

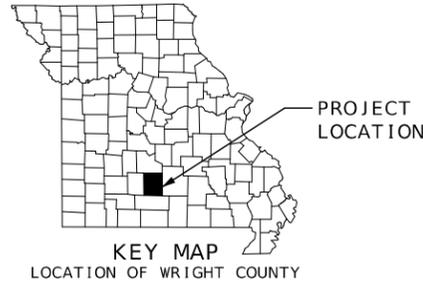
DESIGN DESIGNATION

A.A.D.T. - 2026 = 316
 A.A.D.T. - 2046 = 349
 T = 18%
 V = 55 M.P.H.
 D = 50%/50%

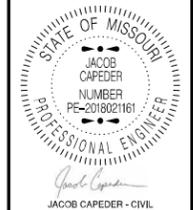
FUNCTIONAL CLASSIFICATION- MINOR ARTERIAL

NO RIGHT OF WAY ACQUISITION

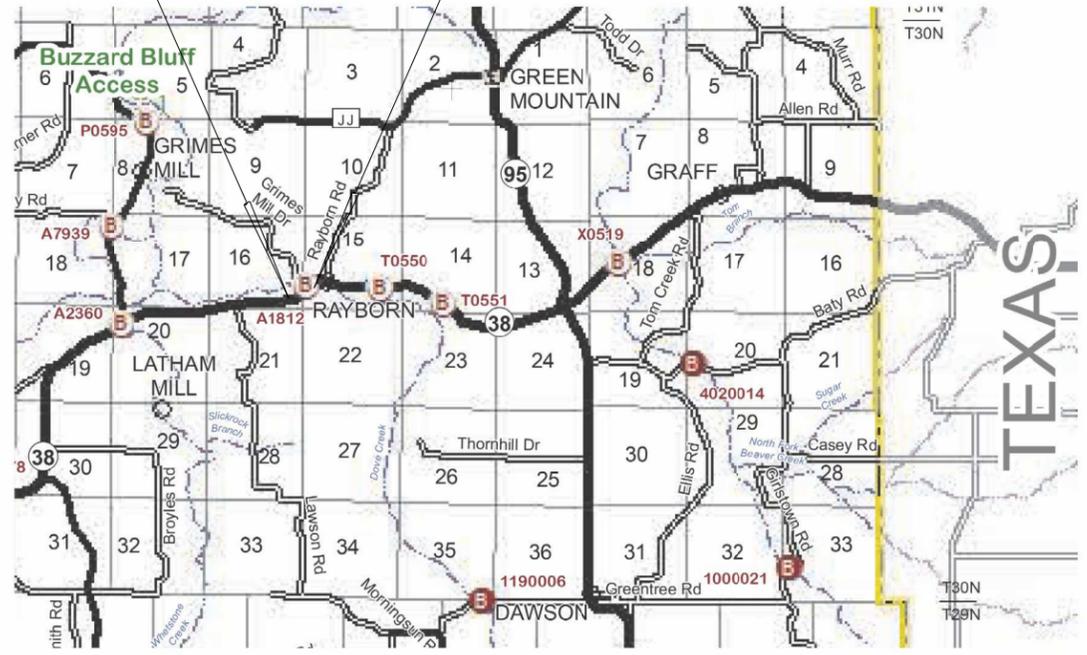
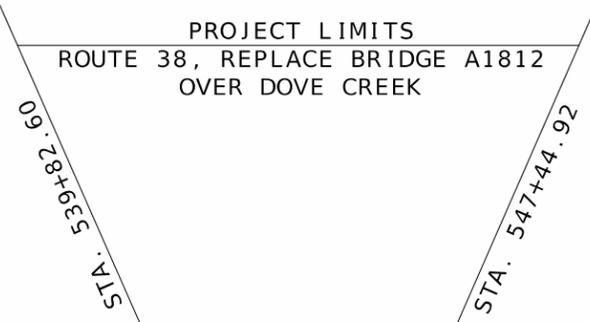
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION PLANS FOR PROPOSED STATE HIGHWAY WRIGHT COUNTY



| DESCRIPTION | SHEET NUMBER |
|-------------------------------------|--------------|
| TITLE SHEET | 1 |
| TYPICAL SECTIONS (TS) (2 SHEETS)--- | 2 |
| QUANTITIES (QU) (3 SHEETS)----- | 3 |
| PLAN-PROFILE (PP)----- | 4-5 |
| REFERENCE POINTS (RP)----- | 6 |
| SPECIAL SHEETS (SS)----- | 7 |
| TRAFFIC CONTROL SHEETS (TC)----- | 8-11 |
| EROSION CONTROL SHEETS (EC)----- | 12 |
| BRIDGE DRAWINGS (B) | |
| A9619----- | 1-28 |
| CROSS SECTIONS (XS)----- | 1-6 |



JACOB CAPEDER - CIVIL
 MO-PE-201802161
 DATE PREPARED
3/12/2026
 ROUTE 38 STATE MO
 DISTRICT SE SHEET NO. 1
 COUNTY WRIGHT
 JOB NO. J9S3689
 CONTRACT ID.
 PROJECT NO.
 BRIDGE NO.



CONVENTIONAL SYMBOLS
(USED IN PLANS)

- | | EXISTING | NEW |
|------------------------------|----------|-------|
| BUILDINGS AND STRUCTURES | | |
| GUARD RAIL | | |
| GUARD CABLE | | |
| CONCRETE RIGHT-OF-WAY MARKER | | |
| STEEL RIGHT-OF-WAY MARKER | | |
| LOCATION SURVEY MARKER | | |
| UTILITIES | | |
| FIBER OPTICS | -FO- | -FO- |
| OVERHEAD CABLE TV | -OTV- | -OTV- |
| UNDERGROUND CABLE TV | -UTV- | -UTV- |
| OVERHEAD TELEPHONE | -OT- | -OT- |
| UNDERGROUND TELEPHONE | -UT- | -UT- |
| OVERHEAD POWER | -OE- | -OE- |
| UNDERGROUND POWER | -UE- | -UE- |
| SANITARY SEWER | -S- | -S- |
| STORM SEWER | -SS- | -SS- |
| GAS | -G- | -G- |
| WATER | -W- | -W- |
| MANHOLE | | |
| FIRE HYDRANT | | |
| WATER VALVE | | |
| WATER METER | | |
| DROP INLET | | |
| DITCH BLOCK | | |
| GROUND MOUNTED SIGN | | |
| LIGHT POLE | | |
| H-FRAME POWER POLE | | |
| TELEPHONE PEDESTAL | | |
| FENCE | | |
| CHAIN LINK | | |
| WOVEN WIRE | | |
| GATE POST | | |
| BENCHMARK | | |

NOTE: DASHED OR OPEN SYMBOLS INDICATE EXISTING FEATURES

THE EXISTENCE AND APPROXIMATE LOCATION OF UTILITY FACILITIES KNOWN TO EXIST, AS SHOWN ON THE PLANS, ARE BASED ON THE BEST INFORMATION AVAILABLE TO THE COMMISSION AT THIS TIME. THIS INFORMATION IS PROVIDED BY THE COMMISSION "AS-IS" AND THE COMMISSION EXPRESSLY DISCLAIMS ANY REPRESENTATION OR WARRANTY AS TO THE COMPLETENESS, ACCURACY, OR SUITABILITY OF THE INFORMATION FOR ANY USE. RELIANCE UPON THIS INFORMATION IS DONE AT THE RISK AND PERIL OF THE USER, AND THE COMMISSION SHALL NOT BE LIABLE FOR ANY DAMAGES THAT MAY ARISE FROM ANY ERROR IN THE INFORMATION. IT IS, THEREFORE, THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE, LOCATION AND STATUS OF ANY FACILITY. SUCH VERIFICATION INCLUDES DIRECT CONTACT WITH THE LISTED UTILITIES.

LENGTH OF PROJECT

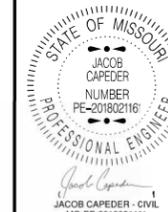
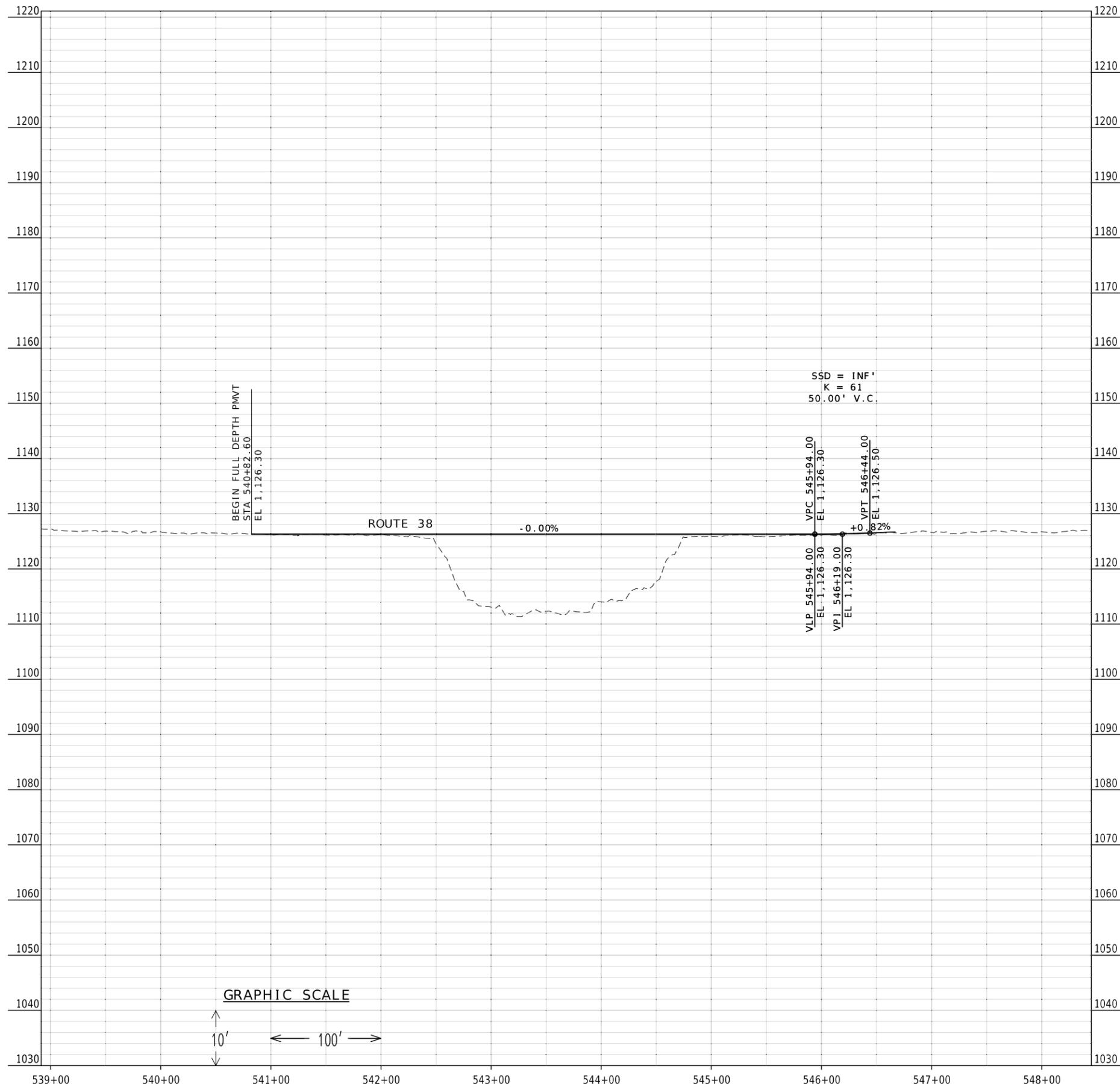
| | |
|---|------------------|
| BEGINNING OF PROJECT | STA. 539 + 82.60 |
| END OF PROJECT | STA. 547 + 44.92 |
| APPARENT LENGTH | 762.32 FEET |
| EQUATIONS AND EXCEPTIONS: | |
| TOTAL CORRECTIONS | 0 FEET |
| NET LENGTH OF PROJECT | 762.32 FEET |
| STATE LENGTH | 0.144 MILES |
| FOR INFORMATION ONLY ESTIMATED DISTURBED ACRES | 0.75 ACRES |

| DATE | DESCRIPTION |
|------|-------------|
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MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
 JEFFERSON CITY, MO 65102
 1-888-ASK-MODOT (1-888-275-6636)

PROPOSED PROFILE GRADE
ROUTE 38



DATE PREPARED
1/9/2026

ROUTE 38 STATE MO

DISTRICT SE SHEET NO. 5

COUNTY
WRIGHT

JOB NO.
J9S3689

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

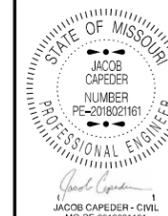
| DATE | DESCRIPTION |
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MISSOURI HIGHWAYS AND TRANSPORTATION
COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-273-6636)

COORDINATE POINT LISTING

| SHEET NO | STATION | LOCATION | OFFSET (USFT) | MODIFIED STATE PLANE (GROUND) | | | DESCRIPTION | GPK POINT ID |
|-------------------------------|-----------|-----------|---------------|-------------------------------|------------------------|--------------------------|--|--------------|
| | | | | NORTHING (US SURVEY FT) | EASTING (US SURVEY FT) | ELEVATION (US SURVEY FT) | | |
| PROJECT CONTROL POINTS | | | | | | | | |
| 4 | 539+79.28 | RTE 38 RT | 17.8030 | 535,006.513 | 1,680,432.985 | 1,125.929 | 5/8" Rebar w/ Pink Plastic Cap set Below Surface South of Route 38 & West of a Private Drive | CP100 |
| 4 | 542+65.05 | RTE 38 RT | 14.9708 | 535,209.656 | 1,680,600.769 | 1,126.060 | 5/8" Rebar w/ Pink Plastic Cap set Below Surface South of Route 38 & West of Bridge A1812 | CP101 |
| 4 | 547+87.90 | RTE 38 RT | 15.1009 | 535,405.517 | 1,680,769.630 | 1,125.896 | 5/8" Rebar w/ Pink Plastic Cap set Below Surface South of Route 38 & East of Bridge A1812 | CP102 |
| 4 | 545+01.43 | RTE 38 RT | 13.2008 | 535,610.472 | 1,680,937.729 | 1,127.811 | 5/8" Rebar w/ Pink Plastic Cap set Below Surface North of Route 38 & West of County Road 330 | CP103 |
| ALIGNMENTS | | | | | | | | |
| 4 | 539+82.60 | RTE 38 CL | 0 | 535,020.510 | 1,680,421.591 | 1,126.300 | BEGIN PROJECT | - |
| 4 | 547+44.92 | RTE 38 CL | 0 | 535,587.453 | 1,680,929.563 | 1,126.746 | END PROJECT | - |
| 4 | 546+09.20 | RTE 38 CL | 0 | 535,495.874 | 1,680,829.827 | 1,126.319 | PC CURVE | - |
| 4 | 553+94.90 | RTE 38 CL | 0 | 535,662.730 | 1,681,455.617 | 1,129.706 | PT CURVE | - |
| 4 | 550+01.73 | RTE 38 CL | 0 | 535,686.680 | 1,681,164.102 | 1,128.634 | PI CURVE | - |



DATE PREPARED
1/9/2026

ROUTE 38 STATE MO
DISTRICT SE SHEET NO. 6

COUNTY
WRIGHT

JOB NO.
J9S3689

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

| | | | |
|--|---|--|--|
| <p>CP-100</p> <p>5/8" REBAR WITH PINK PLASTIC CAP SOUTH SIDE OF RTE 38 AND WEST OF PRIVATE DRIVE</p> <p>STATE PLANE COORDINATES CENTRAL ZONE</p> <p>X 1,680,432.985 Y 535,006.513 Z 1,125.929</p> | <p>CP-101</p> <p>5/8" REBAR WITH PINK PLASTIC CAP SOUTH SIDE OF RTE 38 AND WEST OF BRIDGE A1812</p> <p>STATE PLANE COORDINATES CENTRAL ZONE</p> <p>X 1,680,600.769 Y 535,209.656 Z 1,126.060</p> | <p>CP-102</p> <p>5/8" REBAR WITH PINK PLASTIC CAP EAST SIDE OF RTE 38 AND EAST OF BRIDGE A1812</p> <p>GRID COORDINATES</p> <p>X 1,680,769.630 Y 535,405.517 Z 1,125.896</p> | <p>CP-103</p> <p>5/8" REBAR WITH PINK PLASTIC CAP NORTH SIDE OF RTE 38 AND WEST OF COUNTY ROAD 330</p> <p>GRID COORDINATES</p> <p>X 1,680,937.729 Y 535,610.472 Z 1,127.811</p> |
|--|---|--|--|

ALL PROJECT COORDINATES HAVE BEEN PROJECTED FROM THE MISSOURI STATE PLANE COORDINATE (SPC) SYSTEM OF 1983 USING AN AVERAGE PROJECT PROJECTION (GRID TO GROUND) FACTOR. TO GET BACK TO STATE PLANE COORDINATES MULTIPLY THE PROJECT COORDINATES BY THE AVERAGE GRID FACTOR AS SHOWN IN THE "REFERENCE CONTROL INFORMATION" PORTION OF THIS TABLE.

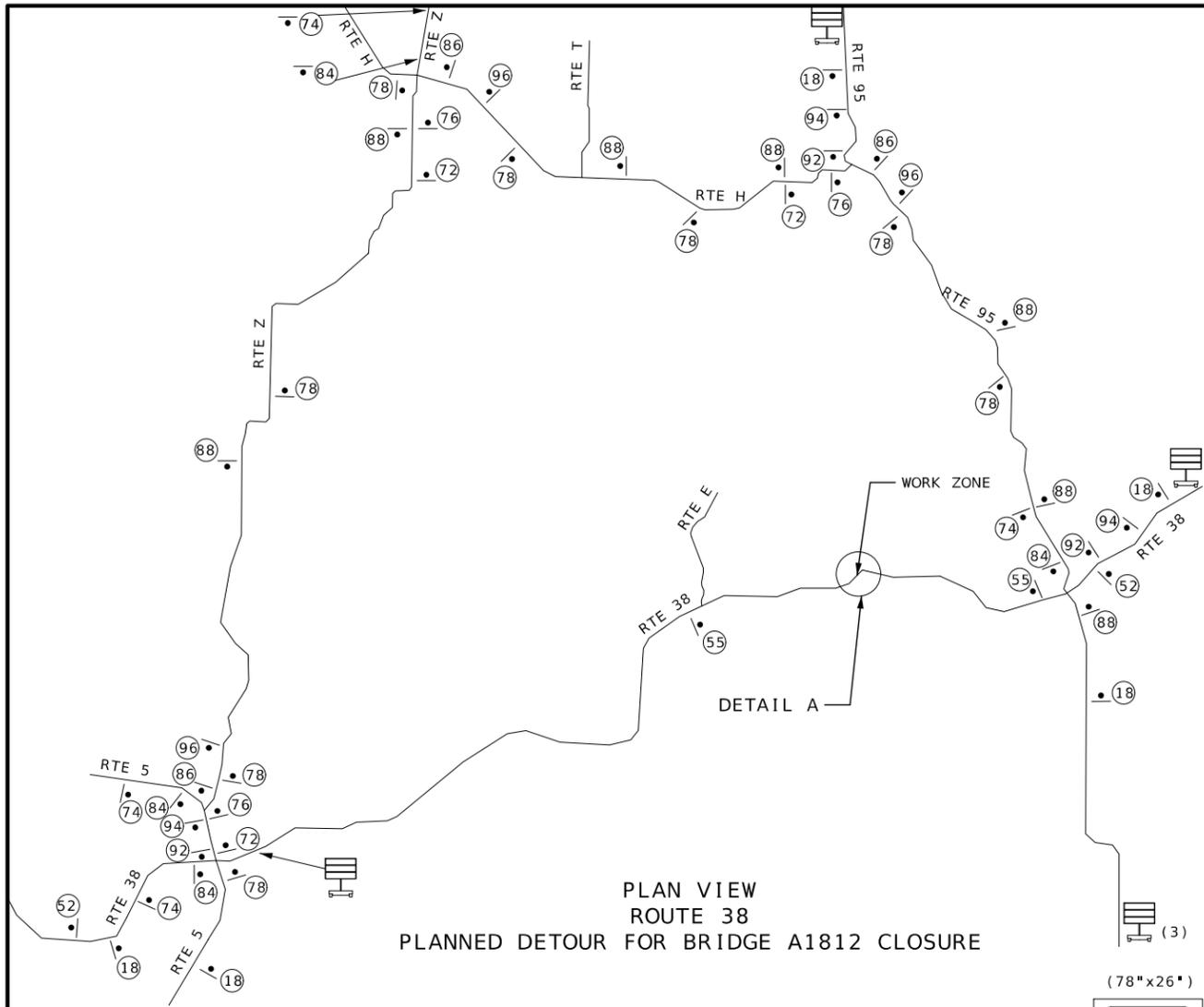
| PROJECT COORDINATE INFORMATION | |
|--------------------------------|--------------------------|
| COORDINATE SYSTEM | MO SPC 83 |
| HORIZONTAL DATUM | NAD83 2011 |
| VERTICAL DATUM | NAVD88 |
| GEOID MODEL | 2018 |
| ELEVATIONS DETERMINED BY | DIFFERENTIAL LEVELING |
| PROJECT PROJECTION FACTOR | 1.00000000 |
| REFERENCE CONTROL INFORMATION | |
| COORDINATE SYSTEM | MO SPC 1983 |
| CONTROL STATION | MOMG |
| DESIGNATION | MODOT MTN GROVE CORS ARP |
| CORS_ID | MOMG |
| PID | DM4680 |
| LATITUDE | 370751.66923 |
| LONGITUDE | 921839.95094 |
| NORTHING (M) | 144008.6820 |
| EASTING (M) | 516784.5240 |
| ZONE | CENTRAL |
| PROJECT AVERAGE GRID FACTOR | 1.00000000 |

DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL JEFFERSON CITY, MO 65102 1-888-ASK-MODOT (1-888-275-6636)



PLAN VIEW
ROUTE 38
PLANNED DETOUR FOR BRIDGE A1812 CLOSURE

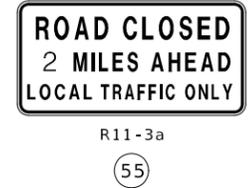
TRAFFIC CONTROL LEGEND

- CHANGEABLE MESSAGE SIGN (CMS) CONTRACTOR FURNISHED/ CONTRACTOR RETAINED
- TYPE 3 MOVEABLE BARRICADE WITH LIGHT
- SIGN (SINGLE SIDED)
- WORK ZONE

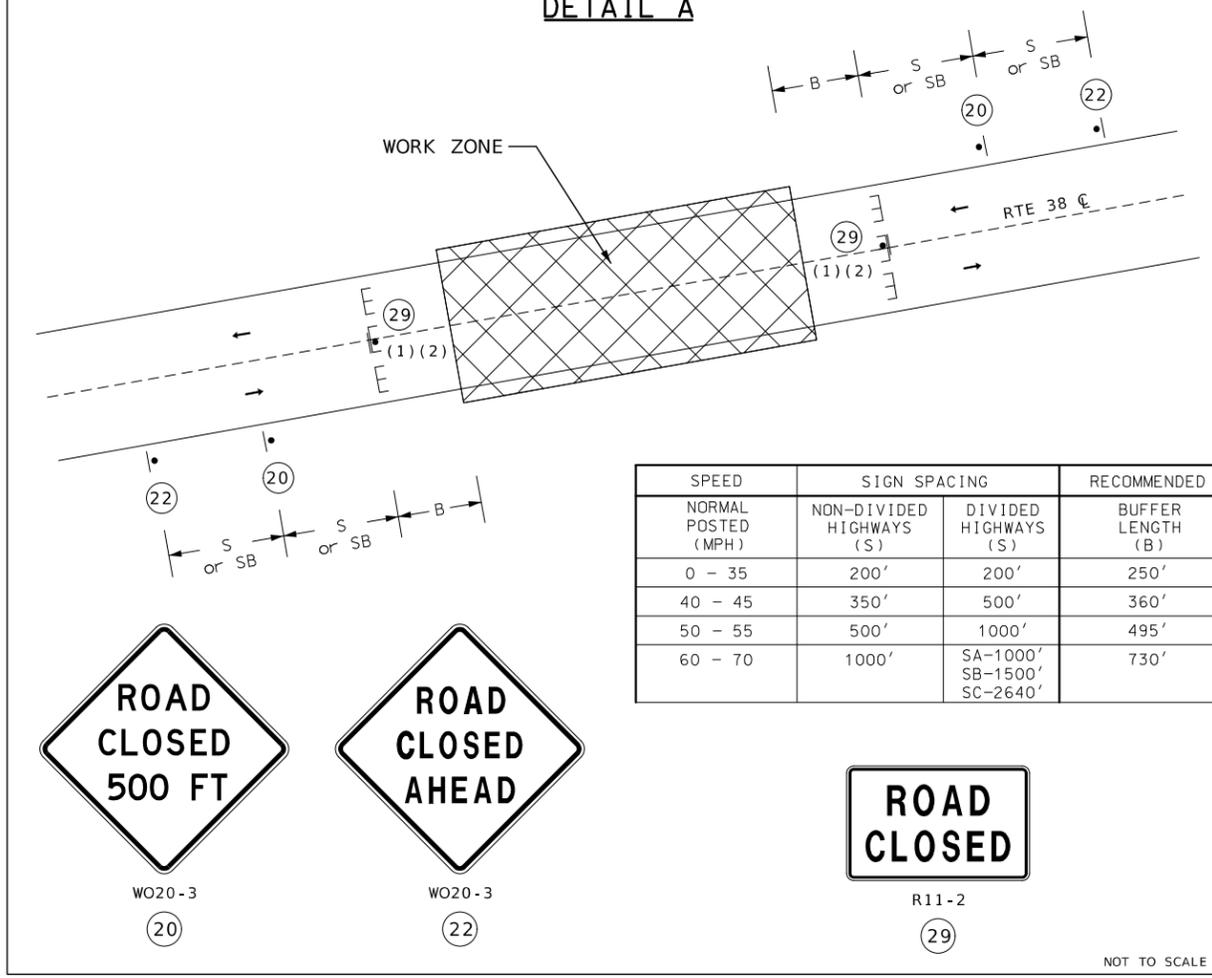
NOTES:

- ANY EXISTING SIGN THAT CONFLICTS WITH THE TRAFFIC CONTROL SIGNAGE SHALL BE COMPLETELY COVERED OR REMOVED.
- PLACEMENT OF TRAFFIC CONTROL SIGNING IS APPROXIMATE AND MAY BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- R11-2 SIGN ASSOCIATED WITH TYPE 3 BARRICADE SHALL BE MOUNTED ON POST 7-10 FEET BEHIND BARRICADE.
- CMS LOCATIONS AND MESSAGES TO BE APPROVED BY ENGINEER.
- ALL SIGNS SHOWN ON THIS SHEET ARE NON-PORTABLE AND SHALL REMAIN IN PLACE FOR THE DURATION OF THE PROJECT.
- (1) "ROAD CLOSED" SIGN MAY BE PLACED 7-10 FEET BEHIND THE BARRICADES AND AT A SIGN HEIGHT APPROPRIATE TO THE TYPE OF ROADWAY. ONE BARRICADE REQUIRED TO CLOSE EVERY 8-FEET OFFPAVEMENT. PAVED SHOULDERS SHALL BE INCLUDED IN THE AREA.
- (2) ADDITIONAL BARRICADES MAY BE USED AND OFFSET TO FACILITATE ACCESS FOR WORK VEHICLES, LOCAL TRAFFIC, TO BUSINESSES, ETC.
- (3) LOCATE CMS BOARD BETWEEN MOUNTAIN GROVE WALMART ENTRANCES (RTE 5-NB, LOG MILE 64.308).
- (4) LOCATE 1000' BEFORE INTERSECTION.

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|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|
| (78"x26") |
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| | | | | | | | | | | |
| (72) | (74) | (76) | (78) | (84) | (86) | (88) | (92) | (94) | (96) | |
| (4) | (4) | | | | | | | (4) | (4) | |



DETAIL A



JACOB CAPEDER - CIVIL
MO-PE-2018021161
PROFESSIONAL ENGINEER

DATE PREPARED
1/9/2026

| | |
|----------------|----------------|
| ROUTE 38 | STATE MO |
| DISTRICT SE | SHEET NO. 9 |

COUNTY
WRIGHT

JOB NO.
J9S3689

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

| DESCRIPTION | DATE |
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MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

NOT TO SCALE

NOTES:

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY PROVIDE ADDITIONAL PROTECTIVE TRUCKS EQUIPPED WITH PROPER WARNING DEVICES.

PROTECTIVE TRUCKS AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

VEHICLE HAZARD WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

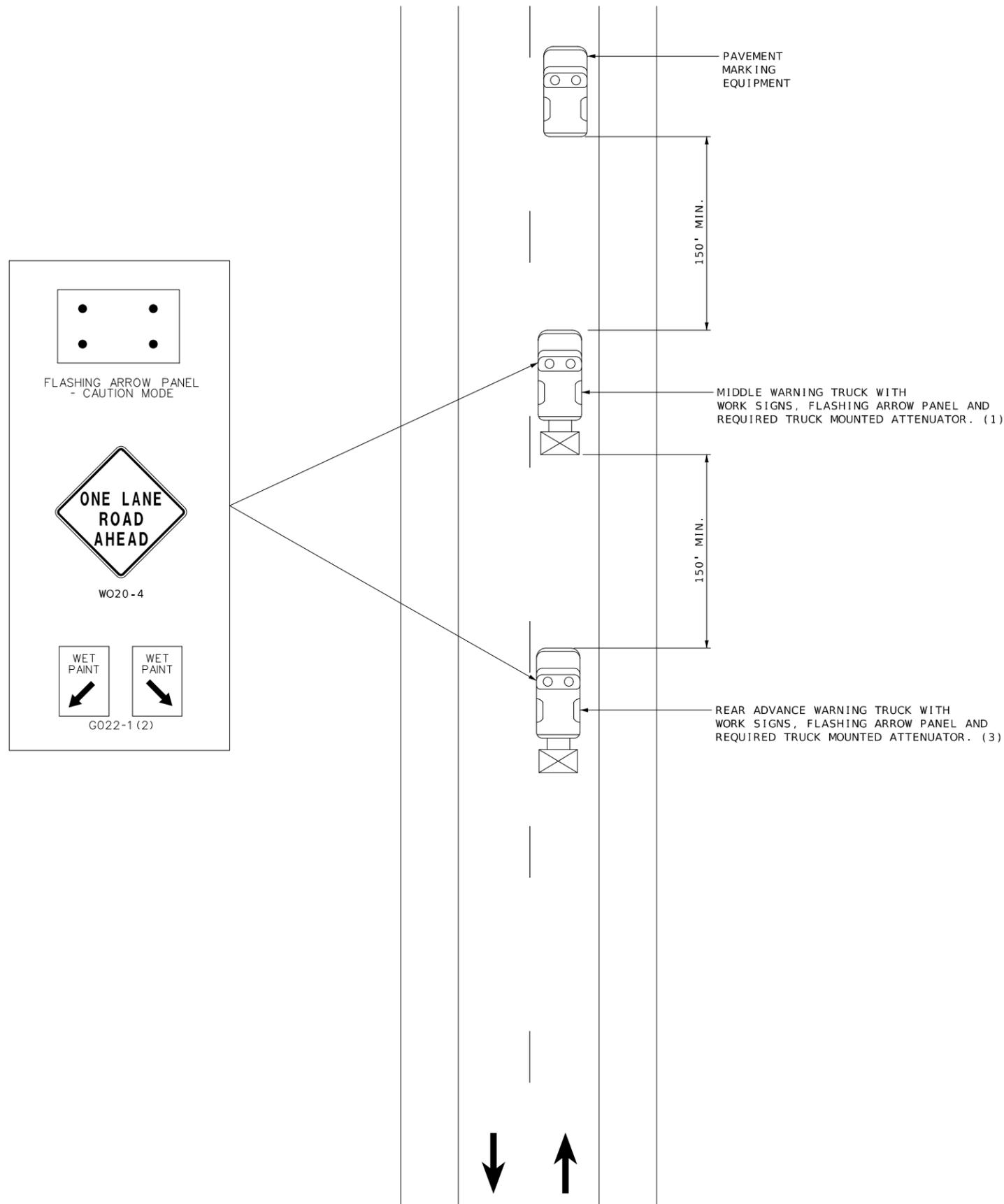
VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED IN A MANNER SUCH THAT THEY ARE NOT OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS ON VEHICLE-MOUNTED SIGNS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.

FLASHING ARROW PANELS SHALL BE INCIDENTAL TO TRUCK MOUNTED ATTENUATORS, WHEREVER USED. NO ADDITIONAL PAYMENT WILL BE MADE.

(1) TRUCK IS OPTIONAL ON TWO-LANE UNDIVIDED HIGHWAYS IF SIGNING AND ARROW BOARD ARE MOUNTED ON THE PAVEMENT MARKING EQUIPMENT.

(2) WET PAINT SIGNS ARE INSTALLED TO INDICATE THE SIDE IN WHICH THE PAVEMENT MARKING MATERIAL IS BEING APPLIED. AT THE CONTRACTOR'S OPTION, A FRONT FACING WET PAINT SIGN MAY BE INSTALLED ON THE LEFT SIDE OF THE PAVEMENT MARKING EQUIPMENT.

(3) REAR ADVANCE WARNING TRUCK IS POSITIONED AT THE NO TRACK POINT OF THE PAVEMENT MARKING MATERIAL, OR VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE, OR SPACING SHOWN, WHICHEVER IS GREATER.



STRIPING ON TWO-LANE HIGHWAY

TEMPORARY TRAFFIC CONTROL
SHEET 4 OF 4

DRAWING NOT TO SCALE. REFER TO DIMENSIONING.



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|---------------------------|-----------------|
| DATE PREPARED 1/9/2026 | |
| ROUTE 38 | STATE MO |
| DISTRICT SE | SHEET NO. 11 |
| COUNTY WRIGHT | |
| JOB NO. J9S3689 | |
| CONTRACT ID. | |
| PROJECT NO. | |
| BRIDGE NO. | |

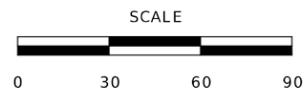
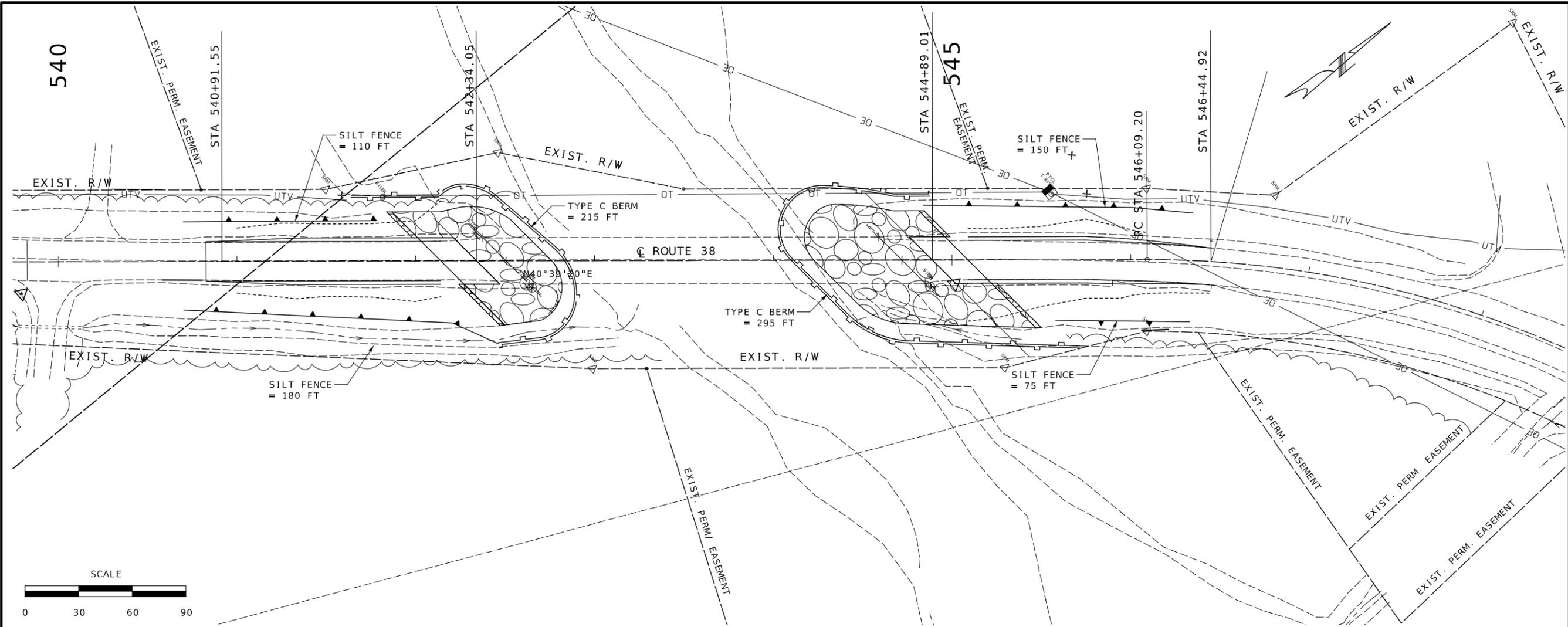
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MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

540

545



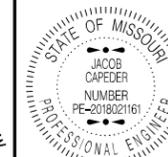
TEMPORARY EROSION CONTROL LEGEND

-  TYPE 1 ROCK DITCH LINER AND EROSION CONTROL GEOTEXTILE (STANDARD PLAN 609.60)
-  TYPE 2 ROCK BLANKET (2'-0" THICK) AND EROSION CONTROL GEOTEXTILE

-  TEMPORARY BERM TYPE C
-  SILT FENCE

NOTES:

- LOCATIONS ARE APPROXIMATE.
- EROSION CONTROL DEVICES SHALL BE PLACED AS APPROVED BY ENGINEER.
- REFER TO STANDARD SHEET 806.10K FOR PLACEMENT OF DITCH CHECKS AND ADDITIONAL TEMPORARY EROSION CONTROL DETAILS.



JACOB CAPEDEP - CIVIL
MO-PE-201802161

DATE PREPARED
1/9/2026

ROUTE 38 STATE MO

DISTRICT SE SHEET NO. 12

COUNTY WRIGHT

JOB NO.

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

| DATE | DESCRIPTION |
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MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION



105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

(55'-70'-70'-55') SDCL COMPOSITE WIDE FLANGE BEAM SPANS
(SKEW: 45°00'00" R.A.)

SEC/SUR 15 TWP 30N RGE 13W



DATE PREPARED
3/16/2026

ROUTE 38 STATE MO

DISTRICT BR SHEET NO. 1

COUNTY WRIGHT

JOB NO. J9S3689

CONTRACT ID.

PROJECT NO.

BRIDGE NO. A9619

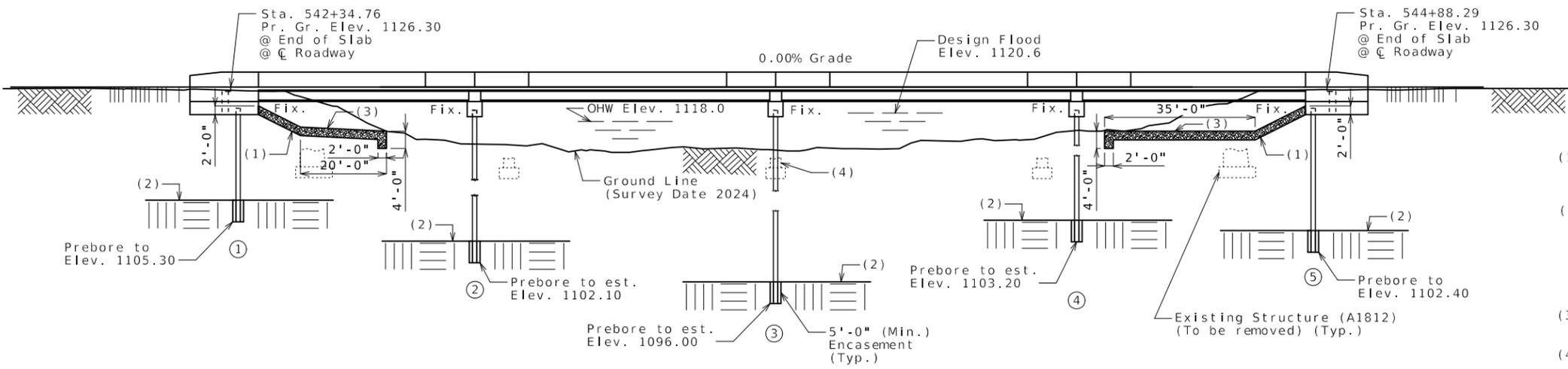
DESCRIPTION

DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

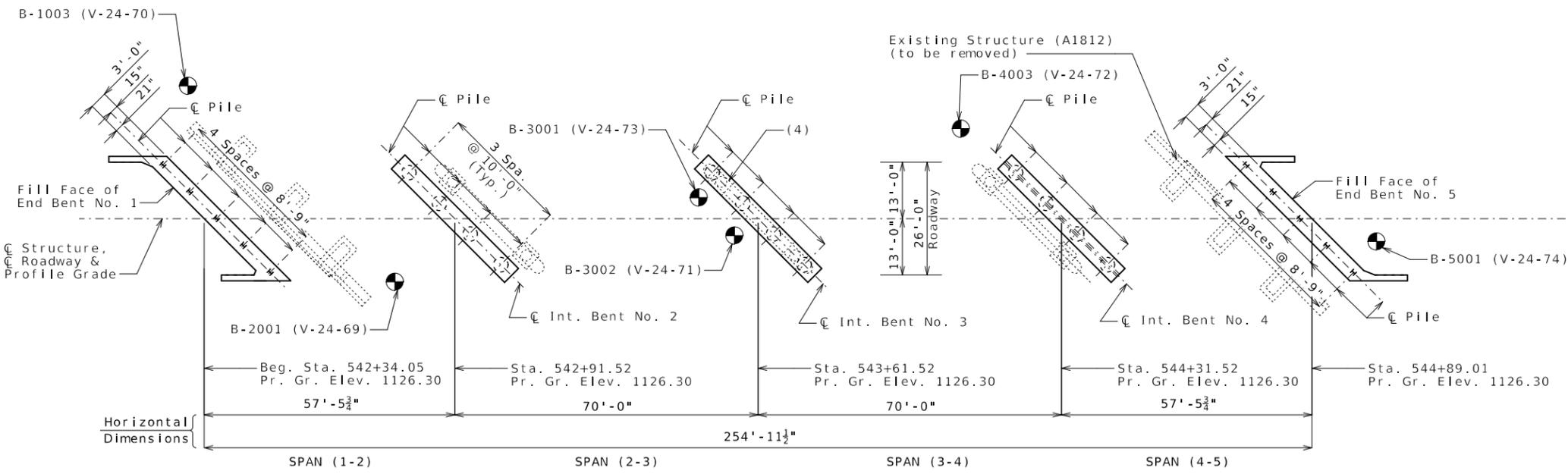
105 WEST CAPITOL JEFFERSON CITY, MO 65102

1-888-ASK-MODOT (1-888-275-6636)



GENERAL ELEVATION

- (1) 2:1 (H:V) Slope (Normal) with 2'-0" Type 2 Rock Blanket with Permanent Erosion Control Geotextile (Rdwy. Item) (Typ.)
- (2) Anticipated Top of Sound Rock
Elev. 1110.30 (Bent No. 1)
Elev. 1107.10 (Bent No. 2)
Elev. 1101.00 (Bent No. 3)
Elev. 1108.20 (Bent No. 4)
Elev. 1107.40 (Bent No. 5)
- (3) Berm Elevation 1114.0 (Slope to drain) (Typ.)
- (4) Remove or prebore thru Existing footings of Int. Bent No. 3



PLAN

⊙ Indicates location of borings.

Notice and Disclaimer Regarding Boring Log Data
The locations of all subsurface borings for this structure are shown on the plan sheet(s) for this structure. The boring data for all locations indicated, as well as any other boring logs or other factual records of subsurface data and investigations performed by the department for the design of the project, are shown on Sheet(s) No. 28 and may be included in the Electronic Bridge Deliverables. They will also be available from the Project Contact upon written request. No greater significance or weight should be given to the boring data depicted on the plan sheets than is given to the subsurface data available from the district or elsewhere.

The Commission does not represent or warrant that any such boring data accurately depicts the conditions to be encountered in constructing this project. A contractor assumes all risks it may encounter in basing its bid prices, time or schedule of performance on the boring data depicted here or those available from the district, or on any other documentation not expressly warranted, which the contractor may obtain from the Commission.

Notes:
For General Notes, Estimated Quantities, Estimated Quantities for Slab on Steel, Foundation Data, Hydrologic Data and Location Sketch, see Sheet No. 2.
Existing roadway fill under the ends of the bridge shall be removed as shown. Removal of existing roadway fill will be considered completely covered by the contract unit price for roadway excavation.
Roadway fill shall be completed to the final roadway section and up to the elevation of the bottom of the concrete beam within the limits of the structure and for not less than 25 feet in back of the fill face of the end bents before any piles are driven for any bents falling within the embankment section.

- BM #1: COTTON PICKER SPINDLE IN THE ROOT OF A 10" ELM TREE
ELEV. = 1127.869'
- BM #4: CHISELED SQUARE ON THE WEST END OF THE NORTH HEADWALL OF A BOX CULVERT
ELEV. = 1125.316'

BRIDGE: ROUTE 38 OVER DOVE CREEK
ROUTE 38 FROM ROUTE 95 TO ROUTE E
ABOUT 2.90 MILES WEST OF ROUTE 95
BEG. STATION 542+34.05

Designed May 2025
Detailed May 2025
Checked Jan. 2026

General Notes:

Design Specifications:

2020 AASHTO LRFD Bridge Design Specifications (9th Ed.)
 2023 AASHTO Guide Specifications for LRFD Seismic Bridge Design (3rd Ed.)
 Seismic Design Category = B (Seismic Details)
 Design earthquake response spectral acceleration coefficient at 1.0 second period, $S_{D1} = 0.166g$
 Acceleration Coefficient (effective peak ground acceleration coefficient), $A_s = 0.125g$

Design Loading:

Vehicular = HL-93
 Future Wearing Surface = 35 lb/sf
 Earth = 120 lb/cf
 Equivalent Fluid Pressure = 45 lb/cf (Min.)
 Superstructure: Simply-Supported, Non-Composite for dead load.
 Continuous Composite for live load.

Design Unit Stresses:

Class B Concrete (Substructure) $f'c = 3,000$ psi
 Class B-1 Concrete (Barrier and CECIP) $f'c = 4,000$ psi
 Class B-2 Concrete (Superstructure except Barrier) $f'c = 4,000$ psi
 Reinforcing Steel (Stainless Steel) (ASTM A955 Grade 75) $f_y = 75,000$ psi
 Structural Steel (ASTM A709 Grade 50) $f_y = 50,000$ psi
 Structural Steel HP Pile (ASTM A709 Grade 50) $f_y = 50,000$ psi

Neoprene Pads:

Neoprene bearing pads shall be 60 durometer and shall be in accordance with Sec 716.

Fabricated Steel Connections:

Field connections shall be made with 3/4-inch diameter ASTM F3125 Grade A325 Type 1 bolts and 13/16-inch diameter holes, except as noted.

Joint Filler:

All joint filler shall be in accordance with Sec 1057 for preformed sponge rubber expansion and partition joint filler, except as noted.

Reinforcing Steel:

Minimum clearance to reinforcing steel shall be 1 1/2", unless otherwise shown.

Minimum clearance between galvanized steel (piles and structural steel, including shear connectors) and stainless reinforcing steel including bar supports shall be 1 1/2". Nylon, PVC, or polyethylene spacers shall be used to maintain clearance. Nylon cable ties shall be used to bind the spacers to the reinforcement.

Structural Steel Protective Coatings:

Structural steel shall be galvanized in accordance with ASTM A123 and Sec 1081.

Traffic Handling:

Structure to be closed during construction. Traffic to be maintained on other routes during construction. See roadway plans for traffic control.

Miscellaneous:

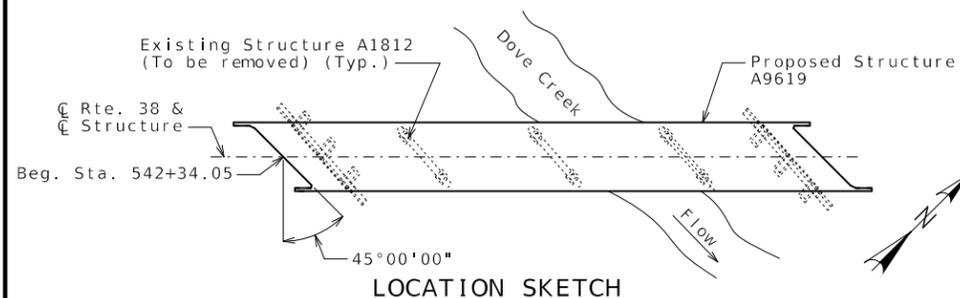
High strength bolts, nuts and washers will be sampled for quality assurance as specified in Sec 106.

| Estimated Quantities Slab on Steel | | |
|---------------------------------------|----------|--------|
| Item | | Total |
| Class B-2 Concrete | cu. yard | 246 |
| Reinforcing Steel (Stainless Steel) | pound | 65,410 |

The table of Estimated Quantities for Slab on Steel represents the quantities used by the State in preparing the cost estimate for concrete slabs. The area of the concrete slab will be measured to the nearest square yard longitudinally from end of slab to end of slab and transversely from out to out of bridge slab (or with the horizontal dimensions as shown on the plan of slab). Payment for stay-in-place corrugated steel forms, conventional forms, all concrete and stainless steel reinforcing steel will be considered completely covered by the contract unit price for the slab. Variations may be encountered in the estimated quantities but the variations cannot be used for an adjustment in the contract unit price.

Method of forming the slab shall be as shown on the plans and in accordance with Sec 703. All hardware for forming the slab to be left in place as a permanent part of the structure shall be coated in accordance with ASTM A123 or ASTM B633 with a thickness class SC 4 and a finish type I, II or III.

Slab shall be cast-in-place with conventional forms or stay-in-place corrugated steel forms. Precast prestressed panels will not be permitted.



Detailed Apr. 2025
 Checked Jan. 2026

Note: This drawing is not to scale. Follow dimensions.

| Estimated Quantities | | | | |
|---|-------------|---------|-----------|---------|
| Item | | Substr. | Superstr. | Total |
| Removal of Miscellaneous ACM (Non-Friable) | sq. foot | | 15 | 15 |
| Class 1 Excavation | cu. yard | 100 | | 100 |
| Removal of Bridges (A1812) | Lump sum | | | 1 |
| Bridge Approach Slab (Minor) | sq. yard | | 118 | 118 |
| Galvanized Structural Steel Piles (12 in.) | linear foot | 175 | | 175 |
| Galvanized Cast-In-Place Concrete Piles (20 in.) | linear foot | 248 | | 248 |
| Pre-Bore for Piling | linear foot | 321 | | 321 |
| Class B Concrete (Substructure) | cu. yard | 77.0 | | 77.0 |
| Slab on Steel | sq. yard | | 808 | 808 |
| Type D Barrier | linear foot | | 546 | 546 |
| Reinforcing Steel (Stainless Steel) | pound | 6,800 | | 6,800 |
| Fabricated Structural Low Alloy Steel (I-Beam) A709, Grade 50 | pound | 125,240 | | 125,240 |
| Slab Drain | each | | 44 | 44 |
| Galvanized Structural Steel | Lump sum | | | 1 |
| Vertical Drain at End Bents | each | 2 | | 2 |
| Laminated Neoprene Bearing Pad | each | | 32 | 32 |

All concrete between the upper and lower construction joints in the end bents is included in the Estimated Quantities for Slab on Steel.

All reinforcement in the end bents is included in the Estimated Quantities for Slab on Steel.

All reinforcement in the intermediate bent concrete diaphragms except reinforcement embedded in the beam cap is included in the Estimated Quantities for Slab on Steel.

All concrete above the intermediate beam cap is included in the Estimated Quantities for Slab on Steel.

Reinforcement for cast-in-place pile at intermediate bents is included in the substructure quantity tables.

Cost of L4x4 ASTM A709 Grade 36 HP pile anchors and 3/4-inch diameter ASTM F3125 Grade A325 Type 1 bolts, complete in place, will be considered completely covered by the contract unit price for Galvanized Structural Steel Piles (12 in.).

| Foundation Data | | | | | | | |
|-------------------|--|-------------|-------------|-------------|-------------|-------------|-------------|
| Type | Design Data | Bent Number | | | | | |
| | | 1 | 2 | 3 | 4 | 5 | |
| Load Bearing Pile | Pile Type and Size | HP 12x53 | CECIP 20" | CECIP 20" | CECIP 20" | HP 12x53 | |
| | Number | ea | 5 | 4 | 4 | 4 | 5 |
| | Approximate Length Per Each | ft | 16 | 19 | 25 | 18 | 19 |
| | Pile Point Reinforcement | ea | - | - | - | - | - |
| | Min. Galvanized Penetration (Elev.) | ft | FULL LENGTH |
| | Est. Max. Scour Depth 100 (Elev.) | ft | - | 1106 | 1106 | 1106 | - |
| | Pile Driving Verification Method | | DF | HARD ROCK * | HARD ROCK * | HARD ROCK * | DF |
| | Resistance Factor | | 0.40 | 0.50 | 0.50 | 0.50 | 0.40 |
| | Minimum Nominal Axial Compressive Resistance | kip | 391 | 524 | 524 | 524 | 391 |

CECIP = Closed Ended Cast-In-Place concrete pile

DF = FHWA-modified Gates Dynamic Pile Formula

Load Bearing Pile:

$$\text{Minimum Nominal Axial Compression Resistance} = \frac{\text{Maximum Factored Loads}}{\text{Resistance Factor}}$$

Estimated Maximum Scour Depth (Elevation) shown is for verifying Minimum Nominal Axial Compressive Resistance using dynamic testing only where pile resistance contribution above this elevation shall not be considered.

All piles shall be galvanized down to the minimum galvanized penetration (elevation).

The contractor shall make every effort to achieve the minimum galvanized penetration (elevation) shown on the plans for all piles. Deviations in penetration less than 5 feet of the minimum will be considered acceptable provided the contractor makes the necessary corrections to ensure the minimum penetration is achieved on subsequent piles.

HP piles are anticipated to be driven to refusal on rock. Review all borings for depth of rock and restrict driving as appropriate to comply with hard rock driving criteria in accordance with Sec 702. When pile refusal on rock occurs, as approved by the engineer, the minimum nominal axial compressive resistance is verified and no additional pile driving verification method is required.

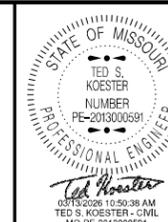
* Prebore for piles at Bents No. 2, 3, and 4 to a minimum of 5'-0" into sound rock. Ensure Piles are seated on sound rock and not rubble. Piles shall be seated with the pile hammer to ensure refusal on hard rock in accordance with Sec 702 and no additional pile driving verification method is required. The annular space of the prebored hole shall be backfilled with Class B-1 Concrete to the top of rock then backfilled with loose sand for the rest of the hole.

Prebore for pile at Bents No. 1 and 5 to elevations 1105.30 and 1102.40, respectively, in accordance with Sec 702.

| Hydrologic Data |
|--|
| Drainage Area = 19.5 mi ² |
| Design Flood Frequency = 50 years |
| Design Flood Discharge = 7,300 cfs |
| Design Flood (D.F.) Elevation = 1120.6 |
| Base Flood (100-year) |
| Base Flood Elevation = 1121.4 |
| Base Flood Discharge = 8,600 cfs |
| Estimated Backwater = 1.5 ft |
| Average Velocity thru Opening = 8.0 ft/s |
| Freeboard (50-year) |
| Freeboard = 1.5 ft |
| Roadway Overtopping |
| Overtopping Flood Discharge ≥ N/A |
| Overtopping Flood Frequency ≥ 500 years |
| 500-Year Flood Elevation ≥ 1122.8 |

GENERAL NOTES AND QUANTITIES

Sheet No. 2 of 28



DATE PREPARED
 3/13/2026

ROUTE 38 STATE MO

DISTRICT BR SHEET NO. 2

COUNTY WR IGH T

JOB NO. J9S3689

CONTRACT ID.

PROJECT NO.

BRIDGE NO.

A9619

DESCRIPTION

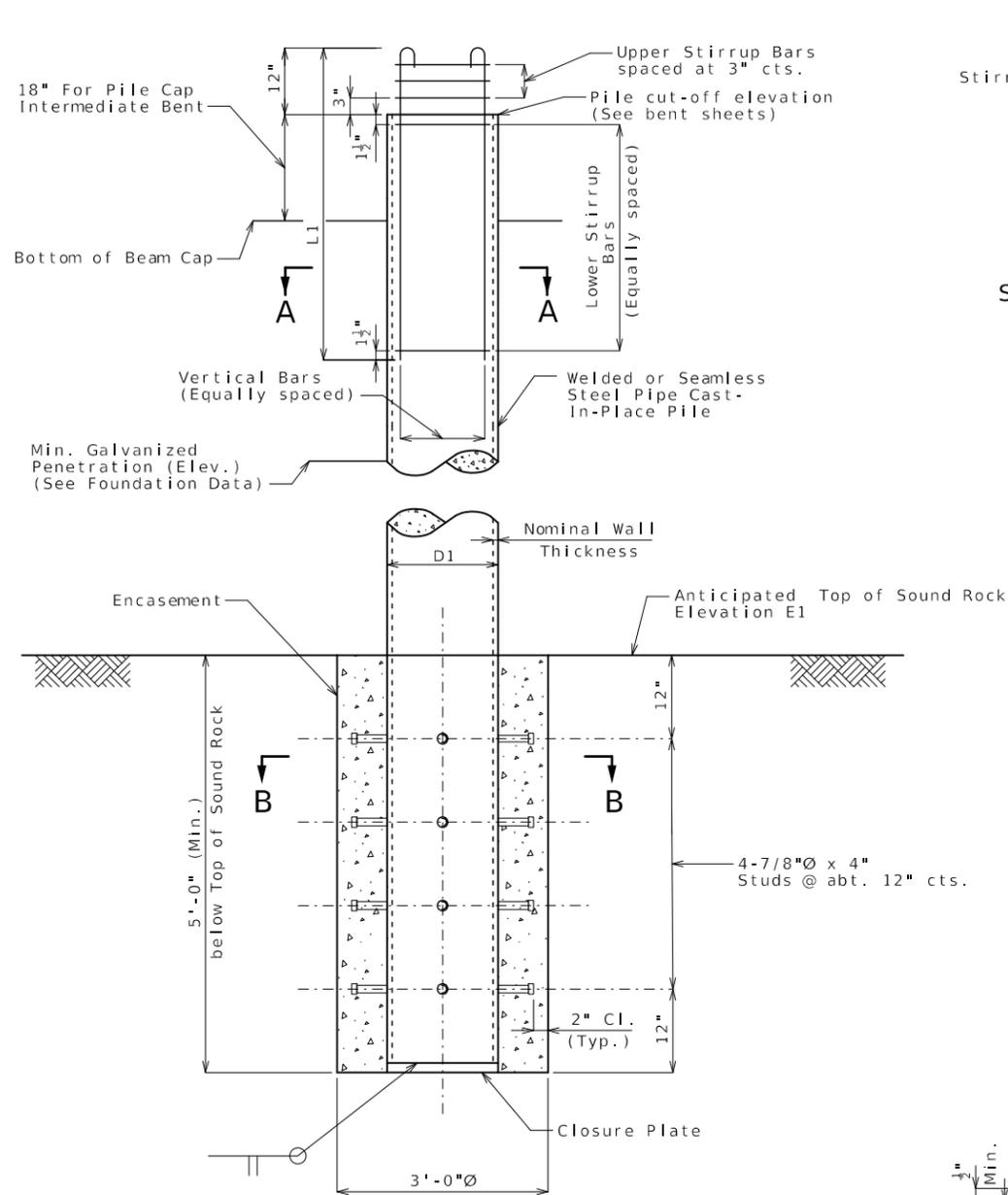
DATE

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

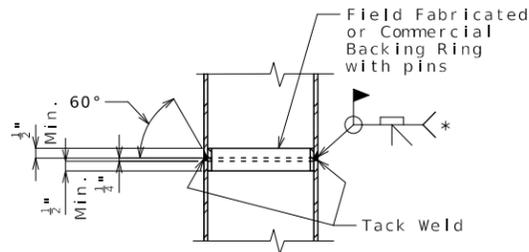
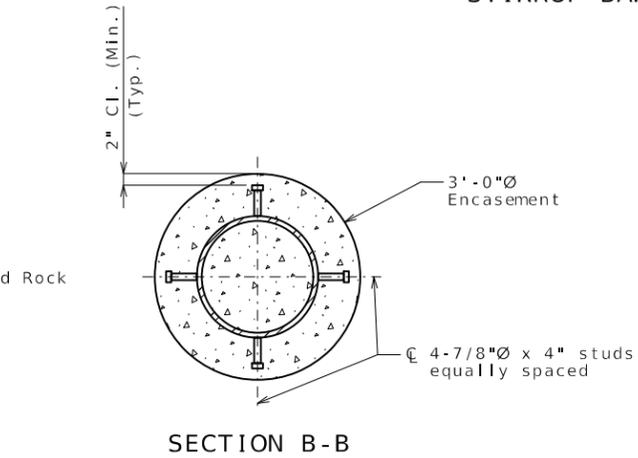
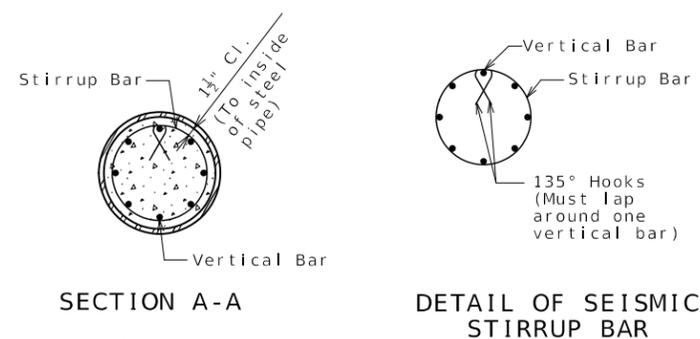
105 WEST CAPITOL JEFFERSON CITY, MO 65102

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GALVANIZED CLOSED ENDED CAST-IN-PLACE (CECIP) CONCRETE PILE WITHOUT PILE POINT REINFORCEMENT



STEEL PIPE PILE SPLICE

* Galvanizing material shall be omitted or removed one inch clear of weld locations in accordance with Sec 702.

| Galvanized Closed Ended Cast-In-Place (CECIP) Concrete Pile Data | | | |
|--|-----------|-----------|-----------|
| Bent Number | 2 | 3 | 4 |
| D1, CECIP Pile (O.D.) | 20" | 20" | 20" |
| Min. Nominal Wall Thickness | 1/2" | 1/2" | 1/2" |
| Closure Plate Thickness | 3/4" | 3/4" | 3/4" |
| Pile Point Reinforcement | None | None | None |
| Vertical Bars | 8-#6-V200 | 8-#6-V300 | 8-#6-V400 |
| L1, Length of Vertical Bars | 7'-3" | 7'-3" | 7'-3" |
| Upper Stirrup Bars | 3-#4-P200 | 3-#4-P300 | 3-#4-P400 |
| Lower Stirrup Bars | 7-#4-P200 | 7-#4-P300 | 7-#4-P400 |
| E1, Anticipated Top of Sound Rock | 1107.10 | 1101.00 | 1108.20 |

Notes:

Welded or seamless steel shell (pipe) shall be ASTM A252 Modified Grade 3 (fy = 50,000 psi) with physical and chemical requirements that meet ASTM A572 Grade 50. Pipe certification and source material certification shall be required.

Concrete for cast-in-place pile and encasement shall be Class B-1.

The minimum wall thickness of any spot or local area of any type shall not be more than 12.5% under the specified nominal wall thickness.

The contractor shall determine the pile wall thickness required to avoid damage from all driving activities, but wall thickness shall not be less than the minimum specified. No additional payment will be made for furnishing a thicker pile wall than specified on the plans.

Closure plate shall not project beyond the outside diameter of the pipe pile. Satisfactory weldments may be made by beveling tip end of pipe or by use of inside backing rings. In either case, proper gaps shall be used to obtain weld penetration full thickness of pipe. Payment for furnishing and installing closure plate will be considered completely covered by the contract unit price for Galvanized Cast-In-Place Concrete Piles (20 in.).

Splices of pipe for cast-in-place concrete pile shall be made watertight and to the full strength of the pipe above and below the splice to permit hard driving without damage. Pipe damaged during driving shall be replaced without cost to the state. Pipe sections used for splicing shall be at least 5 feet in length.

The hooks of vertical bars embedded in the beam cap should not be turned outward, away from the pile core.

Closure plate need not be galvanized.

Reinforcing steel for cast-in-place piles is included in the Bill of Reinforcing Steel.

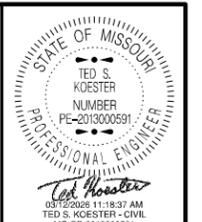
Reinforcement for cast-in-place pile at intermediate bents is included in the substructure quantity tables.

For Foundation Data table, see Sheet No. 2.

The cost of supplying and installing encasement concrete will be considered completely covered by the contract unit price price for Galvanized Cast-In-Place Concrete Piles (20 in.).

The cost of supplying and installing shear connectors will be considered completely covered by the contract unit price price for Galvanized Cast-In-Place Concrete Piles (20 in.).

Shear connectors shall be in accordance with Sec 712, 1037 and 1080.



DATE PREPARED
3/12/2026

ROUTE **38** STATE **MO**

DISTRICT **BR** SHEET NO. **3**

COUNTY
WRIGHT

JOB NO.
J9S3689

CONTRACT ID.

PROJECT NO.

BRIDGE NO.
A9619

| DESCRIPTION | DATE |
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MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

105 WEST CAPITOL
JEFFERSON CITY, MO 65102
1-888-ASK-MODOT (1-888-275-6636)

GALVANIZED CLOSED ENDED CAST-IN-PLACE (CECIP) CONCRETE PILE



DATE PREPARED
3/13/2026

ROUTE 38 STATE MO

DISTRICT BR SHEET NO. 8

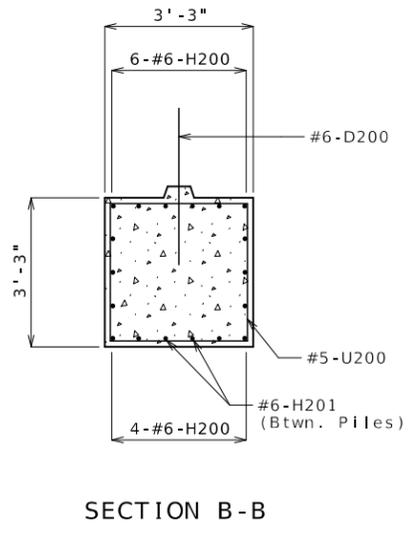
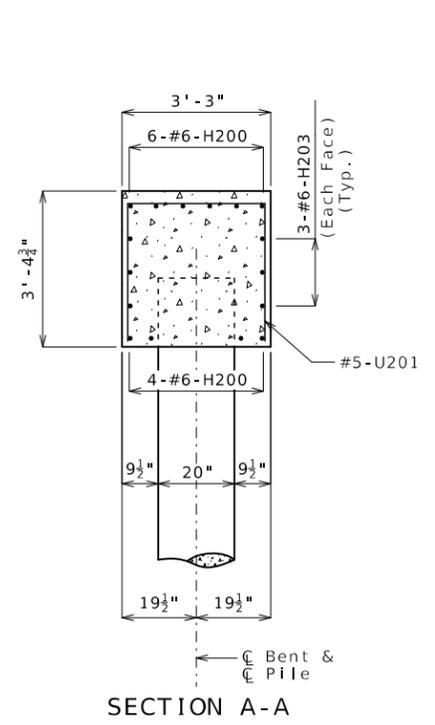
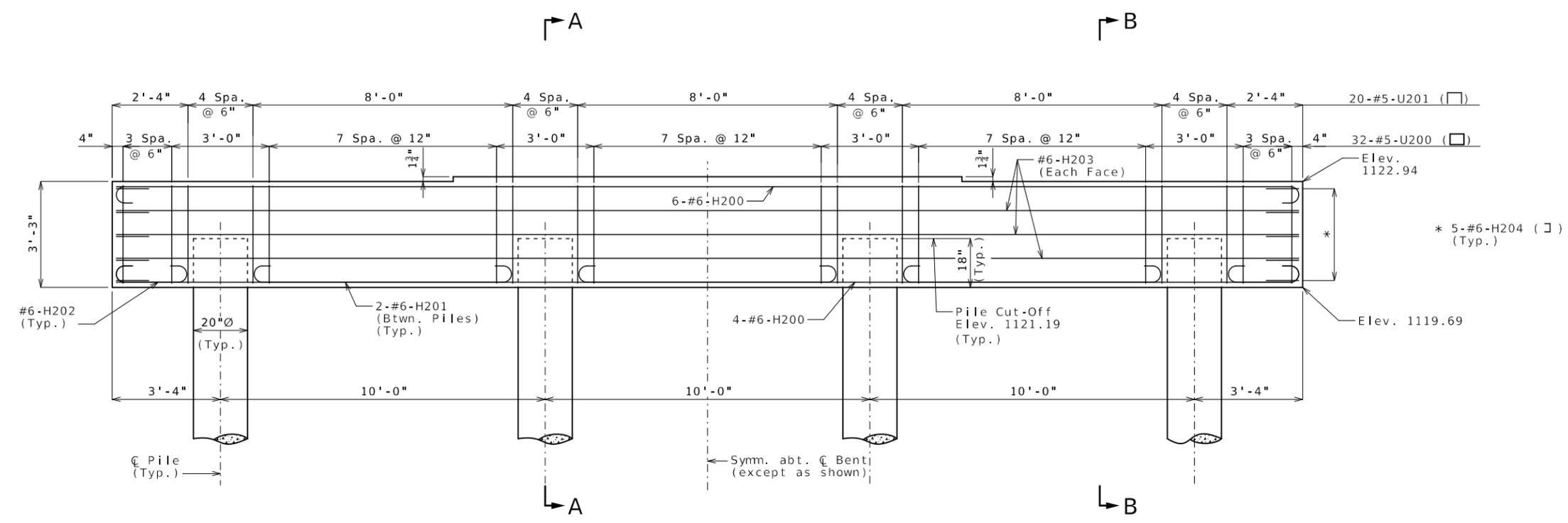
COUNTY
WRIGHT

JOB NO.
J9S3689

CONTRACT ID.

PROJECT NO.

BRIDGE NO.
A9619



| Item | Bent #2 Quantity | Bent #3 Quantity | Bent #4 Quantity |
|--|------------------|------------------|------------------|
| Galvanized Cast-In-Place Concrete Piles (20 in.) | 76 | 100 | 72 |
| Pre-Bore for Piling | 56 | 44 | 56 |
| Class B Concrete (Substructure) | 14.5 | 14.5 | 14.5 |
| Reinforcing Steel (Stainless Steel) | 2,267 | 2,267 | 2,267 |

These quantities are included in the Estimated Quantities table on Sheet No. 2.

Notes:
Work this sheet with Sheet No. 9.
Reinforcing steel shall be shifted to clear piles. U bars shall clear piles by at least 1 1/2 inches.

DETAILS OF INTERMEDIATE BENTS NO. 2, 3 & 4

DESCRIPTION

DATE

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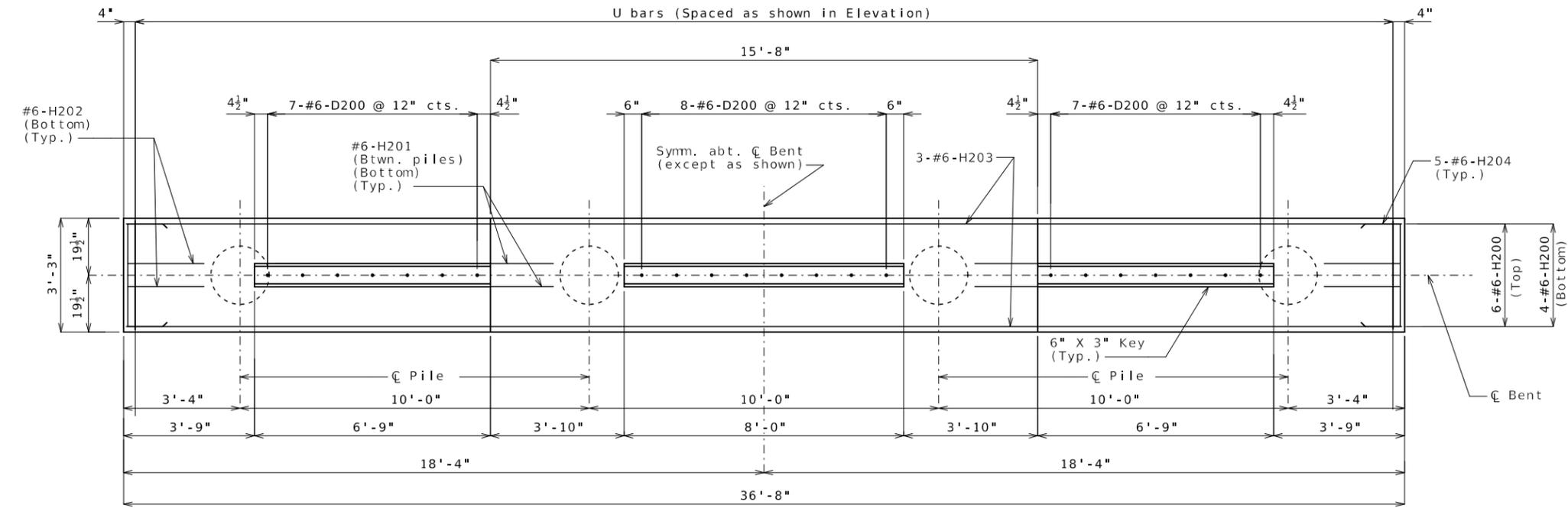
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DISTRICT BR SHEET NO. 9

COUNTY WRIGHT
JOB NO. J9S3689
CONTRACT ID.

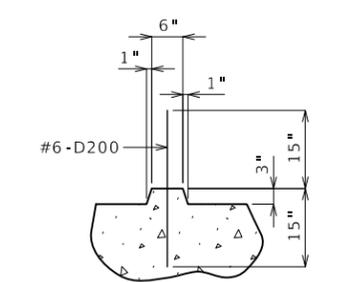
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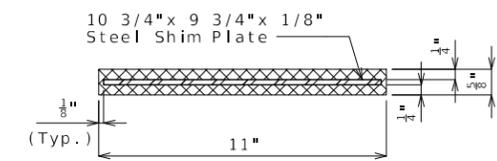
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1-888-ASK-MODOT (1-888-275-6636)



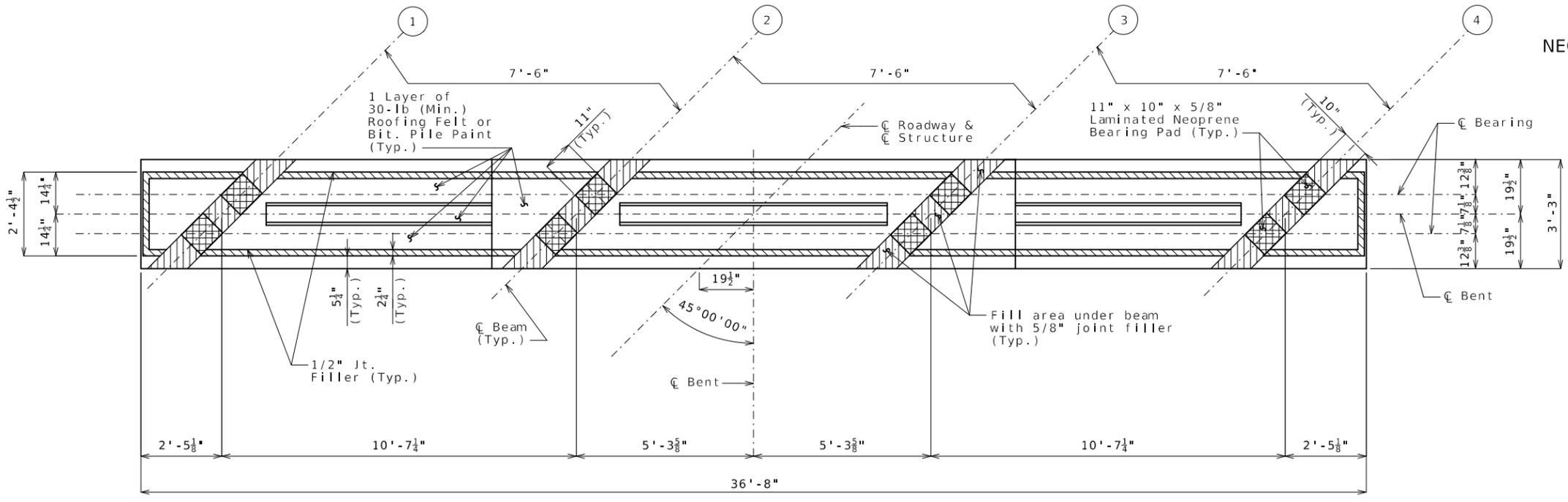
PLAN SHOWING REINFORCEMENT



SECTION THRU KEY



TYPICAL SECTION THRU LAMINATED NEOPRENE BEARING PAD



PLAN OF BEAM

Note:
Work this sheet with Sheet No. 8.

DETAILS OF INTERMEDIATE BENTS NO. 2, 3 & 4

Detailed Jan. 2026
Checked Feb. 2026

Note: This drawing is not to scale. Follow dimensions.

Sheet No. 9 of 28



DATE PREPARED
3/12/2026

ROUTE 38 STATE MO
DISTRICT BR SHEET NO. 12

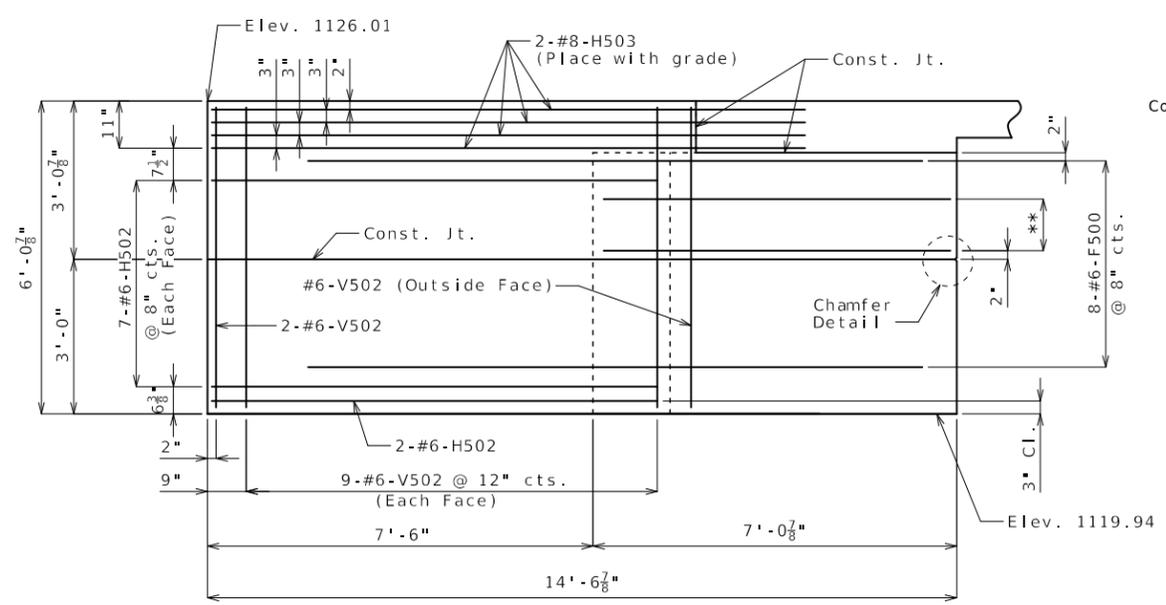
COUNTY WRIGHT
JOB NO. J9S3689
CONTRACT ID.

PROJECT NO.
BRIDGE NO. A9619

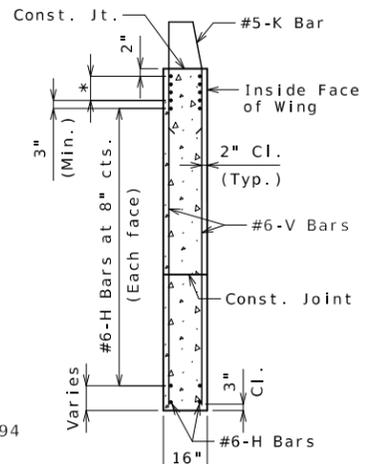
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MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

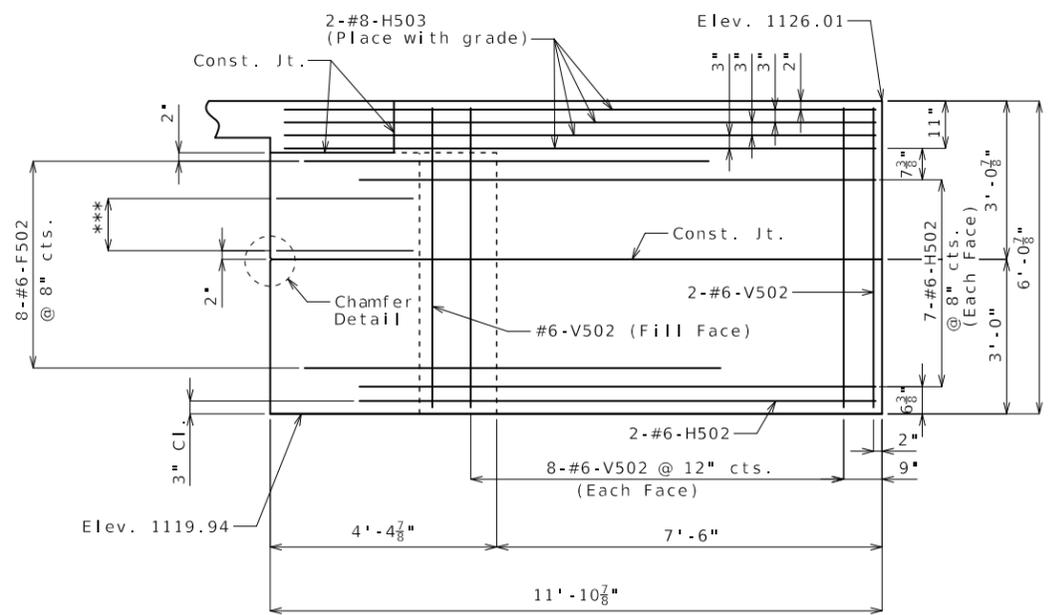
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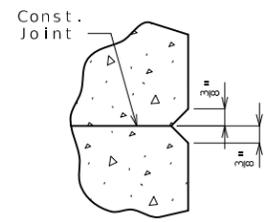
ELEVATION D-D
 ** 3-#6-F501 @ abt. 12" cts.



TYPICAL SECTION THRU WING
 * #8-H Bars at 3" cts. (Each face) (Place with grade)

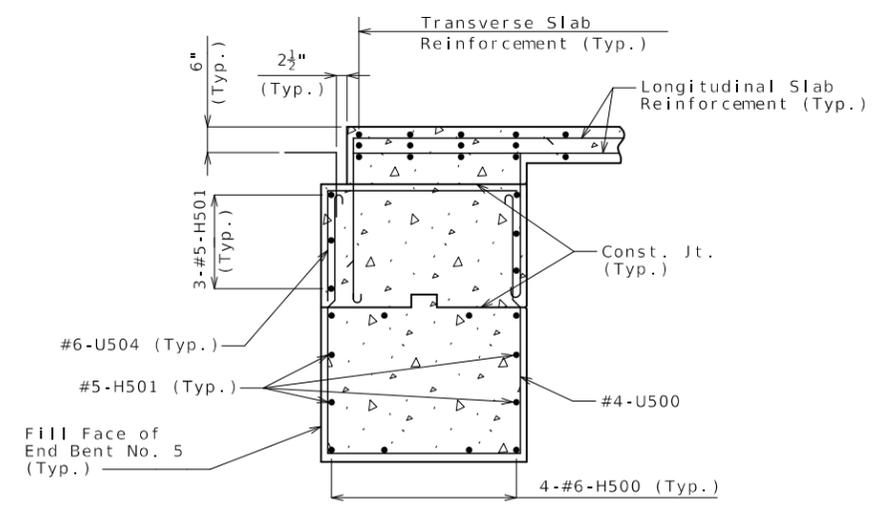


ELEVATION E-E
 *** 3-#6-F503 @ abt. 12" cts.

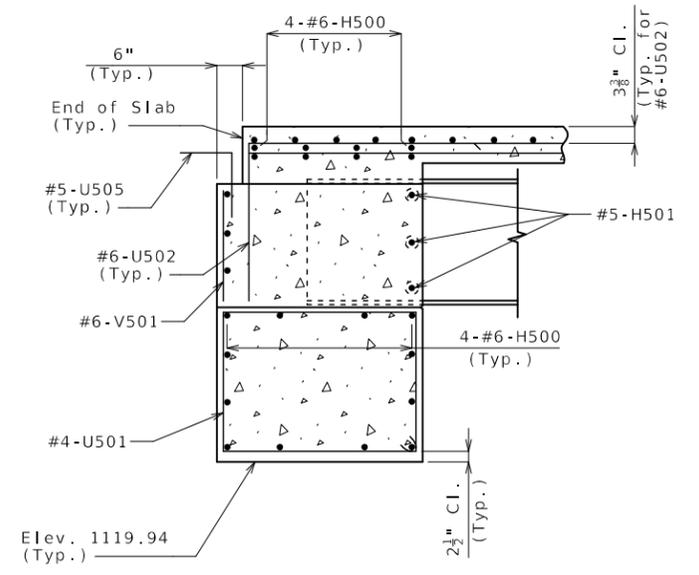


CHAMFER DETAIL

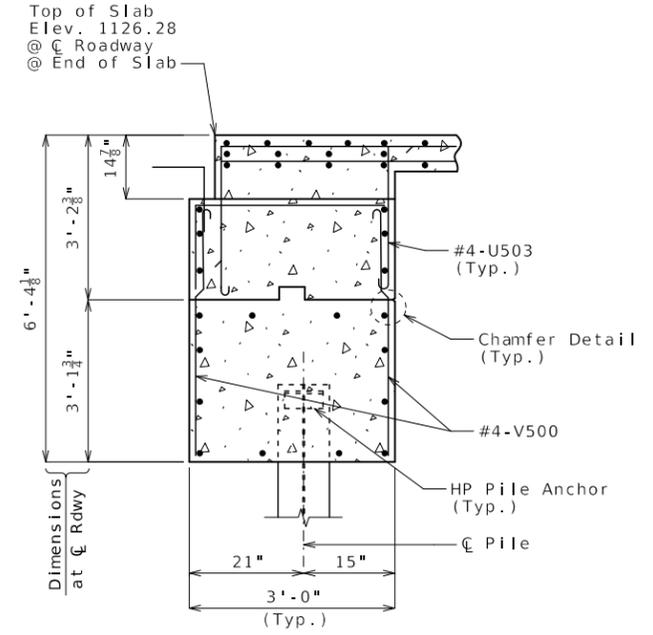
Notes:
 Work this sheet with Sheets No. 10 & 11.
 For reinforcement of the barrier, see Sheets No. 22 & 23.



SECTION A-A

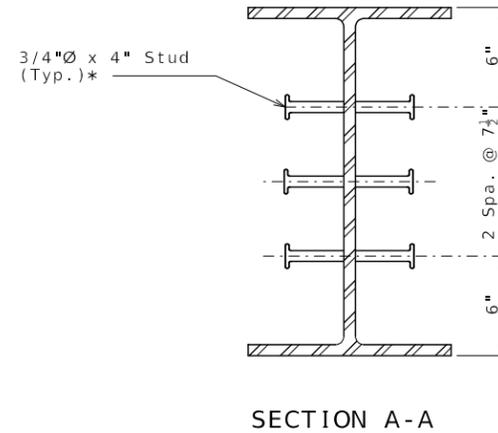
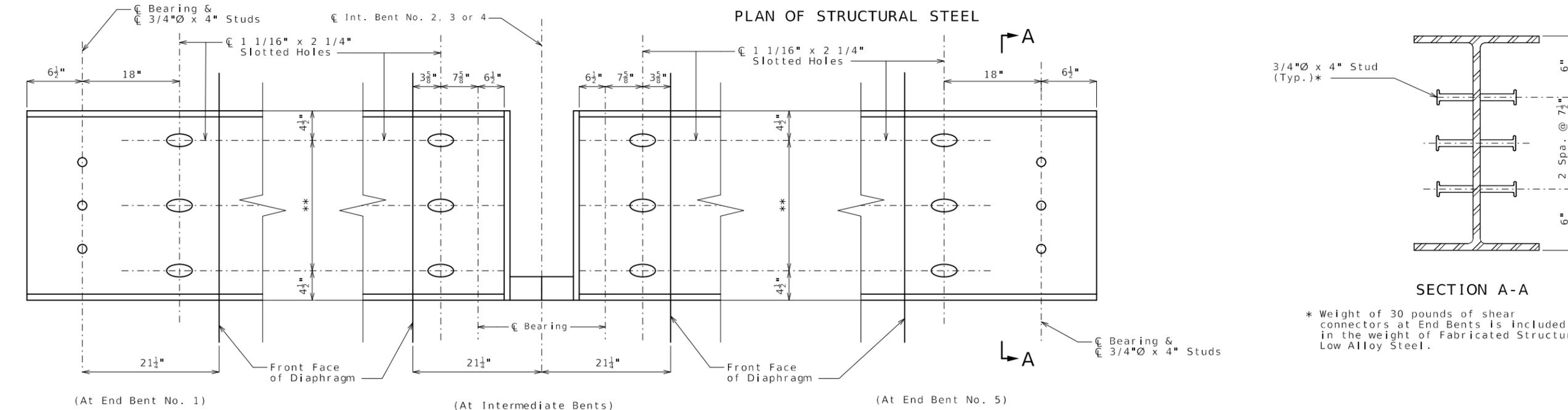
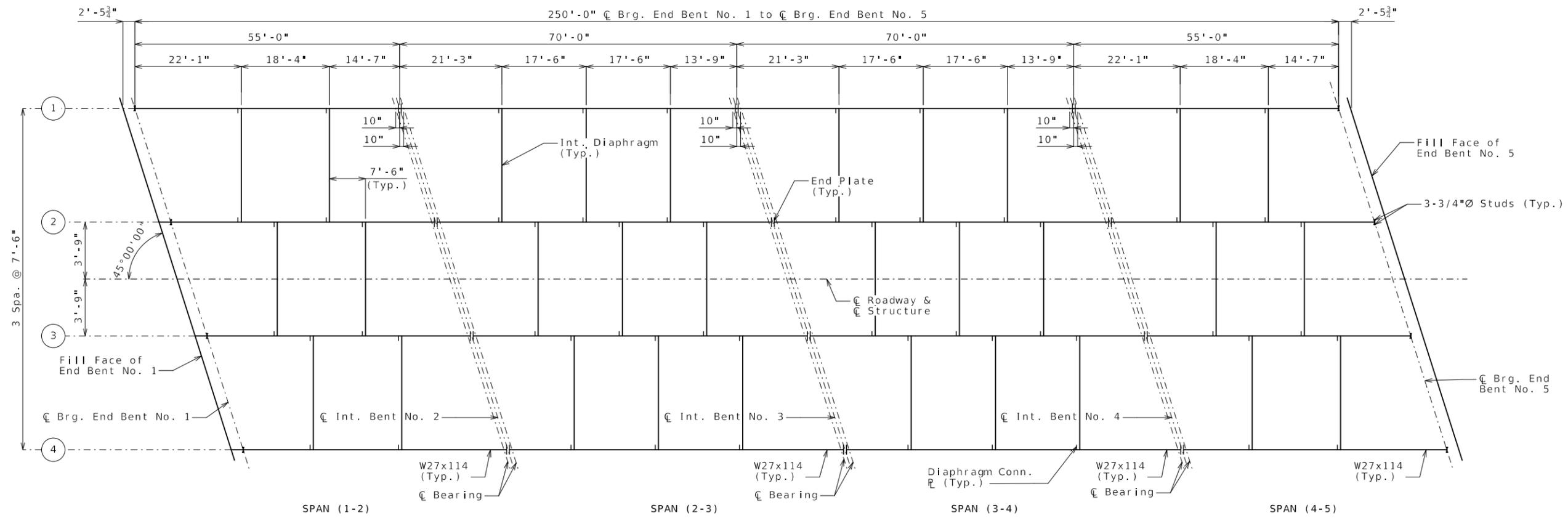


SECTION B-B



SECTION C-C

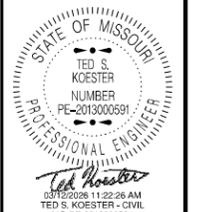
DETAILS OF END BENT NO. 5



* Weight of 30 pounds of shear connectors at End Bents is included in the weight of Fabricated Structural Low Alloy Steel.

Notes:
 Longitudinal dimensions are horizontal from centerline bearing to centerline bearing.
 For details of intermediate diaphragms, connection plates, and end plates, see Sheet No. 15.
 Fabricated Structural Low Alloy Steel shall be ASTM A709 Grade 50, except as noted.
 Structural steel shall be galvanized in accordance with ASTM A123 and Sec 1081.
 Shear connectors shall be in accordance with Sec 712, 1037 and 1080.

Coat in and around the 1 1/16" x 2 1/4" slotted web holes with epoxy mastic primer before running stainless reinforcing steel through to protect from galvanic reactions between the two dissimilar metals.

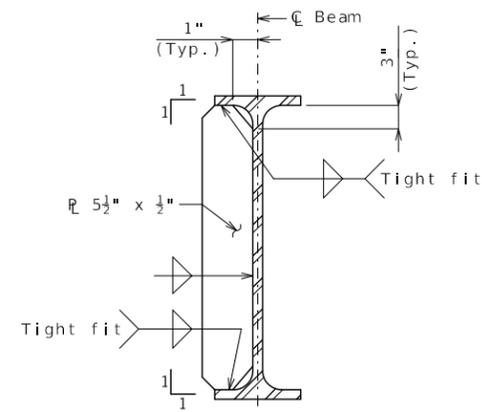


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| DATE PREPARED 3/12/2026 | |
| ROUTE 38 | STATE MO |
| DISTRICT BR | SHEET NO. 13 |
| COUNTY WRIGHT | |
| JOB NO. J9S3689 | |
| CONTRACT ID. | |
| PROJECT NO. | |
| BRIDGE NO. A9619 | |

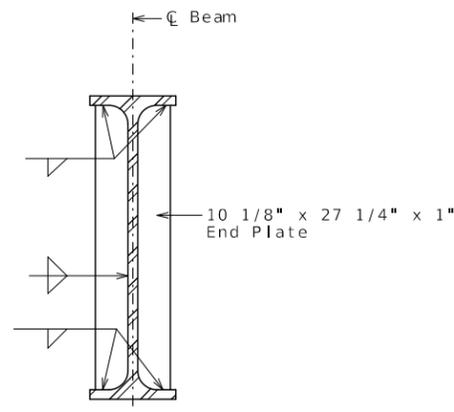
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MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

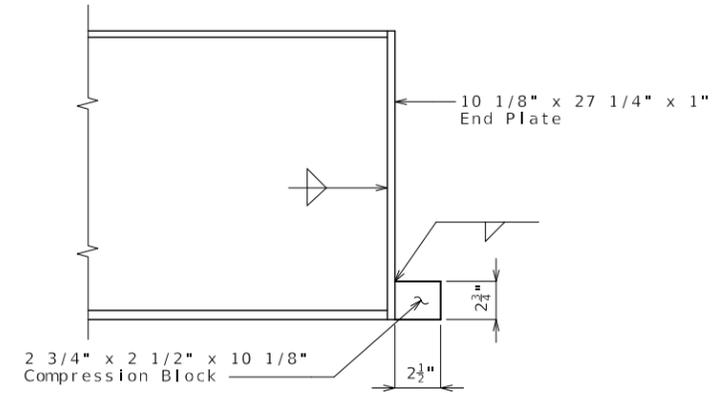
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INTERMEDIATE DIAPHRAGM CONNECTION PLATE

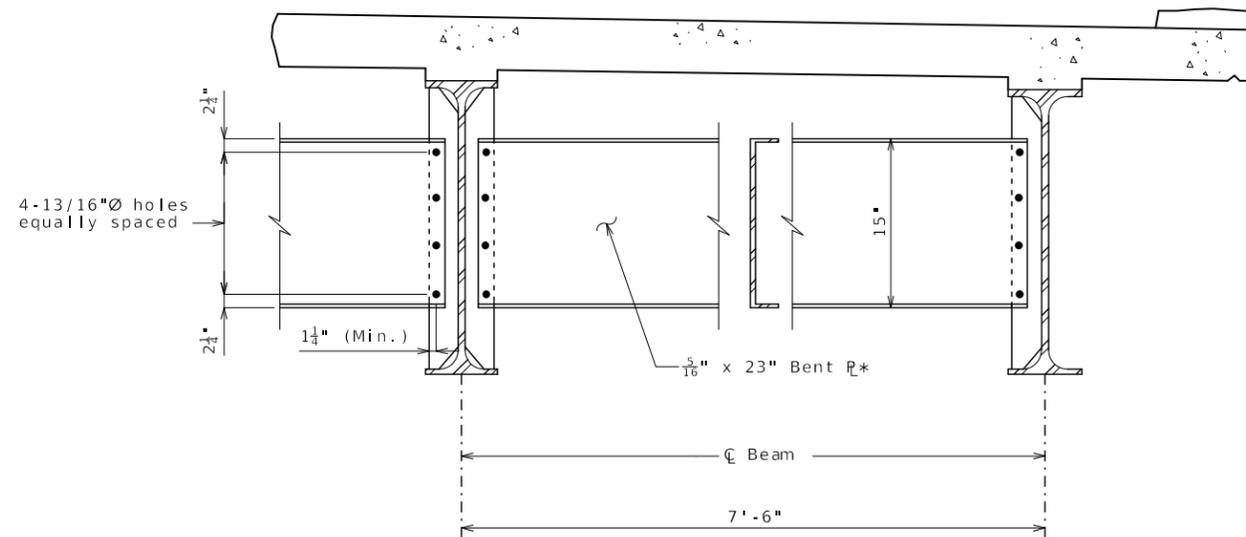


END PLATE AT INTERMEDIATE BENTS
(Looking from midspan toward end of beam)



END PLATE AT INTERMEDIATE BENTS
(Looking transverse to bridge)

WELDING DETAILS



TYPICAL PART SECTION SHOWING INTERMEDIATE DIAPHRAGMS

Notes:

At the contractor's option, holes in the diaphragm plate of non slab bearing diaphragms may be made 3/16 inch larger than the nominal diameter of the bolt. A hardened washer shall be used under the bolt head and nut when this option is used. Holes in the beam diaphragm connection plate or transverse web stiffener shall be standard size.

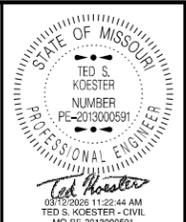
*At the contractor's option, C15 x 33.9 channels may be used in lieu of the 5/16 inch x 23 inch bent plates. No adjustment of the quantity of Fabricated Structural Low Alloy Steel will be allowed for the use of channels.

Fabricated structural steel shall be ASTM A709 Grade 50, except as noted.

Structural steel shall be galvanized in accordance with ASTM A123 & Sec 1081.

For locations of intermediate diaphragms, see Structural Plan on Sheet No. 13.

STEEL DIAPHRAGMS

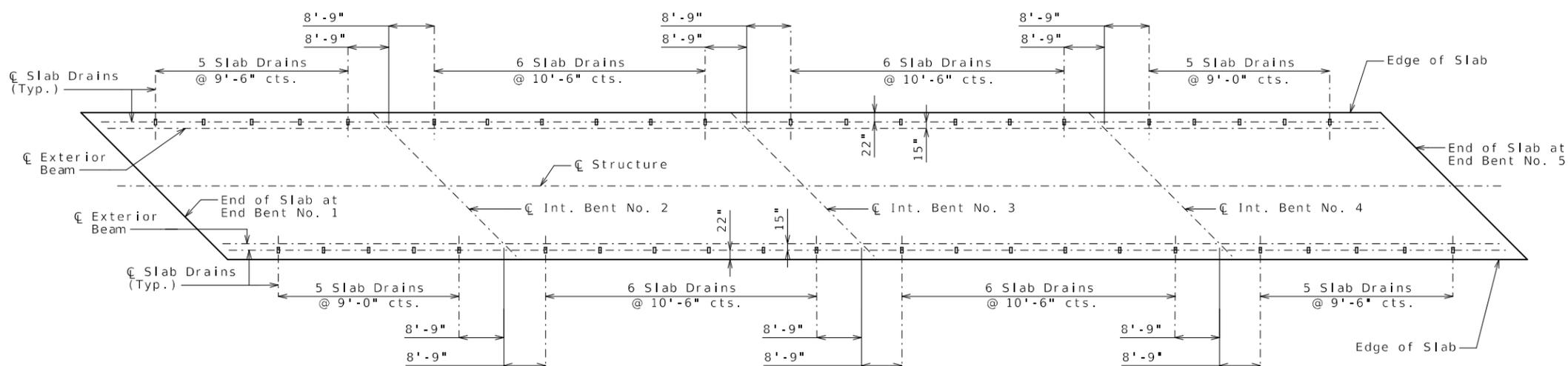


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| DATE PREPARED 3/12/2026 | |
| ROUTE 38 | STATE MO |
| DISTRICT BR | SHEET NO. 15 |
| COUNTY WRIGHT | |
| JOB NO. J9S3689 | |
| CONTRACT ID. | |
| PROJECT NO. | |
| BRIDGE NO. A9619 | |

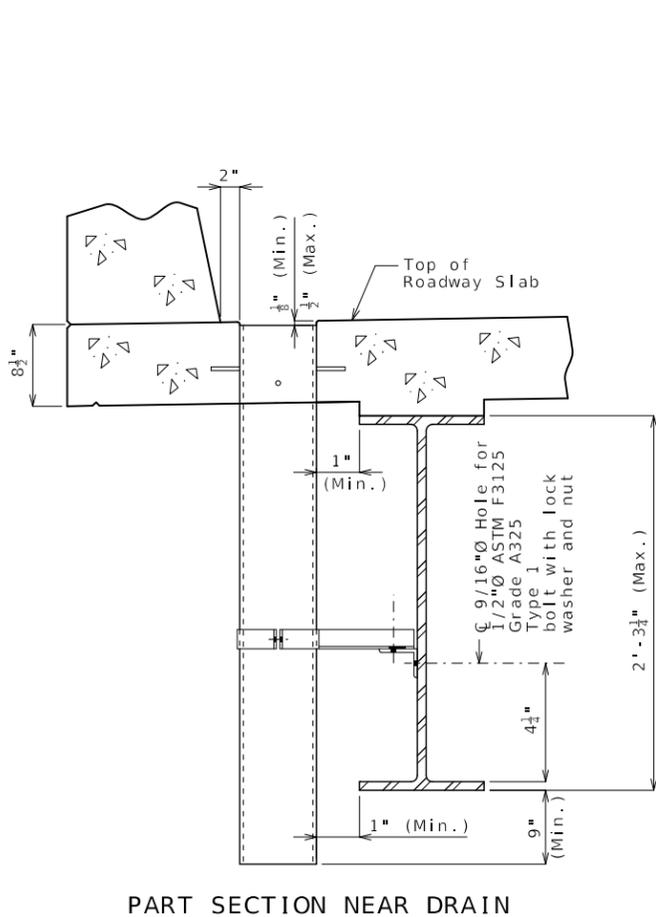
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MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

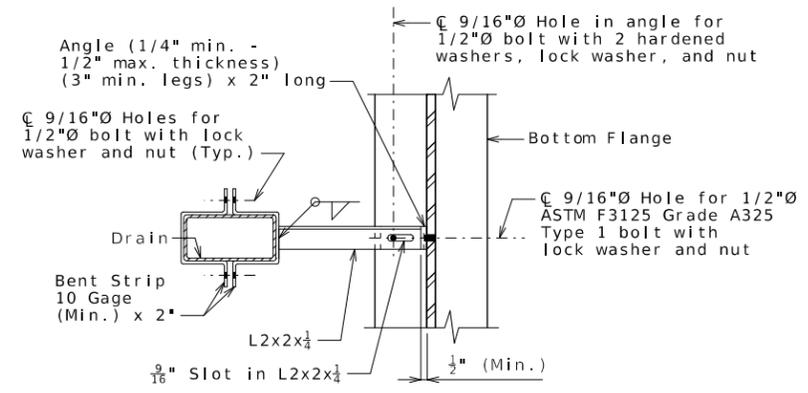
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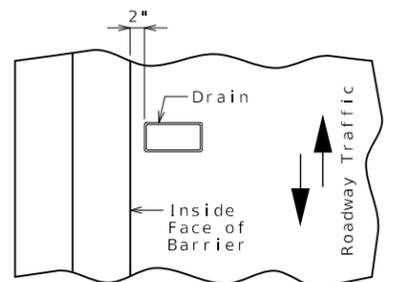
PLAN OF SLAB SHOWING SLAB DRAIN LOCATIONS



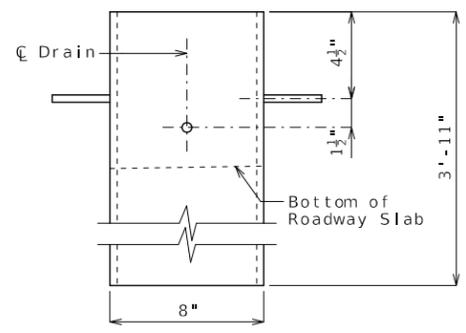
PART SECTION NEAR DRAIN



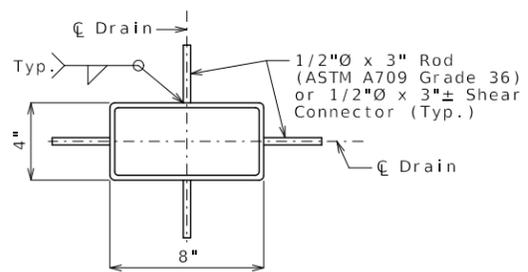
PART SECTION SHOWING BRACKET ASSEMBLY



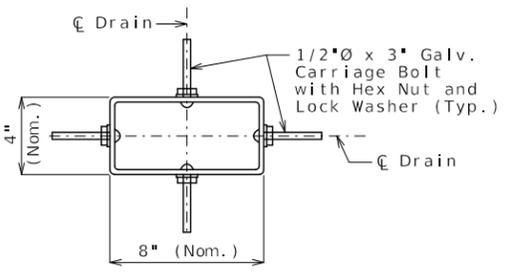
PART PLAN OF SLAB AT DRAIN



ELEVATION OF DRAIN



PLAN OF STEEL DRAIN OPTION



PLAN OF FRP DRAIN OPTION

SLAB DRAINS

General Notes:

Contractor shall have the option to construct either steel or FRP slab drains. All drains shall be of same type.

Slab drain bracket assembly shall be ASTM A709 Grade 36 steel.

Locate drains in slab by dimensions shown in Part Section Near Drain.

Reinforcing steel shall be shifted to clear drains.

The bracket assembly shall be galvanized in accordance with ASTM A123.

All bolts, hardened washers, lock washers and nuts shall be galvanized in accordance with AASHTO M 232 (ASTM A153), Class C.

All 1/2-inch diameter bolts shall be ASTM A307, except as shown.

Shop drawings will not be required for the slab drains and the bracket assembly.

The bolt hole for the bracket assembly attachment shall be located on the WF Beam shop drawings.

Notes for Steel Drain:

Slab drains may be fabricated of either 1/4-inch welded sheets of ASTM A709 Grade 36 steel or from 1/4-inch structural steel tubing ASTM A500 or A501.

Outside dimensions of drains are 8" x 4".

The drains shall be galvanized in accordance with ASTM A123.

Notes for FRP Drain:

Drains shall be machine filament-wound thermosetting resin tubing meeting the requirements of ASTM D2996 with the following exceptions:

Shape of drains shall be rectangular with outside nominal dimensions of 8" x 4".

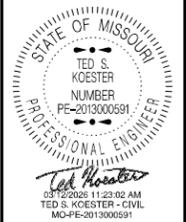
Minimum reinforced wall thickness shall be 1/4 inch.

The resin used shall be ultraviolet (UV) resistant and/or have UV inhibitors mixed throughout. Drains may have an exterior coating for additional UV resistance.

The color of the slab drain shall be gray (Federal Standard #26373). The color shall be uniform throughout the resin and any coating used.

The combination of materials used in the manufacture of the drains shall be tested for UV resistance in accordance with ASTM D4329 Cycle A. The representative material shall withstand at least 500 hours of testing with only minor discoloration and without any physical deterioration. The contractor shall furnish the results of the required ultraviolet testing prior to acceptance of the slab drains.

At the contractor's option, drains may be field cut. The method of cutting FRP slab drain shall be as recommended by the manufacturer to ensure a smooth, chip free cut.



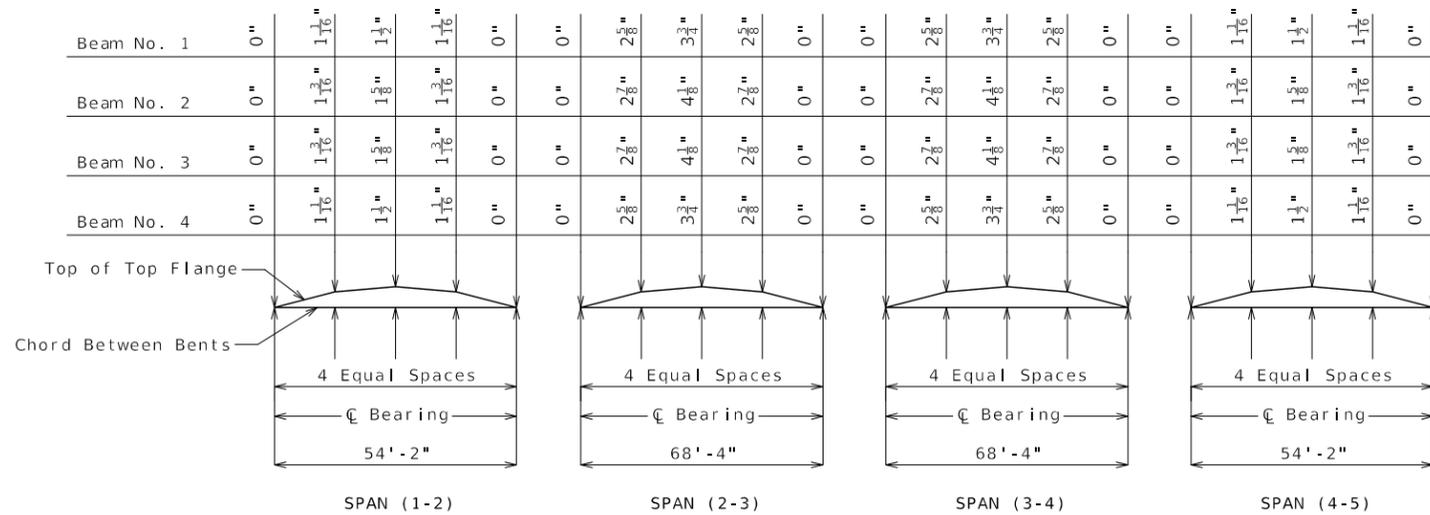
| | | | |
|---------------|-------|-----------|-----------|
| DATE PREPARED | | 3/12/2026 | |
| ROUTE | STATE | DISTRICT | SHEET NO. |
| 38 | MO | BR | 17 |
| COUNTY | | | |
| WRIGHT | | | |
| JOB NO. | | | |
| J9S3689 | | | |
| CONTRACT ID. | | | |
| PROJECT NO. | | | |
| BRIDGE NO. | | | |
| A9619 | | | |

| DESCRIPTION | DATE |
|-------------|------|
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MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

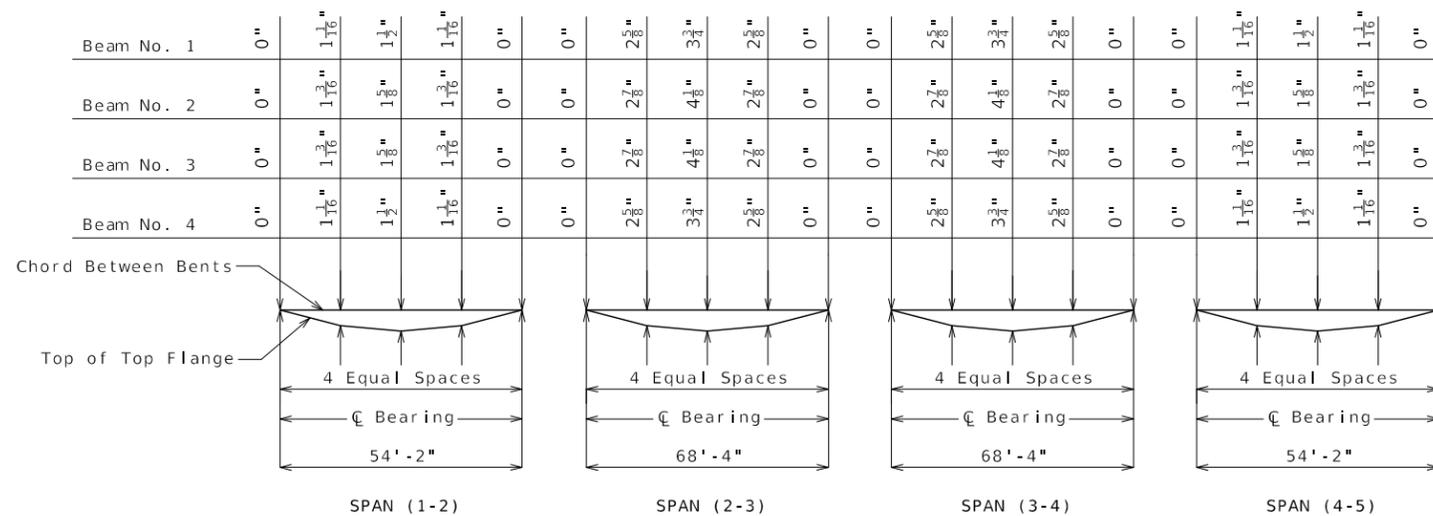
105 WEST CAPITOL JEFFERSON CITY, MO 65102

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BEAM CAMBER DIAGRAM

Camber includes allowance for vertical curve, and for dead load deflection due to the concrete slab, barrier, and structural steel.



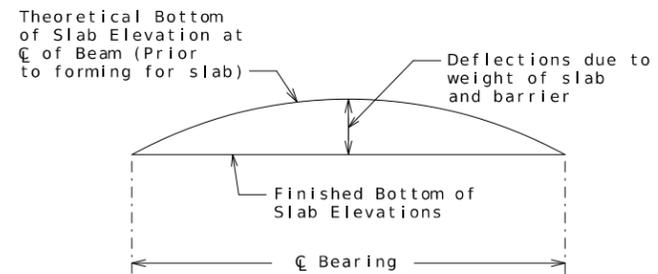
DEAD LOAD DEFLECTION

13 % of dead load deflection is due to the weight of structural steel.

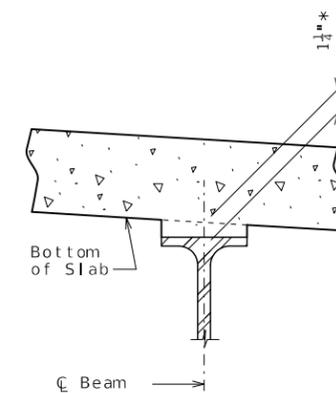
| Theoretical Bottom of Slab Elevations at Centerline of Beam (Prior to forming for slab) | | | | | | | | | | | | | | | | | | | | |
|--|-------------------------------------|---------|---------|---------|---------|-------------------------------------|---------|---------|---------|---------|-------------------------------------|---------|---------|---------|---------|-------------------------------------|---------|---------|---------|---------|
| Beam Number | Span (1-2) (54'-2" C Brg. - C Brg.) | | | | | Span (2-3) (68'-4" C Brg. - C Brg.) | | | | | Span (3-4) (68'-4" C Brg. - C Brg.) | | | | | Span (4-5) (54'-2" C Brg. - C Brg.) | | | | |
| | C Brg. | .25 | .50 | .75 | C Brg. | C Brg. | .25 | .50 | .75 | C Brg. | C Brg. | .25 | .50 | .75 | C Brg. | C Brg. | .25 | .50 | .75 | C Brg. |
| 1 | 1125.37 | 1125.44 | 1125.48 | 1125.44 | 1125.37 | 1125.37 | 1125.56 | 1125.64 | 1125.56 | 1125.37 | 1125.37 | 1125.56 | 1125.64 | 1125.56 | 1125.37 | 1125.37 | 1125.44 | 1125.48 | 1125.44 | 1125.37 |
| 2 | 1125.52 | 1125.60 | 1125.64 | 1125.60 | 1125.52 | 1125.52 | 1125.73 | 1125.82 | 1125.73 | 1125.52 | 1125.52 | 1125.73 | 1125.82 | 1125.73 | 1125.52 | 1125.52 | 1125.60 | 1125.64 | 1125.60 | 1125.52 |
| 3 | 1125.52 | 1125.60 | 1125.64 | 1125.60 | 1125.52 | 1125.52 | 1125.73 | 1125.82 | 1125.73 | 1125.52 | 1125.52 | 1125.73 | 1125.82 | 1125.73 | 1125.52 | 1125.52 | 1125.60 | 1125.64 | 1125.60 | 1125.52 |
| 4 | 1125.37 | 1125.44 | 1125.48 | 1125.44 | 1125.37 | 1125.37 | 1125.56 | 1125.64 | 1125.56 | 1125.37 | 1125.37 | 1125.56 | 1125.64 | 1125.56 | 1125.37 | 1125.37 | 1125.44 | 1125.48 | 1125.44 | 1125.37 |

Elevations are based on a constant slab thickness of 8 1/2" and include allowance for theoretical dead load deflections due to weight of slab and barrier.

BEAM CAMBER, DEAD LOAD DEFLECTION AND SLAB ELEVATIONS

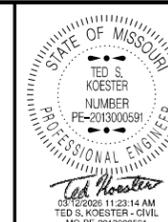


TYPICAL SLAB ELEVATIONS DIAGRAM



THEORETICAL SLAB HAUNCH

* Dimension (bottom of slab to top of top flange) may vary if beam camber after erection differs from plan camber by more than the % of Dead Load Deflection due to weight of structural steel. No payment will be made for any adjustment in forming or additional concrete required for variation in haunching.



DATE PREPARED
3/12/2026

ROUTE 38 STATE MO

DISTRICT BR SHEET NO. 18

COUNTY WRIGHT

JOB NO. J9S3689

CONTRACT ID.

PROJECT NO.

BRIDGE NO. A9619

DESCRIPTION

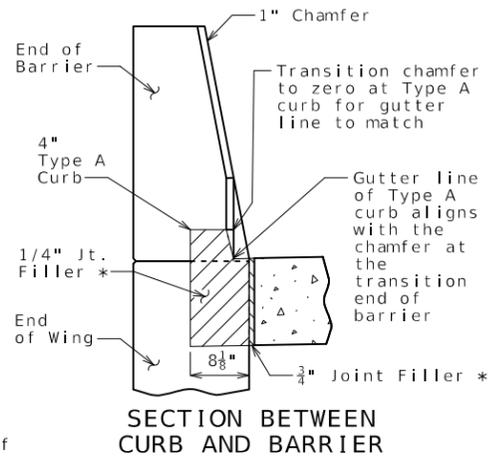
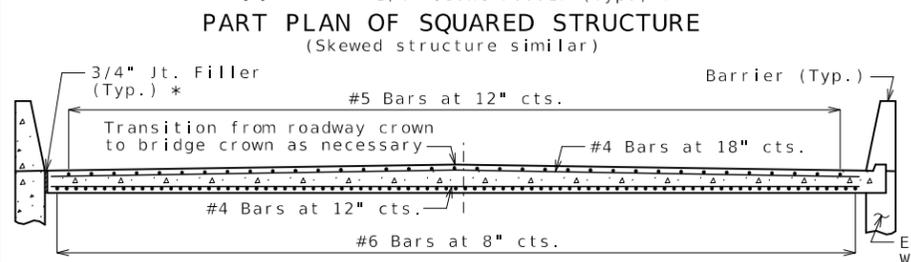
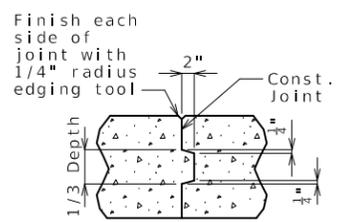
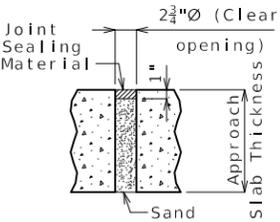
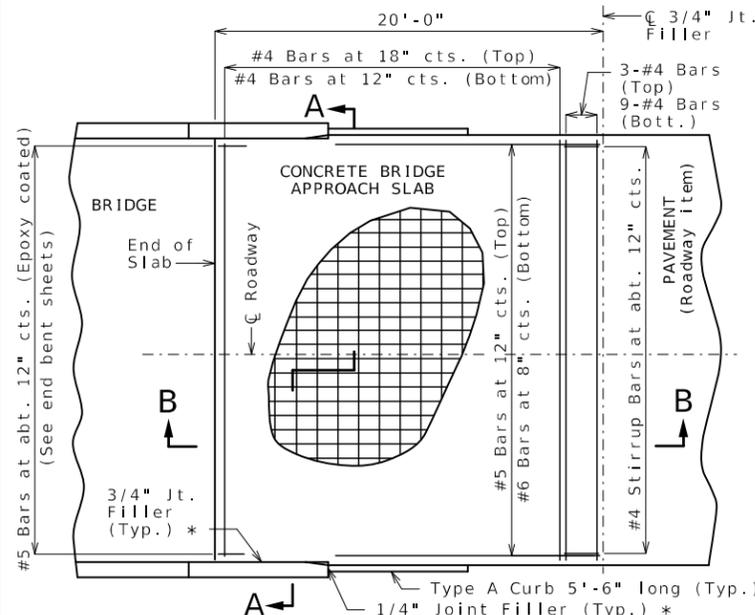
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MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION

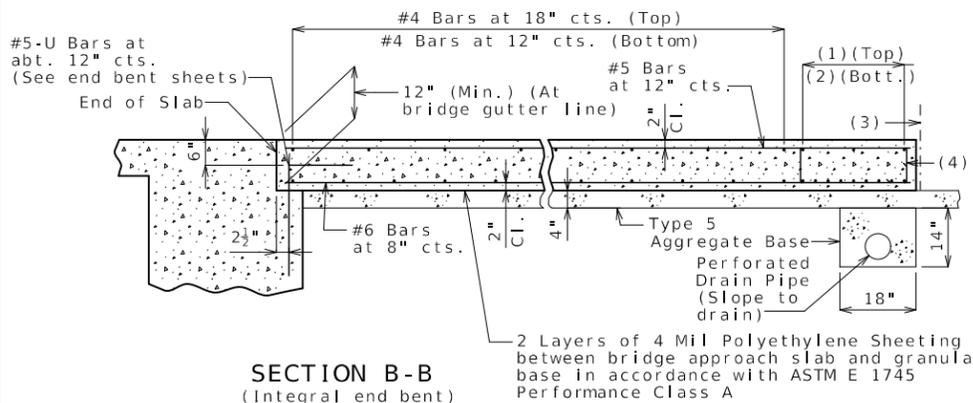
MoDOT

105 WEST CAPITOL JEFFERSON CITY, MO 65102

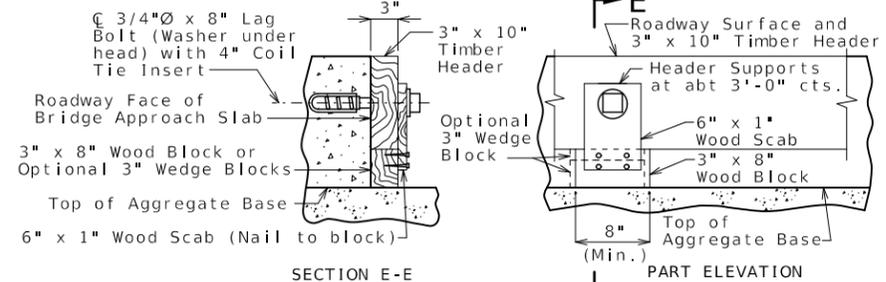
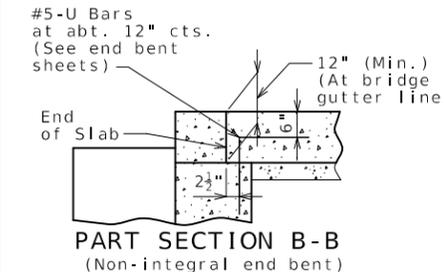
1-888-ASK-MODOT (1-888-275-6636)



With the approval of the engineer, the contractor may crown the bottom of the approach slab to match the crown of the roadway surface.



- (1) 3-#4 Bars
- (2) 9-#4 Bars
- (3) C 3/4" Jt. Filler
- (4) #4 Stirrup Bars at abt. 12" cts.; 2'-0" x 8" (Min.) out to out; Actual length = 5'-10" (Min.); 90° stirrup hook at bottom; Stirrup height (8") and actual length vary due to crown.



Remove timber header when concrete pavement is placed.
OPTIONAL CONCRETE SLAB

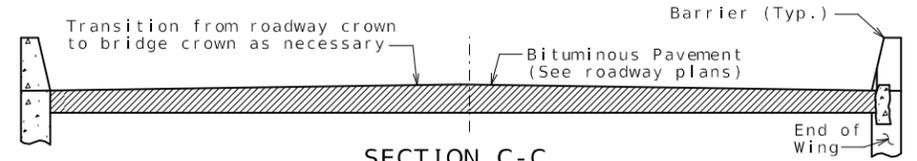
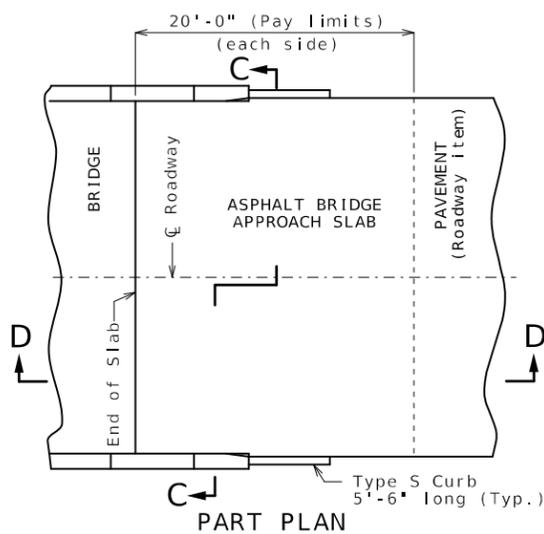
Notes For Concrete Slab Only:
 All concrete for the bridge approach slab shall be in accordance with Sec 503 ($f'c = 4,000$ psi).
 The reinforcing steel in the bridge approach slab shall be epoxy coated Grade 60 with $f_y = 60,000$ psi.
 Longitudinal construction joints in bridge approach slab shall be aligned with longitudinal construction joints in bridge slab.
 Minimum clearance to reinforcing steel shall be 1 1/2", unless otherwise shown.
 The reinforcing steel in the bridge approach slab shall be continuous. The transverse reinforcing steel may be made continuous by providing a minimum lap splice of 26 inches for #4 bars, or by mechanical bar splice.

All joint filler shall be in accordance with Sec 1057 for preformed fiber expansion joint filler except as noted.
 Payment for furnishing all materials, labor and excavation necessary to construct the concrete bridge approach slab, including the timber header, underdrain, Type 5 aggregate base, joint filler, and all other appurtenances and incidental work as shown on this sheet, complete in place, will be considered completely covered by the contract unit price for Bridge Approach Slab (Minor) per square yard.
 See Missouri Standard Plan 609.00 for details of Type A curb.
 Drain pipe may be either 6" diameter corrugated metallic-coated pipe underdrain, 4" diameter corrugated polyvinyl chloride (PVC) drain pipe, or 4" diameter corrugated polyethylene (PE) drain pipe.

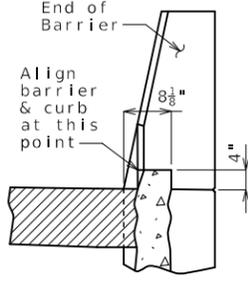
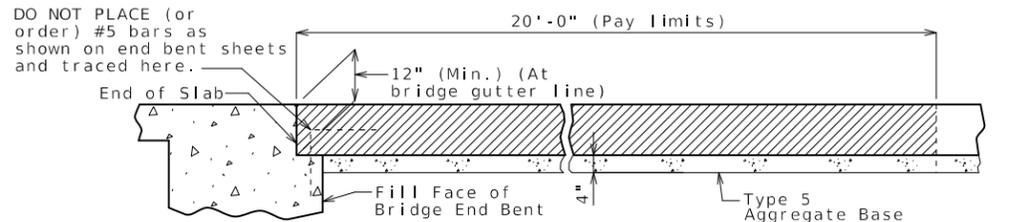
* Seal joint between vertical face of approach slab and wing with sealant in accordance with Sec 717 for silicone joint sealant for saw cut and formed joints.

General Notes:
 Contractor shall have the option to construct either slab except as noted.
 The contractor shall pour and satisfactorily finish the bridge slab before placing the bridge approach slab.
 MoDOT Construction personnel will indicate the bridge approach slab used for this structure:
 Concrete Bridge Approach Slab
 Asphalt Bridge Approach Slab

Notes For Asphalt Slab Only:
 Payment for furnishing all materials, labor and excavation necessary to construct the asphalt bridge approach slab, including tack, curb, and Type 5 aggregate base within the pay limits shown, complete in place, will be considered completely covered by the contract unit price for Bridge Approach Slab (Minor) per square yard.
 Application of tack is required between lifts per Sec 403.



With the approval of the engineer, the contractor may crown the bottom of the approach slab to match the crown of the roadway surface.



See Missouri Standard Plan 609.00 for details of Type S curb.

OPTIONAL ASPHALT SLAB (NOT ALLOWED WITH CONCRETE PAVEMENT)

BRIDGE APPROACH SLAB (MINOR)

Integral end bents shown, non-integral end bent similar.



| | |
|---------------|-----------|
| DATE PREPARED | |
| 3/12/2026 | |
| ROUTE | STATE |
| 38 | MO |
| DISTRICT | SHEET NO. |
| BR | 23 |
| COUNTY | |
| WRIGHT | |
| JOB NO. | |
| J9S3689 | |
| CONTRACT ID. | |
| PROJECT NO. | |
| BRIDGE NO. | |
| A9619 | |

| DATE | DESCRIPTION |
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MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
 105 WEST CAPITOL JEFFERSON CITY, MO 65102
 1-888-ASK-MODOT (1-888-275-6636)



| Bill of Reinforcing Steel | | | | | | | | | | | | | | | | | | |
|---------------------------|-----------|----------------|-------|----|---|------------|-----------|-----------|-----------|-----------|-----------|-----------|---------------------|-----------------------|-----------|----|----|-----|
| No. Req. | Size/Mark | Location | Codes | | | Dimensions | | | | | | | Nom. Length ft. in. | Actual Length ft. in. | Weight lb | | | |
| | | | C | SH | V | B ft. in. | C ft. in. | D ft. in. | E ft. in. | F ft. in. | H ft. in. | K ft. in. | | | | | | |
| Substructure | | | | | | | | | | | | | | | | | | |
| Int Bent 2 | | | | | | | | | | | | | | | | | | |
| 22 | 6 D200 | BEAM KEY | 20 | | | 2 | 6.00 | | | | | | | 2 | 6 | 2 | 6 | 83 |
| 10 | 6 H200 | BEAM | 18 | | | 3 | 6.00 | | | | | | | 3 | 7 | 3 | 7 | 567 |
| 6 | 6 H201 | BEAM | 18 | | | 7 | 11.00 | | | | | | | 9 | 3 | 9 | 3 | 83 |
| 4 | 6 H202 | BEAM | 18 | | | 2 | 2.00 | | | | | | | 3 | 6 | 3 | 6 | 21 |
| 6 | 6 H203 | BEAM | 20 | | | 3 | 6.00 | | | | | | | 3 | 6 | 3 | 6 | 21 |
| 10 | 6 H204 | BEAM | 10S | | | | | 12.00 | 2 | 10.75 | | | | 4 | 11 | 4 | 7 | 69 |
| 40 | 4 P200 | CIP PILES | 34S | | | 4 | 2.25 | 2.00 | | | | | | 5 | 1 | 5 | | 134 |
| 32 | 5 U200 | BEAM | 13S | | | 3 | 0.00 | 3 | 0.00 | 3 | 0.00 | 3 | 0.00 | 12 | 11 | 12 | 7 | 420 |
| 20 | 5 U201 | BEAM | 10S | | | 3 | 0.00 | 3 | 0.00 | | | | | 9 | | 8 | 8 | 181 |
| 32 | 6 V200 | CIP PILES | 17 | | | 7 | 3.00 | | | | | | | 7 | 11 | 7 | 11 | 381 |
| Int Bent 3 | | | | | | | | | | | | | | | | | | |
| 22 | 6 D300 | BEAM KEY | 20 | | | 2 | 6.00 | | | | | | | 2 | 6 | 2 | 6 | 83 |
| 10 | 6 H300 | BEAM | 18 | | | 3 | 6.00 | | | | | | | 3 | 7 | 3 | 7 | 567 |
| 6 | 6 H301 | BEAM | 18 | | | 7 | 11.00 | | | | | | | 9 | 3 | 9 | 3 | 83 |
| 4 | 6 H302 | BEAM | 18 | | | 2 | 2.00 | | | | | | | 3 | 6 | 3 | 6 | 21 |
| 6 | 6 H303 | BEAM | 20 | | | 3 | 6.00 | | | | | | | 3 | 6 | 3 | 6 | 21 |
| 10 | 6 H304 | BEAM | 10S | | | | | 12.00 | 2 | 10.75 | | | | 4 | 11 | 4 | 7 | 69 |
| 40 | 4 P300 | CIP PILES | 34S | | | 4 | 2.25 | 2.00 | | | | | | 5 | 1 | 5 | | 134 |
| 32 | 5 U300 | BEAM | 13S | | | 3 | 0.00 | 3 | 0.00 | 3 | 0.00 | 3 | 0.00 | 12 | 11 | 12 | 7 | 420 |
| 20 | 5 U301 | BEAM | 10S | | | 3 | 0.00 | 3 | 0.00 | | | | | 9 | | 8 | 8 | 181 |
| 32 | 6 V300 | CIP PILES | 17 | | | 7 | 3.00 | | | | | | | 7 | 11 | 7 | 11 | 381 |
| Int Bent 4 | | | | | | | | | | | | | | | | | | |
| 22 | 6 D400 | BEAM KEY | 20 | | | 2 | 6.00 | | | | | | | 2 | 6 | 2 | 6 | 83 |
| 10 | 6 H400 | BEAM | 18 | | | 3 | 6.00 | | | | | | | 3 | 7 | 3 | 7 | 567 |
| 6 | 6 H401 | BEAM | 18 | | | 7 | 11.00 | | | | | | | 9 | 3 | 9 | 3 | 83 |
| 4 | 6 H402 | BEAM | 18 | | | 2 | 2.00 | | | | | | | 3 | 6 | 3 | 6 | 21 |
| 6 | 6 H403 | BEAM | 20 | | | 3 | 6.00 | | | | | | | 3 | 6 | 3 | 6 | 21 |
| 10 | 6 H404 | BEAM | 10S | | | | | 12.00 | 2 | 10.75 | | | | 4 | 11 | 4 | 7 | 69 |
| 40 | 4 P400 | CIP PILES | 34S | | | 4 | 2.25 | 2.00 | | | | | | 5 | 1 | 5 | | 134 |
| 32 | 5 U400 | BEAM | 13S | | | 3 | 0.00 | 3 | 0.00 | 3 | 0.00 | 3 | 0.00 | 12 | 11 | 12 | 7 | 420 |
| 20 | 5 U401 | BEAM | 10S | | | 3 | 0.00 | 3 | 0.00 | | | | | 9 | | 8 | 8 | 181 |
| 32 | 6 V400 | CIP PILES | 17 | | | 7 | 3.00 | | | | | | | 7 | 11 | 7 | 11 | 381 |
| Superstructure | | | | | | | | | | | | | | | | | | |
| End Bent 1 | | | | | | | | | | | | | | | | | | |
| 8 | 6 F100 | WING BRACE | 23S | | | 20.00 | 3 | 7.88 | 20.00 | 18.50 | 7.63 | 18.50 | 7.60 | 7 | 0 | 6 | 10 | 82 |
| 3 | 6 F101 | DIAPHRAGM | 23S | | | 3 | 10.75 | 4 | 4.63 | | | 2 | 9.00 | 8 | 3 | 8 | 1 | 36 |
| 8 | 6 F102 | WING BRACE | 23S | | | 20.00 | 8 | 5.75 | 20.00 | 7.63 | 18.50 | 11 | 10 | 11 | 10 | 11 | 9 | 141 |
| 3 | 6 F103 | DIAPHRAGM | 21S | | | 6 | 5.00 | 3 | 2.00 | 4 | 6.50 | 9 | 7 | 9 | 7 | 9 | 7 | 43 |
| 12 | 6 H100 | BEAM | 20 | | | 4 | 2.00 | | | | | | | 4 | 2 | 4 | 2 | 724 |
| 10 | 5 H101 | BEAM/DIAPHRAGM | 20 | | | 4 | 2.00 | | | | | | | 4 | 2 | 4 | 2 | 419 |
| 32 | 6 H102 | WING | 19S | | | 10 | 1.00 | 12.00 | | | | | | 11 | 1 | 10 | 11 | 525 |
| 16 | 8 H103 | WING | 19 | | | 11 | 6.00 | 12.00 | | | | | | 12 | 6 | 12 | 3 | 523 |
| 24 | 4 U100 | BEAM | 37S | | | 4 | 9.00 | 3 | 10.50 | | | | | 14 | 3 | 14 | | 224 |
| 14 | 4 U101 | BEAM | 13S | | | 3 | 10.50 | 2 | 8.00 | 3 | 10.50 | 2 | 8.00 | 13 | 10 | 13 | 7 | 127 |
| 50 | 6 U102 | DIAPHRAGM | 19S | | | 2 | 8.00 | 5 | 3.00 | | | | | 7 | 11 | 7 | 9 | 582 |
| 35 | 4 U103 | DIAPHRAGM | 37S | | | 2 | 8.00 | 3 | 2.00 | | | | | 9 | 4 | 9 | 2 | 214 |
| 35 | 6 U104 | DIAPHRAGM | 19S | | | 20.00 | 3 | 10.50 | | | | | | 5 | 7 | 5 | 5 | 285 |
| 22 | 4 V100 | BEAM | 17 | | | 4 | 9.00 | | | | | | | 5 | 3 | 5 | 3 | 77 |
| 12 | 6 V101 | DIAPHRAGM | 20 | | | 20.00 | | | | | | | | 1 | 8 | 1 | 8 | 30 |
| 40 | 6 V102 | WING | 20 | | | 5 | 8.00 | | | | | | | 5 | 8 | 5 | 8 | 340 |
| End Bent 5 | | | | | | | | | | | | | | | | | | |

| Bill of Reinforcing Steel | | | | | | | | | | | | | | | | | | | |
|---------------------------|-----------|-----------------|-------|----|---|------------|-----------|-----------|-----------|-----------|-----------|-----------|---------------------|-----------------------|-----------|----|----|--------|-----|
| No. Req. | Size/Mark | Location | Codes | | | Dimensions | | | | | | | Nom. Length ft. in. | Actual Length ft. in. | Weight lb | | | | |
| | | | C | SH | V | B ft. in. | C ft. in. | D ft. in. | E ft. in. | F ft. in. | H ft. in. | K ft. in. | | | | | | | |
| 8 | 6 F500 | WING BRACE | 23S | | | 20.00 | 3 | 7.88 | 20.00 | 18.50 | 7.63 | 18.50 | 7.60 | 7 | 0 | 6 | 10 | 82 | |
| 3 | 6 F501 | DIAPHRAGM | 23S | | | 3 | 10.75 | 4 | 4.63 | | | 2 | 9.00 | 8 | 3 | 8 | 1 | 36 | |
| 8 | 6 F502 | WING BRACE | 23S | | | 20.00 | 8 | 5.75 | 20.00 | 7.63 | 18.50 | 11 | 10 | 11 | 10 | 11 | 9 | 141 | |
| 3 | 6 F503 | DIAPHRAGM | 21S | | | 6 | 5.00 | 3 | 2.00 | 4 | 6.50 | 9 | 7 | 9 | 7 | 9 | 7 | 43 | |
| 12 | 6 H500 | BEAM | 20 | | | 4 | 2.00 | | | | | | | 4 | 2 | 4 | 2 | 724 | |
| 10 | 5 H501 | BEAM/DIAPHRAGM | 20 | | | 4 | 2.00 | | | | | | | 4 | 2 | 4 | 2 | 419 | |
| 32 | 6 H502 | WING | 19S | | | 10 | 1.00 | 12.00 | | | | | | 11 | 1 | 10 | 11 | 525 | |
| 16 | 8 H503 | WING | 19 | | | 11 | 6.00 | 12.00 | | | | | | 12 | 6 | 12 | 3 | 523 | |
| 24 | 4 U500 | BEAM | 37S | | | 4 | 9.00 | 3 | 10.50 | | | | | 14 | 3 | 14 | | 224 | |
| 14 | 4 U501 | BEAM | 13S | | | 3 | 10.50 | 2 | 8.00 | 3 | 10.50 | 2 | 8.00 | 13 | 10 | 13 | 7 | 127 | |
| 50 | 6 U502 | DIAPHRAGM | 19S | | | 2 | 8.00 | 5 | 3.00 | | | | | 7 | 11 | 7 | 9 | 582 | |
| 35 | 4 U503 | DIAPHRAGM | 37S | | | 2 | 8.00 | 3 | 2.00 | | | | | 9 | 4 | 9 | 2 | 214 | |
| 35 | 6 U504 | DIAPHRAGM | 19S | | | 20.00 | 3 | 10.50 | | | | | | 5 | 7 | 5 | 5 | 285 | |
| 22 | 4 V500 | BEAM | 17 | | | 4 | 9.00 | | | | | | | 5 | 3 | 5 | 3 | 77 | |
| 12 | 6 V501 | DIAPHRAGM | 20 | | | 20.00 | | | | | | | | 1 | 8 | 1 | 8 | 30 | |
| 40 | 6 V502 | WING | 20 | | | 5 | 8.00 | | | | | | | 5 | 8 | 5 | 8 | 340 | |
| Int Diaphragms | | | | | | | | | | | | | | | | | | | |
| 18 | 5 H600 | DIAPHRAGM | 20 | | | 36 | 5.00 | | | | | | | 36 | 5 | 36 | 5 | 684 | |
| 24 | 5 H601 | DIAPHRAGM | 10S | | | | | 12.00 | 20.00 | | | | | 3 | 8 | 3 | 5 | 86 | |
| 108 | 4 U600 | DIAPHRAGM | 28S | | | | | 18.00 | 2 | 6.00 | 3 | 2.00 | | 7 | 2 | 7 | | 505 | |
| 48 | 6 U601 | DIAPHRAGM | 28S | | | 2 | 2.00 | 2 | 6.00 | 3 | 2.00 | | | 7 | 10 | 7 | 6 | 541 | |
| 36 | 5 V600 | DIAPHRAGM | 20 | | | 2 | 8.00 | | | | | | | 2 | 8 | 2 | 8 | 100 | |
| Slab | | | | | | | | | | | | | | | | | | | |
| 260 | 5 S1 | SLAB | 20 | | | 52 | 9.00 | | | | | | | 52 | 9 | 52 | 9 | 14,305 | |
| 132 | 7 S2 | SLAB | 20 | | | 36 | 6.00 | | | | | | | 36 | 6 | 36 | 6 | 9,848 | |
| 106 | 6 S3 | SLAB | 20 | 2 | | 28 | 0.00 | | | | | | | 28 | | 28 | | | |
| | | Incr. = 6.000" | | | | 2 | 0.00 | | | | | | | 2 | | 2 | | 2,388 | |
| 450 | 6 S4 | SLAB | 20 | | | 28 | 5.00 | | | | | | | 28 | 5 | 28 | 5 | 19,207 | |
| 64 | 5 S5 | SLAB | 20 | 2 | | 27 | 10.00 | | | | | | | 27 | 10 | 27 | 10 | | |
| | | Incr. = 10.000" | | | | 2 | 0.00 | | | | | | | 2 | | 2 | | 996 | |
| 270 | 5 S6 | SLAB | 20 | | | 28 | 5.00 | | | | | | | 28 | 5 | 28 | 5 | 8,002 | |
| Barrier | | | | | | | | | | | | | | | | | | | |
| Type D | | | | | | | | | | | | | | | | | | | |
| 20 | 5 K1 | BARRIER | 27S | | | 3 | 5.00 | 9.25 | 5.25 | 2 | 11.75 | | 5.25 | 1.00 | 7 | 7 | 7 | 5 | 155 |
| 68 | 5 K2 | BARRIER | 27S | | | 3 | 5.00 | 9.25 | 14.50 | 2 | 2.75 | | 14.25 | 2.75 | 7 | 8 | 7 | 5 | 526 |
| 8 | 5 K3 | BARRIER | 27S | | | 22.50 | 9.25 | 14.50 | 7.75 | 12.00 | 14.25 | 2.75 | 5 | 6 | 5 | 2 | 43 | | |
| 20 | 5 K4 | BARRIER | 19S | 4 | | 2 | 4.25 | 10.00 | | | | | | 3 | 2 | 3 | 1 | | |
| | | Incr. = 0.500" | | | | 2 | 6.25 | 10.00 | | | | | | | | | | | |

