES)												
W012-4	120X60	50.00						LOW CLEARANCE XX FT XX IN XX MILES AHEAD												
W012-5	120X60	50.00						WIDTH RESTRICTION XX FT XX IN XX MILES AHEAD							</					

STATE OF MISSOURI

SHANNON M. KELLNER

NUMBER PE-20101015763

PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY.

DATE PREPARED
8/19/2025

ROUTE
52

STATE
MO

DISTRICT
SW

SHEET NO.
3

COUNTY
BATES

JOB NO.
JSR0310

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

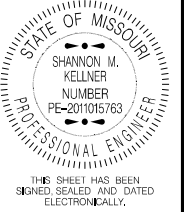
1-888-ASK-MODOT (1-888-275-6636)



NUMBER

PE-2011015763

PROFESSIONAL ENGINEER



THIS SHEET HAS BEEN
SIGNED, SEALED AND DATED
ELECTRONICALLY.

DATE PREPARED

6/17/2025

ROUTE STATE

52 MO

DISTRICT SHEET NO.

SW 4

COUNTY

BATES

JOB NO.

JSR0310

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



LM 1.083-1.111
TRANSITIONAL MILL

LM 1.119
START MILL AND FILL
THRU AMORET



CPKC RR CROSSING

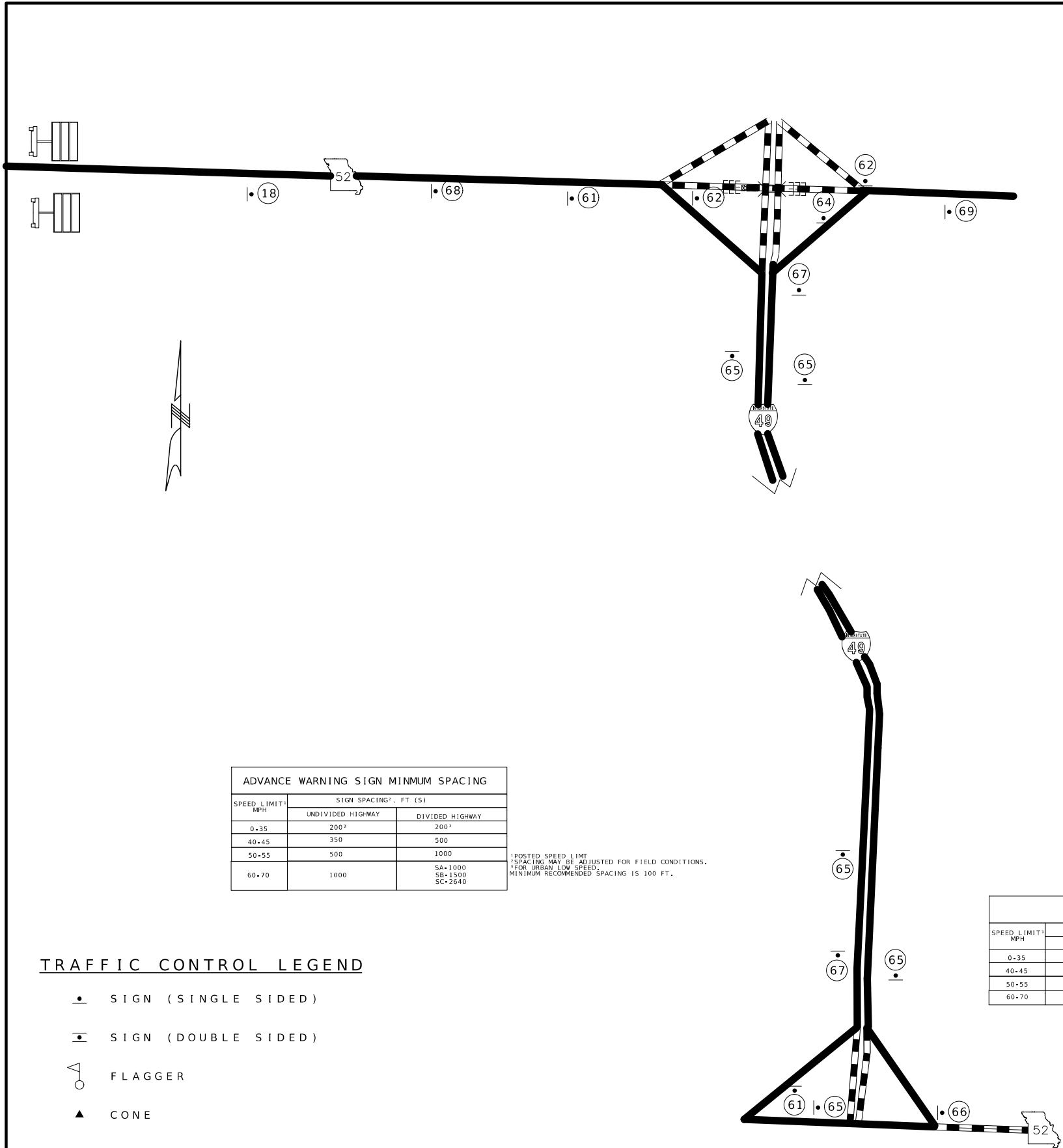
HATCHING LEGEND



TRANSITIONAL MILL



MILL AND FILL THRU
AMORET



ADVANCE WARNING SIGN MINIMUM SPACING		
SPEED LIMIT ¹ MPH	SIGN SPACING ² , FT (S)	
	UNDIVIDED HIGHWAY	DIVIDED HIGHWAY
0-35	200 ³	200 ³
40-45	350	500
50-55	500	1000
60-70	1000	

¹POSTED SPEED LIMIT.
²SPACING MAY BE ADJUSTED FOR FIELD CONDITIONS.
³FOR URBAN LOW SPEED.
MINIMUM RECOMMENDED SPACING IS 100 FT.

TAPER LENGTH AND SPACING					
SPEED LIMIT ¹ MPH	TAPER LENGTH ² , FT		CHANNELIZING SPACING ³ , FT		BUFFER LENGTH, FT
	SHOULDER ⁴ (T1)	LANE ⁵ (T2)	TAPER	BUFFER/ WORK AREA	
0-35	70	245	35'	40'	280
40-45	150	540	40'	80'	400
50-55	185	660	50'	80'	560
60-70	235	840	60'	120'	840

¹POSTED SPEED LIMIT.
²TAPER LENGTH MAY BE ADJUSTED FOR FIELD CONDITIONS.
³SPACING MAY BE ADJUSTED FOR FIELD CONDITIONS.
⁴BASED ON 10 FT SHOULDER WIDTH.
⁵BASED ON 12 FT LANE WIDTH.
⁶SPACING REDUCED TO 1/2 AT INTERSECTIONS.
⁷SPACING MAY BE REDUCED TO 1/2 AT INTERSECTIONS.



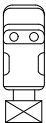
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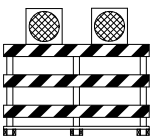
18



69



TRUCK MOUNTED
ATTENUATOR



TYPE III MOVABLE
BARRICADE

DETOUR	DETOUR	DETOUR	DETOUR	DETOUR	DETOUR	DETOUR
MO4-8	MO4-8	MO4-8	MO4-8	MO4-8	MO4-8	MO4-8
EAST	EAST	EAST	EAST	EAST	EAST	EAST
M3-2	M3-2	M3-2	M3-2	M3-2	M3-2	M3-2
52	52	52	52	52	52	52
M1-5	M1-5	M1-5	M1-5	M1-5	M1-5	M1-5
M5-1L	M6-1	M6-1	M5-1R	M6-3	M6-1	M5-2R
61	62	63	64	65	66	67

TRAFFIC CONTROL LEGEND

- SIGN (SINGLE SIDED)
- SIGN (DOUBLE SIDED)
- FLAGGER
- ▲ CONE
- CHANNELIZER
- E BARRICADE
- CHANGEABLE MESSAGE BOARD

EB 52 DETOUR

NOT TO SCALE

TRAFFIC CONTROL
SHEET 1 OF 9



THIS SHEET HAS BEEN
SIGNED, SEALED AND DATED
ELECTRONICALLY.

DATE PREPARED 6/18/2025	
ROUTE 52	STATE MO
DISTRICT SW	SHEET NO. 5
COUNTY BATES	
JOB NO. JSR0310	
CONTRACT ID.	

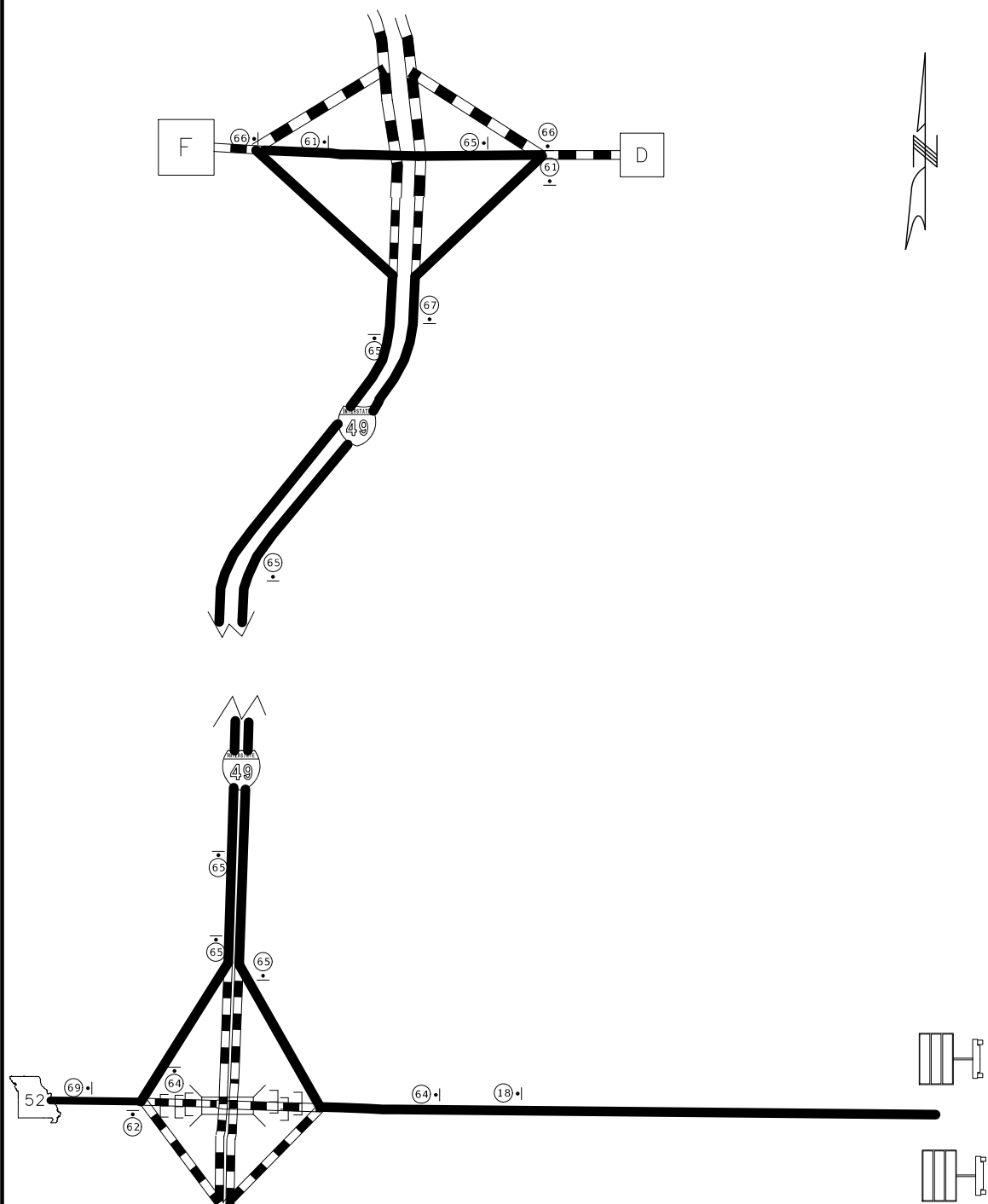
PROJECT NO.

BRIDGE NO.

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

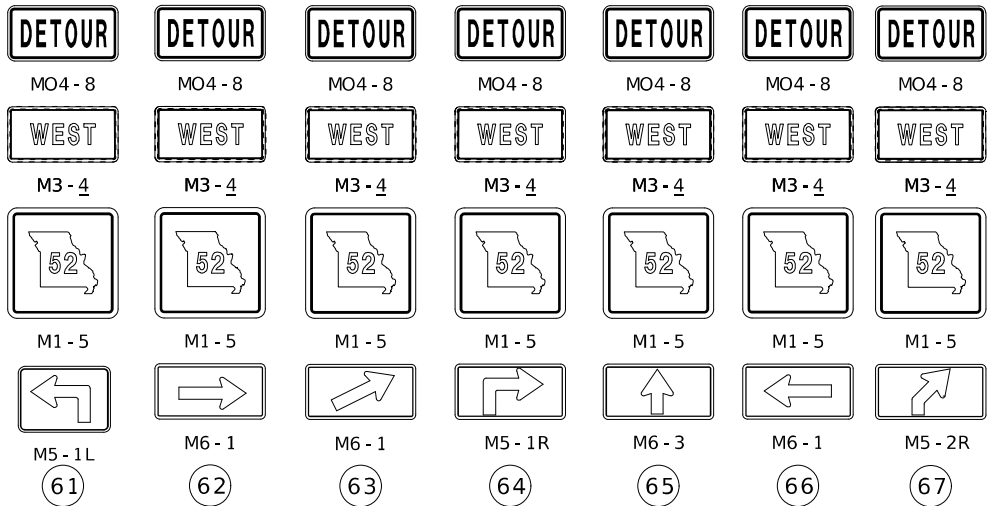
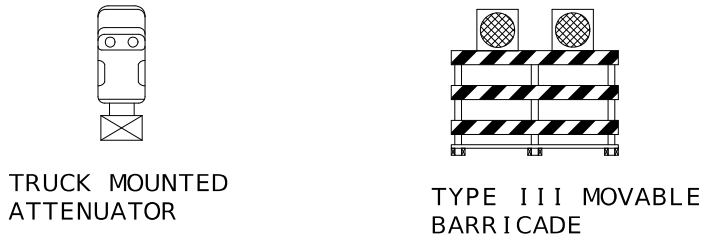
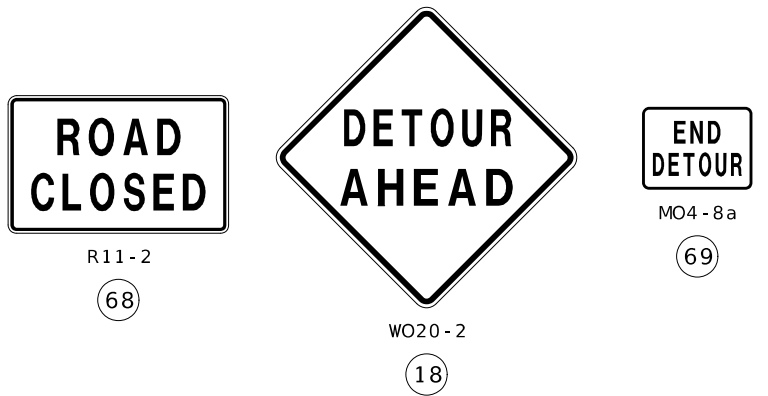


ADVANCE WARNING SIGN MINIMUM SPACING		
SPEED LIMIT ¹ , MPH	SIGN SPACING ² , FT (S)	
	UNDIVIDED HIGHWAY	DIVIDED HIGHWAY
0-35	200 ³	200 ³
40-45	350	500
50-55	500	1000
60-70	1000	SA-1000 SB-1500 SC-2640

¹POSTED SPEED LIMIT
²SPACING MAY BE ADJUSTED FOR FIELD CONDITIONS.
³FOR URBAN LOW SPEED, MINIMUM RECOMMENDED SPACING IS 100 FT.

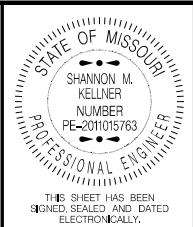
TAPER LENGTH AND SPACING					
SPEED LIMIT ¹ , MPH	TAPER LENGTH ² , FT		CHANNELIZING SPACING ³ , FT		BUFFER LENGTH, FT
	SHOULDER ⁴ (T1)	LANE ⁵ (T2)	TAPER	BUFFER / WORK AREA	
0-35	70	245	35 ⁶	40 ⁶	280
40-45	150	540	40 ⁶	80 ⁶	400
50-55	185	660	50 ⁷	80 ⁷	560
60-70	235	840	60 ⁷	120 ⁷	840

¹POSTED SPEED LIMIT.
²TAPER LENGTH MAY BE ADJUSTED.
³SPACING MAY BE ADJUSTED FOR FIELD CONDITIONS.
⁴BASED ON 10 FT SHOULDER WIDTH.
⁵BASED ON 12 FT LANE WIDTH.
⁶SPACING REDUCED TO 1/2 AT INTERSECTIONS.
⁷SPACING MY BE REDUCED TO 1/2 AT INTERSECTIONS.



WB 52 DETOUR

NOT TO SCALE
TRAFFIC CONTROL
SHEET 2 OF 9

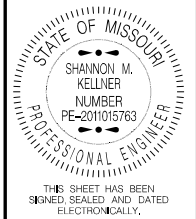
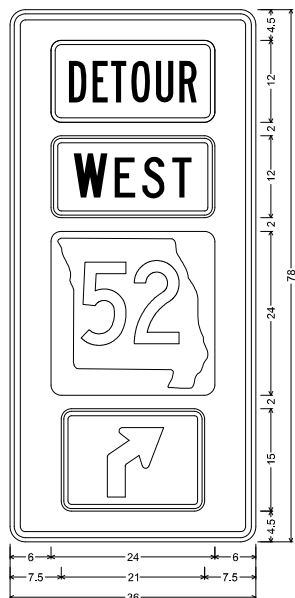
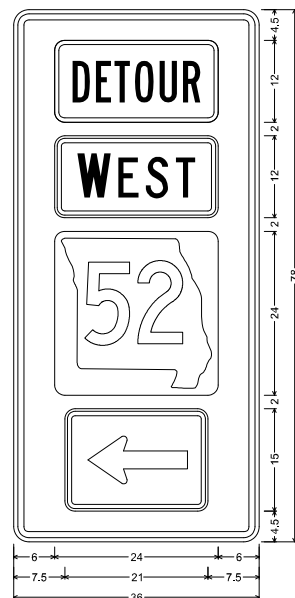
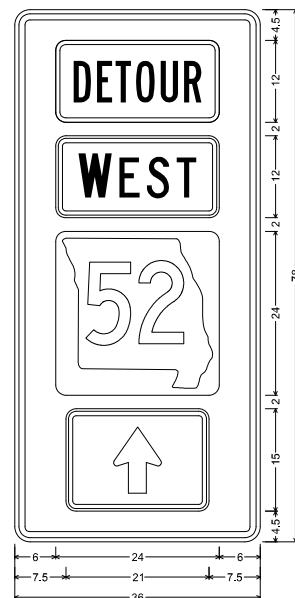
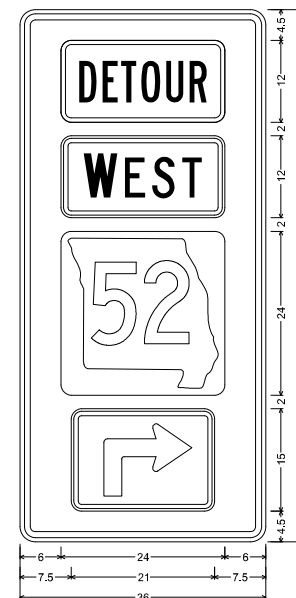
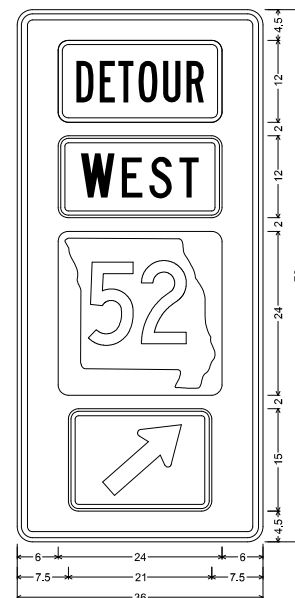
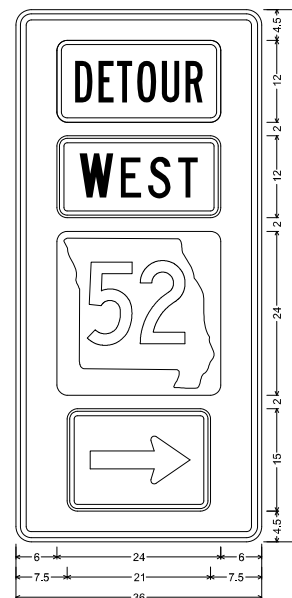
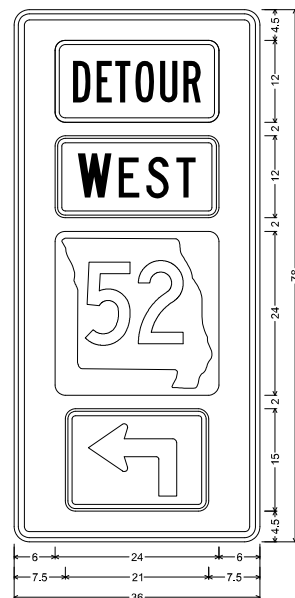
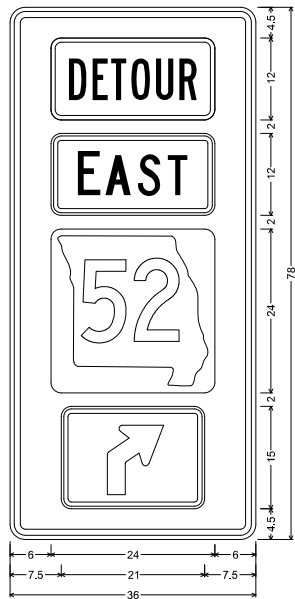
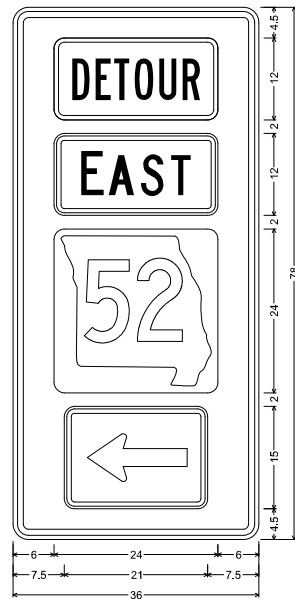
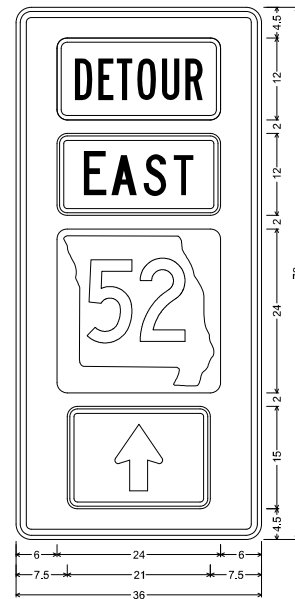
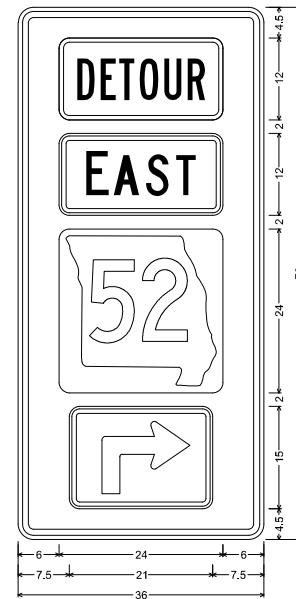
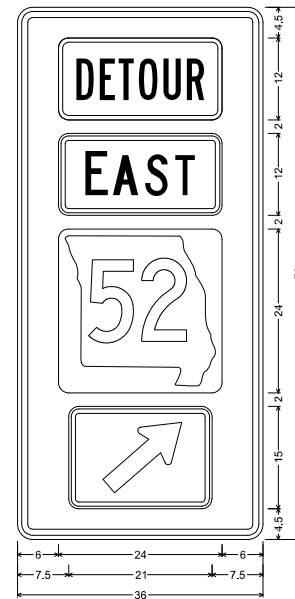
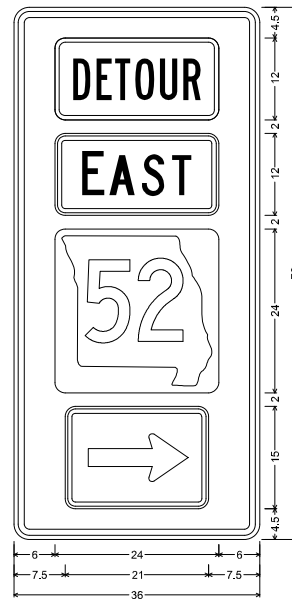
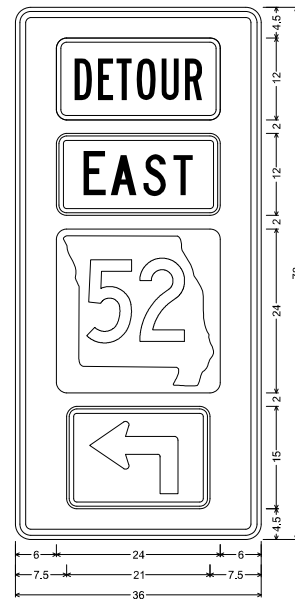


DATE PREPARED 6/18/2025	
ROUTE 52	STATE MO
DISTRICT SW	SHEET NO. 6
COUNTY BATES	
JOB NO. JSR0310	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)





DATE PREPARED	
6/17/2025	
ROUTE	STATE
52	MO
DISTRICT	SHEET NO.
SW	7
COUNTY	
BATES	
JOB NO.	
JSR0310	
CONTRACT ID.	


PROJECT NO.
BRIDGE NO.

[illegible]MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

103 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

NOT TO SCALE

TRAFFIC CONTROL
SHEET 3 OF 9



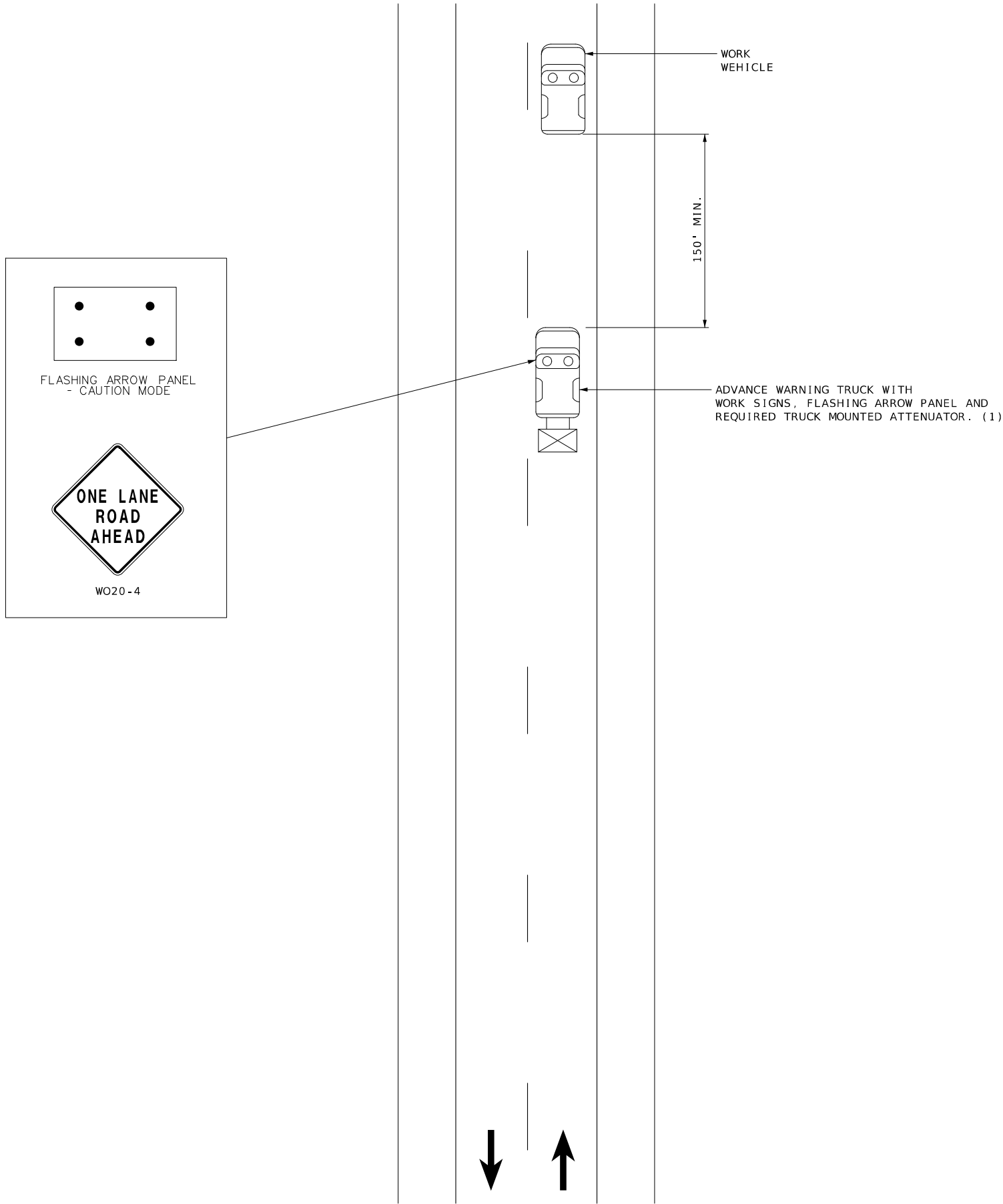
STATE OF MISSOURI
SHANNON M.
KELLNER
NUMBER
PE-2011015763
PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN
SIGNED, SEALED AND DATED
ELECTRONICALLY.

1. ANY EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL SIGNING SHALL BE COMPLETELY COVERED OR REMOVED.
2. TEMPORARY SIGNING SHOWN IS FOR WORK ON ONE SIDE OF THE ROAD. FOR WORK ON THE OTHER SIDE, REVERSE ORDER OF THE SIGNS AND CHANNELIZERS.
3. SIGNING SHOWN SHALL REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE.
4. ALTERNATE TRAFFIC CONTROL MAY BE USED AS NEEDED AT THE APPROVAL OF THE ENGINEER.
5. REFER TO STANDARD DRAWING 616.10, 619.10, AND 620.10 FOR ADDITIONAL DETAILS AND 903.03 FOR SIGN AND SIGN MOUNTING REQUIREMENTS.
6. SPEED LIMIT SIGNS INDICATING THE NORMAL SPEED LIMIT SHALL BE INSTALLED AT THE END OF THE WORK ZONE, PROVIDED NO FURTHER WORK ZONES WILL BE ENCOUNTERED WITHIN THE NEXT 1/2 MILE.
7. TEMPORARY SPEED LIMIT SIGNS SHALL BE COVERED OR REMOVED WHEN THE CONDITIONS REQUIRING REDUCED SPEEDS DO NOT EXIST.
8. NO DIRECT PAYMENT WILL BE MADE FOR THE RELOCATION OF CHANNELIZERS, CONSTRUCTION SIGNS, OR FLASHING ARROW PANEL.
9. ALL SIGNS SHALL BE PORTABLE MOUNT AND ARE TO BE MOVED AS WORK PROGRESSES, UNLESS OTHERWISE NOTED. ALL TRAFFIC CONTROL ITEMS SHALL BE REMOVED FROM THE ROADWAY DURING NON-WORKING HOURS.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING PROPER TRAFFIC CONTROL SETUPS THROUGHOUT CONSTRUCTION AS DESCRIBED IN THESE PLANS OR AS APPROVED BY THE ENGINEER.
11. WHERE MINIMUM LANE WIDTHS CANNOT BE ACHIEVED ON THE SIDE STREETS, THE CONTRACTOR SHALL CLOSE HALF OF THE APPROACH AND POSITION A FLAGGER AT EACH END OF THE WORK AREA. LOCATION TO BE DETERMINED BY THE ENGINEER. ADVANCE FLAGGER SIGNING SHALL BE INSTALLED AS OUTLINED IN THE MUTCD.
12. THE CONTRACTOR SHALL MAINTAIN PUBLIC ACCESS TO ALL BUSINESSES AND STREETS. NO STREET OR ENTRANCE SHALL BE COMPLETELY BLOCKED.
13. SPACING & DISTANCES OF TRAFFIC CONTROL DEVICES ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD & APPROVED BY THE ENGINEER.
14. TRAFFIC CONTROL SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
15. ADJUST ADVANCE WARNING SIGN SPACING TO AVOID SIGN PLACEMENT WITHIN THE LIMITS OF THE RAILROAD RIGHT OF WAY.
16. CONTRACTOR SHALL COORDINATE WITH ADA PROJECT, JXXXXXX.







NOTES:

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY PROVIDE ADDITIONAL PROTECTIVE TRUCKS EQUIPPED WITH PROPER WARNING DEVICES.

PROTECTIVE TRUCKS AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE HAZARD WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED IN A MANNER SUCH THAT THEY ARE NOT OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS ON VEHICLE-MOUNTED SIGNS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.

FLASHING ARROW PANELS AND SIGNS SHALL BE INCIDENTAL TO TRUCK MOUNTED ATTENUATORS, WHEREVER USED. NO ADDITIONAL PAYMENT WILL BE MADE.

(1) ADVANCE WARNING TRUCK IS POSITIONED AT VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE OR SPACING SHOWN, WHICHEVER IS GREATER.



DATE PREPARED	
6/17/2025	
ROUTE	STATE
52	MO
DISTRICT	SHEET NO.
SW	10
COUNTY	
BATES	
JOB NO.	
JSR0310	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION



105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

MOBILE ON TWO-LANE HIGHWAY

NOT TO SCALE

SIDE ROADS ENTERING WORK ZONES

SPEED	SIGN SPACING (FT.)	CHANNELIZER SPACING (FT)
PERMANENT POSTED (MPH)	NON-DIVIDED HIGHWAYS (S)	BUFFER WORK AREA
0-35	200	40
40-45	350	80
50-55	500	80
60-70	1000	120

DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS.

NOTES:
WARNING SIGNS SHALL BE ERECTED AT EACH INTERSECTION WITH ANOTHER STATE HIGHWAY WITHIN THE WORK ZONE.

ADDITIONAL WARNING SIGNS SHALL BE ERECTED AT OTHER INTERSECTIONS WITHIN THE WORK ZONE, AS DIRECTED BY THE ENGINEER.

■ - CHANNELIZERS

◁ - FLAGGER

IF USED AT NIGHT, THE FLAGGER STATIONS SHALL BE ILLUMINATED WITH AN AVERAGE MAINTAINED INTENSITY OF 0.6 FOOTCANDLES (6.5 LUX).



GO20-4a
(58)

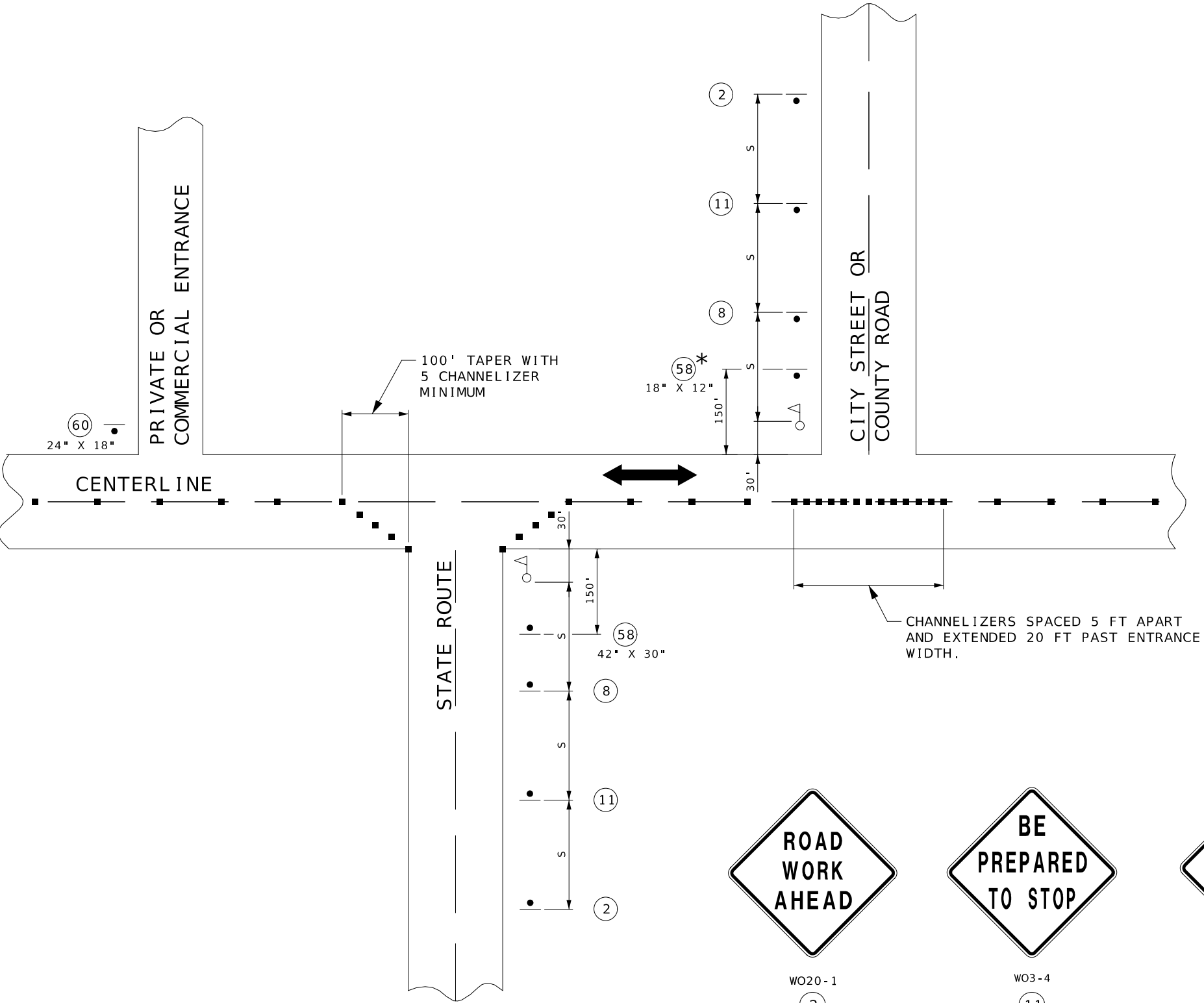
SHALL BE USED ON ALL INTERSECTIONS.

* THE SMALLER 18" X 12" SIGN 58 IS USED ON ALL OTHER NON-STATE ROUTES (CITY STREETS, COUNTY ROADS, ETC.).



24" X 18"
(60)

SHALL ONLY BE USED AT PRIVATE AND COMMERCIAL ENTRANCES. SEE STD PLAN 616.10 FOR SIGN DETAILS.



WO20-1
(2)

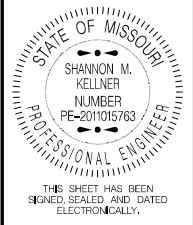


WO3-4
(11)



WO20-7a
(8)

NOT TO SCALE



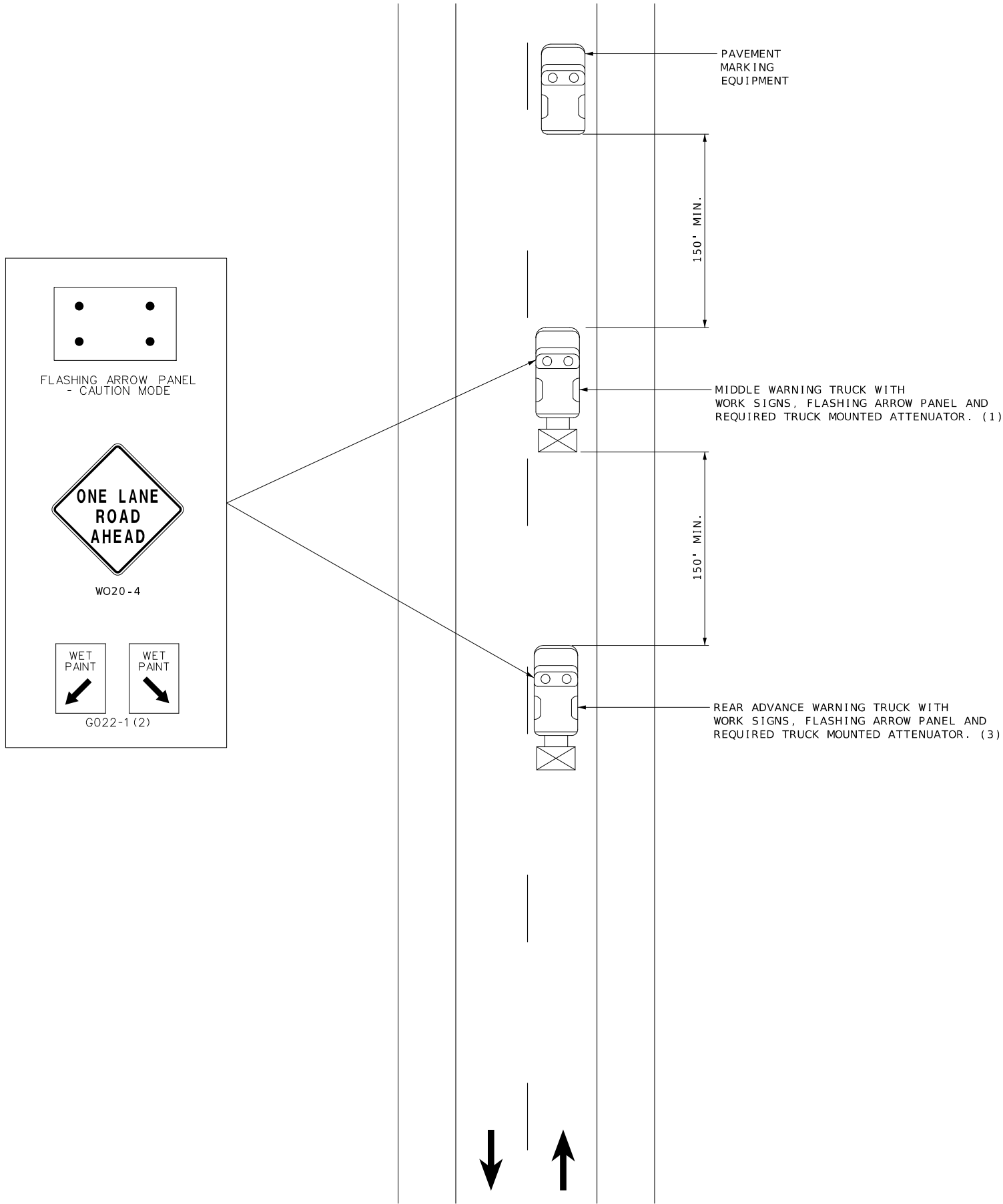
DATE PREPARED 6/17/2025	
ROUTE 52	STATE MO
DISTRICT SW	SHEET NO. 11
COUNTY BATES	
JOB NO. JSR0310	
CONTRACT ID.	

PROJECT NO.
BRIDGE NO.

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)





NOTES:

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY PROVIDE ADDITIONAL PROTECTIVE TRUCKS EQUIPPED WITH PROPER WARNING DEVICES.

PROTECTIVE TRUCKS AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE HAZARD WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED IN A MANNER SUCH THAT THEY ARE NOT OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS ON VEHICLE-MOUNTED SIGNS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.

FLASHING ARROW PANELS AND SIGNS SHALL BE INCIDENTAL TO TRUCK MOUNTED ATTENUATORS, WHEREVER USED. NO ADDITIONAL PAYMENT WILL BE MADE.

(1) TRUCK IS OPTIONAL ON TWO-LANE UNDIVIDED HIGHWAYS IF SIGNING AND ARROW BOARD ARE MOUNTED ON THE PAVEMENT MARKING EQUIPMENT.

(2) WET PAINT SIGNS ARE INSTALLED TO INDICATE THE SIDE IN WHICH THE PAVEMENT MARKING MATERIAL IS BEING APPLIED. AT THE CONTRACTOR'S OPTION, A FRONT FACING WET PAINT SIGN MAY BE INSTALLED ON THE LEFT SIDE OF THE PAVEMENT MARKING EQUIPMENT.

(3) REAR WARNING TRUCK IS POSITIONED AT THE NO TRACK POINT OF THE PAVEMENT MARKING MATERIAL, OR VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE, OR SPACING SHOWN, WHICHEVER IS GREATER.



DATE PREPARED 6/17/2025	
ROUTE 52	STATE MO
DISTRICT SW	SHEET NO. 13
COUNTY BATES	
JOB NO. JSR0310	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

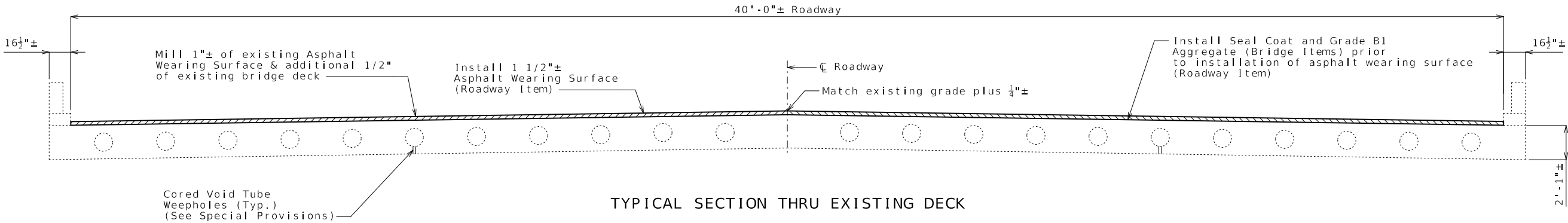
DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION



105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

U.I.P. AND REHABILITATE EXISTING (36'-70'-70'-36') CONTINUOUS CONCRETE VOIDED SLAB SPANS



General Notes:

Design Specifications:
2002 AASHTO LFD (17th Ed.) Standard Specifications
Bridge Deck Rating = 5

Design Loading:
H20-44 (1965) & HS20-44 (New Construction)

Design Unit Stresses:
Class B-1 Concrete f'c = 4,000 psi
Reinforcing Steel (ASTM A615 Grade 60) fy = 60,000 psi

Reinforcing Steel:
Minimum clearance to reinforcing steel shall be 1 1/2", unless otherwise shown.

Miscellaneous:
Roadway surfacing adjacent to bridge ends shall match new bridge wearing surface (Roadway Item).

All exposed edges of barrier shall have either a 1/2-inch radius or a 3/8-inch bevel, unless otherwise shown.

All concrete repairs shall be in accordance with Sec 704, unless otherwise noted.

All reinforcement in barrier end modification shall be epoxy coated.

Bars bonded in existing concrete not removed shall be cleanly stripped and embedded into new concrete where possible. If length is available, existing bars shall extend into new concrete at least 40 diameters for plain bars and 30 diameters for deformed bars, unless otherwise noted.

The area exposed by the removal of concrete and not covered with new concrete shall be coated with a qualified special mortar in accordance with Sec 704.

Outline of existing work is indicated by light dashed lines. Heavy lines indicate new work.

Contractor shall verify all dimensions in field before ordering materials.

Cost of removing existing barrier concrete, and cost of furnishing and installing new concrete, new reinforcing steel, and any other work incidental to the barrier end modification, complete in place, will be considered completely covered by the contract unit price for Barrier End Modification.

Bridge deck repairs to be completed by MoDOT personnel prior to contract letting (Maintenance Item).

Traffic Handling:
Structure to be closed during construction. See roadway plans for traffic control.

Resin Anchor System:
The contractor shall use one of the qualified resin anchor systems in accordance with Sec 1039.

Cost of furnishing and installing the resin anchor systems, complete in place, will be considered completely covered by the contract unit price for Barrier End Modification.

The minimum embedment depth in concrete with f'c = 4,000 psi for the resin anchor systems shall be that required to meet the minimum ultimate pullout strength in accordance with Sec 1039 but shall not be less than 5".

An epoxy coated #5 Grade 60 reinforcing bar shall be substituted for the 5/8"Ø threaded rod.

Estimated Quantities		
Item		Total
Scarification of Bridge Deck	sq. yard	953
Removal of Asphalt Wearing Surface	sq. foot	8580
Emulsified Asphalt, Seal Coat	gallon	381
Seal Coat Aggregate, Grade B1	sq. yard	953
Barrier End Modification	each	4
Cored Void Tube Weep Holes	each	156

REPAIRS TO BRIDGE: ROUTE 52
OVER ROUTE I-49
ROUTE 52 FROM ROUTE K TO ROUTE BUSINESS 49
ABOUT 3.5 MILES EAST OF ROUTE K
BEGINNING STATION 803+95.15± (Match Existing)

STATE OF MISSOURI

TIMOTHY D. LEAF

NUMBER PE-2012000778

PROFESSIONAL ENGINEER

06/10/2025 12:15:02 PM

TIMOTHY D. LEAF - CIVIL

MO-PE-2012000778

DATE PREPARED

6/2/2025

ROUTE 52

DISTRICT BR

STATE MO

SHEET NO. 1

COUNTY BATES

JOB NO. JSR0310

CONTRACT ID.

PROJECT NO.

BRIDGE NO. A21581

DESCRIPTION

DATE

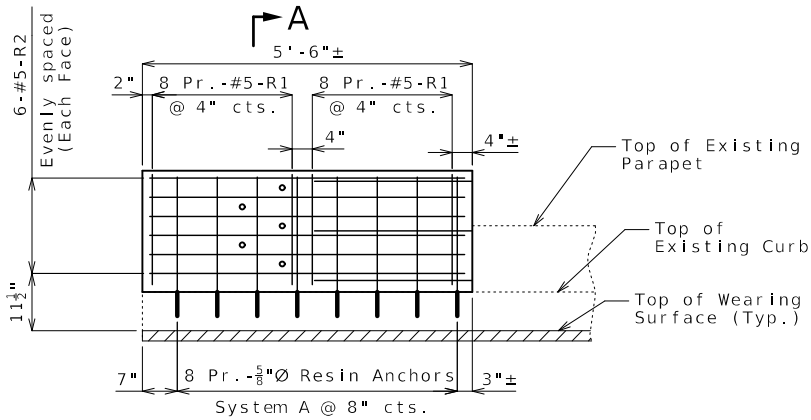
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL

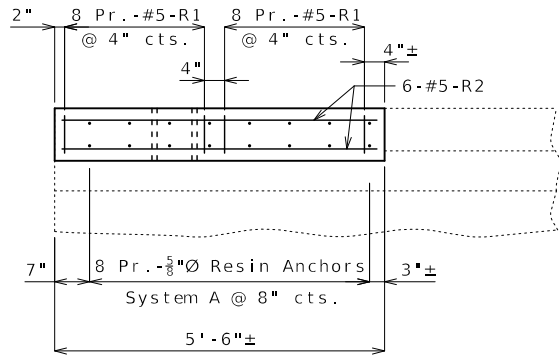
JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

MoDOT

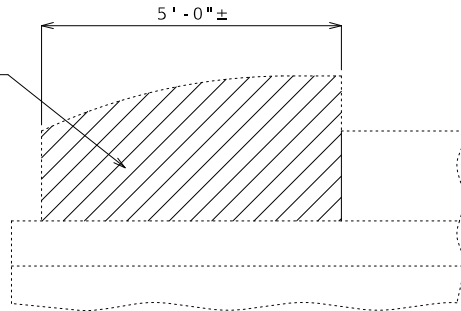


ELEVATION SHOWING REINFORCEMENT
(Right End Post at End Bent No. 5 similar)



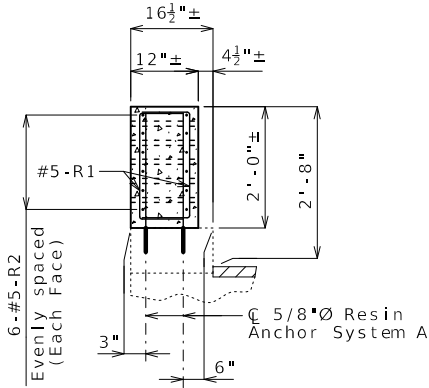
PLAN SHOWING REINFORCEMENT
LEFT END POST AT END BENT NO. 1

Remove concrete
(Hatched area only)
Clean and reuse
reinforcing steel.

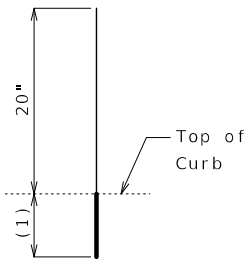


PART ELEVATION SHOWING END POST
CONCRETE REMOVAL

Cost of removing existing end posts will be considered
completely covered by the contract unit price for
Barrier End Modification.



SECTION A-A



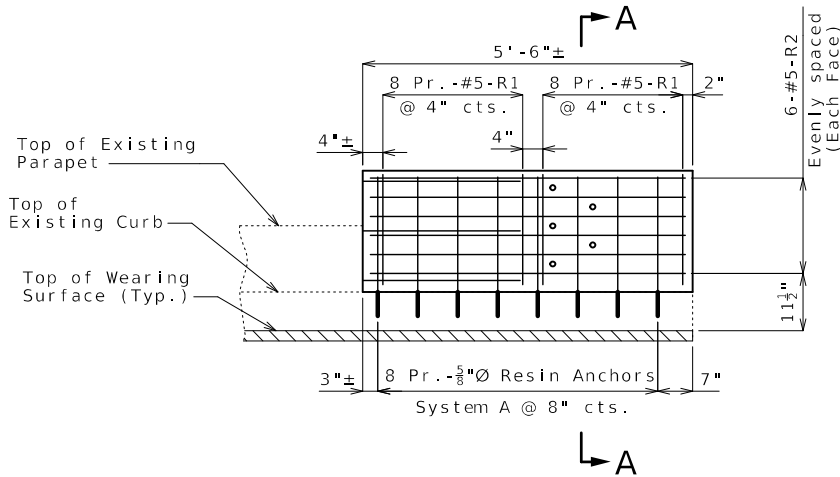
RESIN ANCHOR
SYSTEM A
(64 req'd)
(Install in curb)

DETAILS OF RESIN ANCHORS
(1) Use manufacturer's embedment length
(5" minimum embedment).

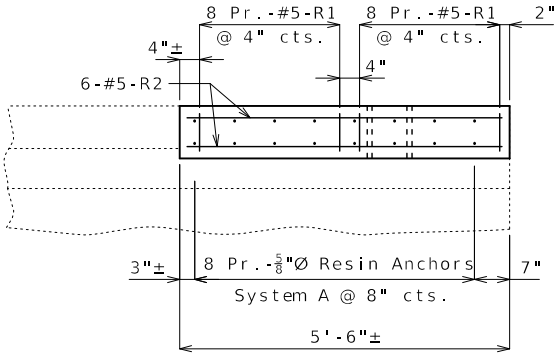
Notes:

Work this sheet with Sheet No. 3.

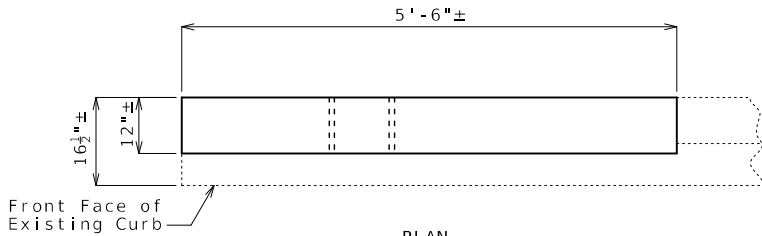
Resin anchors shall be shifted or bent in field to clear one-inch
diameter holes by at least 1/2 inch.



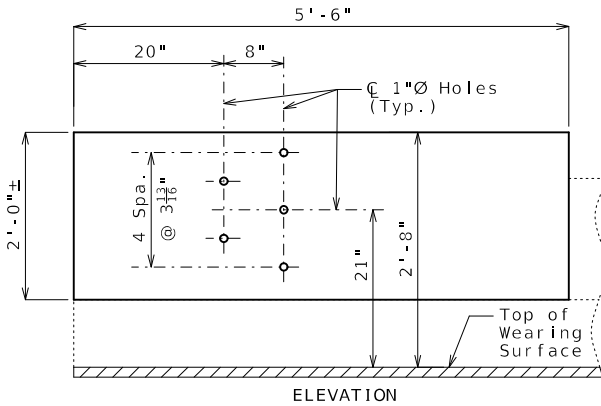
ELEVATION SHOWING REINFORCEMENT
(Right End Post at End Bent No. 1 similar)



PLAN SHOWING REINFORCEMENT
LEFT END POST AT END BENT NO. 5



PLAN



ELEVATION

DETAILS OF END POST AND
GUARD RAIL ATTACHMENT

STATE OF MISSOURI

TIMOTHY D. LEAF

NUMBER

PE-2012000778

PROFESSIONAL ENGINEER

06/02/2025 12:15:22 PM

TIMOTHY D. LEAF - CIVIL

MO-PE-2012000778

DATE PREPARED

6/2/2025

ROUTE

52

STATE

MO

DISTRICT

BR

SHEET NO.

2

COUNTY

BATES

JOB NO.

JSR0310

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

A21581

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

Detailed Jan. 2025

Checked Mar. 2025

Note: This drawing is not to scale. Follow dimensions.

Sheet No. 2 of 3

