

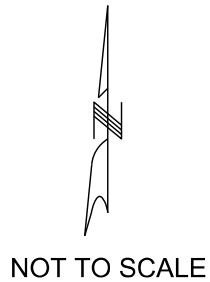
I-270 EASTBOUND	I-270 WESTBOUND
A.A.D.T. - 2022 = 38,700	A.A.D.T. - 2022 = 38,200
A.A.D.T. - 2046 = 42,800	A.A.D.T. - 2046 = 42,300
D.H.V. = 5%	D.H.V. = 3%
T = 18%	T = 18%
V = 60 M.P.H.	V = 60 M.P.H.
FUNCTIONAL CLASSIFICATION - INTERSTATE HIGHWAY	FUNCTIONAL CLASSIFICATION - INTERSTATE HIGHWAY

CONVENTIONAL SYMBOLS (USED IN PLANS)

NOTE: DASHED OR OPEN SYMBOLS INDICATE EXISTING FEATURES

KEY MAP

KEY MAP
LOCATION OF ST. LOUIS COUNTY



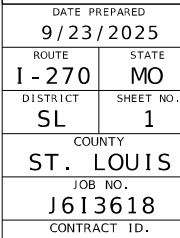
THE EXISTENCE AND APPROXIMATE LOCATION OF FACILITY FACILITIES KNOWN TO EXIST, AS SHOWN ON THE PLANS, ARE BASED ON THE BEST INFORMATION AVAILABLE. TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE. RELIANCE UPON THIS INFORMATION IS DONE AT THE RISK AND PERIL OF THE USER, AND THE COMMISSION SHALL NOT BE LIABLE FOR ANY DAMAGES THAT MAY ARISE FROM ANY ERROR IN THE INFORMATION. IT IS, THEREFORE, THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE, LOCATION AND STATUS OF ANY FACILITY. SUCH VERIFICATION INCLUDES DIRECT CONTACT WITH THE LISTED UTILITIES.

DESCRIPTION	SHEET NUMBER
TITLE SHEET — — — — —	1
QUANTITIES (QU) (2 SHEETS) — — — — —	2
PLAN (PL) — — — — —	3
COORDINATE POINTS (CP) — — — — —	4
RIGHT OF WAY (RW) — — — — —	5
TRAFFIC CONTROL (TC) — — — — —	6 - 8

LENGTH OF PROJECT

TOTAL CORRECTIONS	NONE
NET LENGTH OF PROJECT	985 FEET
STATE LENGTH	0.187 MILES
FOR INFORMATION ONLY ESTIMATED DISTURBED ACRES	0 ACRES

MSD PROJECT NO. 25MSD-00264
MSD BASE MAP NO.: 10G1



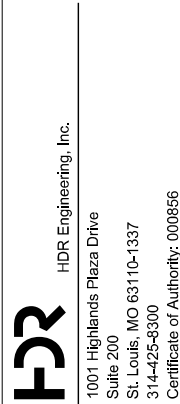
PROJECT NO.
BRIDGE NO.

DATE	DESCRIPTION



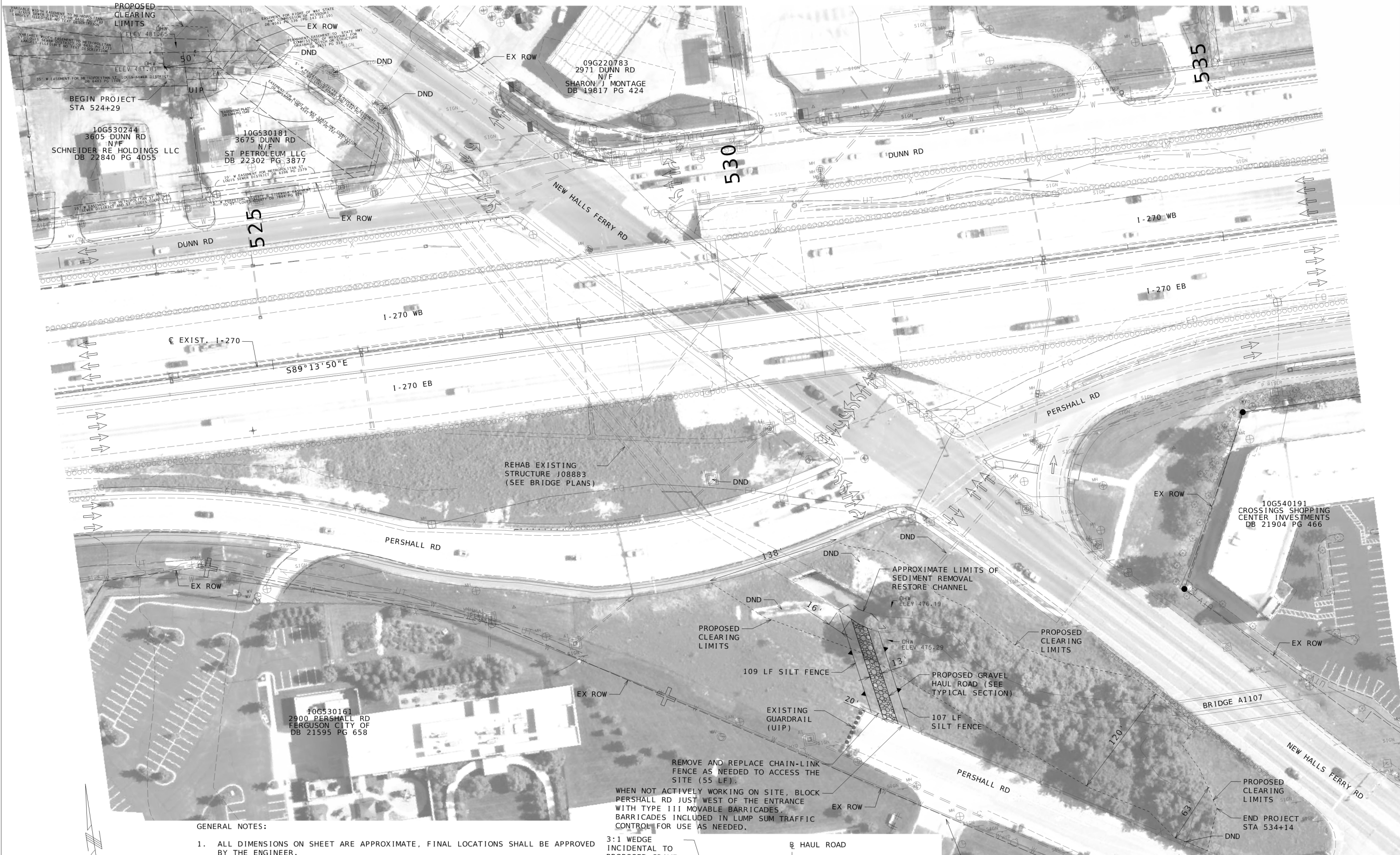
MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT, 899-7215 6526A



IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

SIGN	SIZE	AREA	QTY	TOTAL AREA	RELOC EACH	TOTAL RELOC	SIGN	DESCRIPTION	SIGN	SIZE	AREA	QTY	TOTAL	RELOC EACH	TOTAL RELOC	SIGN	DESCRIPTION	ITEM NUMBER	TOTAL QTY	DESCRIPTION
IN.	SQ. FT.	EACH	SQ. FT.	EACH	SQ. FT.	NO.			IN.	SQ. FT.	EACH	SQ. FT.	EACH	SQ. FT.	NO.					
WARNING SIGNS									GUIDE SIGNS											
WO1-1L	48X48	16.00						TURN (SYMBOL LEFT)	E05-1	36X48	12.00						GORE EXIT	6122008		IMPACT ATTENUATOR 40 MPH (SAND BARRELS)
WO1-1R	48X48	16.00						TURN (SYMBOL RIGHT)	E05-2	48X36	12.00						EXIT OPEN	6122009		IMPACT ATTENUATOR 45 MPH (SAND BARRELS)
WO1-2L	48X48	16.00						CURVE (SYMBOL LEFT)	E05-2a	48X36	12.00						EXIT CLOSED	6122010		IMPACT ATTENUATOR 50 MPH (SAND BARRELS)
WO1-2R	48X48	16.00						CURVE (SYMBOL RIGHT)	GO20-1	60X24	10.00						ROAD WORK NEXT XX MILES	6122012		IMPACT ATTENUATOR 55 MPH (SAND BARRELS)
WO1-3L	48X48	16.00						REVERSE TURN (SYMBOL LEFT)	GO20-2	48X24	8.00						END ROAD WORK	6122014		IMPACT ATTENUATOR 60 MPH (SAND BARRELS)
WO1-3R	48X48	16.00						REVERSE TURN (SYMBOL RIGHT)	GO20-2	48X24	8.00						END ROAD WORK	6122017		IMPACT ATTENUATOR 65 MPH (SAND BARRELS)
WO1-4L	48X48	16.00						REVERSE CURVE (SYMBOL LEFT)	GO20-4	36X18	4.50						PILOT CAR FOLLOW ME	6122019		IMPACT ATTENUATOR 70 MPH (SAND BARRELS)
WO1-4R	48X48	16.00						REVERSE CURVE (SYMBOL RIGHT)	GO20-4a	42X30	8.75						PILOT CAR IN USE WAIT & FOLLOW	6122020		REPLACEMENT SAND BARREL
WO1-4bL	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL LEFT)	GO20-4a	18X12	1.50						PILOT CAR IN USE WAIT & FOLLOW	6122030		IMPACT ATTENUATOR (RELOCATION)
WO1-4bR	48X48	16.00						DOUBLE ARROW REVERSE CURVE (SYMBOL RIGHT)	GO20-5aP	36X24	6.00						WORK ZONE (PLAQUE)	6122040		WORK ZONE CRASH CUSHION (NARROW)
WO1-4cL	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL LEFT)	MO4-8a	24X18	3.00						END DETOUR	6122041		WORK ZONE CRASH CUSHION (RELOCATION)
WO1-4cR	48X48	16.00						TRIPLE ARROW REVERSE CURVE (SYMBOL RIGHT)	MO4-9L	48X36	12.00						DETOUR (LEFT)	6123001	***	TRUCK MOUNTED ATTENUATOR (TMA)
WO1-6	60X30	12.50						HORIZONTAL ARROW (SYMBOL)	MO4-9R	48X36	12.00						DETOUR (RIGHT)	6161012		BUOYS (BOATS KEEP OUT)
WO1-6a	72X36	18.00						HORIZ. ARROW (SYMBOL ON PERMANENT BARRICADE)	MO4-9P	48X12	4.00						STREET NAME (PLAQUE)	6161013		BUOYS (NO WAKE)
WO1-7	60X30	12.50						DOUBLE HEAD HORIZONTAL ARROW (SYMBOL)	MO4-10L	48X18	6.00						DETOUR ARROW (LEFT)	6161014		SPECIAL SIGN ASSEMBLY (BOATS KEEP OUT)
WO1-7a	72X36	18.00						DOUBLE HEAD HORIZ. ARROW (SYMBOL ON PERM. BARR.)	MO4-10R	48X18	6.00						DETOUR ARROW (RIGHT)	6161020		CHANNELIZER (DRUM-LIKE)
									REGULATORY SIGNS									6161022		CHANNELIZER (CONE)
WO1-8	18X24	3.00						CHEVRON (SYMBOL)	R1-1	48X48	13.25						STOP	6161025	30	CHANNELIZER (TRIM-LINE)
WO1-8a	30X36	7.50						CHEVRON (SYMBOL FOR DIVIDED HIGHWAYS)	R1-2	48TRI	6.93						YIELD	6161026		CHANNELIZER (VERTICAL PANEL)
WO3-1	48X48	16.00						STOP AHEAD (SYMBOL)	R1-2a	36X36	9.00						TO ONCOMING TRAFFIC (PLAQUE)	6161030	4	TYPE 3 MOVEABLE BARRICADE
WO3-2	48X48	16.00						YIELD AHEAD (SYMBOL)	R1-3P	30X12	2.50						ALL WAY (PLAQUE)	6161033		DIRECTION INDICATOR BARRICADE
WO3-3	48X48	16.00						SIGNAL AHEAD (SYMBOL)	R2-1	36X48	12.00						SPEED LIMIT XX	6161040		FLASHING ARROW PANEL
WO3-4	48X48	16.00						BE PREPARED TO STOP	R3-1</											



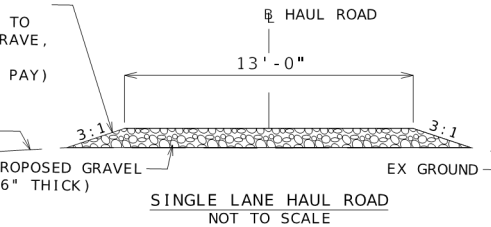
GENERAL NOTES:

1. ALL DIMENSIONS ON SHEET ARE APPROXIMATE, FINAL LOCATIONS SHALL BE APPROVED BY THE ENGINEER.
2. ANY WORK INDICATED ON THE PLANS THAT EXTENDS BEYOND THE PROJECT LIMITS IS CONSIDERED INCIDENTAL TO AND A PART OF THE CONSTRUCTION OF THIS PROJECT.
3. ALL STATIONS AND OFFSETS ARE FROM C EXIST. I-270 UNLESS NOTED.
4. ANY GRADING WORK NECESSARY FOR INSTALLATION OF HAUL ROAD WILL BE ACCOUNTED FOR WITH THE PAY ITEM FOR LINEAR GRADING.
5. ANY DAMAGE TO EXISTING PAVEMENT OR SHOULDER ON PERSHALL RD SHALL BE REMOVED AND REPLACED AS DIRECTED BY ENGINEER (INCIDENTAL).
6. THE WORK PROPOSED ON THE MSD ASSET IS CONSIDERED MAINTENANCE WORK DOES NOT TRIGGER WATER QUALITY REQUIREMENTS UNDER THE MSD REGIONAL MS4 PERMIT. MODOT'S ASSETS AND WORK IN THE ROW ARE COVERED UNDER MODOT'S TS4 PERMIT.

WHEN NOT ACTIVELY WORKING ON SITE, BLOCK PERSHALL RD JUST WEST OF THE ENTRANCE WITH TYPE III MOVABLE BARRICADES. BARRICADES INCLUDED IN LUMP SUM TRAFFIC CONTROL FOR USE AS NEEDED.

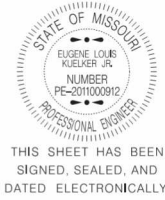
3:1 WEDGE INCIDENTAL TO PROPOSED GRAVE, 6" THICK (NO DIRECT PAY) (TYPICAL)

EX GROUND



SINGLE LANE HAUL ROAD
NOT TO SCALE

I-270 PLAN
SHEET 1 OF 1



DATE PREPARED 9/23/2025	
ROUTE I-270	STATE MO
DISTRICT SL	SHEET NO. 3
COUNTY ST. LOUIS	
JOB NO. J613618	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION



HDR Engineering, Inc.

1001 Highlands Plaza Drive
Suite 200
St. Louis, MO 63110-1337
314-425-6300
Certificate of Authority: 000856

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

ALL PROJECT COORDINATES HAVE BEEN PROJECTED FROM THE MISSOURI STATE PLANE COORDINATE (SPC) SYSTEM OF 1983 USING AN AVERAGE PROJECT PROJECTION (GRID TO GROUND) FACTOR. TO GET BACK TO STATE PLANE COORDINATES, MULTIPLY THE PROJECT COORDINATES BY THE AVERAGE GRID FACTOR AS SHOWN IN THE "REFERENCE CONTROL INFORMATION" PORTION OF THIS TABLE.

PROJECT COORDINATE INFORMATION

COORDINATE SYSTEM	Stateplane MO East FIPS 2401
HORIZONTAL DATUM	NAD 83
VERTICAL DATUM	NAVD 88
GEOID MODEL	Geoid 12B
ELEVATIONS DETERMINED BY	
PROJECT PROJECTION FACTOR	1.0000855

REFERENCE CONTROL INFORMATION

COORDINATE SYSTEM	Missouri Coordinate System of 1983
CONTROL STATION	MOEF
DESIGNATION	MODOT OFALLON CORS ARP
CORS ID	MOOF
PID	DM4688
LATITUDE	38°45'14.06179
LONGITUDE	90°41'44.78025
NORTHING (M)	324131.1530
EASTING (M)	232983.4880
ZONE	2401
PROJECT AVERAGE GRID FACTOR	0.99991451

EXAMPLE OF PROJECT COORDINATE TO S.P.C.

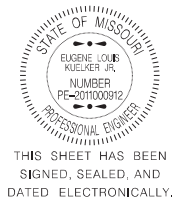
PROJECT NORTHING X AVERAGE GRID FACTOR
= STATE PLANE NORTHING
PROJECT EASTING X AVERAGE GRID FACTOR
= STATE PLANE EASTING
EXAMPLE: CONTROL POINT #3
N 1083739.801 X 0.99991221 = N 1083644.659
E 713463.461 X 0.99991221 = E 713400.826

LINEAR UNIT CONVERSION

1 METER = 3.280833333 US SURVEY FEET (USFT)

COORDINATE POINT LISTING

SHEET NO	STATION	LOCATION	OFFSET (USFT)	MODIFIED STATE PLANE (GROUND)			DESCRIPTION	POINT ID
				NORTHING (US SURVEY FT)	EASTING (US SURVEY FT)	ELEVATION (US SURVEY FT)		
PROJECT CONTROL POINTS								
ALIGNMENTS								
5	520+00.00	EXIST. I-270	0.00'	1069775.1585	885729.0133		BEGINNING OF EXIST. CENTERLINE	
3	522+70.00	EXIST. I-270	0.00'	1069771.5326	885998.9889		POINT	
3	536+20.00	EXIST. I-270	0.00'	1069753.4030	887348.8672		END OF EXIST. CENTERLINE	




DATE PREPARED 9/23/2025	
ROUTE I-270	STATE MO
DISTRICT SL	SHEET NO. 4
COUNTY ST. LOUIS	
JOB NO. J613618	
CONTRACT ID.	
PROJECT NO.	
BRIDGE NO.	

DESCRIPTION	DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION



105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

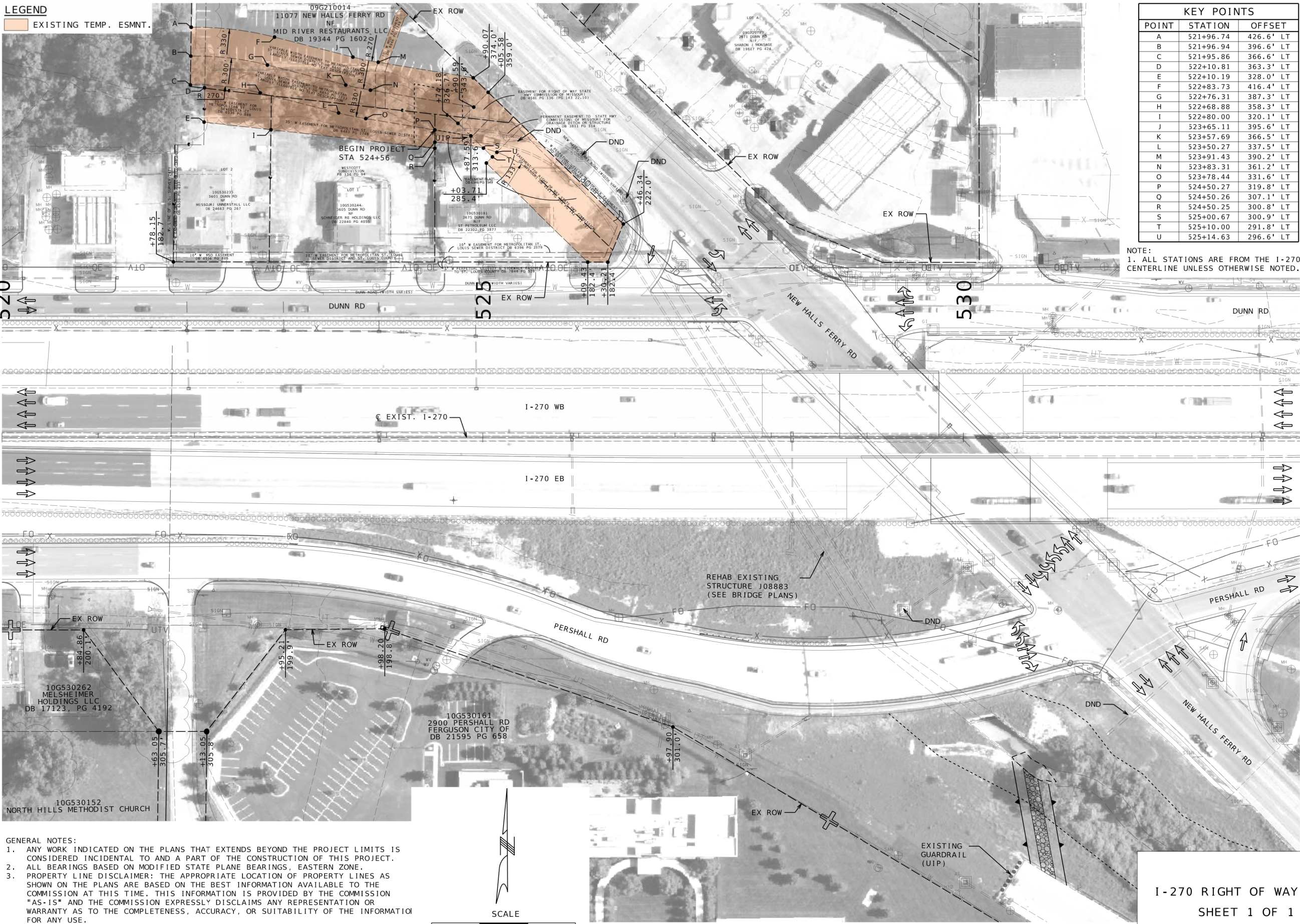
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314-425-9300
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COORDINATE POINT SHEET
SHEET 1 OF 1

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

LEGEND

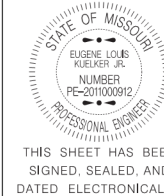
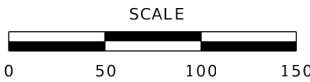
EXISTING TEMP. ESMNT.



KEY POINTS		
POINT	STATION	OFFSET
A	521+96.74	426.6' LT
B	521+96.94	396.6' LT
C	521+95.86	366.6' LT
D	522+10.81	363.3' LT
E	522+10.19	328.0' LT
F	522+83.73	416.4' LT
G	522+76.31	387.3' LT
H	522+68.88	358.3' LT
I	522+80.00	320.1' LT
J	523+65.11	395.6' LT
K	523+57.69	366.5' LT
L	523+50.27	337.5' LT
M	523+91.43	390.2' LT
N	523+83.31	361.2' LT
O	523+78.44	331.6' LT
P	524+50.27	319.8' LT
Q	524+50.26	307.1' LT
R	524+50.25	300.8' LT
S	525+00.67	300.9' LT
T	525+10.00	291.8' LT
U	525+14.63	296.6' LT

NOTE:
1. ALL STATIONS ARE FROM THE I-270 CENTERLINE UNLESS OTHERWISE NOTED.

- GENERAL NOTES:
- ANY WORK INDICATED ON THE PLANS THAT EXTENDS BEYOND THE PROJECT LIMITS IS CONSIDERED INCIDENTAL TO AND A PART OF THE CONSTRUCTION OF THIS PROJECT.
 - ALL BEARINGS BASED ON MODIFIED STATE PLANE BEARINGS, EASTERN ZONE.
 - PROPERTY LINE DISCLAIMER: THE APPROPRIATE LOCATION OF PROPERTY LINES AS SHOWN ON THE PLANS ARE BASED ON THE BEST INFORMATION AVAILABLE TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE.



THIS SHEET HAS BEEN SIGNED, SEALED, AND DATED ELECTRONICALLY.

DATE PREPARED
9/23/2025

ROUTE
I-270

STATE
MO

DISTRICT
SL

SHEET NO.
5

COUNTY
ST. LOUIS

JOB NO.
J613618

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

MoDOT

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

HDR Engineering, Inc.

1001 Highlands Plaza Drive
Suite 200
St. Louis, MO 63110-1337
314-425-8300
Certificate of Authority: 000856

I-270 RIGHT OF WAY PLAN
SHEET 1 OF 1

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.

NOTES :

1. EXISTING GUIDE, WARNING, AND REGULATORY SIGNS WITHIN THE WORK ZONE SHALL NOT CONFLICT WITH THE CONSTRUCTION SIGNING (SUCH AS SPEED LIMIT, BRIDGE CLEARANCE, ETC.) AND SHALL BE COVERED, REMOVED, OR RELOCATED WITH NO DIRECT PAYMENT. CONSTRUCTION SIGNS SHALL NOT OBSTRUCT EXISTING SIGNS REMAINING IN PLACE.
2. ALL SPACING AND DISTANCES ARE APPROXIMATE. THEY MAY BE ADJUSTED BY THE ENGINEER TO FIT FIELD CONDITIONS.
3. SEE STANDARD PLAN 616.10BE FOR TEMPORARY TRAFFIC CONTROL DEVICES DETAILS.
4. SEE STANDARD PLAN 620.10G FOR TEMPORARY PAVEMENT MARKING DETAILS.
5. CONSTRUCTION SIGNS NOTED AS POST-MOUNTED (POST) SHALL BE MOUNTED PER STANDARD PLAN 616.10BE.
6. THE SPEED LIMIT DURING ACTIVE CONSTRUCTION SHALL REMAIN 45 M.P.H. UNLESS OTHERWISE NOTED IN THE TRAFFIC CONTROL PLAN.
7. CONFLICTING STRIPING SHALL BE REMOVED PRIOR TO GUIDING TRAFFIC THROUGH WORK ZONES. ANY STRIPING WHICH IS DAMAGED DURING A DAYS WORK SHALL BE RE-STRIPED NO LATER THAN THE END OF THE SAME DAY.
8. ALL WARNING SIGNS SHALL HAVE A FLUORESCENT SHEETING.
9. WORDING FOR CHANGEABLE MESSAGE SIGNS SHALL BE AS APPROVED OR DIRECTED BY THE ENGINEER.
10. EXACT LOCATIONS FOR CHANGEABLE MESSAGE SIGNS SHALL BE AS DIRECTED BY THE ENGINEER.
11. MISSOURI ONE CALL (800 DIG RITE) SHALL BE CONTACTED FOR LOCATES (800-344-7483 OR 811) BEFORE BEGINNING ANY EXCAVATION OR BORING ACTIVITIES.
12. UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES SHOWN ON PLANS HAVE BEEN DRAFTED FROM AVAILABLE SURVEYS AND RECORDS; THE COMMISSION DOES NOT WARRANT THE LOCATIONS OF THESE FACILITIES AS PRECISE. IT IS POSSIBLE THERE MAY BE OTHERS, THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN OR SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXISTENCE AND PRECISE LOCATION OF ALL FACILITIES AND TO AVOID DAMAGE TO THEM. SEE THE JOB SPECIAL PROVISIONS FOR A LIST OF UTILITY COMPANIES ON OR WITHIN THE VICINITY OF THE PROJECT LIMITS.

TEMPORARY TRAFFIC CONTROL TYPICAL APPLICATIONS NOTES:

1. REMOVE AND/OR MODIFY ANY EXISTING PAVEMENT MARKINGS AS NEEDED.
2. ALTERNATIVE TEMPORARY TRAFFIC CONTROL SET-UPS SHOULD BE CONSIDERED WHEN REDUCTION IN CAPACITY CANNOT BE TOLERATED. SET-UPS MUST BE SUBMITTED TO ENGINEER FOR APPROVAL PRIOR TO IMPLEMENTATION.
3. UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY PROVIDE ADDITIONAL PROTECTIVE TRUCKS EQUIPPED WITH PROPER WARNING DEVICES.

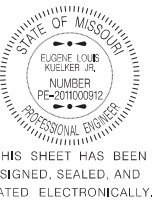
SUGGESTED SEQUENCE OF CONSTRUCTION:

PRIOR TO PERFORMING CULVERT WORK, THE FOLLOWING ACTIVITIES WILL NEED TO OCCUR. TYPICAL APPLICATIONS AS SHOWN ON "TRAFFIC CONTROL PLAN TYPICAL APPLICATIONS SHEET 1 OF 2" WILL BE USED TO PERFORM THE WORK FOR TRAFFIC CONTROL:

1. INSTALL ADVANCE SIGNING.
2. PLACE BARRICADES USING THE TYPICAL APPLICATIONS. COMPLETE THE TREE CLEARING AND CULVERT CLEANOUT.

TRAFFIC CONTROL LEGEND

- | | |
|---|---------------------------------|
|  | SIGN (SINGLE SIDED) |
|  | CHANNELIZER |
|  | DIRECTIONAL INDICATOR BARRICADE |
|  | WORK AREA |
|  | TRAFFIC USAGE |



DATE PREPARED
9/23/2025

ROUTE	STATE
- 270	MO

DISTRICT	SHEET NO.
SL	6

COUNTY
ST. LOUIS


JOB NO.
J6I3618

CONTRACT ID.

PROJECT NO.

BRIDGE NO. _____

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MoDOT

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

COMMISION

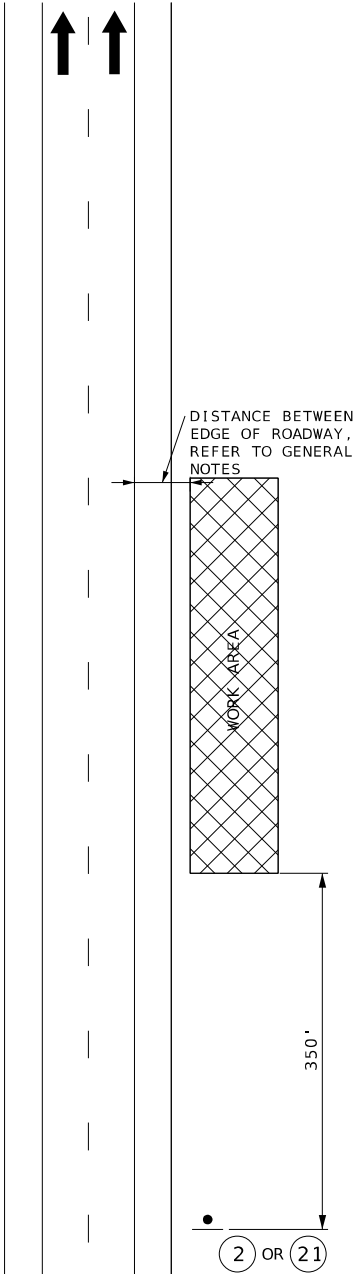
FOR

HDR Engineering, Inc.
1001 Highlands Plaza Drive
Suite 200
St. Louis, MO 63110-1337
314-425-8300
Certificate of Authority: 000856

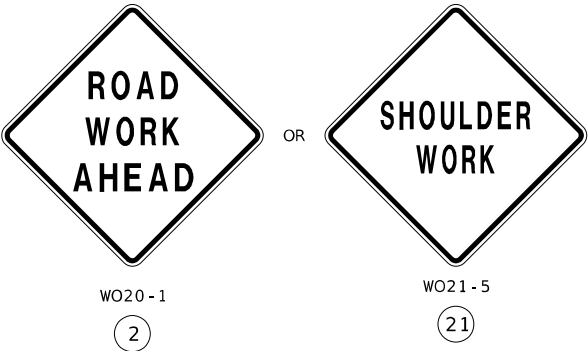
TRAFFIC CONTROL
GENERAL NOTES AND LEGEND
SHEET 1 OF 1

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.






STATIONARY WORK BEYOND THE SHOULDER



- GENERAL NOTES:
- ADVANCE WARNING SIGNS MAY BE ELIMINATED IF AT LEAST ONE OF THE FOLLOWING CONDITIONS ARE MET:
1. WORK SPACE IS 15 FEET OR MORE FROM THE EDGE OF ANY ROADWAY.
 2. WORK SPACE IS BEHIND THE CURB.
 3. WORK SPACE IS BEHIND BARRIER OR GUARDRAIL.
 4. HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS ARE ACTIVATED.




TRAFFIC CONTROL LEGEND

-  SIGN (SINGLE SIDED)
-  CHANNELIZER
-  DIRECTIONAL INDICATOR BARRICADE
-  WORK AREA
-  TRAFFIC USAGE

NOT TO SCALE

TRAFFIC CONTROL PLAN
TYPICAL APPLICATIONS
SHEET 1 OF 2



THIS SHEET HAS BEEN
SIGNED, SEALED, AND
DATED ELECTRONICALLY.

DATE PREPARED
9/23/2025

ROUTE I - 270	STATE MO
DISTRICT SL	SHEET NO. 7

COUNTY
ST. LOUIS

JOB NO.
J613618


CONTRACT ID.

PROJECT NO.


BRIDGE NO.

DATE	DESCRIPTION

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

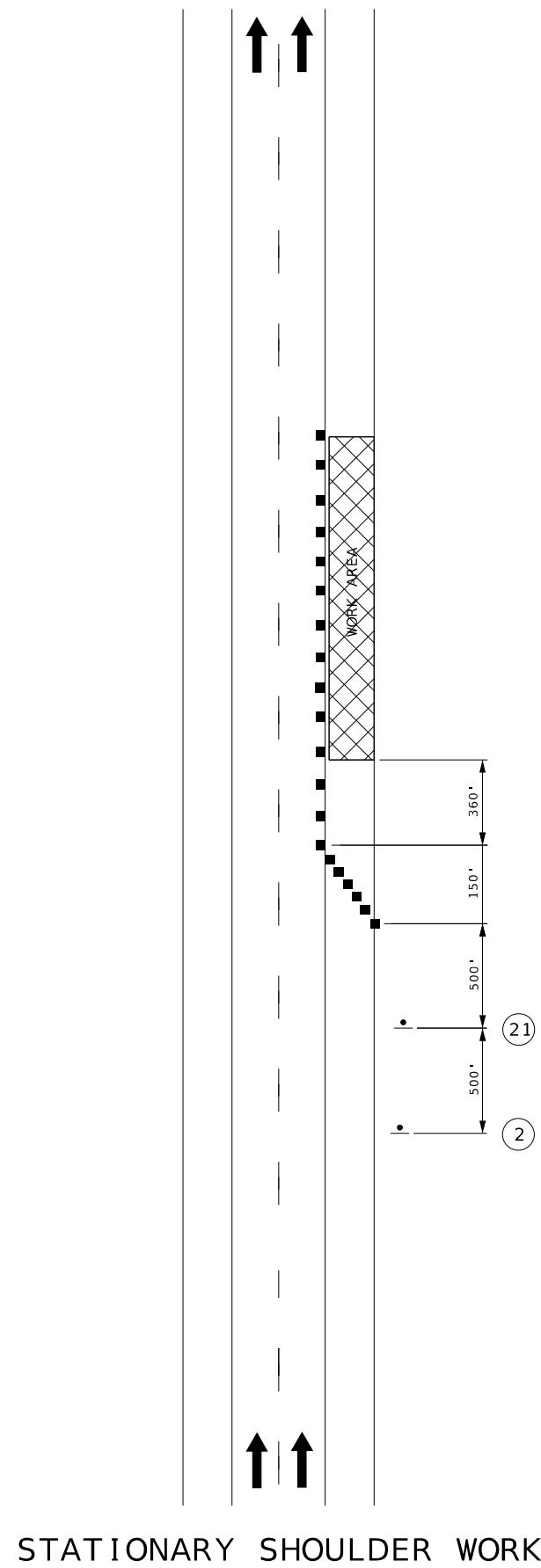


105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)







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St. Louis, MO 63110-1337
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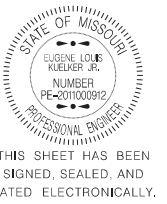


TRAFFIC CONTROL LEGEND

- | | |
|---|---------------------------------|
|  | SIGN (SINGLE SIDED) |
|  | CHANNELIZER |
|  | DIRECTIONAL INDICATOR BARRICADE |
|  | WORK AREA |
|  | TRAFFIC USAGE |

NOT TO SCALE

TRAFFIC CONTROL PLAN
TYPICAL APPLICATIONS
SHEET 2 OF 2

DATE PREPARED
09-08-2007

ROUTE	STATE
- 270	MO

DISTRICT	SHEET NO.
SL	8

COUNTY
ST. LOUIS

JOB NO.
J6I3618


CONTRACT ID.

PROJECT NO.

BRIDGE NO.

[illegible]

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

The logo for the Missouri Department of Transportation (MoDOT) features the word "MoDOT" in a bold, sans-serif font. To the right of the text is a stylized graphic of a road or highway with three curved lines representing lanes, leading towards a horizon line.

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

HDR
HDR Engineering, Inc.
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St. Louis, MO 63110-1337
314-425-9300
Certificate of Authority: 000856

[illegible]

U.I.P. AND REHABILITATE EXISTING 2(10'x15') REINFORCED CONCRETE BOX CULVERT

Estimated Quantities
(MoDOT Sections: 1931 and 1961)

Item	Unit	Total
(*) Dewatering	Lump Sum	1
(*) Culvert Cleanout	Lump Sum	1
Total Surface Hydro Demolition	Sq. Yd.	296
Supplementary Wearing Surface Material	Cu. Yd.	15
Latex Modified Concrete Wearing Surface	Sq. Yd.	296
Class B-1 Concrete (Culverts-Bridge)	Cu. Yd.	10.3
Substructure Repair (Unformed)	Sq. Ft.	576
Superstructure Repair (Unformed)	Sq. Ft.	455
Epoxy Pressure Injecting	Lin. Ft.	40
Clean Out and Seal Transverse Joints	Lin. Ft.	806
Reinforcing Steel (Culverts-Bridge)	Lbs.	1460

Estimated Quantities(MSD Extension, 1968)

Item	Unit	Total
Substructure Repair (Unformed)	Sq. Ft.	262
Superstructure Repair (Unformed)	Sq. Ft.	207
Epoxy Pressure Injecting	Lin. Ft.	20

Estimated Quantities Notes:

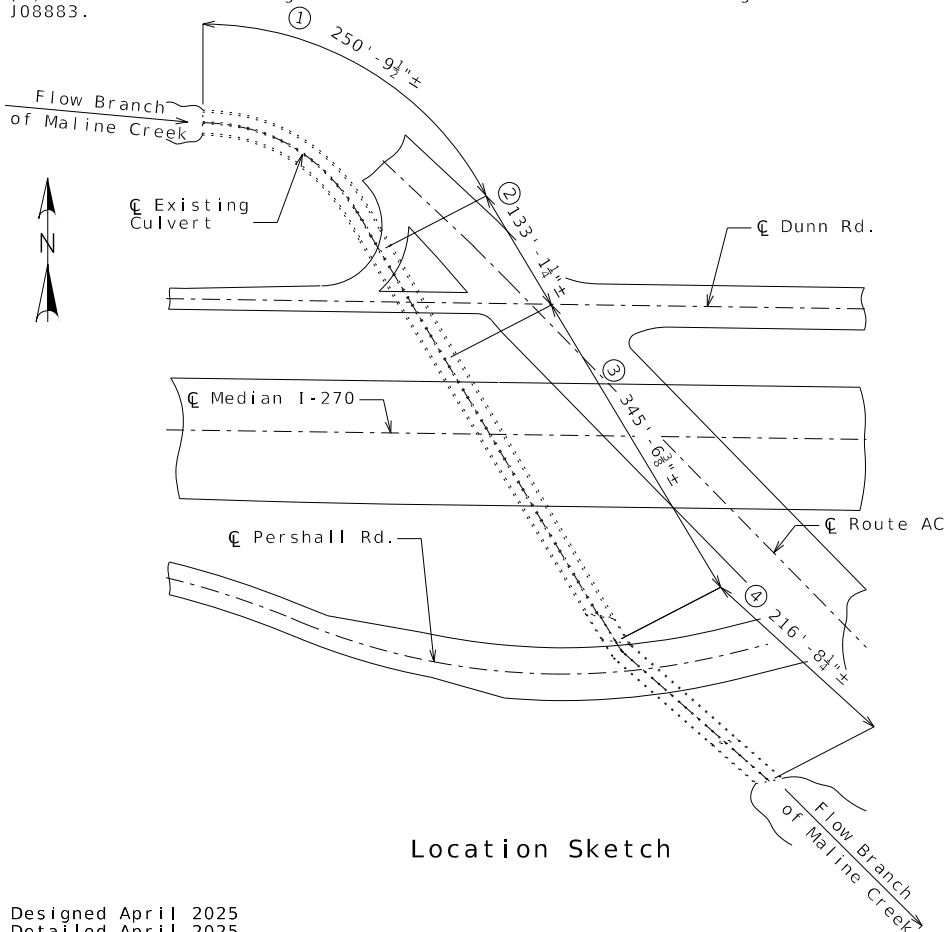
Estimated quantities for MSD Culvert Extension (1968), shall be quantified and paid for separately from the work in the remaining sections of culvert.

Prior to performing Substructure Repair (Unformed), Superstructure Repair (Unformed) and Epoxy Pressure Injecting items the Contractor and Engineer shall agree on the limits for each repair area.

Estimated quantities are based on a limited site visit, a full hands on inspection of the culvert was not performed.

The 2025 edition of the Missouri Department of Transportation's "Missouri Standard Specifications for Highway Construction" and supplemental Interim Specification Revisions, and the job special provisions shall govern for all repairs within the 1968 MSD culvert extension segment.

(*) Limits of Dewatering and Culvert Cleanout extend full length of culvert J08883.



Location Sketch

General Notes:

Design Specifications:
2002 AASHTO LFD (17th Ed.) Standard Specifications (New Construction)
Culvert Condition Rating = 4

Design Loading:

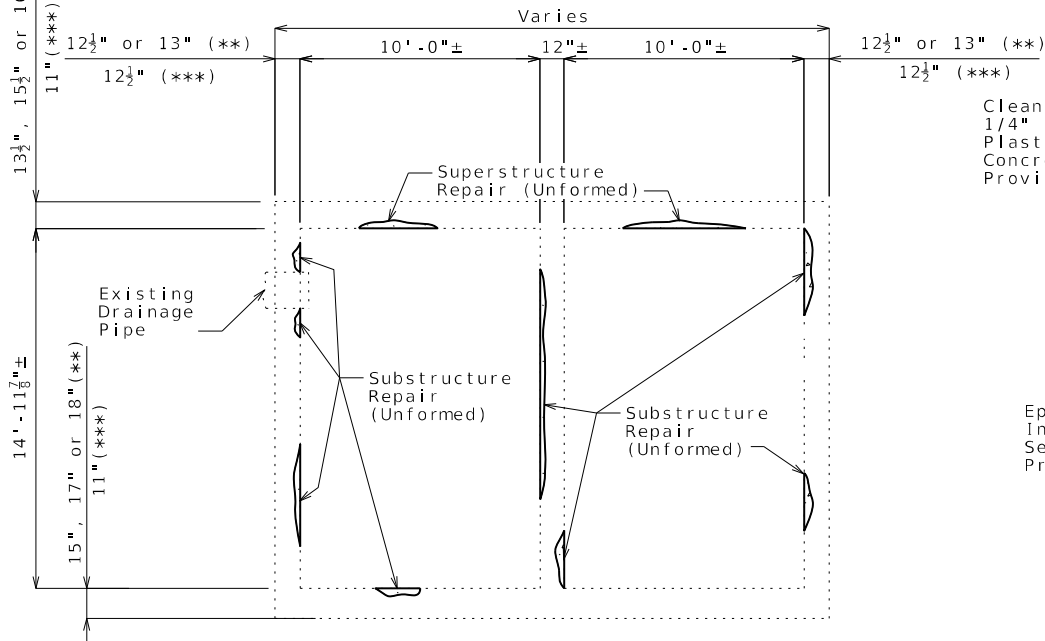
Bridge No. J0888 (1931 Original Culvert): 1931 Specifications
Bridge No. J08881 (1961 Extension): H20-S16-44
(1968 MSD Extension): H20-44, Earth = 120 lb/cf, Equivalent Fluid Pressure = 42 lb/cf
Bridge No. J08882 (2021 Extension): HL-93 minus lane load, Earth = 120 lb/cf, Equivalent Fluid Pressure = 30 lb/cf (min.) 60 lb/cf (max.)

Design Unit Stresses:

Class B-1 Concrete (Box Culvert) f'c = 4,000 psi
Reinforcing Steel (ASTM A706 Grade 60) fy = 60,000 psi

Reinforcing Steel:

Minimum clearance to reinforcing steel shall be 1 1/2", unless otherwise shown.



TYPICAL SECTION OF BOX CULVERT SHOWING REPAIRS TO (1961) EXTENSION AND (1968) MSD EXTENSION

Location Sketch Key:

- ① - Existing MSD Culvert Extension (Constructed in 1968)
- ② - Existing Original Culvert (Constructed in 1931) (Bridge No. J0888)
- ③ - Existing Culvert Extension (Constructed in 1961) (Bridge No. J08881)
- ④ - Existing Culvert Extension (Constructed in 2021) (Bridge No. J08882) Shown for information only, no repairs are anticipated in this section of culvert.

Notes:

Unformed concrete repair areas, crack lengths and locations, and transverse joint details shown are for informational purposes only. Identifying, locating and determining the limits of these repairs will be the responsibility of the Contractor and Engineer. Prior to any unformed concrete repair, crack repair, or joint repair work being performed the Contractor and Engineer shall agree to the repair location and limits proposed.

Epoxy pressure injecting shall be performed in accordance with Special Provisions for Epoxy Pressure Injecting and Section 704.

The cost of all labor, equipment and material necessary to complete the crack repairs shown will be considered completely covered by the contract unit price for Epoxy Pressure Injecting.

Miscellaneous:

Outline of old work is indicated by light dashed lines.
Heavy lines indicate new work.

Contractor shall verify all dimensions in field before ordering new material.

Existing structure dimensions and longitudinal dimensions are based on the original design plans.

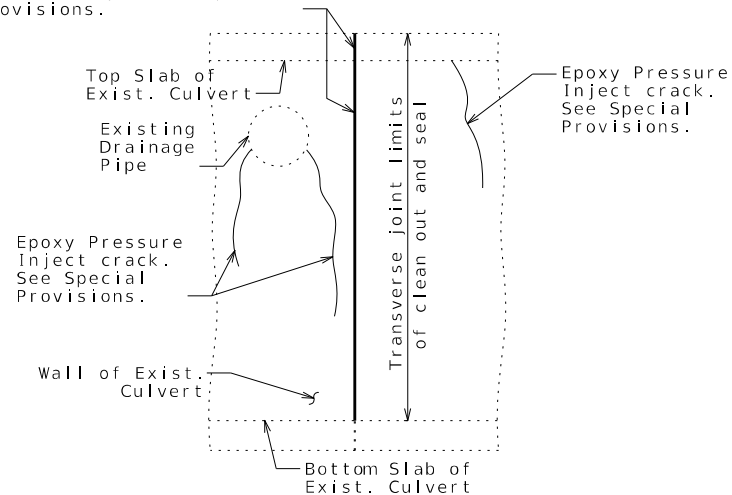
Contractor will be responsible for Dewatering and Culvert Cleanout within the culvert limits in accordance with Special Provisions for Dewatering, Special Provisions for Culvert Cleanout and Sec 206.

For limits and payment of maintenance clean out of the existing channel, including sediment and debris removal and vegetation trimming within the channel limits, see Roadway Plans.

Traffic Handling:

Traffic to be maintained on structure during construction.

Clean out and seal existing 1/4" transverse joint with Plastic Joint Compound for Concrete Pipe. See Special Provisions.



TYPICAL ELEVATION OF BOX CULVERT SHOWING CLEAN OUT AND SEAL OF EXISTING TRANSVERSE JOINTS AND CRACK REPAIRS

(**) Extension (1961), dimensions based on as-built plans ±.

(***) MSD Extension (1968), dimensions based on as-built plans ±.

Notes (continued):

Superstructure Repairs (Unformed) are limited to the existing top slab of culvert.

Substructure Repairs (Unformed) are limited to the existing walls and bottom slab of culvert.

Unformed concrete repairs shall be preformed in accordance with Special Provisions, Shotcrete Concrete Repair and Sec 704.

The cost of all labor, equipment and material necessary to complete the unformed concrete repairs shown will be considered completely covered by the contract unit prices for Superstructure Repair (Unformed) and Substructure Repair (Unformed).

Sealant of transverse joints shall be performed in accordance with Special Provisions for Clean Out and Seal Transverse Joints and Section 1057.

The cost of all labor, equipment and material necessary to complete the joint repairs shown will be considered completely covered by the contract unit price for Clean Out and Seal Transverse Joints.

REPAIRS TO CULVERT - BRIDGE: DUNN ROAD, I-270 & PERSHALL ROAD OVER BRANCH OF MALINE CREEK

ROUTE I-270 FROM ROUTE AC TO ROUTE N

ABOUT 0.0 MILES WEST OF ROUTE AC

☐ MEDIAN I-270 TIE STATION 527+77.29± (MATCH EXISTING)

Designed April 2025
Detailed April 2025
Checked May 2025

Note: This drawing is not to scale. Follow dimensions.

Sheet No. 1 of 4



THIS SHEET HAS BEEN SIGNED, SEALED AND DATED ELECTRONICALLY

DATE PREPARED

9/19/2025

ROUTE STATE

I-270 MO

DISTRICT SHEET NO.

BR 1

COUNTY

ST. LOUIS

JOB NO.

J613618

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

J08883

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)

MoDOT

HDR Engineering, Inc.

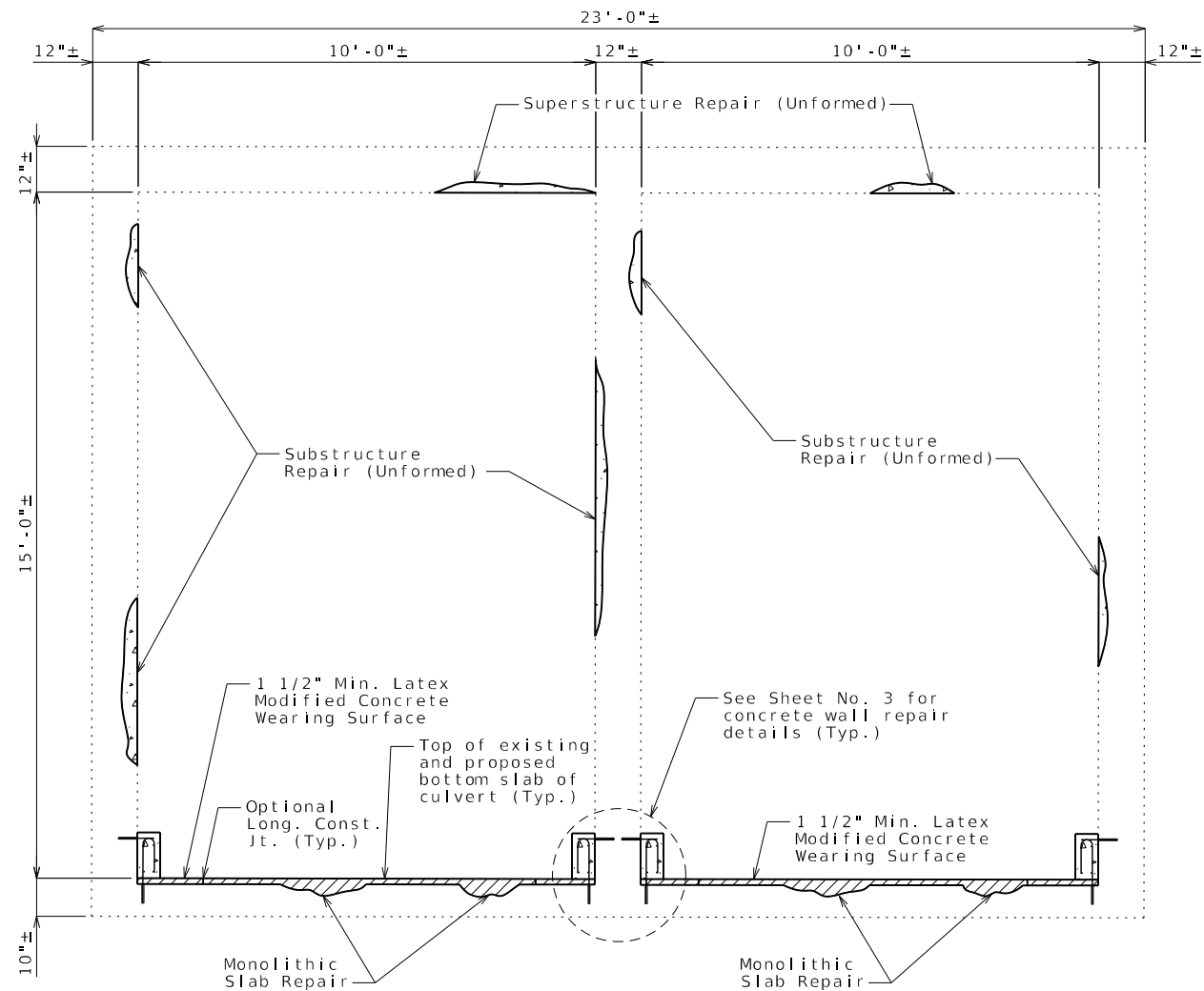
1001 Highlands Plaza Drive

Suite 200

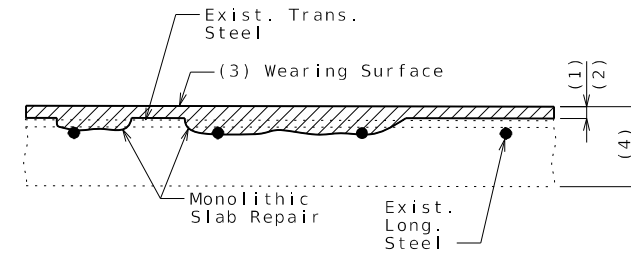
St. Louis, MO 63110

314-425-6300

Certificate of Authority: 000856



TYPICAL SECTION OF BOX CULVERT SHOWING
REPAIRS TO ORIGINAL (1931) SECTION



BOTTOM SLAB OF EXISTING CULVERT
SHOWING MONOLITHIC REPAIR

- (1) Total Surface Hydro Demolition to remove 1 1/2" (min.) and any deteriorated existing slab.
- (2) 1 1/2" (min.) Latex Modified Concrete Wearing Surface.
- (3) Top of proposed Latex Modified Concrete Wearing Surface to match top of original bottom slab of culvert elevation.
- (4) Original and proposed thickness of bottom slab of culvert, 10".

Notes:

Order of Repairs:

1. Dewater and clean out west barrel of culvert.
2. Complete Total Surface Hydro Demolition, within the limits shown.
3. Complete the following repairs:
 - a. Superstructure and Substructure Repairs (Unformed)
 - b. Crack repair, Epoxy Pressure Injecting
 - c. Clean Out and Seal Transverse Joints
4. Install Resin Anchor System for concrete wall repair.
5. Place new Latex Modified Concrete Wearing Surface, within the limits shown, including additional material for areas with monolithic slab repair.
6. Place reinforcement and concrete for concrete wall repair.
7. Repeat steps for east barrel of culvert.

Total Surface Hydro Demolition, installation of Latex Modified Concrete Wearing Surface and concrete wall repair to be performed within the original culvert (1931) limits.

Top of proposed Latex Modified Concrete Wearing Surface to match the top of existing bottom slab elevation.

Superstructure Repairs (Unformed) are limited to the existing top slab of culvert.

Substructure Repairs (Unformed) are limited to the existing walls of culvert.

Identifying, locating and determining the limits of unformed concrete repairs will be the responsibility of the Contractor and Engineer. Prior to any unformed concrete repair being performed the Contractor and Engineer shall agree to the repair location and limits proposed.

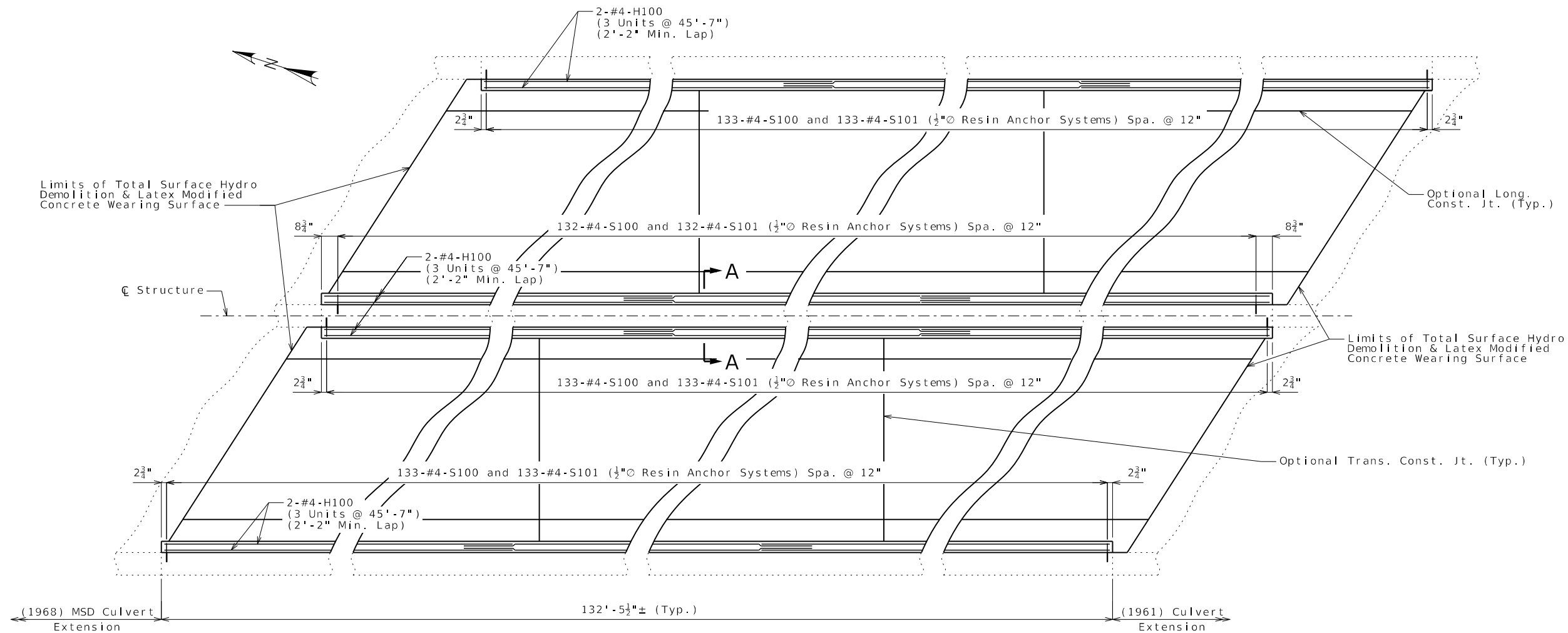
Unformed concrete repairs shall be performed in accordance with Special Provisions, Shotcrete Concrete Repair and Sec 704.

Supplementary wearing surface material for monolithic slab repair will be paid for at the fixed unit price in accordance with Sec 109.

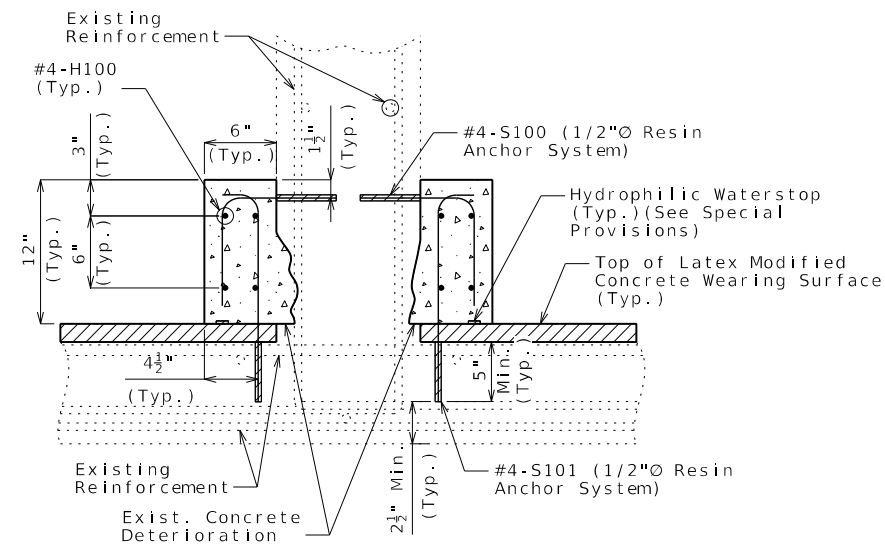
The cost of all labor, equipment and material necessary to complete the unformed concrete repairs shown will be considered completely covered by the contract unit prices for Superstructure Repair (Unformed) and Substructure Repair (Unformed).

Concrete crack filler shall be applied over any optional longitudinal or transverse construction joints in the latex modified concrete wearing surface in accordance with Special Provisions for Concrete Crack Filler and Sec 704. The cost of all labor, equipment and material necessary to complete the application of the concrete crack filler will be considered incidental to the contract unit price for Latex Modified Concrete Wearing Surface.

Work this sheet with Sheet No. 3.



PART PLAN OF BOX CULVERT SHOWING REPAIRS TO ORIGINAL (1931) SECTION



SECTION A-A

(1931) CULVERT SECTION - REPAIR DETAILS

- Notes:
- Dimensions shown are horizontal and based on original design plans.
 - Exposed edge of concrete block shall have either a 1/2" radius or a 3/8" bevel.
 - Concrete for the concrete block shall be Class B-1 Concrete.
 - Cost of furnishing and installing the waterstop, complete in place, will be considered completely covered by the contract unit price for Class B-1 Concrete (Culverts-Bridge).
 - The contractor shall use one of the qualified resin anchor systems in accordance with Sec 1039.
 - Cost of furnishing and installing the resin anchor systems, complete in place, will be considered completely covered by the contract unit price for Class B-1 Concrete (Culverts-Bridge).
 - The minimum embedment depth in concrete with f'c = 3,000 psi for the resin anchor systems shall be that required to meet the minimum ultimate pullout strength in accordance with Sec 1039 but shall not be less than 5" into existing concrete.
 - Epoxy Coated #4-S100 and #4-S101 Grade 60 reinforcing bars shall be substituted for the 1/2" dia threaded rods.
 - #4-S100 and #4-S101 reinforcing bar weights provided on Sheet No. 4 are for informational purposes only. Cost of furnishing and installing the reinforcing bars will be considered completely covered by the contract unit price for Class B-1 Concrete (Culverts-Bridge).
 - Contractor to locate and map existing box culvert reinforcement and ensure resin anchors miss reinforcement. If spacing of resin anchors conflicts with existing reinforcement, the location of resin anchors may be shifted up to 2" along the interior wall, and up to 3" at all other locations to avoid hitting reinforcement.

For bending diagram and quantity of Resin Anchor Systems, see Sheet No. 4.
Work this sheet with Sheet No. 2.

Designed April 2025
Detailed April 2025
Checked May 2025

Note: This drawing is not to scale. Follow dimensions.

Sheet No. 3 of 4



THIS SHEET HAS BEEN
SIGNED, SEALED AND DATED
ELECTRONICALLY

DATE PREPARED
9/19/2025

ROUTE STATE
I-270 MO

DISTRICT SHEET NO.
BR 3

COUNTY
ST. LOUIS

JOB NO.
J613618

CONTRACT ID.

PROJECT NO.

BRIDGE NO.
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DATE

MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
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1-888-ASK-MODOT (1-888-275-6636)

MoDOT

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Certificate of Authority: 000856

2:11:20 PM 9/19/2025

