MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

PLANS FOR PROPOSED STATE HIGHWAY

LAWRENCE COUNTY

DESIGN DESIGNATION

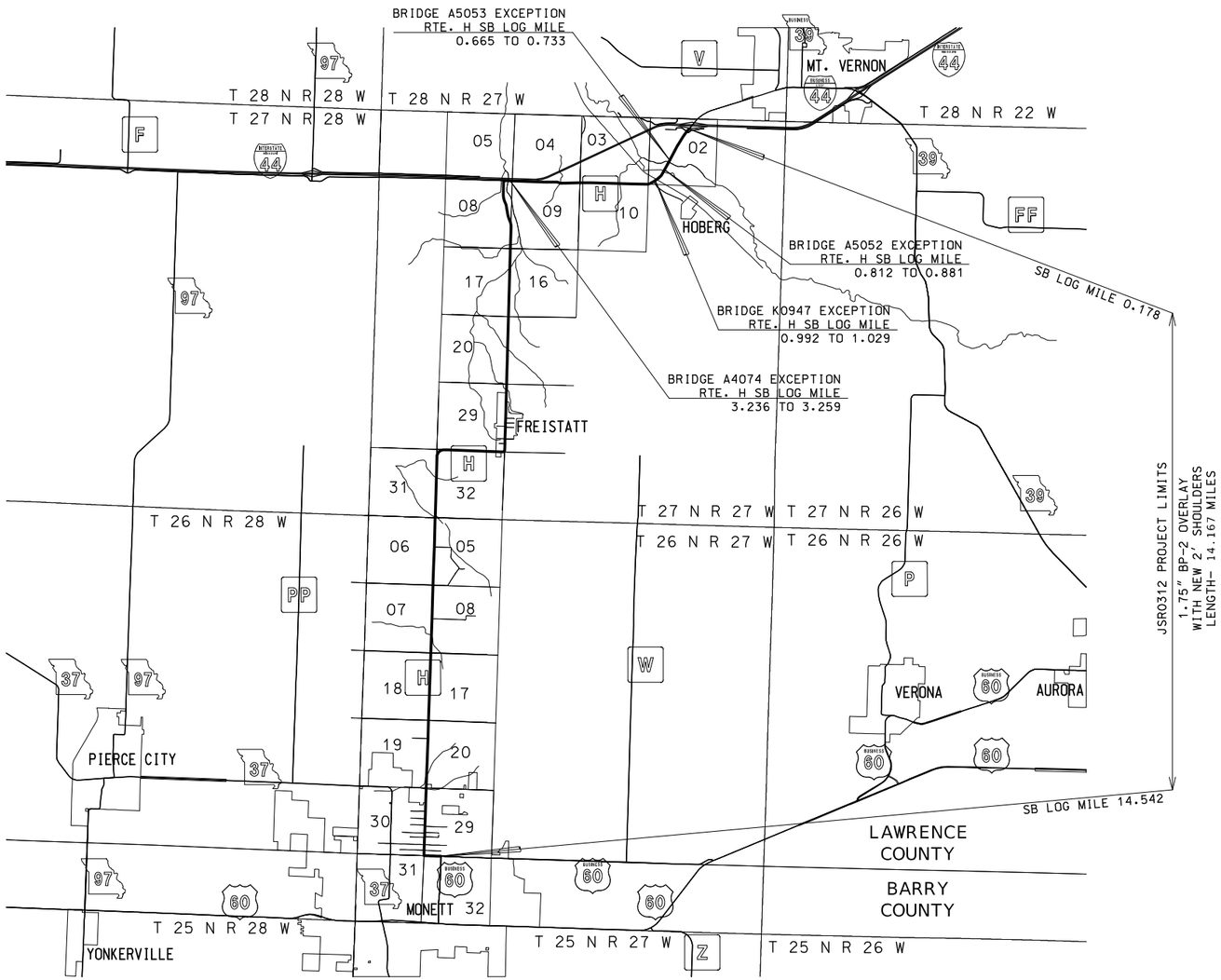
A.A.D.T. - 2024 = 4552

T = 13%

V = 25,35,45,& 55 M.P.H.

FUNCTIONAL CLASSIFICATION-MINOR ARTERIAL

KEY MAP
LOCATION OF LAWRENCE COUNTY



CONVENTIONAL SYMBOLS
(USED IN PLANS)

	EXISTING	NEW
BUILDINGS AND STRUCTURES		
GUARD RAIL		
GUARD CABLE		
CONCRETE RIGHT-OF-WAY MARKER		
STEEL RIGHT-OF-WAY MARKER		
LOCATION SURVEY MARKER		
UTILITIES		
FIBER OPTICS	—FO—	—FO—
OVERHEAD CABLE TV	—OTV—	—OTV—
UNDERGROUND CABLE TV	—UTV—	—UTV—
OVERHEAD TELEPHONE	—OT—	—OT—
UNDERGROUND TELEPHONE	—UT—	—UT—
OVERHEAD POWER	—OE—	—OE—
UNDERGROUND POWER	—UE—	—UE—
SANITARY SEWER	—S—	—S—
STORM SEWER	—SS—	—SS—
GAS	—G—	—G—
WATER	—W—	—W—
MANHOLE	SAN	
FIRE HYDRANT	HYD	
WATER VALVE	WV	
WATER METER	WM	
DROP INLET	DI	
DITCH BLOCK		
GROUND MOUNTED SIGN	SIGN	
LIGHT POLE		
H-FRAME POWER POLE		
TELEPHONE PEDESTAL	PED	
FENCE		
CHAIN LINK	—V—	
WOVEN WIRE	—X—	
GATE POST		
BENCHMARK	BM	

NOTE: DASHED OR OPEN SYMBOLS INDICATE EXISTING FEATURES

THE EXISTENCE AND APPROXIMATE LOCATION OF UTILITY FACILITIES KNOWN TO EXIST, AS SHOWN ON THE PLANS, ARE BASED ON THE BEST INFORMATION AVAILABLE TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE. RELIANCE UPON THIS INFORMATION IS DONE AT THE RISK AND PERIL OF THE USER, AND THE COMMISSION SHALL NOT BE LIABLE FOR ANY DAMAGES THAT MAY ARISE FROM ANY ERROR IN THE INFORMATION. IT IS, THEREFORE, THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE, LOCATION AND STATUS OF ANY FACILITY. SUCH VERIFICATION INCLUDES DIRECT CONTACT WITH THE LISTED UTILITIES.

INDEX OF SHEETS

DESCRIPTION	SHEET NUMBER
TITLE SHEET	1
TYPICAL SECTIONS (TS) (11 SHEETS)	2
QUANTITIES (QU) (4 SHEETS)	3
TRAFFIC CONTROL SHEETS (TC)	4-9

LENGTH OF PROJECT

BEGINNING OF PROJECT	SB LOG MILE 0.178	
END OF PROJECT	SB LOG MILE 14.542	
APPARENT LENGTH	75841.92	FEET
EQUATIONS AND EXCEPTIONS:		
BRIDGE A5053 EXCEPTION	359.04	FEET
BRIDGE A5052 EXCEPTION	364.32	FEET
BRIDGE K0947 EXCEPTION	195.36	FEET
BRIDGE A4074 EXCEPTION	121.44	FEET
TOTAL CORRECTIONS	1040.16	FEET
NET LENGTH OF PROJECT	74801.76	FEET
STATE LENGTH	14.167	MILES
FOR INFORMATION ONLY ESTIMATED DISTURBED ACRES	0.09	ACRES

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER

Darrell R Cook
09/09/2025 08:05:13
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	9/8/2025
ROUTE	H
STATE	MO
DISTRICT	SW
SHEET NO.	1
COUNTY	LAWRENCE
JOB NO.	JSR0312
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

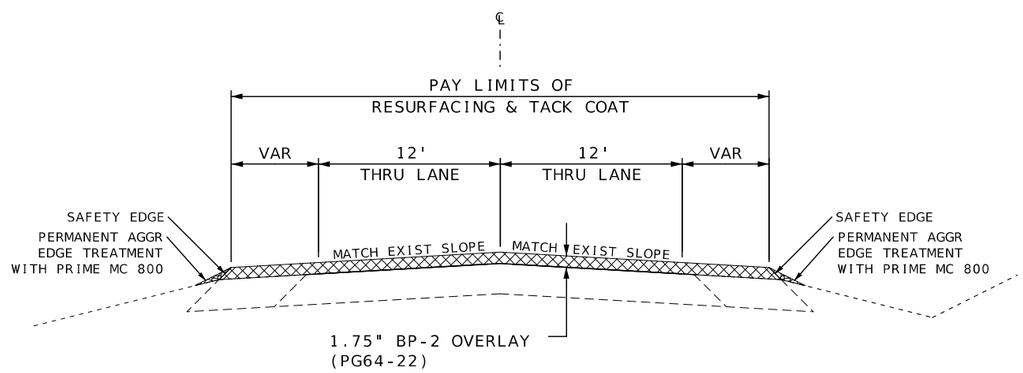
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

NOTES: LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

ASPHALT FACTORS	
COMBINED FACTOR	
BP-2 (PG64-22)	1.970 TONS/CY
TACK COAT	
OVERLAY	0.08 GAL/SY
PRIME LIQUID ASPHALT MC 800	
PERMANENT AGGREGATE EDGE TREATMENT	0.25 GAL/SY
IRREGULARITIES: 75 TONS/MI	



ROUTE H SOUTHBOUND

LOG MILE 0.178 TO LOG MILE 0.665
LOG MILE 1.029 TO LOG MILE 3.236

BRIDGE EXCEPTION

LOG MILE 0.665 TO LOG MILE 0.733

TYPICAL SECTIONS
SHEET 1 OF 11

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
09/09/2025 08:05:28
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
9/8/2025	
ROUTE: H	STATE: MO
DISTRICT: SW	SHEET NO.: 2
COUNTY: LAWRENCE	
JOB NO.: JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

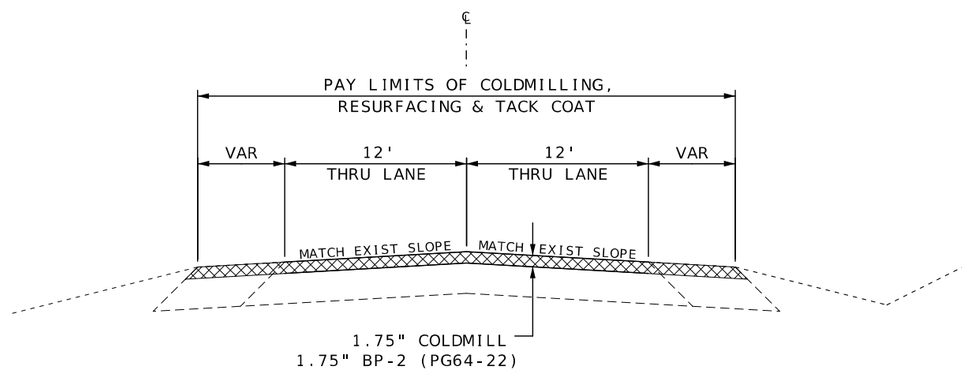
NOTES: LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

ASPHALT FACTORS	
COMBINED FACTOR	
BP-2 (PG64-22)	1.970 TONS/CY
TACK COAT	
MILLING	0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI	



ROUTE H SOUTHBOUND

LOG MILE 0.733 TO LOG MILE 0.812
LOG MILE 0.881 TO LOG MILE 0.992
LOG MILE 3.259 TO LOG MILE 3.316
LOG MILE 9.268 TO LOG MILE 9.288

BRIDGE EXCEPTIONS

LOG MILE 0.812 TO LOG MILE 0.881
LOG MILE 0.992 TO LOG MILE 1.029
LOG MILE 3.236 TO LOG MILE 3.259

TYPICAL SECTIONS
SHEET 2 OF 11

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
09/09/2025 08:05:52
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
9/8/2025	
ROUTE: H	STATE: MO
DISTRICT: SW	SHEET NO.: 2
COUNTY: LAWRENCE	
JOB NO.: JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

NOTES: AS APPROVED BY THE ENGINEER, THE 2' MODIFIED A3 SHOULDER MAY BE REDUCED TO 1' MINIMUM WIDTH IN SPECIFIC AREAS BASED ON EXISTING FIELD CONIDITIONS. THE MODIFIED TYPE A3 SHOULDER SHALL NOT BE BUILT IN AREAS WHERE THE EXISTING SLOPE, GUARDRAIL, OR CULVERT HEADWALLS DO NOT PERMIT. SEE JOB SPECIAL PROVISIONS.

IF THE CONTRACTOR ELECTS TO USE CONCRETE FOR THE SHOULDERS, THE QUANTITY OF ASPHALT AND TACK SHALL BE ADJUSTED IN ACCORDANCE WITH THE JOB SPECIAL PROVISION-OPTIONAL SHOULDER.

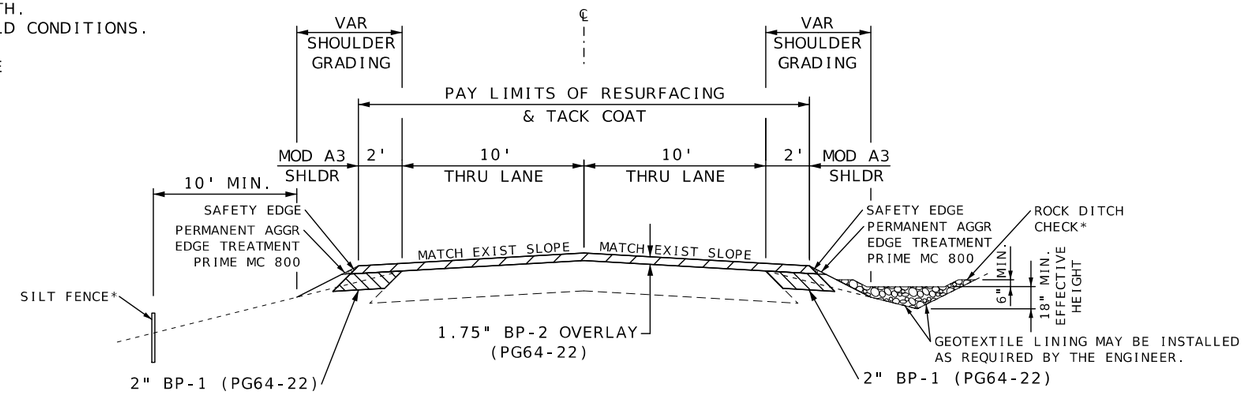
SHOULDER GRADING CONFINED TO FORESLOPE.

LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

SURFACE PLACEMENT - ONE PASS PER LANE

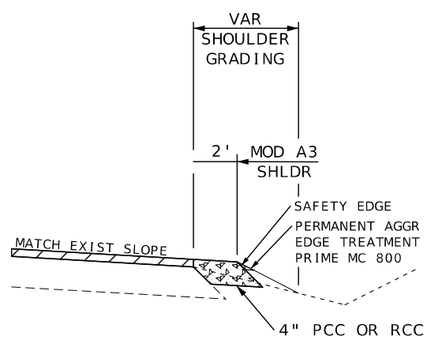
NO S.E. CORRECTION

ASPHALT FACTORS	
COMBINED FACTOR	
BP-1 (PG64-22)	1.970 TONS/CY
BP-2 (PG64-22)	1.970 TONS/CY
TACK COAT	
OVERLAY	0.08 GAL/SY
PRIME LIQUID ASPHALT MC 800	
PERMANENT AGGREGATE EDGE TREATMENT	0.25 GAL/SY
IRREGULARITIES: 75 TONS/MI	



ROUTE H SOUTHBOUND

LOG MILE 3.316 TO LOG MILE 6.496
LOG MILE 7.542 TO LOG MILE 9.268
LOG MILE 9.288 TO LOG MILE 13.348



CONCRETE SHOULDERS

* AT LOCATIONS SHOWN ON THE QUANTITY SHEETS, ALL DEVICES WILL BE INSTALLED AS NECESSARY AT THE DISCRETION OF THE ENGINEER.

TYPICAL SECTIONS
SHEET 3 OF 11

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER
PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
08/11/2025 13:27:41
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
8/11/2025	
ROUTE: H	STATE: MO
DISTRICT: SW	SHEET NO.: 2
COUNTY: LAWRENCE	
JOB NO.: JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DATE	DESCRIPTION

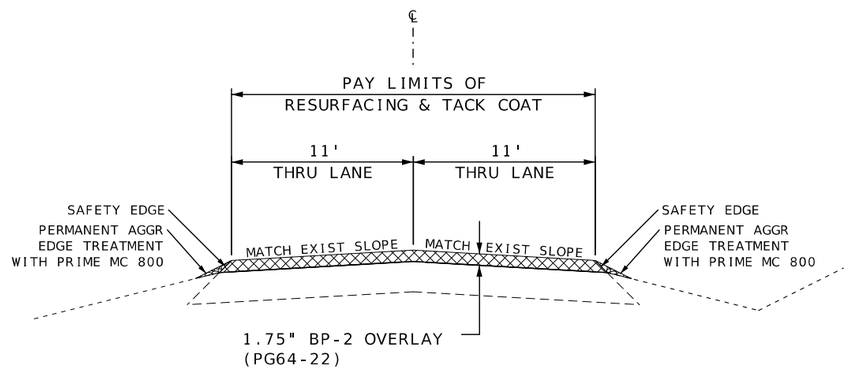
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

NOTES: LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

ASPHALT FACTORS	
COMBINED FACTOR	
BP-2 (PG64-22)	1.970 TONS/CY
TACK COAT	
OVERLAY	0.08 GAL/SY
PRIME LIQUID ASPHALT MC 800	
PERMANENT AGGREGATE EDGE TREATMENT	0.25 GAL/SY
IRREGULARITIES: 75 TONS/MI	



ROUTE H SOUTHBOUND

LOG MILE 6.496 TO LOG MILE 6.830
LOG MILE 7.211 TO LOG MILE 7.542
LOG MILE 13.348 TO LOG MILE 14.133

TYPICAL SECTIONS
SHEET 4 OF 11

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
08/11/2025 13:28:57
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
8/11/2025	
ROUTE	STATE
H	MO
DISTRICT	SHEET NO.
SW	2
COUNTY	
LAWRENCE	
JOB NO.	
JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DATE	DESCRIPTION

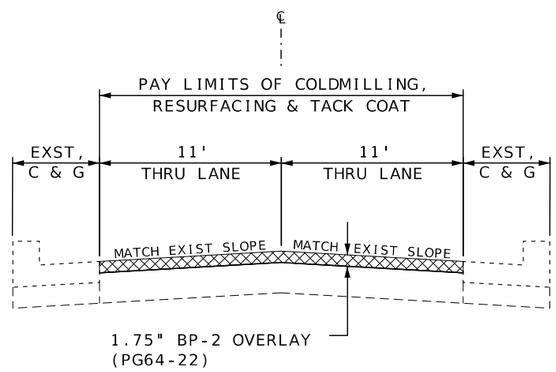
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

NOTES: LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

ASPHALT FACTORS		
	COMBINED FACTOR	
BP-2 (PG64-22)	1.970 TONS/CY	
TACK COAT		
MILLING		0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI		



ROUTE H SOUTHBOUND

LOG MILE 6.830 TO LOG MILE 7.211
LOG MILE 14.133 TO LOG MILE 14.239

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
08/11/2025 13:29:08
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
8/11/2025	
ROUTE H	STATE MO
DISTRICT SW	SHEET NO. 2
COUNTY LAWRENCE	
JOB NO. JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

TYPICAL SECTIONS
SHEET 5 OF 11

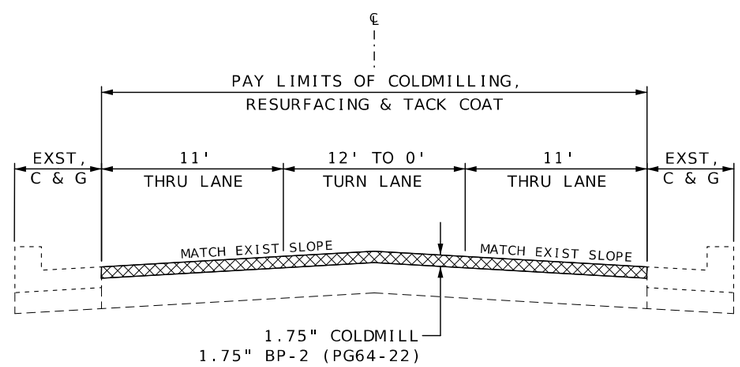
NOTES: LANE WIDTH NOTED IS TYPICAL LANE WIDTH.
ADJUST PAVING WIDTHS TO EXISTING FIELD CONDITIONS.

NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL COLDMILLING (UP TO 3") REQUIRED TO REMOVE ENTIRE TOP LIFT DUE TO DELAMINATION. THIS APPLIES TO ENTIRE PROJECT LIMITS.

SURFACE PLACEMENT - ONE PASS PER LANE

NO S.E. CORRECTION

ASPHALT FACTORS	
COMBINED FACTOR	
BP-2 (PG64-22)	1.970 TONS/CY
TACK COAT	
MILLING	0.10 GAL/SY
IRREGULARITIES: 75 TONS/MI	



ROUTE H SOUTHBOUND

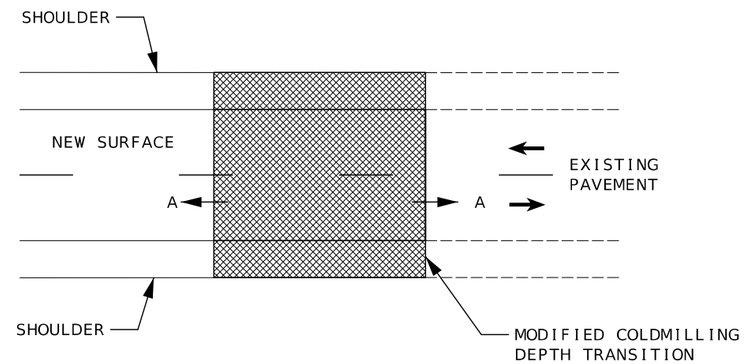
LOG MILE 14.239 TO LOG MILE 14.298
LOG MILE 14.303 TO LOG MILE 14.542

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
08/11/2025 13:29:21
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

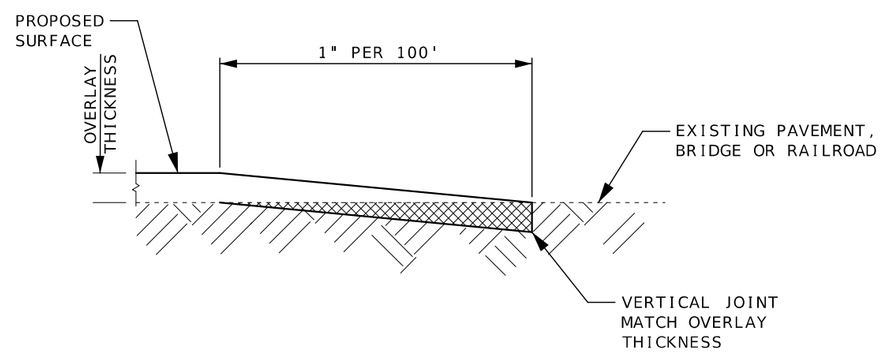
DATE PREPARED	
8/11/2025	
ROUTE: H	STATE: MO
DISTRICT: SW	SHEET NO.: 2
COUNTY: LAWRENCE	
JOB NO.: JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



MAINLINE, BRIDGE, AND RAILROAD PLAN



SECTION A-A

TRANSVERSE JOINT TRANSITION FOR SINGLE LIFT
PROJECT BEGINNING, END, BRIDGE, OR RAILROAD

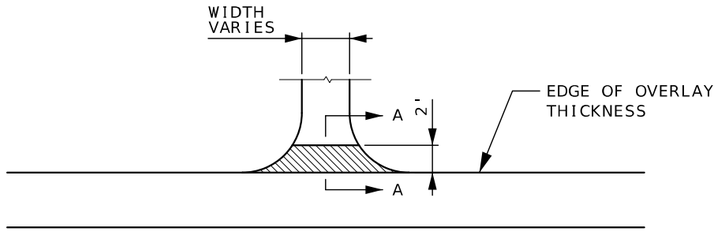
NOTE: MILL EXISTING CONCRETE 1" MAXIMUM.

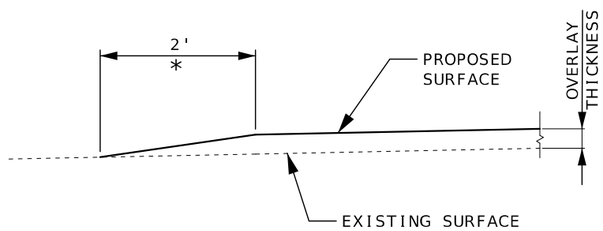
STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
08/11/2025 13:29:33
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
8/11/2025	
ROUTE: H	STATE: MO
DISTRICT: SW	SHEET NO.: 2
COUNTY: LAWRENCE	
JOB NO.: JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



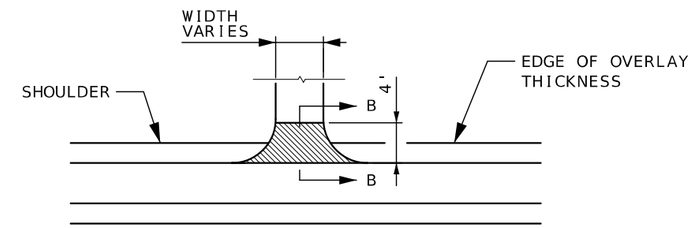


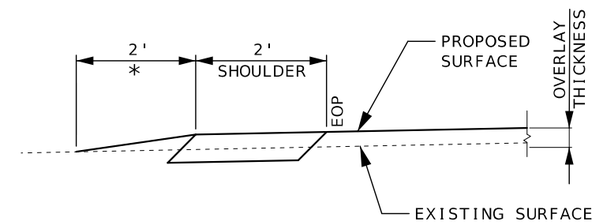
SECTION A-A

TYPICAL ENTRANCE - NO SHOULDER
(FIELD, PRIVATE OR COUNTY ROAD)
*TAPER AT 1:1 FOR FIELD ENTRANCE

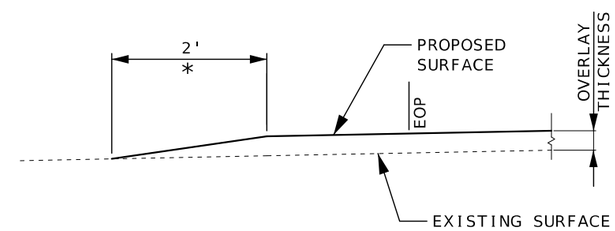
NOTE: 4 TONS GRAVEL A OR CRUSHED STONE
CALCULATED PER ENTRANCE.

6 TONS CALCULATED PER COUNTY ROAD.





AGGREGATE & EARTH ENTRANCES



ASPHALT & CONCRETE ENTRANCES

SECTION B-B

TYPICAL ENTRANCE - EXIST/NEW SHOULDER
(FIELD, PRIVATE OR COUNTY ROAD)
*TAPER AT 1:1 FOR FIELD ENTRANCE

NOTES: DO NOT BUILD 2' MODIFIED A3 SHOULDERS
ACROSS CONCRETE OR ASPHALT ENTRANCES,
STREETS OR ROADS.

4 TONS GRAVEL A OR CRUSHED STONE B
CALCULATED PER ENTRANCE.

6 TONS CALCULATED PER COUNTY ROAD.

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
08/11/2025 13:29:42
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	8/11/2025
ROUTE	H
STATE	MO
DISTRICT	SW
SHEET NO.	2
COUNTY	LAWRENCE
JOB NO.	JSR0312
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

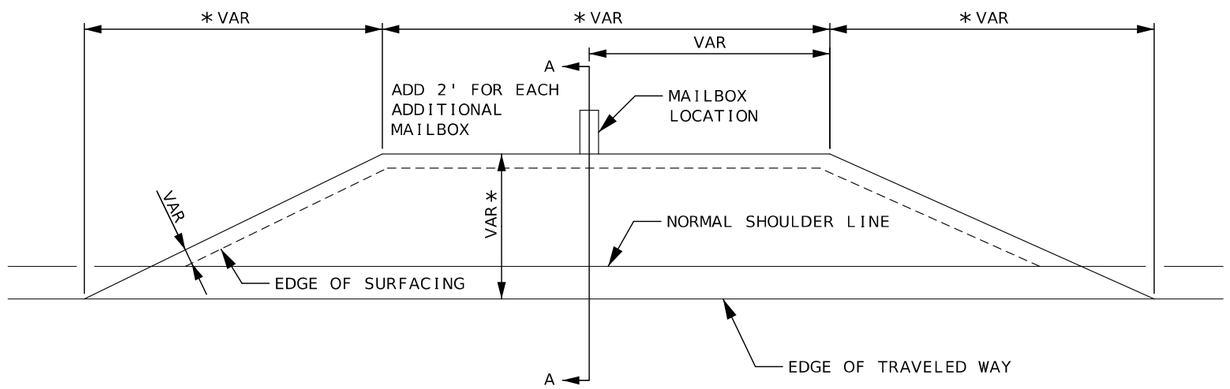
DESCRIPTION	DATE

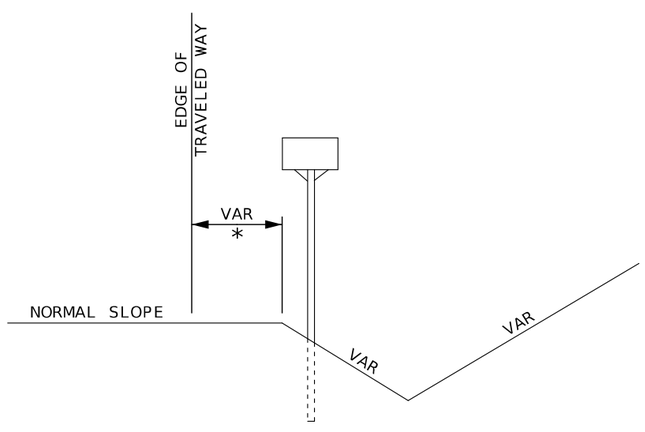
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

NOTE: MAILBOX TURNOUT QUANTITIES BASED ON 2' WIDTH AND 15' LENGTH. ADD 2' IN LENGTH PER ADDITIONAL MAILBOX AT SAME LOCATION, AS APPROVED BY THE ENGINEER.

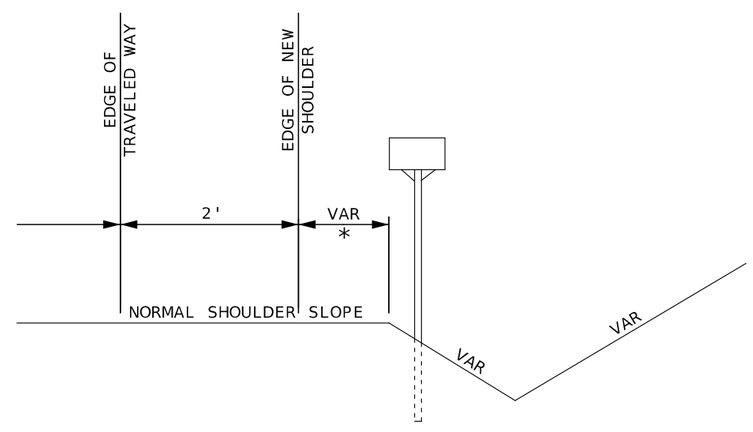
TYPICAL MAILBOX TURNOUT

*AS APPROVED BY THE ENGINEER





SECTION A-A
NO SHOULDER



SECTION A-A
SHOULDER

TYPICAL SECTIONS
SHEET 9 OF 11

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
08/11/2025 13:29:52
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
8/11/2025	
ROUTE: H	STATE: MO
DISTRICT: SW	SHEET NO.: 2
COUNTY: LAWRENCE	
JOB NO.: JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

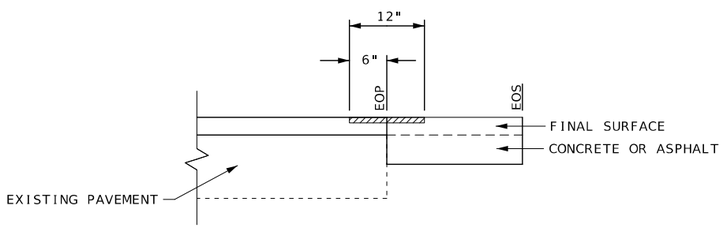
DESCRIPTION	DATE

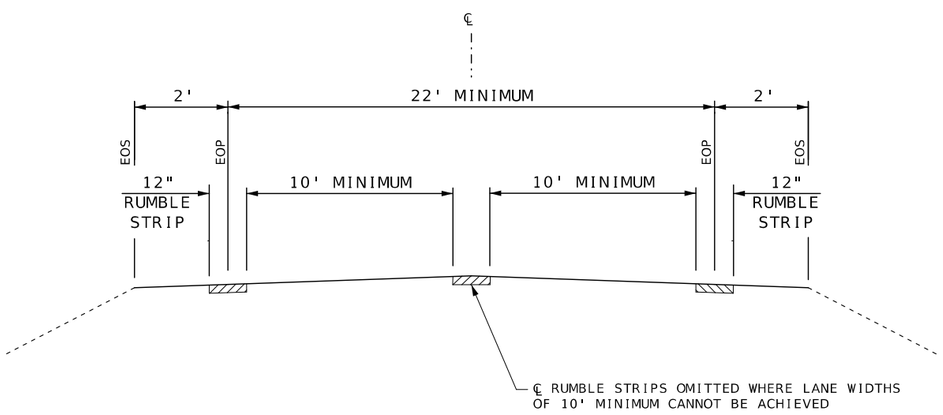
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

RUMBLE STRIPS FOR 2' MODIFIED A3 SHOULDERS

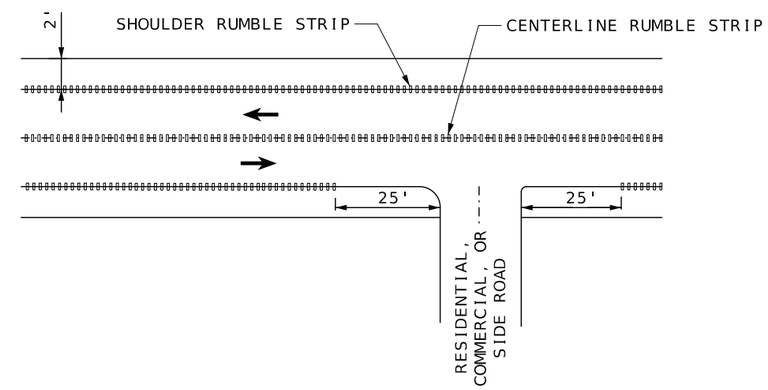
AS APPROVED BY THE ENGINEER, A 6" MINI RUMBLE STRIP
MAY BE USED WHERE A 12" RUMBLE STRIP DOES NOT FIT.

RUMBLE STRIP DETAILS





INTERSECTION RUMBLE STRIP LAYOUT



TYPICAL SECTIONS
SHEET 10 OF 11

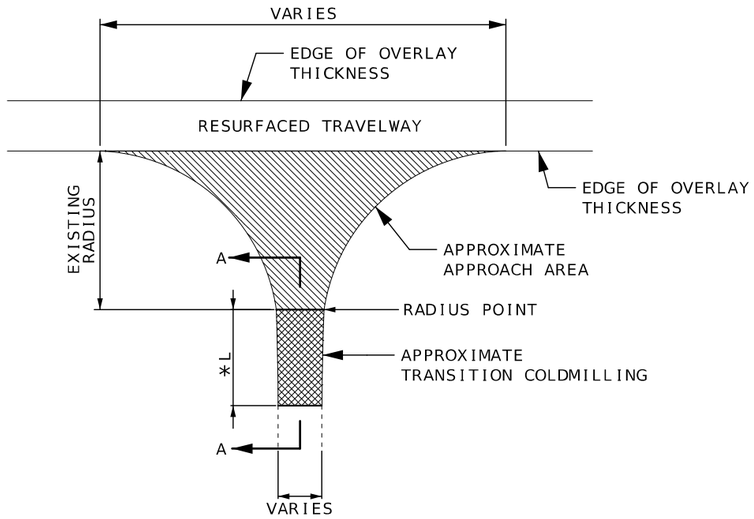
STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
08/11/2025 13:30:03
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

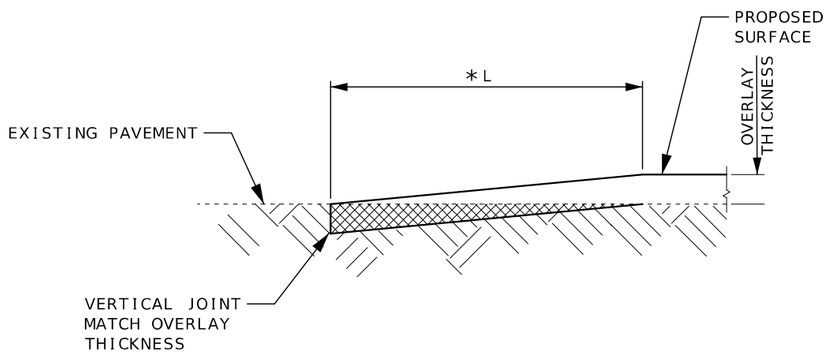
DATE PREPARED	8/11/2025
ROUTE	H
STATE	MO
DISTRICT	SW
SHEET NO.	2
COUNTY	LAWRENCE
JOB NO.	JSR0312
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



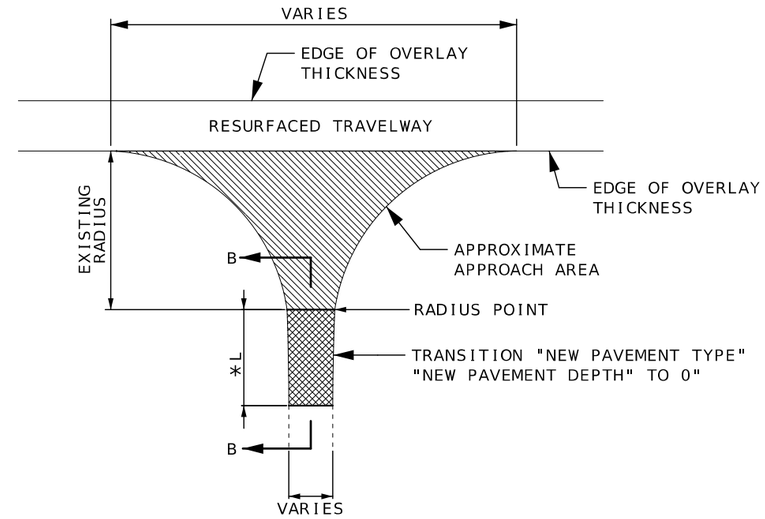


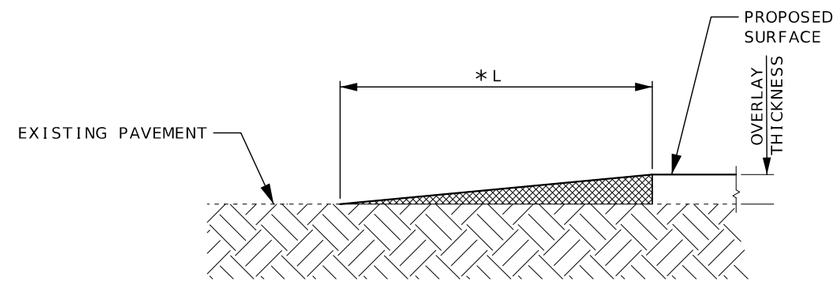
SECTION A-A

SURFACING REQUIREMENTS FOR
OVERLAYS AT STATE ROUTES.
SEE QUANTITY SHEETS FOR LOCATIONS.

*TRANSITION LENGTHS

OVERLAY THICKNESS	DEPTH TRANS. LENGTH (L)
1"	20.0'
1 ½"	40.0'
1 ¾"	50.0'
2"	60.0'





SECTION B-B

SURFACING REQUIREMENTS FOR OVERLAYS
AT EXISTING COLD MIX STATE ROUTES.
SEE QUANTITY SHEETS FOR LOCATIONS.

*TRANSITION LENGTHS

OVERLAY THICKNESS	DEPTH TRANS. LENGTH (L)
1"	20.0'
1 ½"	40.0'
1 ¾"	50.0'
2"	60.0'

TYPICAL SECTIONS
SHEET 11 OF 11

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
08/11/2025 13:30:12
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
8/11/2025	
ROUTE H	STATE MO
DISTRICT SW	SHEET NO. 2
COUNTY LAWRENCE	
JOB NO. JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

REMOVAL OF IMPROVEMENTS

LOG MILE	LOCATION	DESCRIPTON	UNITS	TOTAL	REMARKS
RTE. H SB					
0.173	0.234	GUARDRAIL	LF	388.0	GUARDRAIL & 2 CWT
0.173	0.202	GUARDRAIL	LF	238.0	GUARDRAIL & 2 CWT
0.587	0.631	GUARDRAIL	LF	275.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
0.592	0.631	GUARDRAIL	LF	250.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
0.734	0.772	GUARDRAIL	LF	200.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
0.734	0.772	GUARDRAIL	LF	175.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
0.783	0.811	GUARDRAIL	LF	150.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
0.783	0.811	GUARDRAIL	LF	150.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
0.882	0.910	GUARDRAIL	LF	150.0	BRIDGE ANCHOR & GUARDRAIL
0.882	0.910	GUARDRAIL	LF	150.0	BRIDGE ANCHOR & GUARDRAIL
1.067	1.195	GUARDRAIL	LF	738.0	GUARDRAIL & 2 CWT
1.067	1.155	GUARDRAIL	LF	463.0	GUARDRAIL & CWT
3.213	3.235	GUARDRAIL	LF	125.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
3.213	3.235	GUARDRAIL	LF	125.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
3.260	3.288	GUARDRAIL	LF	125.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
3.260	3.304	GUARDRAIL	LF	500.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
4.559	4.584	GUARDRAIL	LF	175.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
4.565	4.579	GUARDRAIL	LF	125.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
4.586	4.610	GUARDRAIL	LF	175.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
4.599	4.623	GUARDRAIL	LF	175.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
9.251	9.270	GUARDRAIL	LF	75.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
9.262	9.270	GUARDRAIL	LF	25.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
9.288	6.252	GUARDRAIL	LF	50.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
9.288	9.316	GUARDRAIL	LF	150.0	BRIDGE ANCHOR, GUARDRAIL, & CWT
1 LUMP SUM					

MODIFIED A3 SHOULDERS (GRADING & BASE LIFT ONLY)

LOG MILE	LOG MILE	LENGTH (MI)	WIDTH (FT)	AREA (SQFT)	AREA (SQYD)	LG CL 2 FOR SHOULDERS (STA)	2" BP-1 1.970 TONS/CY (TON)	TACK COAT (GAL)	REMARKS
RTE. H SB									
RIGHT SIDE									
3.316	6.496	3.180	2	33580.8	3731.2	167.9	408.36	298.5	NEW 2' SHOULDER
7.542	13.348	5.806	2	61311.4	6812.4	306.6	745.58	545.0	NEW 2' SHOULDER
LEFT SIDE									
3.316	6.496	3.180	2	33580.8	3731.2	167.9	408.36	298.5	NEW 2' SHOULDER
7.542	13.348	5.806	2	61311.4	6812.4	306.6	745.58	545.0	NEW 2' SHOULDER
					TOTAL	949.0	2307.88	1687.0	
					USE	949	2307.9	SEE TACK COAT	

SHAPING SLOPES

BEGIN LOG MILE	END LOG MILE	(LF)	CLASS III (100 FT)
RTE. H SB			
LEFT SIDE			
0.588		100	1
1.063		100	1
	1.199	100	1
3.209		100	1
	3.292	100	1
4.561		100	1
	4.614	100	1
9.258		100	1
	9.297	100	1
RIGHT SIDE			
0.583		100	1
	1.159	100	1
3.209		100	1
	3.308	100	1
4.555		100	1
	4.627	100	1
9.247		100	1
	9.311	100	1
		TOTAL	17
		USE	17

PERMANENT AGGREGATE EDGE TREATMENT

LOG MILE	LOG MILE	LENGTH (MI)	WIDTH (FT)	AREA (SQYD)	AGGR. 200 TONS/MILE (TON)	PRIME LIQUID ASPHALT MC 800 590 GAL/MILE (GAL)
RTE. H SB						
0.178	14.133	13.955	4	32747.7	2791.00	8233.5
				TOTAL	2791.00	8233.5
				USE	2791.0	8234

GRAVEL (A) OR CRUSHED STONE (B)

ROUTE	ITEM	(TON)
H	GAVEL OR CRUSHED STONE	568
	TOTAL	568

TACK COAT

	(GAL)
MODIFIED A3 SHOULDERS	1687.0
PAVEMENT RESURFACING	20530.2
TOTAL	22217.2
USE	22218

ADDITIONAL MOBILIZATION FOR SEEDING & MULCHING

EACH	4

TEMPORARY EROSION CONTROL

ITEM	QUANTIY	UNIT
RTE. H SB		
TEMPORARY SEEDING & MULCH	0.4	CY
SEDIMENT REMOVAL	90.0	CY
SILT FENCE	1200.0	LF
ROCK DITCH CHECK	750.0	LF
SEEDING - COOL SEASON MIX	0.9	ACRES
ROCK DITCH CHECKS PLACED AS APPROVED BY THE ENGINEER		

MOBILIZATION

1 LUMP SUM

CONTRACTOR FURNISHED SURVEYING AND STAKING

1 LUMP SUM

SUMMARY OF QUANTITIES

SHEET 1 OF 4

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER

Darrell R Cook
09/09/2025 08:06:07
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED 9/8/2025

ROUTE	STATE
H	MO

DISTRICT	SHEET NO.
SW	3

COUNTY: LAWRENCE

JOB NO. JSR0312

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

PAVEMENT RESURFACING

LOG MILE	LOG MILE	LENGTH (MI)	WIDTH (FT)	AREA (SQFT)	AREA (SQYD)	1.75" BP-2 1.970 TONS/CY (TON)	TACK COAT (GAL)	REMARKS
RTE. H SB								
0.178	0.211	0.033	28	4878.7	542.1	51.91	54.2	TRANSITION 1.75" TO 0.0"
0.211	0.632	0.421	28	62240.6	6915.6	662.26	553.2	MAINLINE AND EXISTING SHOULDERS
0.632	0.665	0.033	28	4878.7	542.1	51.91	54.2	TRANSITION 0.0" TO 1.75"
0.665	0.733	0.068	-	-	-	-	-	BRIDGE A5053 EXCEPTION
0.733	0.812	0.079	28	11679.4	1297.7	124.27	129.8	MILL & FILL 1.75"
0.812	0.881	0.069	-	-	-	-	-	BRIDGE A5052 EXCEPTION
0.881	0.992	0.111	28	16410.2	1823.4	174.62	182.3	MILL & FILL 1.75"
0.992	1.029	0.037	-	-	-	-	-	BRIDGE K0947 EXCEPTION
1.029	1.062	0.033	28	4878.7	542.1	51.91	54.2	TRANSITION 1.75" TO 0.0"
1.062	3.203	2.141	28	316525.4	35169.5	3367.97	2813.6	MAINLINE AND EXISTING SHOULDERS
1.062	3.236	2.174	28	321404.2	35711.6	3419.88	3571.2	TRANSITION 0.0" TO 1.75"
3.236	3.259	0.023	-	-	-	-	-	BRIDGE A4074 EXCEPTION
3.259	3.316	0.057	VAR.	14621.1	1624.6	155.58	162.5	MILL & FILL 1.75" FROM BRIDGE & APRONS
3.316	3.349	0.033	24	4181.8	464.6	44.49	46.5	TRANSITION 1.75" TO 0.0"
3.349	6.496	3.147	24	398787.8	44309.8	4243.28	3544.8	MAINLINE AND NEW 2' SHOULDERS
6.496	6.797	0.301	22	34964.2	3884.9	372.03	310.8	MAINLINE ONLY
6.797	6.830	0.033	22	3833.3	425.9	40.79	42.6	TRANSITION 0.0" TO 1.75"
6.830	7.211	0.381	22	44257.0	4917.4	470.91	491.7	MILL & FILL 1.75"
7.211	7.244	0.033	22	3833.3	425.9	40.79	42.6	TRANSITION 1.75" TO 0.0"
7.244	7.542	0.298	22	34615.7	3846.2	368.33	307.7	MAINLINE ONLY
7.542	9.235	1.693	24	214537.0	23837.4	2282.76	1907.0	MAINLINE AND NEW 2' SHOULDERS
9.235	9.268	0.033	24	4181.8	464.6	44.49	46.5	TRANSITION 0.0" TO 1.75"
9.268	9.288	0.020	22	2323.2	258.1	24.72	25.8	MILL & FILL 1.75"
9.288	9.321	0.033	24	4181.8	464.6	44.49	46.5	TRANSITION 1.75" TO 0.0"
9.312	13.348	4.036	24	511441.9	56826.9	5441.96	4546.2	MAINLINE AND NEW 2' SHOULDERS
13.348	14.100	0.752	22	87352.3	9705.8	929.47	776.5	MAINLINE ONLY
14.100	14.133	0.033	22	3833.3	425.9	40.79	34.1	TRANSITION 0.0" TO 1.75"
14.133	14.239	0.106	22	12313.0	1368.1	131.01	136.8	MILL & FILL 1.75"
14.239	14.298	0.059	VAR.	13518.6	1502.1	143.85	150.2	MILL & FILL 1.75" WIDTH TAPER & APRONS
14.303	14.542	0.239	34	42905.3	4767.3	456.54	476.7	MILL & FILL 1.75" THREE LANES
TACK ESTIMATED @ 0.10 GALS PER SQYD FOR COLDMILLING								
& 0.08 GALS PER SQYD FOR ALL OTHERS								
IRREGULARITIES								
					952.9	91.25	76.2	ROADS & ENTRANCES 2' WIDE
		14.364				1077.30	-	IRREGULAEITIES 75 TONS/MILE
					TOTAL	24297.65	20530.2	
					USE	24297.7	SEE TACK COAT	
NOTE: ANY AND ALL IRREGULARITIES MAY BE UNDERRUN AND NO ADJUSTMENT TO ANY UNIT BID COST WILL BE MADE								

FULL DEPTH COLD MILLING (≤ 3")

BEGIN LOG MILE	END LOG MILE	LENGTH (MI)	WIDTH (FT)	AREA (SQFT)	AREA (SQYD)	REMARKS
RTE. H SB						
0.733	0.812	0.079	28	11679.4	1297.7	1.75" BETWEEN BRIDGES
0.881	0.992	0.111	28	16410.2	1823.4	1.75" BETWEEN BRIDGES
3.259	3.316	0.057	VAR.	14621.1	1624.6	1.75" FROM BRIDGE & APRONS
6.830	7.211	0.381	22	44257.0	4917.4	1.75" CURB & GUTTER SECTION
9.268	9.288	0.020	22	2323.2	258.1	1.75" ON BRIDGE
14.133	14.239	0.106	22	12313.0	1368.1	1.75" CURB & GUTTER SECTION
14.239	14.298	0.059	VAR.	13518.6	1502.1	1.75" TAPER TO INTERSECTION & RT TURN LANE
14.303	14.542	0.239	34	42905.3	4767.3	1.75" THREE LANE SECTION
				TOTAL	17558.7	
				USE	17559	

PREFORMED THERMOPLASTIC PAVEMENT MARKING TAPE

LOG MILE	LOCATION	24" WHITE (L.F.)	LEFT/RIGHT ARROW (EACH)	REMARKS
RTE. H SB				
6.951	CL	42		CROSSWALK
7.073	CL	30		CROSSWALK
14.133	CL	30		CROSSWALK
14.272	CL		1	LT TURN LANE ARROW
14.289	CL	10		STOP BAR
14.297	RT	12		STOP BAR
14.305	LT	12		STOP BAR
14.311	CL	11		STOP BAR
14.333	CL		1	LT TURN LANE ARROWS
14.419	CL		1	LT TURN LANE ARROWS
14.423	CL		1	LT TURN LANE ARROWS
14.503	CL		1	LT TURN LANE ARROWS
14.524	CL		1	LT TURN LANE ARROWS
14.539	CL	11		STOP BAR
14.542	RT	15		STOP BAR
	TOTAL	173	6	
	USE	173	6	

STANDARD WATERBORNE PAVEMENT MARKING PAINT, TYPE P BEADS

LOG MILE	LOG MILE	LOCATION	4" WHITE (L.F.)	4" YELLOW (L.F.)	REMARKS
RTE. H SB					
0.178	14.218	RT	74131.2		RIGHT EDGELINE
0.178	14.218	LT	74131.2		LEFT EDGELINE
0.178	14.297	CL		149096.6	CENTERLINE
14.271	14.297	CL RT	137.3		TURN LANELINE
14.297	14.298	LT	100.0		WHITE AROUND ISLAND
14.305	14.334	CL LT	153.1		TURN LANELINE
14.311	14.355	CL RT		464.6	DOUBLE SOLID CENTERLINE
14.355	14.491	CL		1436.2	CENTER LANELINES
14.491	14.539	CL LT		506.9	DOUBLE SOLID CENTERLINE
14.502	14.542	CL RT	211.2		TURN LANELINE
		TOTAL	148864.0	151504.3	
		USE	148864	151505	

MODIFIED COLD MILLING (DEPTH TRANSITION)

BEGIN LOG MILE	END LOG MILE	LENGTH (MI)	WIDTH (FT)	AREA (SQFT)	AREA (SQYD)	REMARKS
RTE. H SB						
0.178	0.211	0.033	28	4900.0	544.4	TRANSITION 1.75" TO 0"
0.632	0.665	0.033	28	4900.0	544.4	TRANSITION 0" TO 1.75"
1.029	1.062	0.033	28	4900.0	544.4	TRANSITION 1.75" TO 0"
3.203	3.236	0.033	28	4900.0	544.4	TRANSITION 0" TO 1.75"
3.316	3.349	0.033	20	3500.0	388.9	TRANSITION 1.75" TO 0"
6.797	6.830	0.033	20	3500.0	388.9	TRANSITION 0" TO 1.75"
7.211	7.244	0.033	20	3500.0	388.9	TRANSITION 1.75" TO 0"
9.235	9.268	0.033	22	3850.0	427.8	TRANSITION 0" TO 1.75"
9.288	9.321	0.033	22	3850.0	427.8	TRANSITION 1.75" TO 0"
14.100	14.133	0.033	22	3850.0	427.8	TRANSITION 0" TO 1.75"
				TOTAL	4627.7	
				USE	4628	

SUMMARY OF QUANTITIES
SHEET 2 OF 4

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
09/09/2025 08:06:16
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED 9/8/2025

ROUTE	STATE
H	MO
DISTRICT	SHEET NO.
SW	3

COUNTY: LAWRENCE
JOB NO.: JSR0312
CONTRACT ID.
PROJECT NO.
BRIDGE NO.

DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

BITUMINOUS RUMBLE STRIPS

BEGIN LOG MILE	END LOG MILE	LENGTH (LF)	CENTER LINE (STA.)	SHOULDER (STA.)	REMARKS
RTE. H SB					
LEFT SIDE					
0.178	6.496	33359.040		301.04	EDGE OF TRAVELWAY
7.542	13.348	30655.680		274.01	EDGE OF TRAVELWAY
RIGHT SIDE					
0.178	6.496	33359.040		311.79	EDGE OF TRAVELWAY
7.542	13.348	30655.680		284.76	EDGE OF TRAVELWAY
CENTER					
0.178	6.496	33359.040	330.59		CENTER LINE
7.542	13.348	30655.680	303.56		CENTER LINE
		TOTAL	634.15	870.56	
		USE	634.2	870.6	

GUARDRAIL

BEGIN LOG MILE	END LOG MILE	GUARDRAIL (LF)	HEIGHT & BLOCK TRANS. (EACH)	BRIDGE APPROACH TRANS. SECTION (EACH)	BRIDGE APPROACH TRANS. SECTION THRIE-BEAM (EACH)	END ANCHOR (EACH)	CWT (EACH)
RTE. H SB							
LEFT SIDE							
0.173	0.234	287.5					2
0.592	0.631	150.0		1			1
0.734	0.772	200.0		1		1	
0.783	0.811	150.0		1		1	
0.882	0.910	50.0	1	1			
1.067	1.195	637.5					2
3.213	3.235	25.0		1			1
3.260	3.288	50.0		1			1
4.565	4.579	25.0			1		1
4.586	4.610	75.0			1		1
9.262	9.270	37.5			1	1	
9.288	9.297	75.0			1		1
RIGHT SIDE							
0.173	0.202	137.5					2
0.587	0.631	175.0		1			1
0.734	0.772	200.0		1		1	
0.783	0.811	150.0		1		1	
0.882	0.910	50.0	1	1			
1.067	1.155	362.5	1				1
3.213	3.235	25.0		1			1
3.260	3.304	412.5		1			1
4.559	4.584	75.0			1		1
4.599	4.623	75.0			1		1
9.251	9.270	25.0			1		1
9.288	9.307	67.5			1	1	
	TOTAL	3517.5	3	12	8	6	19
	USE	3518	3	12	8	6	19

CABLE, LOOP DETECTOR, IN DUCT

LOG MILE	LOCATION	1C #14 (L.F.)	REMARKS
RTE. H SB			
14.289	CL	346	6 X 30 LOOP
14.297	RT	305	6 X 30 LOOP
14.305	LT	318	6 X 30 LOOP
14.311	CL	356	6 X 30 LOOP
14.539	CL	345	6 X 30 LOOP
14.542	RT	307	6 X 30 LOOP
	TOTAL	1977	
	USE	1980	

SUMMARY OF QUANTITIES
SHEET 3 OF 4

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
09/09/2025 08:06:28
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED
9/8/2025

ROUTE	STATE
H	MO
DISTRICT	SHEET NO.
SW	3

COUNTY
LAWRENCE

JOB NO.
JSR0312

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

SIGN	SIZE IN.	AREA SQ.FT.	QTY EACH	TOTAL AREA SQ.FT.	QTY RELOC EACH	TOTAL RELOC SQ.FT.	SIGN NO.	DESCRIPTION
WARNING SIGNS								
WO1-1L	48X48	16.00						TURN (SYMBOL LEFT)
WO1-1R	48X48	16.00						TURN (SYMBOL RIGHT)
WO1-2L	48X48	16.00						CURVE (SYMBOL LEFT)
WO1-2R	48X48	16.00						CURVE (SYMBOL RIGHT)
WO1-3L	48X48	16.00						REVERSE TURN (SYMBOL LEFT)
WO1-3R	48X48	16.00						REVERSE TURN (SYMBOL RIGHT)
WO1-4L	48X48	16.00						REVERSE CURVE (SYMBOL LEFT)
WO1-4R	48X48	16.00						REVERSE CURVE (SYMBOL RIGHT)
WO1-4bL	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL LEFT)
WO1-4bR	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL RIGHT)
WO1-4cL	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL LEFT)
WO1-4cR	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL RIGHT)
WO1-6	60X30	12.50						HORIZONTAL ARROW (SYMBOL)
WO1-6a	72X36	18.00						HORIZ. ARROW (SYMBOL ON PERMANENT BARRICADE)
WO1-7	60X30	12.50						DOUBLE HEAD HORIZONTAL ARROW (SYMBOL)
WO1-7a	72X36	18.00						DOUBLE HEAD HORIZ. ARROW (SYMBOL ON PERM. BARR.)
WO1-8	18X24	3.00						CHEVRON (SYMBOL)
WO1-8a	30X36	7.50						CHEVRON (SYMBOL FOR DIVIDED HIGHWAYS)
WO3-1	48X48	16.00						STOP AHEAD (SYMBOL)
WO3-2	48X48	16.00						YIELD AHEAD (SYMBOL)
WO3-3	48X48	16.00						SIGNAL AHEAD (SYMBOL)
WO3-4	48X48	16.00	2	32.00				BE PREPARED TO STOP
WO3-5	48X48	16.00						SPEED LIMIT AHEAD
WO4-1L	48X48	16.00						MERGE (SYMBOL FROM LEFT)
WO4-1R	48X48	16.00						MERGE (SYMBOL FROM RIGHT)
WO4-1aL	48X48	16.00						MERGE (LEFT)
WO4-1aR	48X48	16.00						MERGE (RIGHT)
WO5-1	48X48	16.00	2	32.00				ROAD/BRIDGE/RAMP NARROWS
WO5-3	48X48	16.00						ONE LANE BRIDGE
WO5-5	48X48	16.00						NARROW LANES
WO6-1	48X48	16.00						DIVIDED HIGHWAY (SYMBOL)
WO6-2	48X48	16.00						DIVIDED HIGHWAY END (SYMBOL)
WO6-3	48X48	16.00						TWO WAY TRAFFIC (SYMBOL)
WO7-3a	30X24	5.00						NEXT XX MILES (PLAQUE)
WO8-1	48X48	16.00	4	64.00				BUMP
WO8-2	48X48	16.00						DIP
WO8-3	48X48	16.00						PAVEMENT ENDS
WO8-4	48X48	16.00						SOFT SHOULDER
WO8-5	48X48	16.00						SLIPPERY WHEN WET (SYMBOL)
WO8-6	48X48	16.00						TRUCK CROSSING
WO8-6c	48X48	16.00						TRUCK ENTRANCE
WO8-7	36X36	9.00						LOOSE GRAVEL
WO8-7a	36X36	9.00						FRESH OIL / LOOSE GRAVEL
WO8-9	48X48	16.00						LOW SHOULDER
WO8-11	48X48	16.00	6	96.00				UNEVEN LANES
WO8-12	48X48	16.00	8	128.00				NO CENTER LINE
WO8-15	48X48	16.00						GROOVED PAVEMENT
WO8-15P	30X24	5.00						MOTORCYCLE (PLAQUE)
WO8-17L	48X48	16.00						SHOULDER DROP-OFF (SYMBOL LEFT)
WO8-17R	48X48	16.00	10	160.00				SHOULDER DROP-OFF (SYMBOL RIGHT)
WO8-17P	30X24	5.00	10	50.00				SHOULDER DROP-OFF (PLAQUE)
W10-1	42RND.	9.62						RAILROAD CROSSING
WO12-1	24X24	4.00						DOUBLE DOWN ARROW (SYMBOL)
WO12-2	48X48	16.00						LOW CLEARANCE (SYMBOL)
WO12-2x	24X18	3.00						LOW CLEARANCE (PLAQUE)
WO12-2a	84X24	14.00						OVERHEAD LOW CLEARANCE (FEET AND INCHES)
WO12-4	120X60	50.00						LOW CLEARANCE XX FT XX IN XX MILES AHEAD
WO12-5	120X60	50.00						WIDTH RESTRICTION XX FT XX IN XX MILES AHEAD
WO13-1	30X30	6.25						ADVISORY SPEED (PLAQUE)
WO16-2	30X24	5.00						XXX FEET (PLAQUE)
WO16-3	30X24	5.00						X MILE (PLAQUE)
WO20-1	48X48	16.00	50	800.00				ROAD/BRIDGE/RAMP WORK AHEAD
WO20-2	48X48	16.00						DETOUR AHEAD
WO20-3	48X48	16.00						ROAD CLOSED AHEAD
WO20-4	48X48	16.00	4	64.00				ONE LANE ROAD AHEAD
WO20-5	48X48	16.00						RIGHT/CENTER/LEFT LANE CLOSED AHEAD
WO20-5a	48X48	16.00						2 RIGHT/CENTER/LEFT LANES CLOSED AHEAD
WO20-6a	48X48	16.00						RIGHT/CENTER/LEFT LANE CLOSED
WO20-7a	48X48	16.00	4	64.00				FLAGGER (SYMBOL)
WO21-2	36X36	9.00						FRESH OIL
WO21-5	48X48	16.00	2	32.00				SHOULDER WORK / SHOULDER WORK AHEAD
WO22-1	48X48	16.00						BLASTING ZONE AHEAD
WO22-2	42X36	10.50						TURN OFF 2-WAY RADIO AND PHONE
WO22-3	42X36	10.50						END BLASTING ZONE
GO22-1	21X15	2.19						WET PAINT (ARROW PIVOTS)

SIGN	SIZE IN.	AREA SQ.FT.	QTY EACH	TOTAL SQ.FT.	QTY RELOC EACH	TOTAL RELOC SQ.FT.	SIGN NO.	DESCRIPTION
GUIDE SIGNS								
E05-1	36X48	12.00						GORE EXIT
E05-2	48X36	12.00						EXIT OPEN
E05-2a	48X36	12.00						EXIT CLOSED
GO20-1	60X24	10.00	2	20.00				ROAD WORK NEXT 15 MILES
GO20-2	48X24	8.00	2	16.00				END ROAD WORK
GO20-4	36X18	4.50	1	4.50				PILOT CAR FOLLOW ME
GO20-4a	42X30	8.75	15	131.25				PILOT CAR IN USE WAIT & FOLLOW
GO20-4a	18X12	1.50	30	45.00				PILOT CAR IN USE WAIT & FOLLOW
GO20-5aP	36X24	6.00	2	12.00				WORK ZONE (PLAQUE)
MO4-8a	24X18	3.00						END DETOUR
MO4-9L	48X36	12.00						DETOUR (LEFT)
MO4-9R	48X36	12.00						DETOUR (RIGHT)
MO4-9P	48X12	4.00						STREET NAME (PLAQUE)
MO4-10L	48X18	6.00						DETOUR ARROW (LEFT)
MO4-10R	48X18	6.00						DETOUR ARROW (RIGHT)
REGULATORY SIGNS								
R1-1	48X48	13.25						STOP
R1-2	48TRI.	6.93						YIELD
R1-2a	36X36	9.00						TO ONCOMING TRAFFIC (PLAQUE)
R1-3P	30X12	2.50						ALL WAY (PLAQUE)
R2-1	36X48	12.00	6	72.00				SPEED LIMIT 2@35, 2@45, 2@55
R3-1	48X48	16.00						NO RIGHT TURN (SYMBOL)
R3-2	48X48	16.00						NO LEFT TURN (SYMBOL)
R3-3	36X36	9.00						NO TURNS
R3-4	48X48	16.00						NO U-TURN (SYMBOL)
R3-7L	30X30	6.25						LEFT LANE MUST TURN LEFT
R3-7R	30X30	6.25						RIGHT LANE MUST TURN RIGHT
R4-1	36X48	12.00						DO NOT PASS
R4-2	36X48	12.00						PASS WITH CARE
R4-7a	36X48	12.00						KEEP RIGHT (HORIZONTAL ARROW)
R4-8a	36X48	12.00						KEEP LEFT (HORIZONTAL ARROW)
R5-1	30X30	6.25						DO NOT ENTER
R5-1a	36X24	6.00						WRONG WAY
R6-1L	54X18	6.75						ONE WAY ARROW (LEFT)
R6-1R	54X18	6.75						ONE WAY ARROW (RIGHT)
R6-2L	24X30	5.00						ONE WAY (LEFT)
R6-2R	24X30	5.00						ONE WAY (RIGHT)
R9-9	24X12	2.00						SIDEWALK CLOSED
R9-11L	24X18	3.00						SIDEWALK CLOSED AHEAD, (ARROW LEFT) CROSS HERE
R9-11R	24X18	3.00						SIDEWALK CLOSED AHEAD, (ARROW RIGHT) CROSS HERE
R10-6	24X36	6.00						STOP HERE ON RED (45^ ARROW)
R11-2	48X30	10.00						ROAD CLOSED
R11-3a	60X30	12.50						ROAD CLOSED XX MILES AHEAD LOCAL TRAFFIC ONLY
R11-4	60X30	12.50						ROAD CLOSED TO THRU TRAFFIC
CONST-3A	60X48	20.00						FINE SIGN
CONST-3X	56X12	4.67						SPEEDING/PASSING (PLATE)
MISCELLANEOUS SIGNS								
CONST-5	48X36	12.00						POINT OF PRESENCE
CONST-5	96X48	32.00						POINT OF PRESENCE
CONST-8	48X36	12.00	2	24.00				WORK ZONE NO PHONE ZONE

	TOTAL
616-10.05 CONSTRUCTION SIGNS	1847

	TOTAL
616-10.10 RELOCATED SIGNS	

EFFECTIVE: 07-01-2025

ITEM NUMBER	TOTAL QTY	DESCRIPTION
6122008		IMPACT ATTENUATOR 40 MPH (SAND BARRELS)
6122009		IMPACT ATTENUATOR 45 MPH (SAND BARRELS)
6122010		IMPACT ATTENUATOR 50 MPH (SAND BARRELS)
6122012		IMPACT ATTENUATOR 55 MPH (SAND BARRELS)
6122014		IMPACT ATTENUATOR 60 MPH (SAND BARRELS)
6122017		IMPACT ATTENUATOR 65 MPH (SAND BARRELS)
6122019		IMPACT ATTENUATOR 70 MPH (SAND BARRELS)
6122020		REPLACEMENT SAND BARREL
6122030		IMPACT ATTENUATOR (RELOCATION)
6122040		WORK ZONE CRASH CUSHION (NARROW)
6122041		WORK ZONE CRASH CUSHION (RELOCATION)
6123001		TRUCK MOUNTED ATTENUATOR (TMA)
6161012		BUOYS (BOATS KEEP OUT)
6161013		BUOYS (NO WAKE)
6161014		SPECIAL SIGN ASSEMBLY (BOATS KEEP OUT)
6161020		CHANNELIZER (DRUM-LIKE)
6161022		CHANNELIZER (CONE)
6161025	100	CHANNELIZER (TRIM-LINE)
6161026		CHANNELIZER (VERTICAL PANEL)
6161030		TYPE 3 MOVEABLE BARRICADE
6161033		DIRECTION INDICATOR BARRICADE
6161040		FLASHING ARROW PANEL
6161047		TYPE 3 OBJECT MARKER
6161055		SEQUENTIAL FLASHING WARNING LIGHT
6161070		TUBULAR MARKER
6161095		RADAR SPEED ADVISORY SYSTEM
6161096		CHANGEABLE MESSAGE SIGN, COMMISSION FURNISHED/RETAINED
6161098A		CHANGEABLE MESSAGE SIGN WITHOUT COMM. INTERFACE, CONTRACTOR FURNISHED/RETAINED
6161099	3	CHANGEABLE MESSAGE SIGN WITH COMM. INTERFACE, CONTRACTOR FURNISHED/RETAINED
6162000A		WORK ZONE TRAFFIC SIGNAL SYSTEM
6162002		TEMPORARY LONG-TERM RUMBLE STRIPS
6173600D		TEMPORARY TRAFFIC BARRIER, CONTRACTOR FURNISHED/RETAINED
6173700B		TEMP. TRAFFIC BARRIER ANCHORED, CONTRACTOR FURNISHED/RETAINED
6173706		TEMP. TRAFFIC BARRIER STIFFNESS TRANSITION CONTRACTOR FURNISHED/RETAINED
6174000A		TEMP. TRAFFIC BARRIER HEIGHT TRANSITION, CONTRACTOR FURNISHED/RETAINED
6175010A		RELOCATING TEMPORARY TRAFFIC BARRIER
6175011B		RELOCATING TEMP. TRAFFIC BARRIER ANCHORED
6175013		RELOCATING TEMP. TRAFFIC BARRIER STIFFNESS
6175020A		RELOCATING TEMP. TRAFFIC BARRIER HEIGHT
6208064A		TEMPORARY RAISED PAVEMENT MARKER
9029400		TEMPORARY TRAFFIC SIGNALS
9029401		TEMPORARY TRAFFIC SIGNALS AND LIGHTING

SUMMARY OF QUANTITIES

SHEET 4 OF 4

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
09/09/2025 08:06:37
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED 9/8/2025

ROUTE	STATE
H	MO
DISTRICT	SHEET NO.
SW	3

COUNTY: LAWRENCE

JOB NO. JSR0312

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

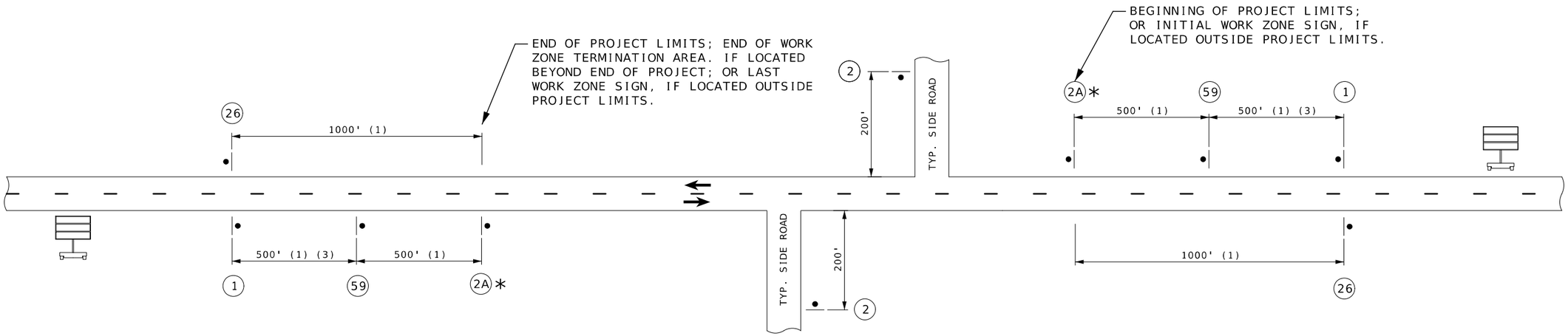
DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

GENERAL NOTES:

1. ANY EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL SIGNING SHALL BE COMPLETELY COVERED OR REMOVED.
2. TEMPORARY SIGNING SHOWN IS FOR WORK ON ONE SIDE OF THE ROAD. FOR WORK ON THE OTHER SIDE, REVERSE ORDER OF THE SIGNS AND CHANNELIZERS.
3. SIGNING SHOWN SHALL REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE.
4. ALTERNATE TRAFFIC CONTROL MAY BE USED AS NEEDED AT THE APPROVAL OF THE ENGINEER.
5. REFER TO STANDARD DRAWING 616.10, 619.10, AND 620.10 FOR ADDITIONAL DETAILS AND 903.03 FOR SIGN AND SIGN MOUNTING REQUIREMENTS.
6. SPEED LIMIT SIGNS INDICATING THE NORMAL SPEED LIMIT SHALL BE INSTALLED AT THE END OF THE WORK ZONE, PROVIDED NO FURTHER WORK ZONES WILL BE ENCOUNTERED WITHIN THE NEXT 1/2 MILE.
7. TEMPORARY SPEED LIMIT SIGNS SHALL BE COVERED OR REMOVED WHEN THE CONDITIONS REQUIRING REDUCED SPEEDS DO NOT EXIST.
8. NO DIRECT PAYMENT WILL BE MADE FOR THE RELOCATION OF CHANNELIZERS, CONSTRUCTION SIGNS, OR FLASHING ARROW PANEL.
9. ALL SIGNS SHALL BE PORTABLE MOUNT AND ARE TO BE MOVED AS WORK PROGRESSES, UNLESS OTHERWISE NOTED. ALL TRAFFIC CONTROL ITEMS SHALL BE REMOVED FROM THE ROADWAY DURING NON-WORKING HOURS.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING PROPER TRAFFIC CONTROL SETUPS THROUGHOUT CONSTRUCTION AS DESCRIBED IN THESE PLANS OR AS APPROVED BY THE ENGINEER.
11. WHERE MINIMUM LANE WIDTHS CANNOT BE ACHIEVED ON THE SIDE STREETS, THE CONTRACTOR SHALL CLOSE HALF OF THE APPROACH AND POSITION A FLAGGER AT EACH END OF THE WORK AREA. LOCATION TO BE DETERMINED BY THE ENGINEER. ADVANCE FLAGGER SIGNING SHALL BE INSTALLED AS OUTLINED IN THE MUTCD.
12. THE CONTRACTOR SHALL MAINTAIN PUBLIC ACCESS TO ALL BUSINESSES AND STREETS. NO STREET OR ENTRANCE SHALL BE COMPLETELY BLOCKED.
13. SPACING & DISTANCES OF TRAFFIC CONTROL DEVICES ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD & APPROVED BY THE ENGINEER.
14. TRAFFIC CONTROL SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
15. ADJUST ADVANCE WARNING SIGN SPACING TO AVOID SIGN PLACEMENT WITHIN THE LIMITS OF THE RAILROAD RIGHT OF WAY.



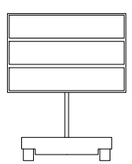
TYPICAL BEGINNING AND END OF PROJECT

(UNDIVIDED HIGHWAY)

(1) DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS. WHERE TRAFFIC BACKUPS ARE EXPECTED BEYOND THE ADVANCE WARNING AREA, ADDITIONAL SIGNING MAY BE NEEDED.

(2) THE "WORK ZONE NO PHONE ZONE" SIGN IS PLACED A MINIMUM OF 500 FEET BEFORE THE ROAD WORK AHEAD SIGN.

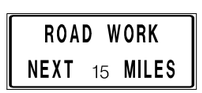
(3) CMS PLACEMENT SHALL BE APPROVED BY THE ENGINEER.



(3)

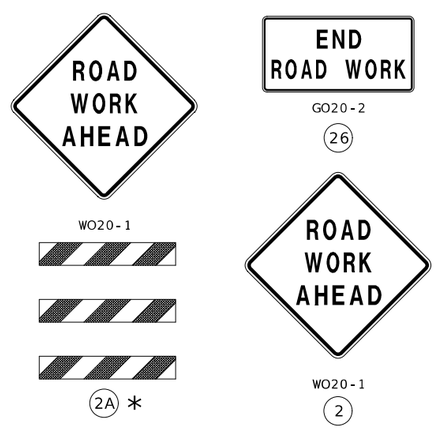
CONST-8

59 (2)



GO20-1

1



WO20-1

2A *

GO20-2

26

WO20-1

2

*SIGN 2A WITH AWRS MUST BE GROUND OR SKID MOUNTED.

NOT TO SCALE

TRAFFIC CONTROL
SHEET 1 OF 6

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER

Darrell R Cook
08/11/2025 13:31:12
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
8/11/2025	
ROUTE	STATE
H	MO
DISTRICT	SHEET NO.
SW	4
COUNTY	
LAWRENCE	
JOB NO.	
JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

WO20-1

2

WO20-4

7

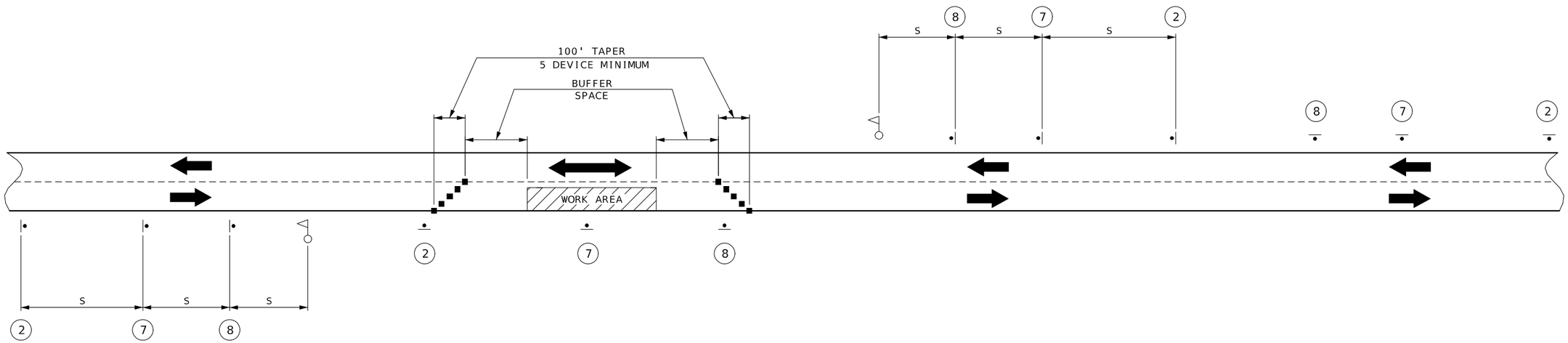
WO20-7a

8

GO20-4

53

SIGN 53, "PILOT CAR FOLLOW ME" SHALL BE IN CONTRACTOR'S RESPONSIBILITY WHEN IN USE DURING OPERATION.



SET 1	SET 2	SET 3	SET 4
ACTIVE	INACTIVE	ACTIVE	INACTIVE

NOTES:
THIS PLAN IS FOR DAYLIGHT FLAGGING OPERATIONS ONLY, WHEN NIGHT WORK IS REQUIRED, OR OTHERWISE ALLOWED BY THE ENGINEER, WORK ZONE LIGHTING SHALL BE PROVIDED IN ACCORDANCE WITH SEC 616.5.2.

CHANNELIZING DEVICES LOCATED DOWNSTREAM OF THE ONE-LANE, TWO-WAY TAPER ARE OPTIONAL. THESE DEVICES SHOULD BE ELIMINATED WHEN THEIR USE WILL REDUCE THE USABLE LANE WIDTH, INCLUDING ANY ACCEPTABLE SHOULDERS, TO LESS THAN 10' OR WILL SIGNIFICANTLY AFFECT THE RESURFACING OPERATION.

SIGN SETS 1 AND 3 ARE ACTIVE AND (I.E., SIGNS FACE ONCOMING TRAFFIC). SIGN SETS 2 AND 4 ARE INACTIVE (I.E., SIGNS DO NOT FACE EITHER DIRECTION OF TRAFFIC) WHEN THE RESURFACING OPERATION IS LOCATED BETWEEN SIGN SETS 1 AND 3.

WHEN SIGN SETS 2 AND 4 ARE ACTIVE, SIGN SETS 1 AND 3 BECOME INACTIVE AND ARE ADVANCED TO BECOME SETS 2 AND 4 WITH SIGN LEGENDS TURNED AWAY FROM BOTH DIRECTIONS OF TRAFFIC.
WHEN THE RESURFACING OPERATION ADVANCES TO BETWEEN SIGN SETS 2 AND 4, SIGN SETS 2 AND 4 BECOME ACTIVE (I.E., NEW SIGN SETS 1 AND 3) AND SIGN SETS 1 AND 3 ADVANCED IN THE DIRECTION OF THE OPERATION (I.E., NEW SIGN SETS 2 AND 4).

■ - CHANNELIZERS

- FLAGGER

SPEED	SIGN SPACING (FT) (1)	BUFFER SPACE
PERMANENT POSTED (MPH)	NON-DIVIDED HIGHWAYS (S)	LENGTH (FT)
0-35	200	250
40-45	350	360
50-55	500	495
60-70	1000	730

SPACING NOTES:

(1) SPACING BETWEEN SIGNS, BETWEEN LAST SIGN AND FLAGGER, BEGINNING OF TAPER OR SIGNED CONDITION.

SPACINGS MAY BE ADJUSTED AS NECESSARY TO MEET FIELD CONDITIONS, AS APPROVED BY ENGINEER.

MOVING FLAGGER OPERATION
(UNDIVIDED HIGHWAY)

NOT TO SCALE

TRAFFIC CONTROL
SHEET 2 OF 6

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
09/09/2025 08:06:44
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

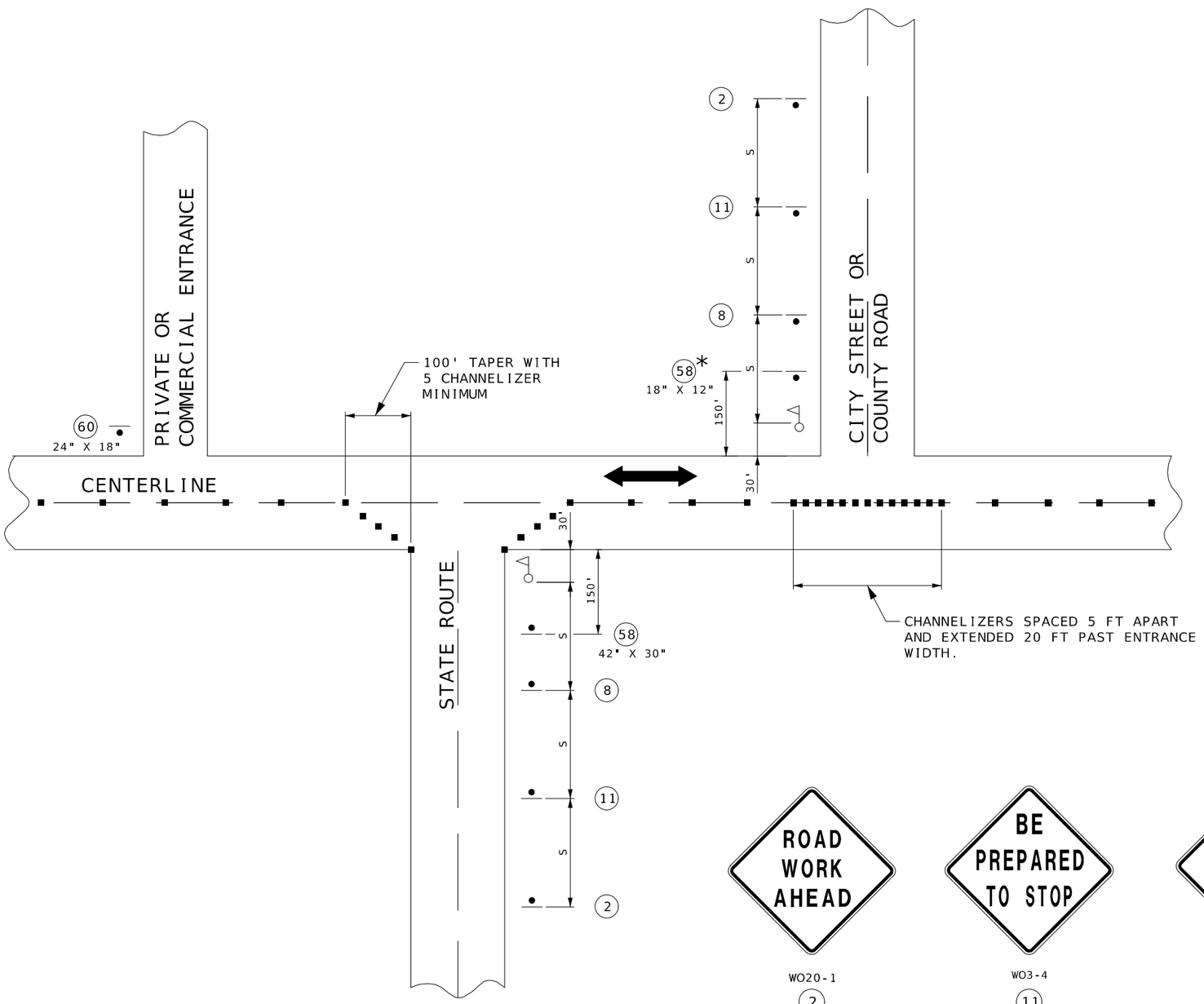
DATE PREPARED	
8/11/2025	
ROUTE H	STATE MO
DISTRICT SW	SHEET NO. 5
COUNTY LAWRENCE	
JOB NO. JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

SIDE ROADS ENTERING WORK ZONES



SPEED	SIGN SPACING (FT.)	CHANNELIZER SPACING (FT)
PERMANENT POSTED (MPH)	NON-DIVIDED HIGHWAYS (S)	BUFFER WORK AREA
0-35	200	40
40-45	350	80
50-55	500	80
60-70	1000	120

DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS.

NOTES:
WARNING SIGNS SHALL BE ERECTED AT EACH INTERSECTION WITH ANOTHER STATE HIGHWAY WITHIN THE WORK ZONE.

ADDITIONAL WARNING SIGNS SHALL BE ERECTED AT OTHER INTERSECTIONS WITHIN THE WORK ZONE, AS DIRECTED BY THE ENGINEER.

■ - CHANNELIZERS

- FLAGGER

IF USED AT NIGHT, THE FLAGGER STATIONS SHALL BE ILLUMINATED WITH AN AVERAGE MAINTAINED INTENSITY OF 0.6 FOOTCANDLES (6.5 LUX).

G020-4a

58

SHALL BE USED ON ALL INTERSECTIONS.

* THE SMALLER 18" X 12" SIGN 58 IS USED ON ALL OTHER NON-STATE ROUTES (CITY STREETS, COUNTY ROADS, ETC.).

24" X 18"

60

SHALL ONLY BE USED AT PRIVATE AND COMMERCIAL ENTRANCES. SEE STD PLAN 616.10 FOR SIGN DETAILS.

WO20-1

2

WO3-4

11

WO20-7a

8

NOT TO SCALE

TRAFFIC CONTROL
SHEET 3 OF 6

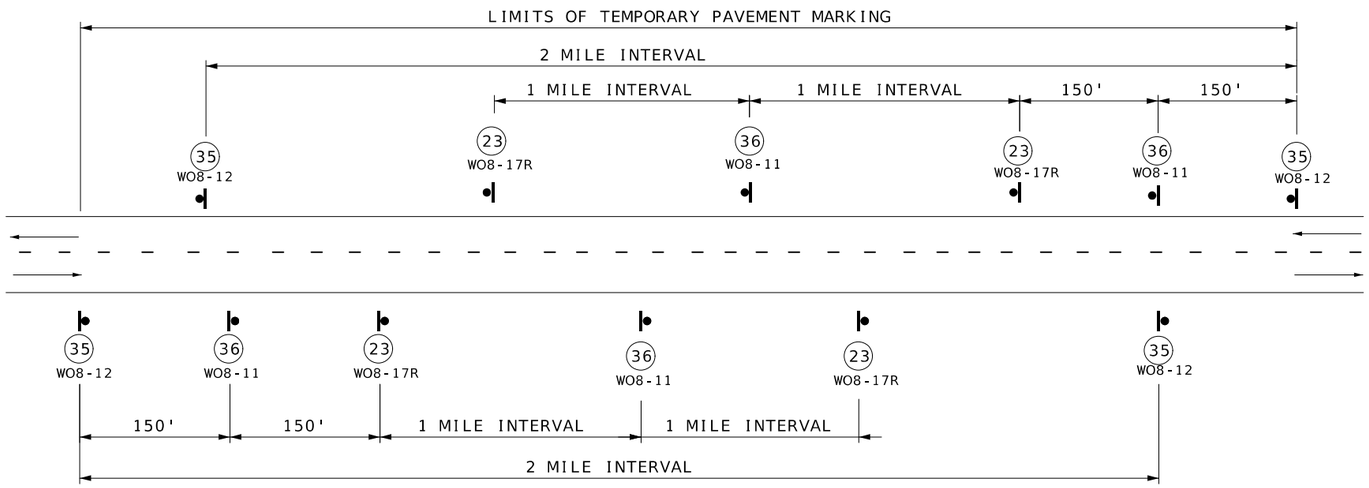
STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER

Darrell R Cook
09/09/2025 08:06:53
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
8/11/2025	
ROUTE: H	STATE: MO
DISTRICT: SW	SHEET NO.: 6
COUNTY: LAWRENCE	
JOB NO.: JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

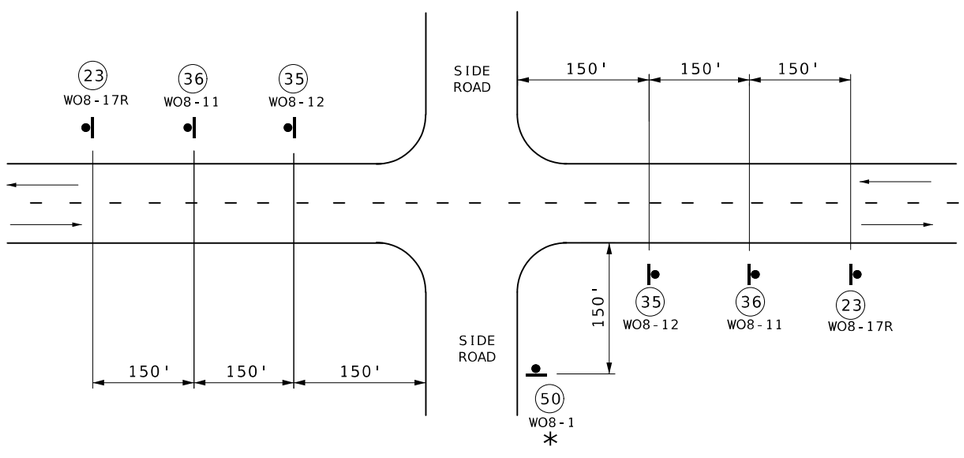
DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



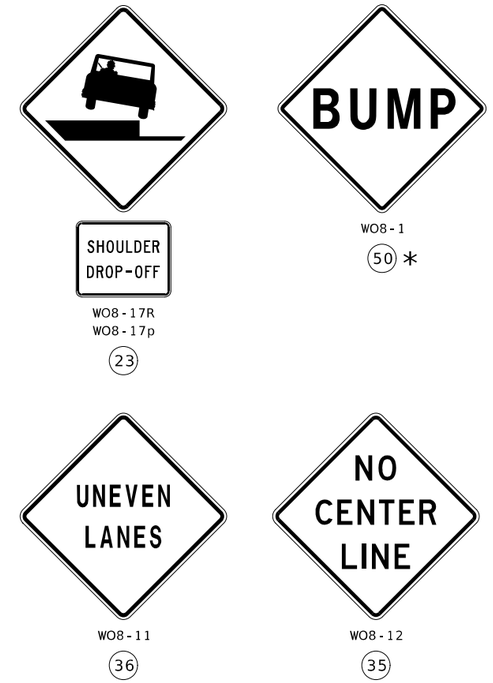
SIGN SPACING FOR MAINLINE

(DETAIL SHOWN IS BASED ON A PROJECT MEETING ALL CONDITIONS: NO CENTER STRIPE, UNEVEN LANES, SHOULDER DROP-OFF AND BUMP.)
WHEN BOTH UNEVEN LANES AND SHOULDER DROP-OFF SIGNS ARE USED, BOTH SIGHS SHALL STAY IN PLACE UNTIL BOTH CONDITIONS NO LONGER EXIST.
IF ONLY ONE CONDITION EXISTS (UNEVEN LANES OR SHOULDER DROP-OFF). THE SIGN SPACING SHALL BE AT 1 MILE INTERVALS.



SIGN SPACING AT STATE ROUTE INTERSECTIONS

* BUMP SIGN SHOULD BE IN ACCORDANCE WITH STANDARD PLAN 619.10.
UNLESS REQUIRED BY STANDARD PLAN 619.10, BUMP SIGN AT CONTRACTOR'S EXPENSE, NO DIRECT PAY. SEE STANDARD SPECIFICATION 622.2.4.



GENERAL NOTES:

FOR DETAILS OF TEMPORARY PAVEMENT MARKING, SEE STD. 620.10, SHEET 1 OF 5.

SIGN 35 AND TEMPORARY PAVEMENT MARKING INSTALLED WHERE CENTERLINE STRIPING HAS BEEN COVERED OR REMOVED. SIGNS ARE TO REMAIN IN PLACE UNTIL THE PERMANENT CENTERLINE PAVEMENT MARKINGS ARE IN PLACE. SIGNS SHALL BE COVERED OR REMOVED WHEN PAVEMENT CENTERLINE MARKING HAS BEEN INSTALLED.

SIGN 35 IS PLACED AT APPROXIMATELY TWO-MILE INTERVALS AND AT STATE ROUTE JUNCTIONS. WHEN THE INSTALLATION AT A JUNCTION IS WITHIN ONE-EIGHTH MILE OF THE NORMAL MAINLINE SIGN 35 , THE LATTER MAY BE ELIMINATED.

ALL SIGNS SHALL BE POST MOUNTED AND IN ACCORDANCE WITH STANDARD PLAN 616.10 AND 903.03.

WHEN SHOULDER DROP-OFF SIGNS ARE IN PLACE FOR GREATER THAN THREE DAYS, THE SHOULDER DROP-OFF PLAQUE SHOULD BE USED IN ADDITION WITH THE SHOULDER DROP-OFF SIGN.

FOR BRIDGE EXCEPTIONS AND COLDMILLED AREAS, LOCATION OF SIGN 50 TO BE APPROVED BY THE ENGINEER. UNLESS REQUIRED BY STANDARD PLAN 619.10, SIGN 50 AT CONTRACTOR'S EXPENSE, NO DIRECT PAY. SEE STANDARD SPECIFICATION 622.2.4.

NOT TO SCALE

TRAFFIC CONTROL
SHEET 4 OF 6

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
09/09/2025 08:07:03
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
8/11/2025	
ROUTE H	STATE MO
DISTRICT SW	SHEET NO. 7
COUNTY LAWRENCE	
JOB NO. JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

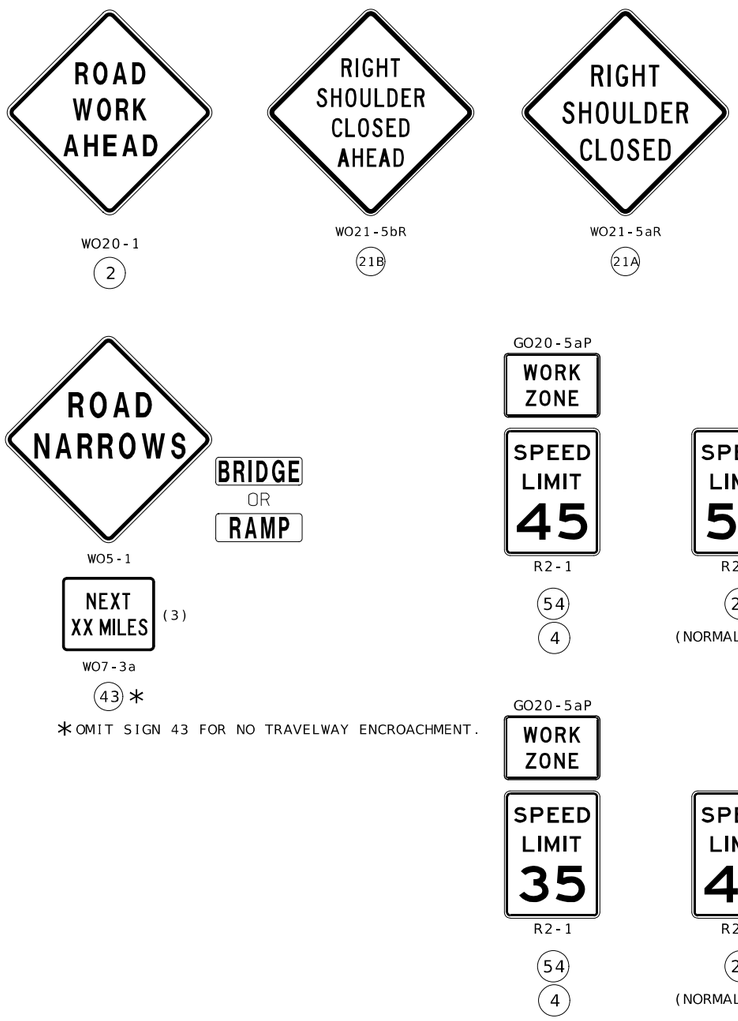
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

SHOULDER WORK WITH MINOR TRAVELWAY ENCROACHMENT OR NO ENCROACHMENT
(DIVIDED HIGHWAY)

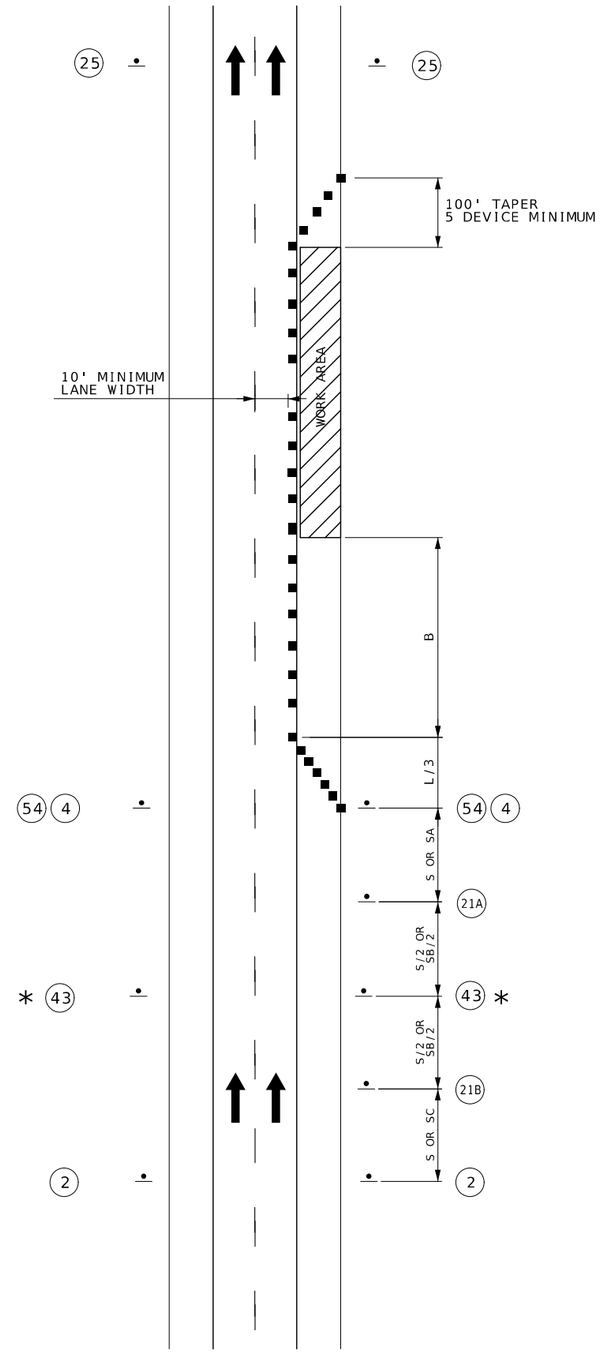
SPEED	SIGN SPACING (FT)	TAPER LENGTH (FT)		OPTIONAL	CHANNELIZER SPACING (FT)	
PERMANENT POSTED (MPH)	DIVIDED HIGHWAYS (S)	SHOULDER(1) (T1)	LANE(2) (T2)	BUFFER LENGTH (FT) (B)	TAPERS	BUFFER/ WORK AREAS
0-35	200	70	245	250	35	40
40-45	500	150	540	360	40	80
50-55	1000	185	660	495	50	80
60-70	SA - 1000 SB - 1500 SC - 2640	235	840	730	60	120

NOTES:

(1) SHOULDER TAPER LENGTH BASED ON 10 FT. (STANDARD SHOULDER WIDTH) OFFSET.
(2) LANE TAPER LENGTH BASED ON 12 FT. (STANDARD LANE WIDTH) OFFSET.
(3) NEXT XX MILES SIGN NOT REQUIRED FOR NARROW LANE SECTIONS LESS THAN ONE MILE.



*OMIT SIGN 43 FOR NO TRAVELWAY ENCROACHMENT.



NOT TO SCALE

TRAFFIC CONTROL
SHEET 5 OF 6

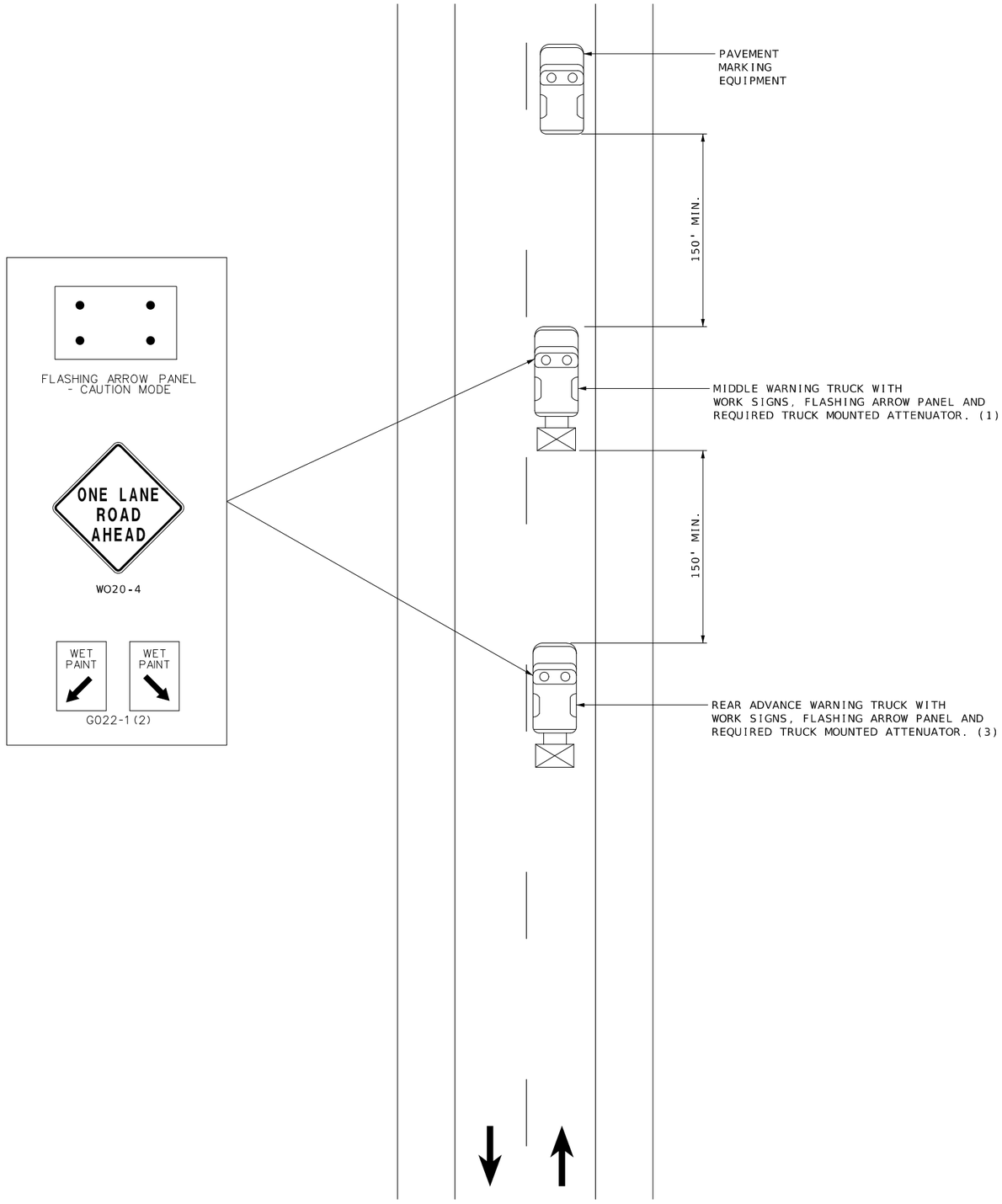
STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER
Darrell R Cook
09/09/2025 08:07:10
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
8/11/2025	
ROUTE H	STATE MO
DISTRICT SW	SHEET NO. 8
COUNTY LAWRENCE	
JOB NO. JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)



STRIPING ON TWO-LANE HIGHWAY

NOTES:

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY PROVIDE ADDITIONAL PROTECTIVE TRUCKS EQUIPPED WITH PROPER WARNING DEVICES.

PROTECTIVE TRUCKS AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE HAZARD WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED IN A MANNER SUCH THAT THEY ARE NOT OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS ON VEHICLE-MOUNTED SIGNS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.

FLASHING ARROW PANELS AND SIGNS SHALL BE INCIDENTAL TO TRUCK MOUNTED ATTENUATORS, WHEREVER USED. NO ADDITIONAL PAYMENT WILL BE MADE.

(1) TRUCK IS OPTIONAL ON TWO-LANE UNDIVIDED HIGHWAYS IF SIGNING AND ARROW BOARD ARE MOUNTED ON THE PAVEMENT MARKING EQUIPMENT.

(2) WET PAINT SIGNS ARE INSTALLED TO INDICATE THE SIDE IN WHICH THE PAVEMENT MARKING MATERIAL IS BEING APPLIED. AT THE CONTRACTOR'S OPTION, A FRONT FACING WET PAINT SIGN MAY BE INSTALLED ON THE LEFT SIDE OF THE PAVEMENT MARKING EQUIPMENT.

(3) REAR WARNING TRUCK IS POSITIONED AT THE NO TRACK POINT OF THE PAVEMENT MARKING MATERIAL, OR VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE, OR SPACING SHOWN, WHICHEVER IS GREATER.

NOT TO SCALE

TRAFFIC CONTROL
SHEET 6 OF 6

STATE OF MISSOURI
DARRELL RAY COOK
NUMBER PE-2005000971
PROFESSIONAL ENGINEER

Darrell R Cook
08/11/2025 13:31:22
DARRELL RAY COOK - CIVIL
MO-PE-2005000971

DATE PREPARED	
8/11/2025	
ROUTE	STATE
H	MO
DISTRICT	SHEET NO.
SW	9
COUNTY	
LAWRENCE	
JOB NO.	
JSR0312	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
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