

DESIGN DESIGNATION

I-44

A.A.D.T. - 2024 = 36,224

D.H.V. = 8%

T = 30%

V = 70 M.P.H.

FUNCTIONAL CLASSIFICATION- INTERSTATE

ROUTE D

A.A.D.T. - 2024 = 9,463

D.H.V. = 9%

T = 8%

V = 55 M.P.H.

FUNCTIONAL CLASSIFICATION- MAJOR COLLECTOR

NO R/W REQUIRED

CONVENTIONAL SYMBOLS
(USED IN PLANS)

	EXISTING	NEW
BUILDINGS AND STRUCTURES		
GUARD RAIL		
GUARD CABLE		
CONCRETE RIGHT-OF-WAY MARKER		
STEEL RIGHT-OF-WAY MARKER		
LOCATION SURVEY MARKER		
UTILITIES		
FIBER OPTICS	-FO-	-FO-
OVERHEAD CABLE TV	-OTV-	-OTV-
UNDERGROUND CABLE TV	-UTV-	-UTV-
OVERHEAD TELEPHONE	-OT-	-OT-
UNDERGROUND TELEPHONE	-UT-	-UT-
OVERHEAD POWER	-OE-	-OE-
UNDERGROUND POWER	-UE-	-UE-
SANITARY SEWER	-S-	-S-
STORM SEWER	-SS-	-SS-
GAS	-G-	-G-
WATER	-W-	-W-
MANHOLE		
FIRE HYDRANT		
WATER VALVE		
WATER METER		
DROP INLET		
DITCH BLOCK		
GROUND MOUNTED SIGN		
LIGHT POLE		
H-FRAME POWER POLE		
TELEPHONE PEDESTAL		
FENCE		
CHAIN LINK		
WOVEN WIRE		
GATE POST		
BENCHMARK		

NOTE: DASHED OR OPEN SYMBOLS INDICATE EXISTING FEATURES

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

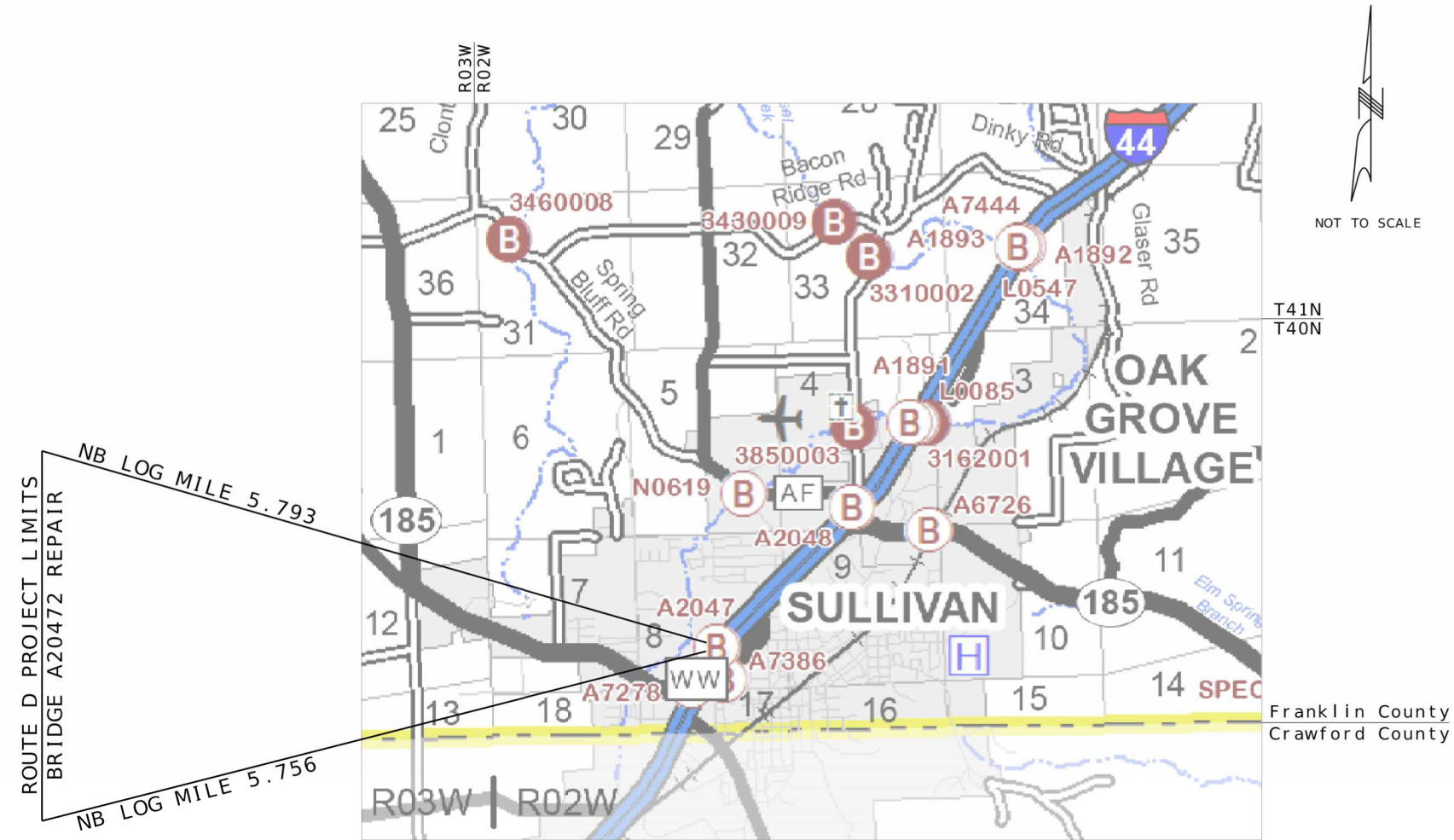
PLANS FOR PROPOSED

STATE HIGHWAY

FRANKLIN COUNTY



TOWNSHIP 40N, RANGE 02W, SECTION 08



ROUTE D PROJECT LIMITS
BRIDGE A20472 REPAIR

NB LOG MILE 5.793

NB LOG MILE 5.756

THE EXISTENCE AND APPROXIMATE LOCATION OF UTILITY FACILITIES KNOWN TO EXIST, AS SHOWN ON THE PLANS, ARE BASED ON THE BEST INFORMATION AVAILABLE TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE. RELIANCE UPON THIS INFORMATION IS DONE AT THE RISK AND PERIL OF THE USER, AND THE COMMISSION SHALL NOT BE LIABLE FOR ANY DAMAGES THAT MAY ARISE FROM ANY ERROR IN THE INFORMATION. IT IS, THEREFORE, THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE, LOCATION AND STATUS OF ANY FACILITY. SUCH VERIFICATION INCLUDES DIRECT CONTACT WITH THE LISTED UTILITIES.

INDEX OF SHEETS	
DESCRIPTION	SHEET NUMBER
TITLE SHEET -----	1
TYPICAL SECTION-----	2
QUANTITIES (QU) (3 SHEETS)-----	3
TRAFFIC CONTROL SHEETS (TC)-----	4-10
PAVEMENT MARKING (PM)-----	11
BRIDGE DRAWINGS (B)	
A20472-----	1-5

LENGTH OF PROJECT	
BEGINNING OF PROJECT	LOG MILE 5.756
END OF PROJECT	LOG MILE 5.793
APPARENT LENGTH	196.83 FEET
EQUATIONS AND EXCEPTIONS:	

THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY

DATE PREPARED
8/8/2025

ROUTE D	STATE MO
DISTRICT SL	SHEET NO. 1

COUNTY
FRANKLIN

JOB NO.
JSLM0087

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL ST., 300
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

16305 SWINGLEY RIDGE RD
ST. 300
CHESTERFIELD, MO 63017
314.231.0100
GBAteam.com

GEORGE BUTLER
ASSOCIATES, INC.
PRO. ENGINEER 000133
ARCHITECT 000212
PRO. LAND SURVEYOR 000059

BRIAN KIERATH
PROFESSIONAL
ENGINEER
PE-2023008624

DISCLAIMER
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DATE PREPARED 8/8/2025	
ROUTE D	STATE MO
DISTRICT SL	SHEET NO. 2
COUNTY FRANKLIN	
JOB NO. JSLM0087	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

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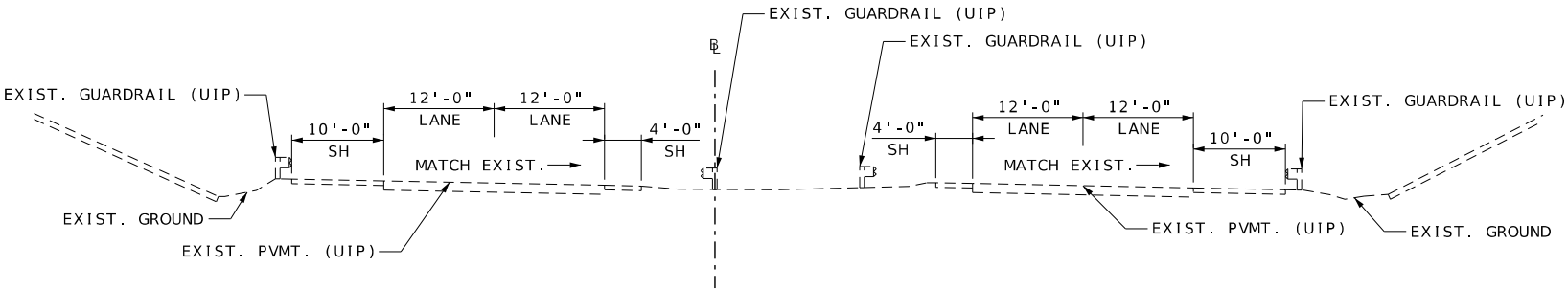
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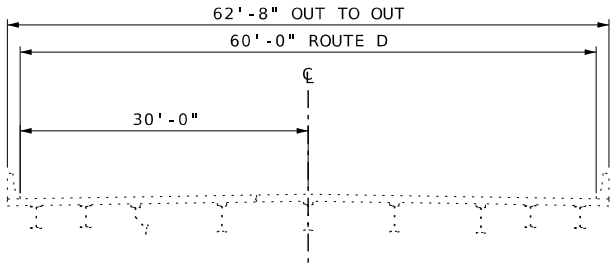
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WB I-44

EB I-44



EXIST. TYPICAL SECTION I-44



EXIST. TYPICAL SECTION ROUTE D
BRIDGE A20472
BEGINNING OF PROJECT LOG MILE 5.756
END OF PROJECT LOG MILE 5.793

TYPICAL SECTION

ROUTE D
OVER I-44

MOBILIZATION	
TOTAL	1 LUMP SUM

CONTRACTOR FURNISHED SURVEYING AND STAKING	
TOTAL	1 LUMP SUM



THIS SHEET HAS BEEN
SIGNED, SEALED, AND DATED
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DATE PREPARED
8/8/2025

ROUTE D	STATE MO
DISTRICT SL	SHEET NO. 3

COUNTY
FRANKLIN

JOB NO.
JSLM0087

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION	DATE

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SUMMARY OF QUANTITIES
SHEET 1 OF 3

BRIAN KIERATH
PROFESSIONAL
ENGINEER
PE-2023008624

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED. REV.

REMOVAL OF IMPROVEMENTS			
ROADWAY	ITEM	QUANTITY	UNIT
RTE D BRIDGE	CHANNELIZER (TRIM LINE)	20	EA
RTE D BRIDGE	TYPE III MOVEABLE BARRICADE	5	EA
PROJECT TOTAL		1	LUMP SUM

NOTE: REMOVAL OF TEMPORARY TRAFFIC CONTROL DEVICES IS FOR COMISSION AND THIRD PARTY INSTALLED DEVICES ESTABLISHED ON THE PROJECT SITE PRIOR TO THE BEGINNING OF THE PROJECT. ALL QUANTITIES SHOWN IN THIS TABLE ARE APPROXIMATE. COSTS SHALL BE CONSIDERED SUBSIDIARY TO BID ITEM 202-20.10 "REMOVAL OF IMPROVEMENTS" (LUMP SUM). SEE JOB SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

PERMANENT PAVEMENT MARKING					
LOCATION	FROM	TO	HIGH BUILD WATERBORNE PAVEMENT MARKING TYPE L BEADS		PREFORMED THERMOPLASTIC PAVEMENT MARKING
			6" WHITE	4" YELLOW	LEFT ARROW
			SOLID	DOUBLE SOLID	
			(LF)	(LF)	
RTE. D	BRIDGE OVER I-44		1,084	716	4
PROJECT TOTAL			1,084	716	4
BID ITEM TOTAL			1,084	716	4

PAVEMENT MARKING REMOVAL			
	PAVEMENT MARKING REMOVAL		PAVEMENT MARKING REMOVAL (SYMBOLS)
	TEMPORARY PAVEMENT MARKING*	PERMANENT PAVEMENT MARKING	PERMANENT PAVEMENT MARKING
	(LF)	(LF)	(EA)
PROJECT TOTAL	406	700	2
BID ITEM TOTAL	1,106		2

*REMOVAL OF TEMPORARY PAVEMENT MARKING QUANTITY SHOWN IS FOR COMMISSION-INSTALLED TEMPORARY PAVEMENT MARKING ESTABLISHED ON THE PROJECT SITE PRIOR TO BEGINNING OF THE PROJECT. ALL QUANTITIES SHOWN IN THIS TABLE ARE APPROXIMATE.

SUMMARY OF QUANTITIES
SHEET 2 OF 3



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SIGNED, SEALED, AND DATED
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DATE PREPARED
8/8/2025

ROUTE D	STATE MO
DISTRICT SL	SHEET NO. 3

COUNTY
FRANKLIN

JOB NO.
JSLM0087

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION									
DATE									

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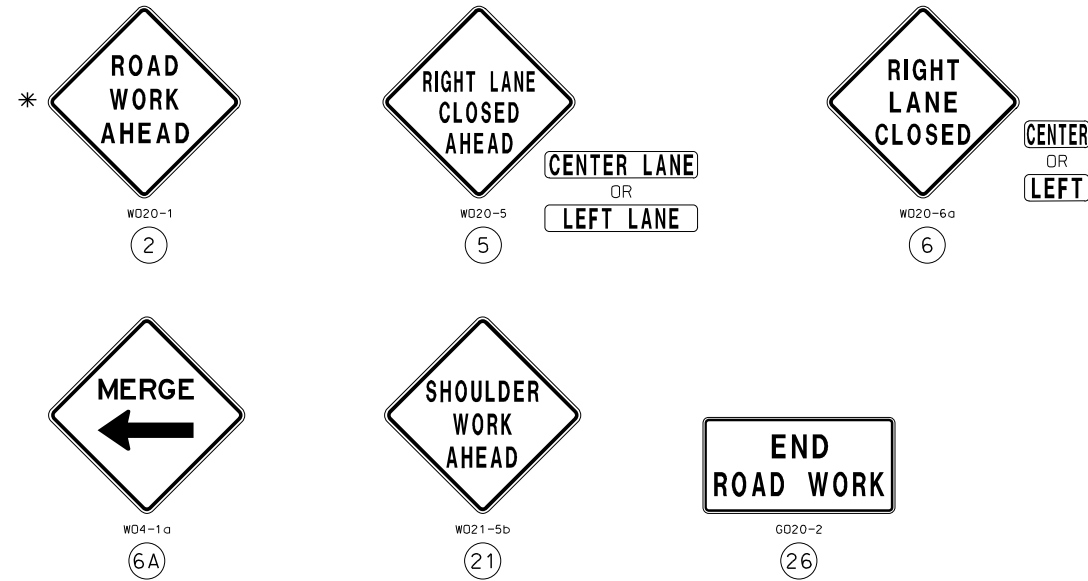
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ENGINEER
PE-2018024421

REV.

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

SIGN	SIZE IN.	AREA SQ. FT.	QTY EACH	TOTAL AREA SQ. FT.	QTY RELOC EACH	TOTAL RELOC SQ. FT.	SIGN NO.	DESCRIPTION	SIGN	SIZE IN.	AREA SQ. FT.	QTY EACH	TOTAL SQ. FT.	QTY RELOC EACH	TOTAL RELOC SQ. FT.	SIGN NO.	DESCRIPTION	ITEM NUMBER	TOTAL QTY	DESCRIPTION
WARNING SIGNS									GUIDE SIGNS									6122008		IMPACT ATTENUATOR 40 MPH (SAND BARRELS)
WO1-1L	48X48	16.00						TURN (SYMBOL LEFT)	E05-1	36X48	12.00						GORE EXIT	6122009		IMPACT ATTENUATOR 45 MPH (SAND BARRELS)
WO1-1R	48X48	16.00						TURN (SYMBOL RIGHT)	E05-2	48X36	12.00						EXIT OPEN	6122010		IMPACT ATTENUATOR 50 MPH (SAND BARRELS)
WO1-2L	48X48	16.00						CURVE (SYMBOL LEFT)	E05-2a	48X36	12.00						EXIT CLOSED	6122012		IMPACT ATTENUATOR 55 MPH (SAND BARRELS)
WO1-2R	48X48	16.00						CURVE (SYMBOL RIGHT)	GO20-1	60X24	10.00						ROAD WORK NEXT XX MILES	6122014		IMPACT ATTENUATOR 60 MPH (SAND BARRELS)
WO1-3L	48X48	16.00						REVERSE TURN (SYMBOL LEFT)	GO20-2	48X24	8.00						END ROAD WORK	6122017		IMPACT ATTENUATOR 65 MPH (SAND BARRELS)
WO1-3R	48X48	16.00						REVERSE TURN (SYMBOL RIGHT)	GO20-4	36X18	4.50						PILOT CAR FOLLOW ME	6122019		IMPACT ATTENUATOR 70 MPH (SAND BARRELS)
WO1-4L	48X48	16.00						REVERSE CURVE (SYMBOL LEFT)	GO20-4a	42X30	8.75						PILOT CAR IN USE WAIT & FOLLOW	6122020		REPLACEMENT SAND BARREL
WO1-4R	48X48	16.00	2	32.00			15	REVERSE CURVE (SYMBOL RIGHT)	GO20-4a	18X12	1.50						PILOT CAR IN USE WAIT & FOLLOW	6122030		IMPACT ATTENUATOR (RELOCATION)
WO1-4bL	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL LEFT)	GO20-5aP	36X24	6.00	3	18.00			54	WORK ZONE (PLAQUE)	6122040		WORK ZONE CRASH CUSHION (NARROW)
WO1-4bR	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL RIGHT)	MO4-8a	24X18	3.00	2	6.00			52	END DETOUR	6122041		WORK ZONE CRASH CUSHION (RELOCATION)
WO1-4cL	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL LEFT)	MO4-9L	48X36	12.00						DETOUR (LEFT)	6123001		TRUCK MOUNTED ATTENUATOR (TMA)
WO1-4cR	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL RIGHT)	MO4-9R	48X36	12.00						DETOUR (RIGHT)	6161012		BUOYS (BOATS KEEP OUT)
WO1-6	60X30	12.50	1	12.50			16	HORIZONTAL ARROW (SYMBOL)	MO4-9P	48X12	4.00						STREET NAME (PLAQUE)	6161013		BUOYS (NO WAKE)
WO1-6a	72X36	18.00						HORIZ. ARROW (SYMBOL ON PERMANENT BARRICADE)	MO4-10L	48X18	6.00						DETOUR ARROW (LEFT)	6161014		SPECIAL SIGN ASSEMBLY (BOATS KEEP OUT)
WO1-7	60X30	12.50						DOUBLE HEAD HORIZONTAL ARROW (SYMBOL)	MO4-10R	48X18	6.00						DETOUR ARROW (RIGHT)	6161020		CHANNELIZER (DRUM-LIKE)
WO1-7a	72X36	18.00						DOUBLE HEAD HORIZ. ARROW (SYMBOL ON PERM. BARR.)	REGULATORY SIGNS									6161022		CHANNELIZER (CONE)
WO1-8	18X24	3.00						CHEVRON (SYMBOL)	R1-1	48X48	13.25						STOP	6161025	82	CHANNELIZER (TRIM-LINE)
WO1-8a	30X36	7.50						CHEVRON (SYMBOL FOR DIVIDED HIGHWAYS)	R1-2	48TRI	6.93						YIELD	6161026		CHANNELIZER (VERTICAL PANEL)
WO3-1	48X48	16.00						STOP AHEAD (SYMBOL)	R1-2a	36X36	9.00						TO ONCOMING TRAFFIC (PLAQUE)	6161030	40	TYPE 3 MOVEABLE BARRICADE
WO3-2	48X48	16.00						YIELD AHEAD (SYMBOL)	R1-3P	30X12	2.50						ALL WAY (PLAQUE)	6161033	24	DIRECTION INDICATOR BARRICADE
WO3-3	48X48	16.00						SIGNAL AHEAD (SYMBOL)	R2-1	36X48	12.00	3	36.00			25	SPEED LIMIT XX	6161040	2	FLASHING ARROW PANEL
WO3-4	48X48	16.00						BE PREPARED TO STOP	R3-1	48X48	16.00	2	32.00			56	NO RIGHT TURN (SYMBOL)	6161047		TYPE 3 OBJECT MARKER
WO3-5	48X48	16.00	2	32.00			4	SPEED LIMIT AHEAD	R3-2	48X48	16.00	4	64.00			55	NO LEFT TURN (SYMBOL)	6161055	24	SEQUENTIAL FLASHING WARNING LIGHT
WO4-1L	48X48	16.00						MERGE (SYMBOL FROM LEFT)	R3-3	36X36	9.00						NO TURNS	6161070		TUBULAR MARKER
WO4-1R	48X48	16.00						MERGE (SYMBOL FROM RIGHT)	R3-4	48X48	16.00						NO U-TURN (SYMBOL)	6161095		RADAR SPEED ADVISORY SYSTEM
WO4-1aL	48X48	16.00						MERGE (LEFT)	R3-7L	30X30	6.25						LEFT LANE MUST TURN LEFT	6161096		CHANGEABLE MESSAGE SIGN, COMMISSION FURNISHED/RETAINED
WO4-1aR	48X48	16.00	1	16.00			6A	MERGE (RIGHT)	R3-7R	30X30	6.25						RIGHT LANE MUST TURN RIGHT			
WO5-1	48X48	16.00						ROAD/BRIDGE/RAMP NARROWS	R4-1	36X48	12.00						DO NOT PASS	6161098A	2*	CHANGEABLE MESSAGE SIGN WITHOUT COMM. INTERFACE, CONTRACTOR FURNISHED/RETAINED
WO5-3	48X48	16.00						ONE LANE BRIDGE	R4-2	36X48	12.00						PASS WITH CARE			
WO5-5	48X48	16.00						NARROW LANES	R4-7a	36X48	12.00						KEEP RIGHT (HORIZONTAL ARROW)	6161099		CHANGEABLE MESSAGE SIGN WITH COMM. INTERFACE, CONTRACTOR FURNISHED/RETAINED
WO6-1	48X48	16.00						DIVIDED HIGHWAY (SYMBOL)	R4-8a	36X48	12.00						KEEP LEFT (HORIZONTAL ARROW)			
WO6-2	48X48	16.00						DIVIDED HIGHWAY END (SYMBOL)	R5-1	30X30	6.25						DO NOT ENTER	6162000A		WORK ZONE TRAFFIC SIGNAL SYSTEM
WO6-3	48X48	16.00						TWO WAY TRAFFIC (SYMBOL)	R5-1a	36X24	6.00						WRONG WAY	6162002		TEMPORARY LONG-TERM RUMBLE STRIPS
WO7-3a	30X24	5.00						NEXT XX MILES (PLAQUE)	R6-1L	54X18	6.75						ONE WAY ARROW (LEFT)	6173600D		TEMPORARY TRAFFIC BARRIER, CONTRACTOR FURNISHED/RETAINED
WO8-1	48X48	16.00						BUMP	R6-1R	54X18	6.75						ONE WAY ARROW (RIGHT)			
WO8-2	48X48	16.00						DIP	R6-2L	24X30	5.00						ONE WAY (LEFT)	6173700B		TEMP. TRAFFIC BARRIER ANCHORED, CONTRACTOR FURNISHED/RETAINED
WO8-3	48X48	16.00						PAVEMENT ENDS	R6-2R	24X30	5.00						ONE WAY (RIGHT)			
WO8-4	48X48	16.00						SOFT SHOULDER	R9-9	24X12	2.00						SIDEWALK CLOSED	6173706		TEMP. TRAFFIC BARRIER STIFFNESS TRANSITION CONTRACTOR FURNISHED/RETAINED
WO8-5	48X48	16.00						SLIPPERY WHEN WET (SYMBOL)	R9-11L	24X18	3.00						SIDEWALK CLOSED AHEAD, (ARROW LEFT) CROSS HERE			
WO8-6	48X48	16.00						TRUCK CROSSING	R9-11R	24X18	3.00						SIDEWALK CLOSED AHEAD, (ARROW RIGHT) CROSS HERE	6174000A		TEMP. TRAFFIC BARRIER HEIGHT TRANSITION, CONTRACTOR FURNISHED/RETAINED
WO8-6c	48X48	16.00						TRUCK ENTRANCE	R10-6	24X36	6.00						STOP HERE ON RED (45° ARROW)	6175010A		RELOCATING TEMPORARY TRAFFIC BARRIER
WO8-7	36X36	9.00						LOOSE GRAVEL	R11-2	48X30	10.00	7	70.00			29	ROAD CLOSED	6175011B		RELOCATING TEMP. TRAFFIC BARRIER ANCHORED
WO8-7a	36X36	9.00						FRESH OIL / LOOSE GRAVEL	R11-3a	60X30	12.50						ROAD CLOSED XX MILES AHEAD	6175013		RELOCATING TEMP. TRAFFIC BARRIER STIFFNESS
WO8-9	48X48	16.00						LOW SHOULDER	R11-4	60X30	12.50						LOCAL TRAFFIC ONLY	6175020A		RELOCATING TEMP. TRAFFIC BARRIER HEIGHT
WO8-11	48X48	16.00						UNEVEN LANES	CONST-3A	60X48	20.00						ROAD CLOSED TO THRU TRAFFIC	6208064A		TEMPORARY RAISED PAVEMENT MARKER
WO8-12	48X48	16.00						NO CENTER LINE	CONST-3X	56X12	4.67						FINE SIGN	9029400		TEMPORARY TRAFFIC SIGNALS
WO8-15	48X48	16.00						GROOVED PAVEMENT									SPEEDING/PASSING (PLATE)	9029401		TEMPORARY TRAFFIC SIGNALS AND LIGHTING
WO8-15P	30X24	5.00						MOTORCYCLE (PLAQUE)	MISCELLANEOUS SIGNS									*NOT PART OF LUMP SUM TRAFFIC CONTROL		
WO8-17L	48X48	16.00						SHOULDER DROP-OFF (SYMBOL LEFT)	CONST-5	48X36	12.00						POINT OF PRESENCE			
WO8-17R	48X48	16.00						SHOULDER DROP-OFF (SYMBOL RIGHT)	CONST-5	96X48	32.00						POINT OF PRESENCE			
WO8-17P	30X24	5.00						SHOULDER DROP-OFF (PLAQUE)	CONST-8	48X36	12.00	7	94.00				WORK ZONE NO PHONE ZONE			
W10-1	42RND	9.62						RAILROAD CROSSING	SP-58	96X48	32.00	3	96.00			58	RTE D CLOSED AHEAD FOLLOW DETOUR			
WO12-1	24X24	4.00						DOUBLE DOWN ARROW (SYMBOL)	SP-58	96X48	32.00	1	32.00			59	RTE D CLOSED			

	SIGN (SINGLE SIDED)
	CHANNELIZER
	DIRECTIONAL INDICATOR BARRICADES
	TYPE III BARRICADE
	TYPE II ADA BARRICADE
	IMPACT ATTENUATOR
	LANE USE ARROW
	WORK AREA
	FLASHING ARROW PANELS
	FLAGGER
	CHANGEABLE MESSAGE BOARD
	BARRIER HEIGHT TRANSITION



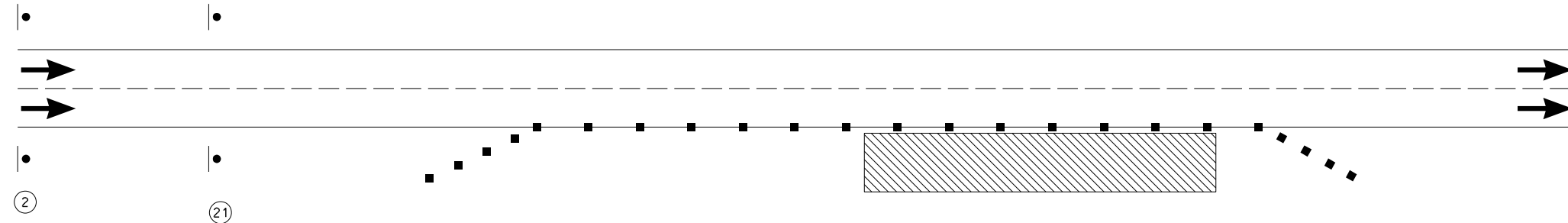
CHANNELIZING DEVICE SPACING ¹		
POSTED SPEED PRIOR TO CONSTRUCTION, MPH (P)	MAX. CHANNELIZER SPACING THROUGH TAPER (X)	MAX. CHANNELIZER SPACING THROUGH WORK AREA (Y)
0 TO 35	35' ²	40' ²
40 TO 45	40' ²	80' ²
50 TO 55	50' ³	80' ³
60 TO 70	60' ³	120' ³

- L = W x P FOR 40 MPH OR MORE
 L = $\frac{W^2}{50}$ FOR 35 MPH OR LESS
 L = TAPER LENGTH IN FEET
 W = LATERAL SHIFT IN FEET
 P = POSTED SPEED PRIOR TO CONSTRUCTION

BUFFER SPACE	
POSTED SPEED PRIOR TO CONSTRUCTION, MPH (P)	RECOMMENDED LENGTH OF LONGITUDINAL BUFFER
0 TO 35	280 FT
40 TO 45	400 FT
50 TO 55	560 FT
60 TO 70	840 FT

Diagram illustrating the layout of a 100 FT. TAPER section for 5 DEVICES, showing distances and stationing:

- 1500' (Distance from station 2 to station 21)
- 1000' (Distance from station 21 to the start of the taper)
- L/3 (Distance from station 21 to the start of the taper)
- OPTIONAL BUFFER AREA
- 150' MIN (Minimum distance from the start of the taper to the end of the buffer area)
- WORK AREA
- 100 FT. TAPER (Total length of the taper section)
- 5 DEVICES (Number of devices in the taper section)

[illegible]

BRIDGE NO.

DESCRIPTION

DATE _____

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

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JEFFERSON CITY, MO 65102
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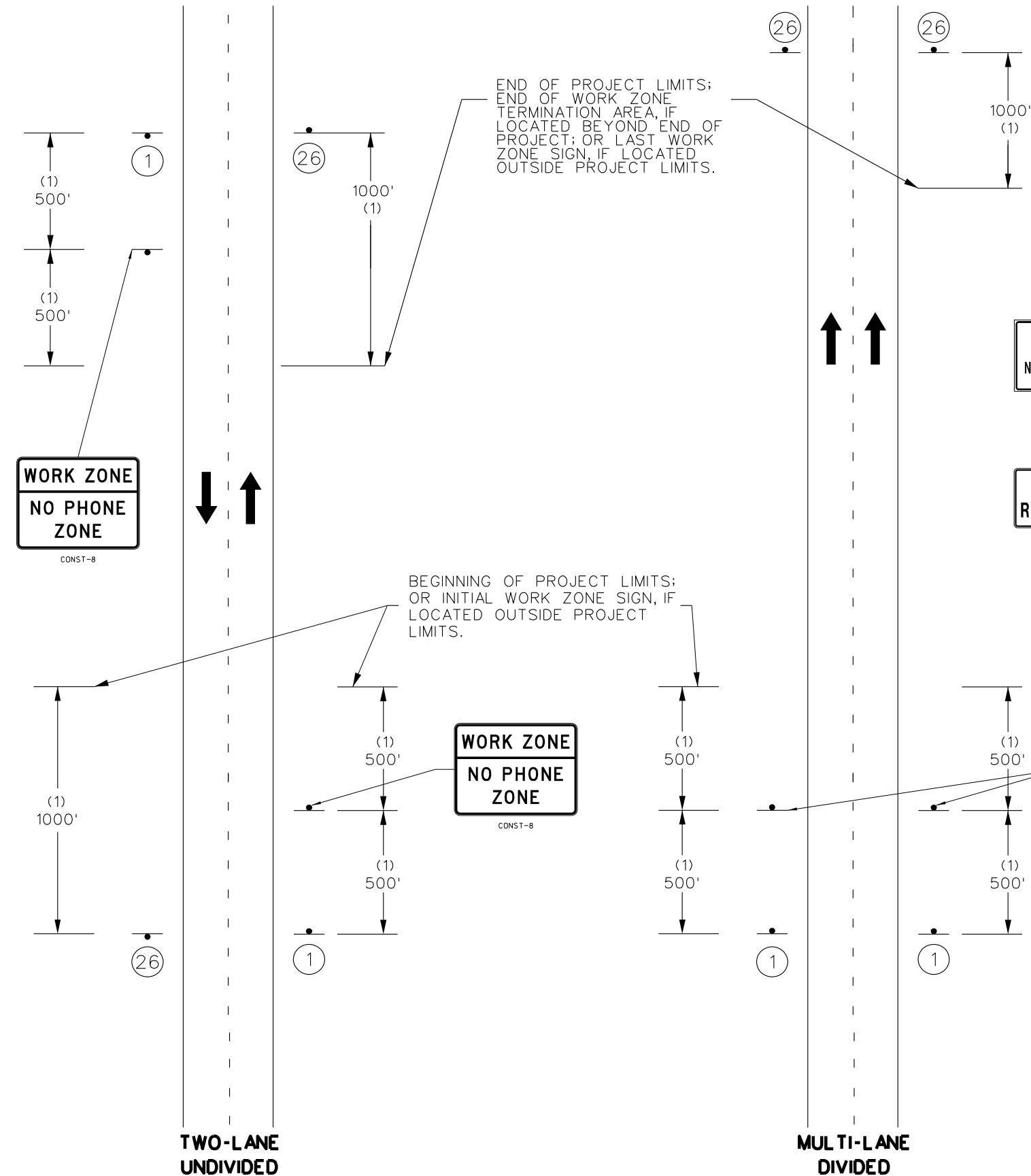
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RISTOPHER NOVOSEL
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PE-2018024421

REV. _____ IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.



NOTES:

SIGN 1 IS ONLY REQUIRED ON PROJECTS OF 2 MILES OR GREATER IN LENGTH.

SIGN 26 IS USED ON ALL PROJECTS WHERE SIGN 1 IS USED.

OTHER SIGNS SUCH AS DETOUR OR ALTERNATE ROUTE SIGNING MAY BE USED OUTSIDE THE PROJECT LIMITS.

ANY EXISTING SIGNING THAT CONFLICTS WITH THE TRAFFIC CONTROL SIGNING SHALL BE COMPLETELY COVERED OR REMOVED.

(1) DISTANCE MAY BE ADJUSTED ACCORDING TO FIELD CONDITIONS. WHERE TRAFFIC BACKUPS ARE EXPECTED BEYOND THE ADVANCE WARNING AREA, ADDITIONAL SIGNING MAY BE NEEDED.

(2) SIGN CONST-7-72 IS PLACED 500 FEET BEFORE THE BEGINNING OF PROJECT LIMITS OR THE ROAD WORK AHEAD SIGN OR ROAD WORK NEXT XX MILES SIGN. IF USED, WHEN THESE SIGNS ARE LOCATED OUTSIDE THE PROJECT LIMITS.

(3) THE "WORK ZONE NO PHONE ZONE" SIGN IS PLACED A MINIMUM OF 500 FEET BEFORE THE ROAD WORK AHEAD SIGN.

BEGIN/END OF PROJECT SIGNING SHALL BE INSTALLED FOR ALL DIRECTIONS OF ROUTE D AND I-44.

STATE OF MISSOURI

CHRISTOPHER NOVOSEL

NUMBER PE-2018024421

PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY.

DATE PREPARED

8/8/2025

ROUTE

D

STATE

MO

DISTRICT

SL

SHEET NO.

5

COUNTY

FRANKLIN

JOB NO.

JSLM0087

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL ST. 300 CHESTERFIELD, MO 63017 314.231.0100 GBAteam.com

1-888-ASK-MODOT (1-888-275-6636)

GBA

16305 SWINGLEY RIDGE RD ST. 300 CHESTERFIELD, MO 63017 314.231.0100 GBAteam.com

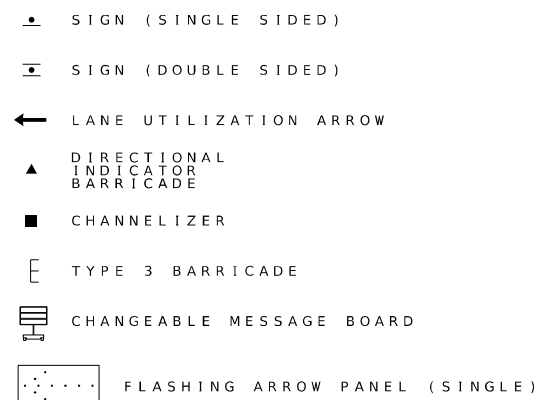
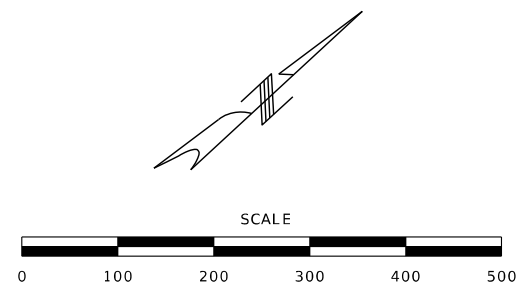
GEORGE BUTLER ASSOCIATES, INC. PRO. ENGINEER 000133 ARCHITECT 000212 PRO. LAND SURVEYOR 000059

TEMPORARY TRAFFIC CONTROL BEGIN/END OF PROJECT SIGNING

CHRISTOPHER NOVOSEL PROFESSIONAL ENGINEER PE-2018024421

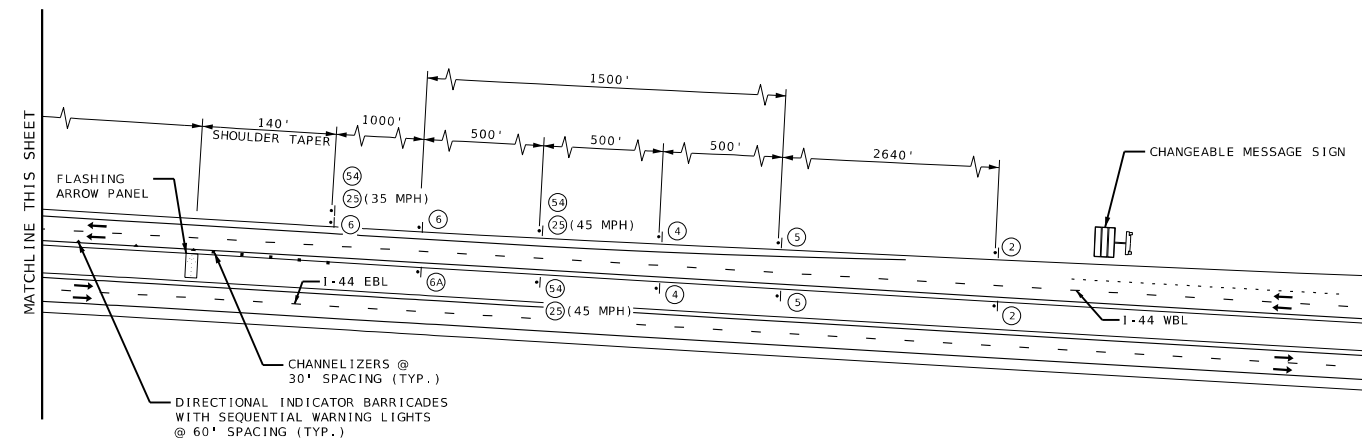
IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

REV.



TEMPORARY
TRAFFIC CONTROL
SHEET 1 OF 2





DETOUR

MO4-8

WEST

TO

44

M(113-4

M(113-1

M(116-2R

57A

DETOUR

MO4-8

TO

D

M1-5

M1-5a

M6-3

57C

DETOUR

MO4-8

TO

D

M4-5

M1-5a

M6-1

57D

DETOUR

MO4-8

TO

D

M4-5

M1-5a

M6-1

57E

DETOUR

MO4-8

TO

44

M(114-5

M(113-1

M(116-1

57G

DETOUR

MO4-8

TO

44

M(114-5

M(113-1

M(116-1

57H

DETOUR

MO4-8

TO

44

M(114-5

M(113-1

M(116-3

57I

DETOUR

MO4-8

TO

44

M(114-5

M(113-4

M(113-1

57J

DETOUR

MO4-8

TO

44

M(114-5

M(113-4

M(116-1

57K

END

DETOUR

MO4-8a

TO

EAST

44

M(114-5

M(113-2

M(113-1

57M

CLOSED AHEAD

FOLLOW DETOUR

SP-58

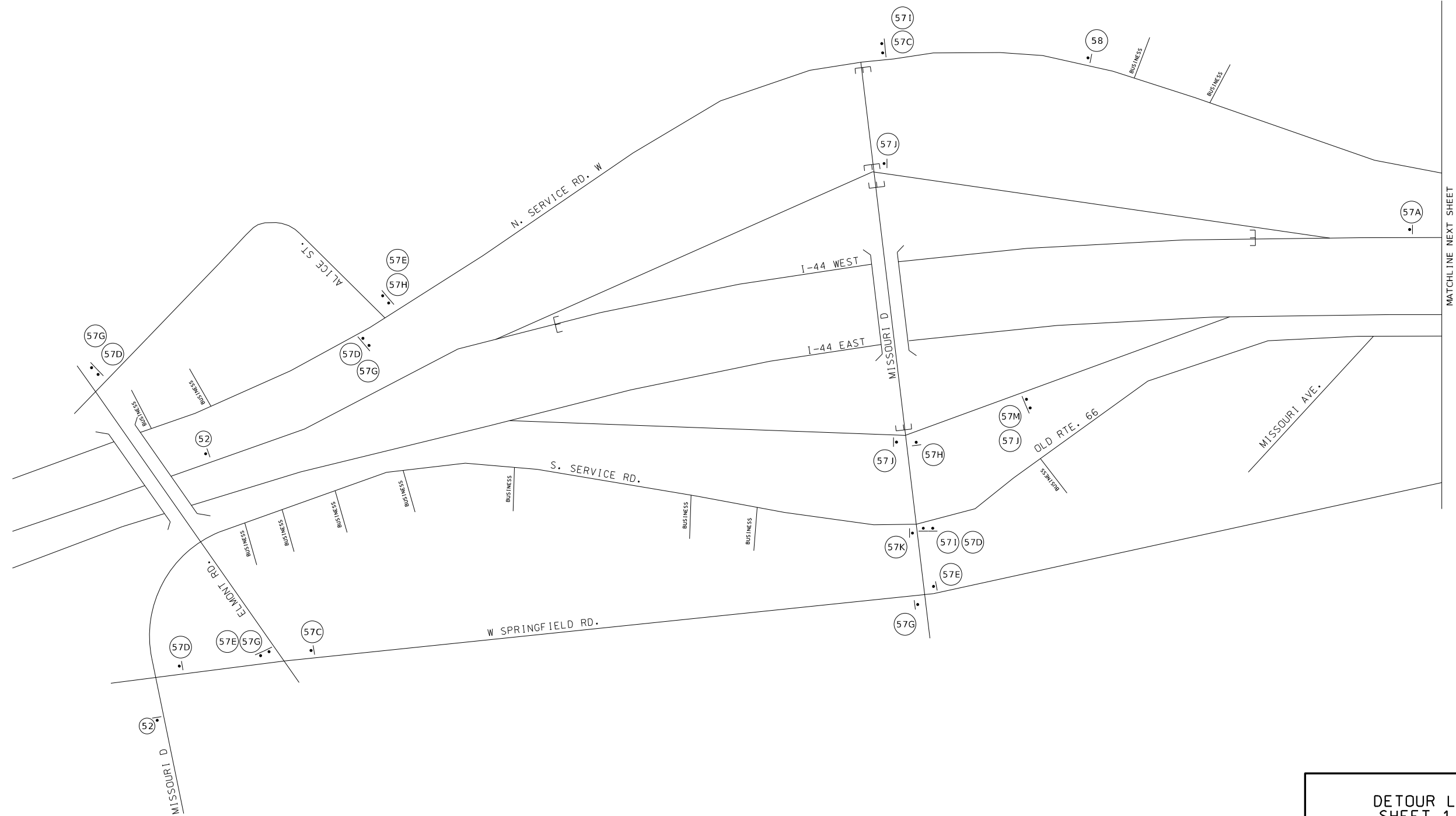
58

END

DETOUR

MO4-8a

52



DETOUR LAYOUT
SHEET 1 OF 3

STATE OF MISSOURI

CHRISTOPHER NOVOSEL

NUMBER PE-2018024421

PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY.

DATE PREPARED 8/8/2025

ROUTE D STATE MO

DISTRICT SL SHEET NO. 8

COUNTY FRANKLIN

JOB NO. JSLM0087

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

MODOT

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

GBA

16305 SWINGLEY RIDGE RD

ST. 300

CHESTERFIELD, MO 63017

314.231.0100

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GEORGE BUTLER ASSOCIATES, INC.

PRO. ENGINEER 000133

ARCHITECT 000212

PRO. LAND SURVEYOR 000059

CHRISTOPHER NOVOSEL

PROFESSIONAL ENGINEER

PE-2018024421

DETOUR

MO4+8

WEST

INTERSTATE 44

M(1)3+4

M(1)1+1

M(1)5+2R

57B

DETOUR

MO4+8

TO

D

M4+5

M1+5a

M6+3

57C

DETOUR

MO4+8

TO

D

M4+5

M1+5a

M6+1

57D

DETOUR

MO4+8

TO

D

M4+5

M1+5a

M6+1

57E

DETOUR

MO4+8

TO

INTERSTATE 44

M(1)4+5

M(1)1+1

M(1)6+1

57G

DETOUR

MO4+8

TO

INTERSTATE 44

M(1)4+5

M(1)1+1

M(1)6+1

57H

DETOUR

MO4+8

TO

INTERSTATE 44

M(1)4+5

M(1)1+1

M(1)6+1

57I

DETOUR

MO4+8

TO

WEST

M(1)4+5

M(1)3+4

M(1)1+1

M(1)6+3

57J

DETOUR

MO4+8

TO

WEST

M(1)4+5

M(1)3+4

M(1)1+1

M(1)6+1

57K

END DETOUR

MO4+8a

TO

EAST

M(1)4+5

M(1)3+2

M(1)1+1

57M

DETOUR

MO4+8

TO

WEST

M(1)4+5

M(1)3+4

M(1)1+1

M(1)5+2R

57O

INTERSTATE 44

CLOSED AHEAD

FOLLOW DETOUR

SP-59

59

END DETOUR

MO4+8a

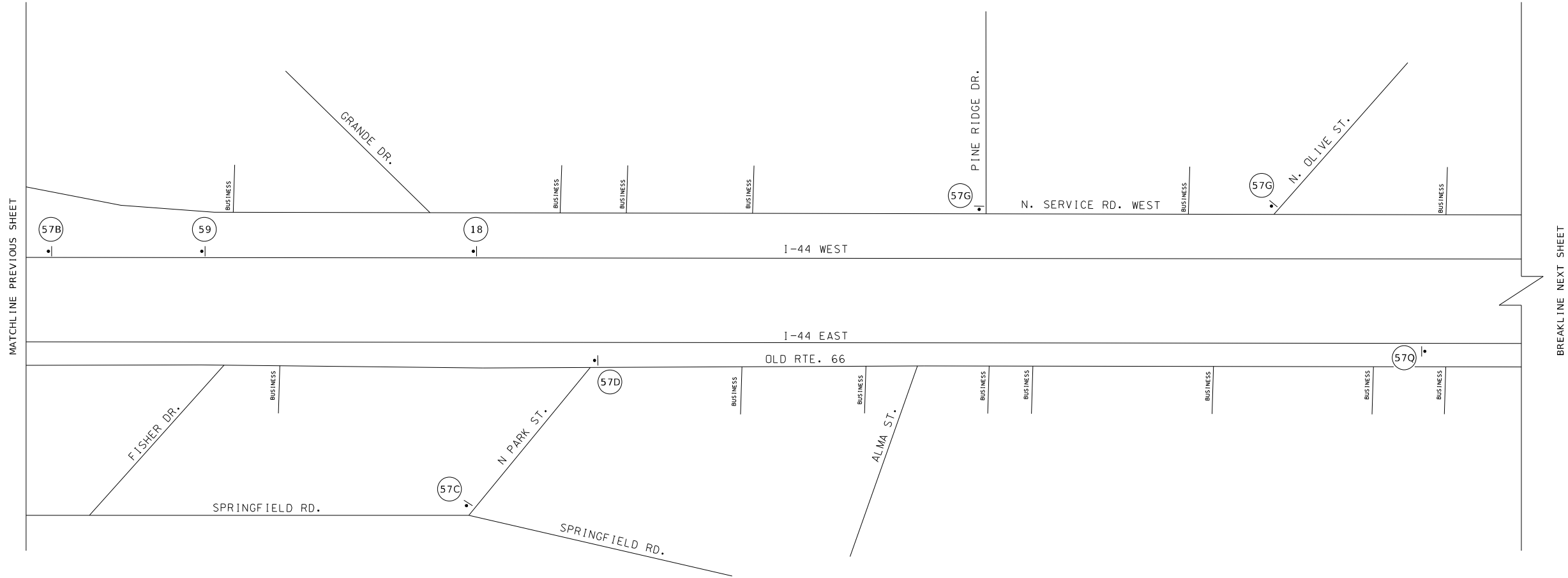
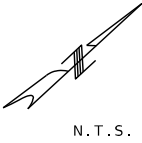
52

DETOUR AHEAD

W020+2

18

DETOUR SIGNING NOTE:
COVER ALL EXISTING
CONFLICTING ROUTE SIGNS



DETOUR LAYOUT
SHEET 2 OF 3

STATE OF MISSOURI

CHRISTOPHER NOVOSEL

NUMBER PE-2018024421

PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY.

DATE PREPARED

8/8/2025

ROUTE

D

STATE

MO

DISTRICT

SL

SHEET NO.

9

COUNTY

FRANKLIN

JOB NO.

JSLM0087

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

MODOT

105 WEST CAPITOL

JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

GBA

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CHESTERFIELD, MO 63017

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ASSOCIATES, INC.

PRO. ENGINEER 000133

ARCHITECT 000212

PRO. LAND SURVEYOR 000059

CHRISTOPHER NOVOSEL

PROFESSIONAL

ENGINEER

PE-2018024421

REV.

DETOUR

MO4+8

TO

MO4+5

D

M1+5a

M6+1

57D

DETOUR

MO4+8

TO

MO4+5

D

M1+5a

M6+1

57E

DETOUR

MO4+8

TO

WEST

M113+4

INTERSTATE 44

M113+1

M116+2R

57N

DETOUR

MO4+8

TO

WEST

M113+4

INTERSTATE 44

M113+1

M116+2

57P

DETOUR

MO4+8

SOUTH

D

M1+5a

M116+2R

57R

DETOUR

MO4+8

SOUTH

D

M1+5a

M115+2R

57S

D

CLOSED AHEAD

FOLLOW DETOUR

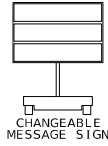
SP+58

58

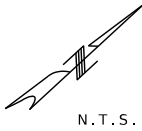
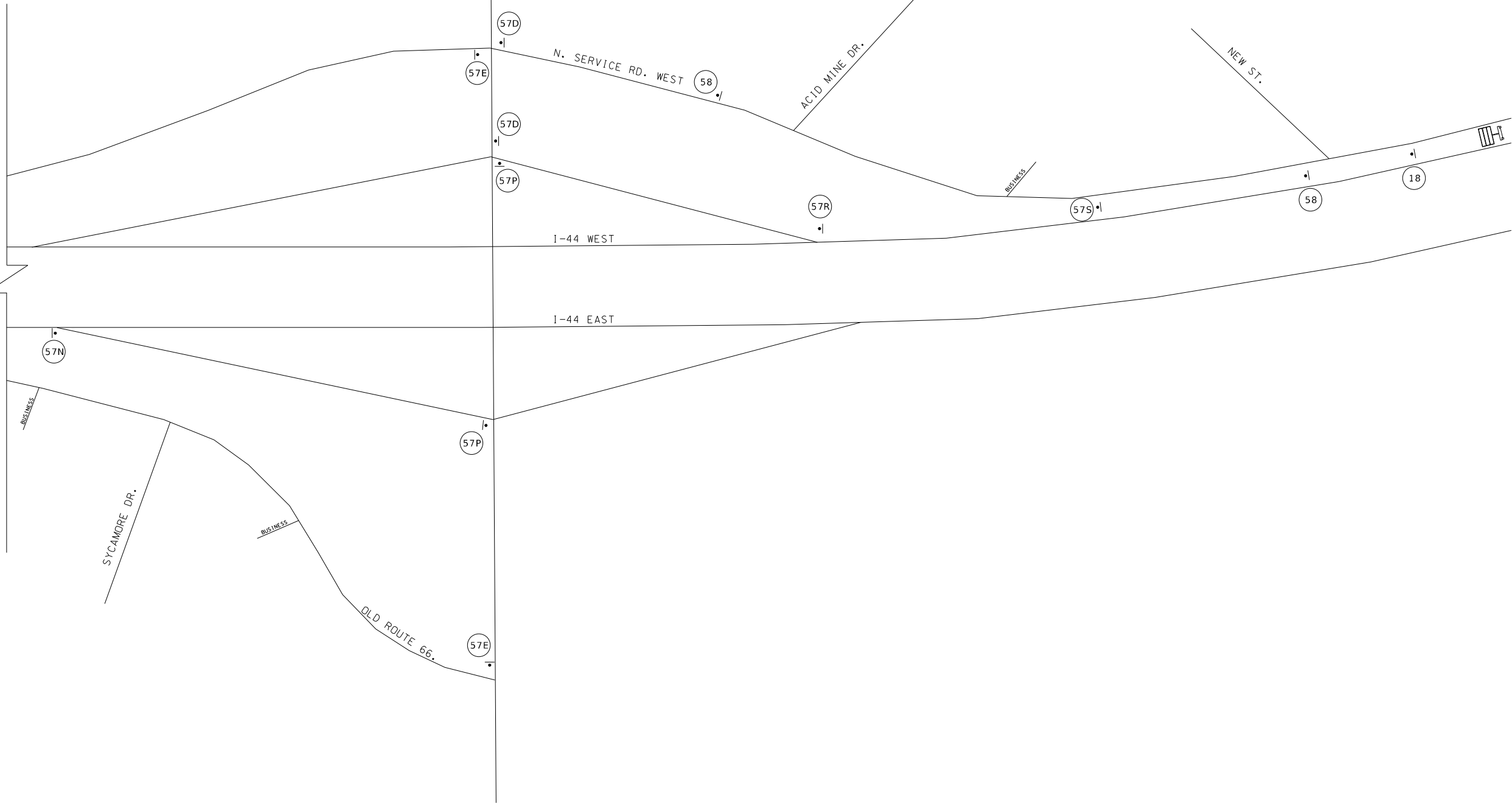
DETOUR AHEAD

W020+2

18



BREAKLINE PREVIOUS SHEET



DETOUR SIGNING NOTE:
COVER ALL EXISTING
CONFLICTING ROUTE SIGNS

STATE OF MISSOURI

CHRISTOPHER NOVOSSEL

NUMBER PE-2018024421

PROFESSIONAL ENGINEER

THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY.

DATE PREPARED 8/8/2025

ROUTE D

STATE MO

DISTRICT SL

SHEET NO. 10

COUNTY FRANKLIN

JOB NO. JSLM0087

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL ST., 300
JEFFERSON CITY, MO 65102
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CHESTERFIELD, MO 63017
314.231.0100
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ASSOCIATES, INC.
PRO. ENGINEER 000133
ARCHITECT 000212
PRO. LAND SURVEYOR 000059

CHRISTOPHER NOVOSSEL
PROFESSIONAL
ENGINEER
PE-2018024421

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DETOUR LAYOUT
SHEET 3 OF 3

c:\bms\gba-pw-01\chris novosel\dms26734\010_TC_DT03_JSLM0087_I100.dgn 10:40:55 AM 8/8/2025

6" SOLID WHITE
EDGE LINE = 350'

6" SOLID WHITE
EDGE LINE = 350'

LEFT TURN ARROW

TEMPORARY 4" SOLID
WHITE LINE EDGE LINE = 406'

TYPE 3 MOVABLE BARRICADE

LEFT TURN ARROW

CHANNELIZERS

4" TEMPORARY DOUBLE
SOLID YELLOW LINE = 700'

NOTES

THE COMMISSION HAS A
TRAFFIC CONTROL ON
SITE FOR THE PURPOSES
ROUTE D TO ONE LANE
IN EACH DIRECTION OF
OVER 1-44 AS SHOWN.
TRAFFIC CONTROL MAY
PLACE THROUGHOUT THE
THE PROJECT. FOLLOW
NOTICE TO PROCEED.
THE CONTRACTOR'S RES
TO MONITOR, MAINTAIN
AND SUPPLEMENT THE
ZONE AS NECESSARY TO
CONTRACTOR OPERATION
REQUIRED TO SAFELY C
PROJECT.

MONITORING, ADJUSTING
MAINTAINING COMMISS
TRAFFIC CONTROL DEVS
PAID FOR AT THE CON
PRICE FOR ITEM 616-
MAINTENANCE OF COMM
TRAFFIC CONTROL DEV
LUMP SUM.

THE CONTRACTOR WILL
COMMISSION-OWNED TR
DEVICES WHEN NO LONG
FOR THE PROJECT.

THE COMMISSION HAS ESTABLISHED TRAFFIC CONTROL ON THE PROJECT SITE FOR THE PURPOSE OF REDUCING ROUTE D TO ONE LANE OF TRAFFIC IN EACH DIRECTION ON THE BRIDGE OVER I-44 AS SHOWN. THIS TRAFFIC CONTROL MAY REMAIN IN PLACE THROUGHOUT THE DURATION OF THE PROJECT. FOLLOWING THE NOTICE TO PROCEED, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN, ADJUST, AND SUPPLEMENT THE EXISTING WORK ZONE AS NECESSARY TO FACILITATE CONTRACTOR OPERATIONS AS REQUIRED TO SAFELY COMPLETE THE PROJECT.

THE CONTRACTOR WILL REMOVE
COMMISSION-OWNED TRAFFIC CONTROL
DEVICES WHEN NO LONGER REQUIRED
FOR THE PROJECT.

72'

31'

27'

16°

38°

1, 2, 3, 4, 5

DETAIL A

EDGE OF BRIDGE

6" SOLID WHITE
EDGE LINE = 350'

12'

12'

6'

6" SOLID WHITE
EDGE LINE = 350'

12'

LEFT TURN ARROW

4" SOLID DOUBLE
YELLOW LINE = 716' TOTAL
SEE DETAIL A

LEFT TURN ARROW

6" SOLID WHITE
EDGE LINE = 192'

6" SOLID WHITE
EDGE LINE = 192'

LEFT TURN ARROW

LEFT TURN ARROW

6' 12' 12'

N.T.S.



THIS SHEET HAS BEEN
SIGNED, SEALED, AND DATED
ELECTRONICALLY.

DATE PREPARED
8/8/2025

ROUTE	STATE
D	MO

DISTRICT	SHEET NO.
SL	11

COUNTY
FRANKLIN

JOB NO.
JSLM0087

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

[illegible]MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITAL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

GBA

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O. LAND SURVEYOR 000059

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PROFESSIONAL
ENGINEER
PE-2018024421

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REV.

